

#13309294

## Nielsen Architects

Staged demolition of all existing buildings and the staged construction of a new ALDI supermarket and Chemist Warehouse building with shared car parking for up to 120 cars, signage, fencing and landscaping.

**1150-1154 & 1156-1158 South Road, Clovelly Park and 17-19 Wingfield Street, Clovelly Park, and 1-5 Norrie Avenue, Clovelly Park.**

DA 100/E074/18

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## OVERVIEW

<b>Application No</b>	100/E074/18
<b>Unique ID/KNET ID</b>	#13309294; 2018/19210/01
<b>Applicant</b>	Nielsen Architects
<b>Proposal</b>	Staged demolition of all existing buildings and staged construction of a new ALDI supermarket and Chemist Warehouse building with shared car parking for up to 120 cars, signage, fencing and landscaping.
<b>Subject Land</b>	1150-1154 & 1156-1158 South Road, Clovelly Park and 17-19 Wingfield Street, Clovelly Park and 1-5 Norrie Avenue, Clovelly Park.
<b>Zone/Policy Area</b>	Neighbourhood Centre Zone.
<b>Relevant Authority</b>	State Commission Assessment Panel.
<b>Lodgement Date</b>	31/08/2018
<b>Council</b>	City of Marion.
<b>Development Plan</b>	Consolidated 20/02/2018.
<b>Type of Development</b>	Merit
<b>Public Notification</b>	Category 2.
<b>Representations</b>	Nine (9) representations received of which three (3) wish to be heard.
<b>Referral Agencies</b>	Commissioner of Highways.
<b>Report Author</b>	Malcolm Govett, Planning Officer.
<b>RECOMMENDATION</b>	Development Plan Consent subject to conditions.

## EXECUTIVE SUMMARY

Nielsen Architects has applied for Development Plan consent to undertake the staged demolition of all existing buildings on the site and the staged construction of a new ALDI supermarket and Chemist Warehouse building with associated car parking for up to 120 cars, signage, fencing and landscaping within the Neighbourhood Centre Zone at South Road, Clovelly Park.

The new shop buildings will operate seven days per week and are classified as a form of merit development. The key planning issues in respect of the proposed development are the bulk and scale of the built form and the potential noise impacts at the interface between land uses.

Category 2 public notification was undertaken on two separate occasions and the State Commission Assessment Panel received a total of nine (9) written submissions of which three (3) wish to be heard.

Marion Council has indicated it generally supports the proposed development, but has raised several concerns including bulk and scale of proposed built form, potential for increased noise impacts, increased volume of vehicle movements in the surrounding road network, the increased movements of delivery vehicles along Wingfield Street and Norrie Avenue, safe movement of large delivery vehicles into Norrie Avenue, and the need for additional landscaping throughout the development.

On balance, it is considered the application has sufficient merit to warrant support. It is considered the proposed development is an envisaged land use activity within the Neighbourhood Centre Zone and is consistent with the relevant Concept Plan for Clovelly Park. It is also considered the proposal would provide a sufficient number of off-street car parks and the predicted increase in traffic movement would not adversely impact the surrounding road network.

Furthermore, it is considered the proposed built form would provide a suitable standard of design and appearance, mitigation of potential noise impacts to conform to the Environmental Protection (Noise) Policy, and sufficient landscaping would be provided throughout the site.

It is recommended the Panel grant Development Plan Consent subject to the application of appropriate conditions.

## **ASSESSMENT REPORT**

### **1. BACKGROUND**

#### **1.1 Pre-Lodgement Process**

At the date of lodgement of the application the State Commission Assessment Panel (SCAP) is the relevant planning authority for this application pursuant to Item 20 (1) under Schedule 10 of the *Development Regulations 2008*, because:

- The total amount to be applied to any work within the area of Metropolitan Adelaide exceeds \$5 million, and
- The State Coordinator-General determined the assessment of the development would be best achieved under a scheme established by the Department of the Minister to facilitate the assessments of such developments.

#### **1.2 Amended Proposal**

In December 2018, the applicant decided to amend the proposed development and made the following key changes:

	<b>Original Proposal</b>	<b>Amended Proposal</b>
<b>Development Site:</b>	Ten (10) allotments. Site area of 8,422m <sup>2</sup> .	Eleven (11) allotments. Addition of 17 Wingfield Street. Site area of 9,207m <sup>2</sup> .
<b>Car Parking:</b>	Up to 117 parks (shared).	Up to 120 parks (shared).
<b>Floor Area:</b>	Chemist Warehouse building of 700m <sup>2</sup> .	Chemist Warehouse building of 1101m <sup>2</sup> .
<b>Vehicle Access:</b>	New dual access crossover to South Road. Existing dual access crossover to be deleted.	Existing South Road dual access crossover retained. New dual access crossover deleted.
<b>Built Form (ALDI):</b>	Western elevation to Wingfield Street is 6.3m high and 63m long.	Unchanged.
	Southern elevation to Norrie Avenue is 6.3m high and 31m long.	Unchanged.
	Pre-cast panels in single colour.	Pre-cast panels in three colours. Western elevation incorporates steel feature screens with perforated metal panels.
<b>Built Form (Chemist Warehouse):</b>	Western elevation to Wingfield Street is 6.3m high and 17.3m long.	33m long
	Northern elevation is 6.3m high and 41m long.	34.7m long
	Pre-cast panels in single colour.	Pre-cast panels in three colours. Western elevation incorporates steel feature screens with perforated metal panels.
<b>Building Setback (ALDI):</b>	3.1m from Wingfield Street.	Unchanged.
	3.125 from Norrie Avenue.	Unchanged.
<b>Building Setback (Chemist warehouse):</b>	0m from Wingfield Street.	1.5m from Wingfield Street.
	0m from shared northern boundary.	1.5m from shared northern boundary.
<b>Landscaping:</b>	No treatment for Chemist Warehouse building.	Upright trees to 9m height to be planted for the Chemist Warehouse building along its Wingfield Street (western) boundary and the shared (northern) boundary with #15 Wingfield Street.

### **1.3 State Coordinator-General**

When the applicant amended the proposal to include an additional allotment (i.e. 17 Wingfield Street) as part of the development site, the Department sought clarification on whether the proposed amendment would necessitate a formal re-assessment by the State Coordinator-General.

The State Coordinator-General advised the proposed amendment would not:

- be at odds with the call-in criteria outlined in Schedule 10 of the *Development Regulations 2008*,
- change the essential nature of the development,
- alter the merit designation from the original call-in to non-complying

It is therefore an appropriate amendment that does not require a new call-in process.

## **2. DESCRIPTION OF PROPOSAL**

Application details are contained in **Attachment 2**.

### **2.1 The Development**

The proposed development involves the staged demolition of all of the existing buildings on the site (i.e. three dwellings and three commercial buildings) and the staged construction of two buildings in the form of an ALDI supermarket and a Chemist Warehouse building with associated off-street car parking, signage and landscaping.

<b>FEATURES OF THE ALDI SUPERMARKET</b>	
<b>Floor Area:</b>	<ul style="list-style-type: none"> <li>• Gross leasable area of 1,732m<sup>2</sup></li> <li>• Retail floor area of 1,187m<sup>2</sup></li> </ul>
<b>Building Height:</b>	<ul style="list-style-type: none"> <li>• Single storey.</li> <li>• Generally 6.275m.</li> <li>• Tower element along Wingfield Street will be 8.5m.</li> </ul>
<b>Building Setbacks:</b>	<ul style="list-style-type: none"> <li>• 3.1m from Wingfield Street.</li> <li>• 3.1m from Norrie Avenue.</li> <li>• 52m from South Road.</li> </ul>
<b>External Finishes:</b>	<ul style="list-style-type: none"> <li>• Precast concrete panel walls in alternating (3) reddish brown tones.</li> <li>• Precast concrete panels for tower element in dark grey.</li> <li>• Pre-coated metal roof sheets.</li> <li>• Steel feature screens with perforated metal panels along western elevation.</li> </ul>
<b>Car Parking:</b>	<ul style="list-style-type: none"> <li>• Up to 120 car parks across the site.</li> <li>• 58 parks allocated to ALDI.</li> <li>• Development deed prepared between the two parties to ensure vehicle access and car parking will be managed across the site.</li> </ul>
<b>Service Vehicles:</b>	<ul style="list-style-type: none"> <li>• 19m long semi-trailers.</li> <li>• Small rigid vehicles for bakery products.</li> </ul>
<b>Service Deliveries:</b>	<ul style="list-style-type: none"> <li>• Loading area at the south-west corner of the site adjacent to Norrie Avenue.</li> <li>• Require 24 hour delivery access with 2 main deliveries per day.</li> </ul>
<b>Operating Hours:</b>	<ul style="list-style-type: none"> <li>• 7 days per week.</li> </ul>



	<ul style="list-style-type: none"> <li>7.00am to 9.00pm.</li> </ul>
<b>Landscaping:</b>	<ul style="list-style-type: none"> <li>2m wide buffer to Wingfield Street &amp; Norrie Avenue.</li> <li>4.5m wide buffer to South Road.</li> <li>Pockets throughout the car park.</li> </ul>

FEATURES OF THE CHEMIST WAREHOUSE SHOP	
<b>Floor Area:</b>	<ul style="list-style-type: none"> <li>Retail floor area of 1,101m<sup>2</sup></li> </ul>
<b>Building Height:</b>	<ul style="list-style-type: none"> <li>Single storey.</li> <li>Generally 6.275m.</li> <li>Tower element will be 7.8m.</li> </ul>
<b>Building Setbacks:</b>	<ul style="list-style-type: none"> <li>1.5m from Wingfield Street.</li> <li>1.5m from shared boundary with #15 Wingfield Street.</li> <li>50m from South Road.</li> </ul>
<b>External Finishes:</b>	<ul style="list-style-type: none"> <li>Precast concrete panels in alternating (3) grey tones.</li> <li>Precast concrete panels in yellow.</li> <li>Zincalume metal roof sheets.</li> <li>Steel feature screens with perforated metal panels along western elevation.</li> </ul>
<b>Car Parking:</b>	<ul style="list-style-type: none"> <li>Up to 120 car parks across the site.</li> <li>62 parks allocated to Chemist Warehouse.</li> <li>Development deed prepared between the two parties to ensure vehicle access and car parking will be managed across the site.</li> </ul>
<b>Service Vehicles:</b>	<ul style="list-style-type: none"> <li>Trucks and vans up to 10m long.</li> </ul>
<b>Service Deliveries:</b>	<ul style="list-style-type: none"> <li>Loading area adjacent to Wingfield Street at western end of the building.</li> <li>Maximum of 2 deliveries per day.</li> </ul>
<b>Operating Hours:</b>	<ul style="list-style-type: none"> <li>7 days per week.</li> <li>7.30am to 8.00pm and 10.00am to 6.00pm on Sundays.</li> </ul>
<b>Landscaping:</b>	<ul style="list-style-type: none"> <li>1.5m wide buffer to Wingfield Street and the shared northern boundary.</li> </ul>

## 2.2 Traffic and Vehicle Access

Future vehicle access to the development site will be provided via South Road, Norrie Avenue and Wingfield Street:

1. Along South Road, the existing dual access point will be re-installed in its current location and facilitate left turn ingress and egress for light vehicles only.
2. In Norrie Avenue, a new access point will be created to facilitate the movement of light vehicles and the larger service vehicles up to 19m long, as well as unrestricted turning movements.
3. In Wingfield Street, the two existing access points will be consolidated into a single access with un-restricted turning movements for light vehicles and large service vehicles up to 10m long.

The traffic consultant for the applicant considers that all of the proposed access points will operate suitably. To enable the 19m long service vehicles to make deliveries to the ALDI supermarket via the new Norrie Avenue access point, a section of the existing on-street parking area along Norrie Avenue will need to be removed.

## 2.3 Advertising Signs

External advertising signage for the development includes the following:

- Two free standing pylon signs of 8m high by 3m wide with shared advertising along the South Road frontage to the site.
- A lifestyle graphic of 3.3m high by 7m wide on the northern elevation of the ALDI building.
- Large gable signs of 2.4m high by 2m wide on the western, northern and eastern elevations of the ALDI building's tower element.
- A small gable sign of 1.4m high by 1.2m wide on the southern elevation of the ALDI building.
- The signage for the Chemist Warehouse building is wall painted/graphic and not illuminated.

## 2.4 Environmental Noise

The acoustic consultant for the applicant finds the predicted noise levels from the proposed development will achieve the relevant requirements of the *Environmental Protection (Noise) Policy 2007*, but subject to:

1. Construction of a 2.4m high "colorbond" fence along part of the northern boundary of the site which is to be sealed airtight at all junctions.
2. Construction of a 2.4m high "colorbond" fence along part of the Wingfield Street boundary of the site adjacent to the Chemist Warehouse loading bay, which is to be sealed airtight at all junctions.
3. Construction of a 2.4m high "colorbond" fence along part of the Norrie Avenue boundary of the site adjacent to the ALDI loading bay, which is to be sealed airtight at all junctions.
4. Installation of acoustic absorption on the ALDI delivery fence along Norrie Avenue.
5. Screening of the ALDI mechanical plant.
6. Restricting the times for rubbish collection.

## 2.5 Staging of Development

The staged development will be undertaken in the following manner:

- Stage 1: Demolition of all buildings except for the southern portion of the Chemist Warehouse which is to continue trading with 24 car parks.
- Stage 2: Construction of new ALDI supermarket and Chemist Warehouse building.
- Stage 3: Construction of most of the off-street car parking area.
- Stage 4: Remaining Chemist Warehouse building and car park demolished. ALDI supermarket and new Chemist Warehouse building commence trading.
- Stage 5: Balance of off-street car parking area constructed.

## 3. SITE AND LOCALITY

### 3.1 Site Description

The site consists of eleven (11) allotments, described as follows:

Lot No	Section	Street	Suburb	Hundred	Title
A3 D19270	x	1150-1154 South Road	Clovelly Park	Adelaide	CT5252/910
A29 D3034	x	1156-1158 South Road	Clovelly Park	Adelaide	CT5148/210
A30 D3034	x	1156-1158 South Road	Clovelly Park	Adelaide	CT5148/211
A218 F12391	x	1156-1158 South Road	Clovelly Park	Adelaide	CT5148/214

A217 F12391	x	1156-1158 South Road	Clovelly Park	Adelaide	CT5148/217
A216 F12391	x	1156-1158 South Road	Clovelly Park	Adelaide	CT5148/47
A1 D19270	x	17 Wingfield Street	Clovelly Park	Adelaide	CT5191/11
A2 D19270	x	19 Wingfield Street	Clovelly Park	Adelaide	CT5450/122
A28 D3034	x	5 Norrie Avenue	Clovelly Park	Adelaide	CT5258/302
A27 D3034	x	3 Norrie Avenue	Clovelly Park	Adelaide	CT5676/287
A26 D3034	x	1 Norrie Avenue	Clovelly Park	Adelaide	CT5132/506

**Figure 1: The Development Site.**



The proposed development site is of irregular shape and has a land area of 9,207 square metres. It contains commercial buildings and detached residential dwellings, a vacant allotment, and a contiguous area of sealed car park which provides up to 81 car parks. The existing Chemist Warehouse and Rite Price buildings have a combined gross leasable floor area of about 2,100 square metres.

The vacant allotment at the corner of Norrie Avenue and Wingfield Street previously contained a detached dwelling. Currently, the two detached dwellings at 19 and 17 Wingfield Street are unoccupied.

The site has road frontage on three of its sides; 78 metres along South Road to the east which is a primary arterial road, 50 metres along Norrie Avenue to the south, and 120 metres along Wingfield Street to the west.

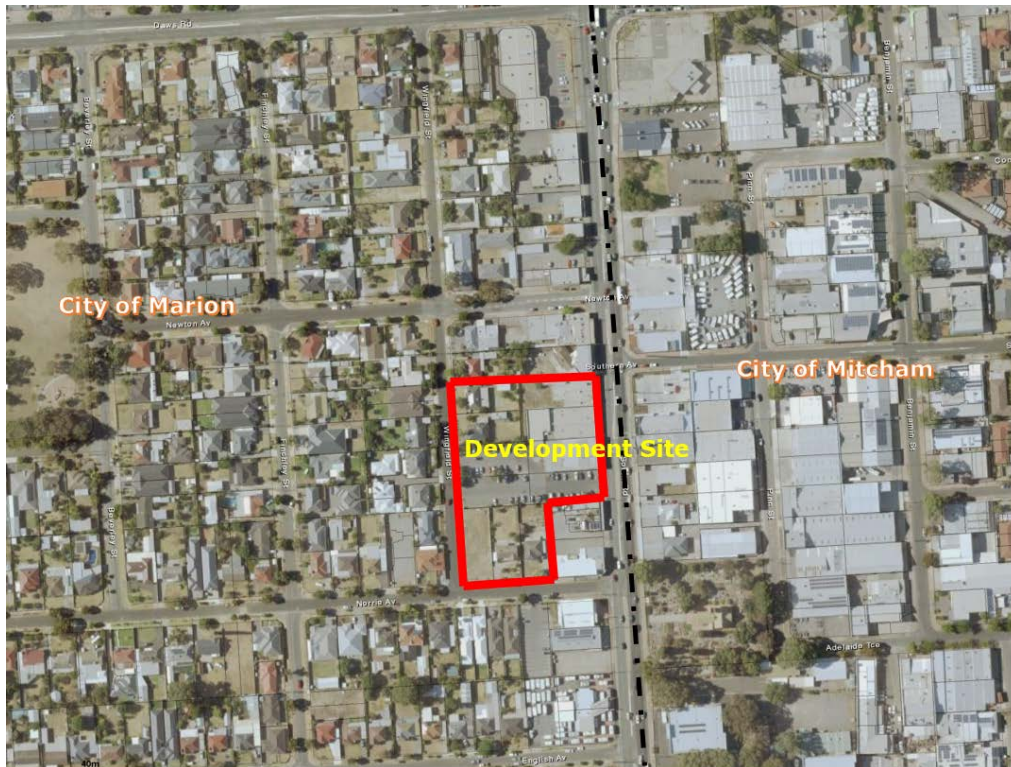
Vehicle access to the site is currently available from South Road via a dual crossover and from Wingfield Street via two separate crossovers. There is a pedestrian actuated crossing adjacent to the South Road frontage of the site. Public transport bus stops are located along South Road about 100 metres north and 250 metres south of the site.

Parking controls along the southern side of Norrie Street and the western side of Wingfield Street restrict parking to one hour during business hours on any day. The eastern side of Wingfield Street is a no stopping zone during business hours.

Due to its developed state the site is relatively flat and does not present any distinguishing physical features or vegetation cover of any significance.

### **3.2 Locality**

**Figure 2: Locality Map.**



The subject land is situated within the Neighbourhood Centre Zone. The Zone is located on the western side of South Road and extends for a distance of about 180 metres between Norrie Avenue and Newton Avenue.

The Zone is a relatively small part of a non-residential land use precinct which continues for about 1.2 kilometres south from Daws Road to the former Mitsubishi site at Tonsley Park. This spread of non-residential land use activity is also reflected on the eastern side of South Road.

Behind the Zone, further west from South Road, the land use activity is predominantly residential and mainly in the form of single storey detached dwellings.

St Mary's Anglican Church and Cemetery, designated as a State heritage place, is located across South Road to the south east of the subject land.

## **4. STATUTORY REFERRAL BODY COMMENTS**

### **4.1 Safety and Services Division, DPTI**



The Commissioner of Highways is a mandatory referral in accordance with Item 3 under Schedule 8 of the *Development Regulations 2008*. The State Commission Assessment Panel must have regard to this advice.

The Department advises of its qualified support for the proposed development.

The Department advises the adjacent section of South Road forms part of the strategically important National Land Transport Network and is classified as a Major Traffic Route, a Primary Freight Route, a Major Cycling Route, and a High Frequency Public Transport Corridor under the Department's '*A Functional Hierarchy for South Australia's Land Transport Network*'. South Road carries approximately 44,700 vehicles per day at this location.

While the subject site is not currently affected by any requirements under the Metropolitan Adelaide Road Widening Plan, both the South Australian and Australian Governments have indicated clear commitments to progress a non-stop North-South Corridor for Adelaide and are working together to identify the next priority section of the corridor. At this stage, the nature and timing of potential improvements to this section of South Road are yet to be determined and any potential future land requirements are unknown. Accordingly, the Department cannot guarantee the development will not be impacted in the future.

The South Road access will be located adjacent to the southern end of the site at the location of an existing access serving the site. The Department supports this design as it maximises the separation between the access and the South Road/Southern Avenue junction as well as the adjacent pedestrian actuated crossing. However, the connection to the first north-south aisle should be modified to operate on a left turn in and left turn out basis only, to ensure vehicles entering the site are not impeded by vehicles attempting to turn right.

The Department does not object to the location or operation of the other proposed access points to the site. However, given the potential for movements associated with the ALDI loading bay to interfere with traffic utilising the Norrie Avenue access, it suggests that appropriate traffic management arrangements will need to be put in place to ameliorate this issue.

The Department is satisfied that the development will not adversely impact on the adjacent road network provided the recommended modification to the car park design is undertaken.

All conditions and advisory notes, except for those pertaining to outdoor signage, have been included in the recommendation. A full copy of the referral response is contained in **Attachment 3**.

## 5. COUNCIL COMMENTS

### 5.1 City of Marion

The Council is generally supportive of the proposed development but acknowledges there are several elements which require further attention before any decision can be made.

#### Built form

Council is concerned the redevelopment of the site will present a considerable expanse of blank wall to a height of 6.25m. This means the existing occupants along Norrie Avenue and Wingfield Street will experience a changed outlook, particularly in relation to the bulk/scale of the built form and the minimal setbacks to boundaries.

#### Traffic and Access

The on-site car parking to be provided would exceed the minimum requirements recommended in the Development Plan. Council recommends that appropriate reciprocal access rights should be placed on the relevant titles to ensure the full balance of the parking provided remains accessible for the whole of the mixed use development.

The proposal would result in an increase in traffic movements throughout the local road network, and an increase of delivery trucks and heavy vehicles particularly along Norrie Avenue and Wingfield Street.

The request for removal of on-street parking along the southern side of Norrie Avenue should be resolved prior to the issuing of Development Plan Consent.

#### Land use interface

The proposed development would considerably change the built form appearance and likely intensity of use, for example hours of operation, noise and traffic movements. A condition of consent should seek to limit/control the proposed delivery and collection hours for service vehicles so as to ensure there are no unreasonable impacts on adjacent allotments.

#### Landscaping

The extent of landscaping proposed adjacent the western façade of the ALDI building and along the South Road frontage is limited, and does not significantly assist in complementing the built form and reducing the visual impact of the larger buildings or enhance the appearance of the primary road frontage. Additional ground level plantings should be incorporated to assist in providing an improved streetscape outcome and to increase the overall attractiveness of the site. Such plantings should occur adjacent the southern and western ALDI facades, and adjacent the South Road frontage.

#### Stormwater

The stormwater calculations and discharge points look acceptable, although approval to connect into the pits along South Road will have to come from the Department.

Most of the conditions recommended by the Council have been included in the recommendation to SCAP. A full copy of the Council's referral response is contained in **Attachment 4**.

## **6. PUBLIC NOTIFICATION**

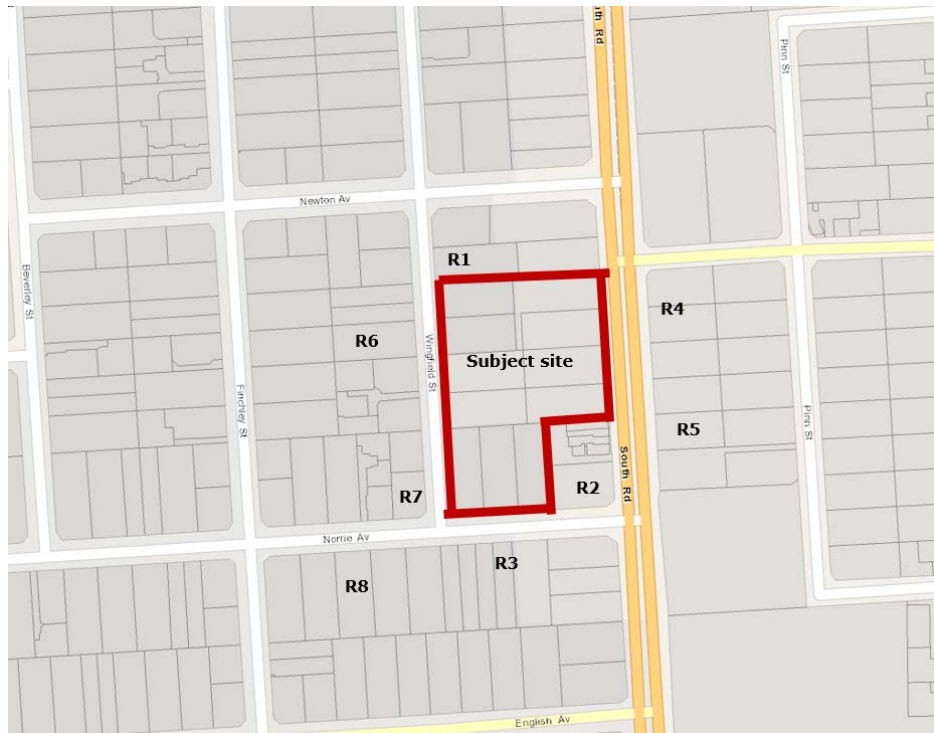
The application was notified as a Category 2 development pursuant to Item 19 in Part 2 under Schedule 9 of the *Development Regulations 2008*. Public notification was undertaken by directly contacting adjoining owners and occupiers of the land) on two separate occasions, firstly in September 2018 and then in January 2019 after the applicant amended the proposal.

A total of nine (9) representations were received, although one of the representors made a submission during each period of notification. Two (2) of the representors support the development, four (4) of them support it with some concerns, and two (2) of them oppose the development.

Three (3) of the representors wish to be heard by the State Commission Assessment Panel.



**Figure 3: Map of Representors.**



A copy of each representation and the applicant's response are respectively contained in **Attachment 5** and **Attachment 6**.

**Figure 4: Summary of Submissions & Responses by Applicant.**

<b>Representor ID</b>	<b>Issue</b>	<b>Applicant's Response</b>
R1	<i>Size and location of proposed supermarket.</i>	<i>Additional gross leasable floor area of about 700m<sup>2</sup> is appropriate and would not compromise the intent of the zone.</i>
	<i>Demolition of dwelling at 19 Wingfield Street.</i>	<i>A detached dwelling is not envisaged or desired within the zone.</i>
R2	<i>Does not want to be charged for replacement of the existing fence to a 2.4m high fence.</i>	<i>The proponent will bear all costs associated with the replacement of any existing fencing.</i>
R3	<i>Noise associated with service vehicles would adversely affect residential amenity.</i>	<i>Subject to certain acoustic treatments, the proposal will satisfy the requirements of the Environment Protection (Noise) Policy.</i>
	<i>Proposal would significantly increase traffic movements and congestion along Norrie Avenue.</i>	<i>Traffic consultant advises the additional traffic generated will not adversely impact on the surrounding road network. Also, there will be no adverse impacts from the right turn lane along Norrie Street into the site.</i>
	<i>Increased traffic movements along Norrie Avenue will</i>	<i>Traffic consultant advises the right hand turn from Norrie Avenue into</i>

<b>Representor ID</b>	<b>Issue</b>	<b>Applicant's Response</b>
	<i>create a major problem for those exiting onto South Road.</i>	<i>South Road will experience a marginal increase in queue length.</i>
	<i>Concerned about noise generated by the ALDI rubbish compactor and waste storage bins.</i>	<i>Subject to certain acoustic treatments, the proposal will satisfy the requirements of the Environment Protection (Noise) Policy.</i>
	<i>Graffiti is an ongoing problem and ALDI must ensure to arrange for its immediate removal if and when it occurs.</i>	<i>Any graffiti which appears on the site will be removed by store management as soon as practical.</i>
	<i>Concerned their privacy and amenity will be compromised by light-spill and security sensors and CCTV.</i>	<i>The advertising signs to be used throughout the development will incorporate a low level of illumination and will comply with relevant Australian Standards. DPTI not opposed to car park area being illuminated provided it complies with Australian Standards regarding illumination.</i>
	<i>The existing short-term parking strip along Norrie Avenue should be converted to a residents only permit parking area to overcome the difficulties experienced by the Council's refuse collection contractor.</i>	<i>Car parking restrictions within the surrounding road network is a matter for Marion Council to address and not relevant to the application.</i>
	<i>Value of their property will significantly reduce due to the close proximity of proposed supermarket.</i>	<i>Impact on property values is not a planning consideration.</i>
<i>R4</i>	<i>Redevelopment should improve the parking situation for customers of the Chemist Warehouse.</i>	<i>The proposed car parking comfortably exceeds the Development Plan requirements.</i>
<i>R5</i>	<i>The design takes into consideration the residential development on the western side of Wingfield Street and increases the visual amenity in the locality.</i>	<i>Agreed.</i>
<i>R6</i>	<i>Increased car movements will adversely affect access to private property.</i>	<i>Traffic consultant advises the additional traffic generated will not adversely impact on the surrounding road network.</i>
<i>R7</i>	<i>Noise generated by service vehicles and other associated activities outside of 7am to 7pm.</i>	<i>Subject to certain acoustic treatments, the proposal will satisfy the requirements of the Environment Protection (Noise) Policy.</i>
	<i>Inadequate number of car parks provided.</i>	<i>The proposed car parking comfortably exceeds the Development Plan requirements.</i>

<b>Representor ID</b>	<b>Issue</b>	<b>Applicant's Response</b>
<i>R8</i>	<i>Narrow streets and excess traffic.</i>	<i>Traffic consultant advises the additional traffic generated will not adversely impact on the surrounding road network.</i>

## **7. POLICY OVERVIEW**

The subject site is within the Neighbourhood Centre Zone as described within the Marion Council Development Plan Consolidated 20 February 2018. Relevant planning policies are contained in **Attachment 7** and are summarised below.

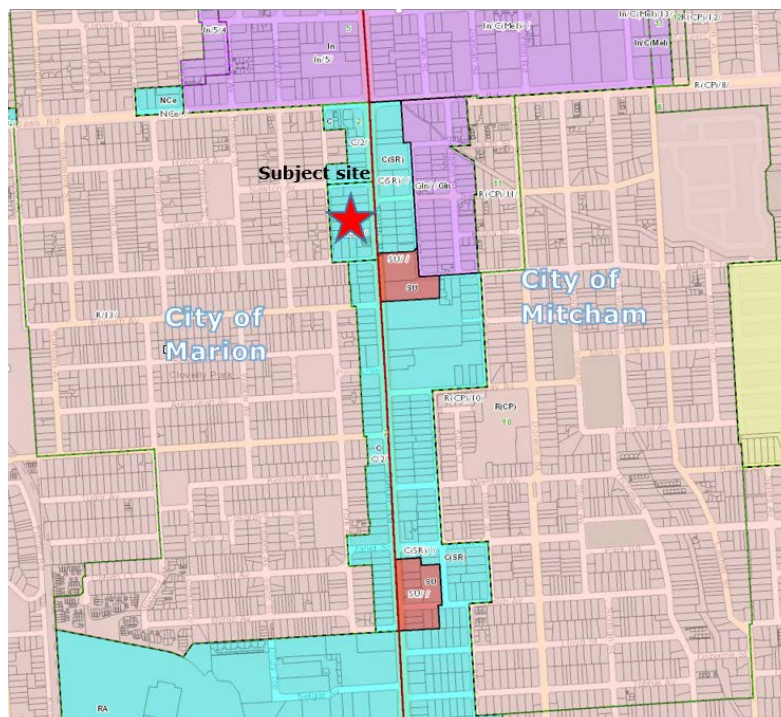
### **7.1 Zone**

The key objectives of the Neighbourhood Centre Zone are to provide a range of facilities to meet the shopping, community, business and recreational needs of the surrounding neighbourhood and to provide the main focus of business and community life outside of a district centre (Objectives 1 and 2).

Residential development is envisaged within the Zone only where it is in conjunction with non-residential development. That is, it should be located only behind or above non-residential uses on the same allotment. Where residential development is undertaken it should not prejudice the operation of existing and future retail activity within the Zone (Objective 3 & Principles of Development Control 1, 2, 3 and 4).

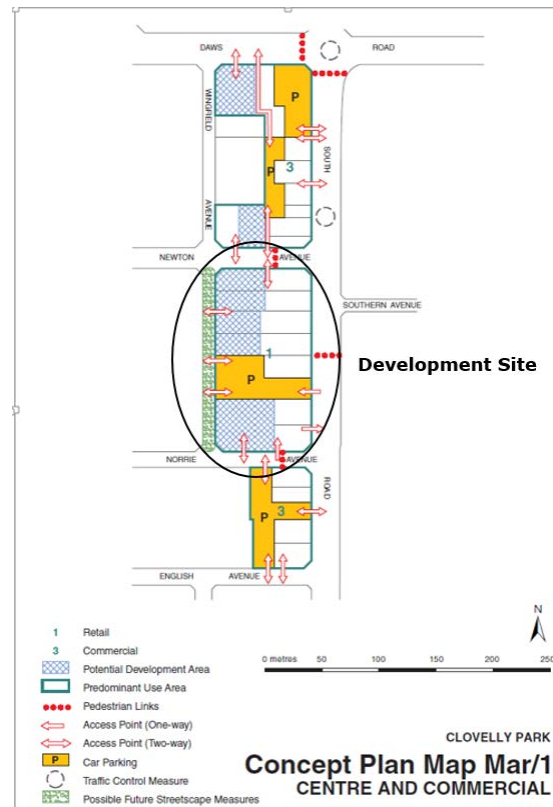
The recommended maximum gross leasable retail floor area for any building at the Clovelly Park Neighbourhood Centre should be in the order of 2,000m<sup>2</sup> (Principle of Development Control 5). A discount can be applied in respect of off-street car parking where a site is classified as a Designated Area (Principle of Development Control 7).

**Figure 5: Zoning Map.**



Development within the centre should be carried out in accordance with the concept plan provided in below in Figure 6:

**Figure 6: Concept Plan (Marion Council Development Plan).**



## 7.2 Council Wide

The General Section (Council Wide) of the Marion Council Development Plan contains broad policies relating to 'Advertisements', 'Centres and Retail Development', 'Design and Appearance', 'Interface between Land Uses', 'Landscaping, Fences and Walls', and 'Transportation and Access' which are considered to be relevant to the proposed development.

## 8. PLANNING ASSESSMENT

The application has been assessed against the relevant provisions of the Marion Council Development Plan, consolidated on 20 February 2018, which are contained in **Attachment 7**.

### 8.1 Land Use and Character

Objectives 1 and 2 and Principle of Development Control 1 for the Neighbourhood Centre Zone recommend development of shopping facilities that meet the needs of the community within the precinct.

In Schedule 1 of the *Development Regulations 2008*, a "shop" means "premises used primarily for the sale by retail, rental or display of goods, foodstuffs, merchandise or materials". It is considered the proposed ALDI supermarket and the Chemist Warehouse building would comply with the meaning of a shop. As a result, it is considered the proposed development is envisaged within the Neighbourhood Centre Zone and would be consistent with the Marion Council Development Plan.

Principle of Development Control 6 for the Zone recommends that development should be carried out in accordance with Clovelly Park Centre and Commercial Concept Plan Map Mar/1. The Concept Plan identifies the current under-developed allotments containing detached dwellings as being a potential development area and that any additional vehicle access points should be from Wingfield Street and Norrie Avenue rather than South Road.

It is considered the proposed development is consistent with the Concept Plan because the under-developed allotments are targeted for retail development and no additional vehicle access points would be created to/from South Road.

Principle of Development Control 5 for the Zone recommends the gross leasable floor area within the Clovelly Park precinct should generally be in the order of 2,000 square metres. Notwithstanding that the proposed development would provide about 2,800 square metres of gross leasable floor area, it is considered the proposal is generally consistent with this policy recommendation.

This is primarily because the proposal would provide sufficient off-street car parks to accommodate the increase in gross leasable floor area. Furthermore, it is considered the proposal would represent a regeneration of the precinct by providing a lift in the standard of retail services and overall shopping experience for customers.

## 8.2 Design and Appearance

The Marion Council Development, through the Concept Plan Map Mar/1, envisages the full development of the Clovelly Park shopping precinct. Moreover, Objective 3 and Principles of Development Control 3 and 4 for the Neighbourhood Centre Zone recommend that while non-residential activity may be appropriate in some circumstances, it should not prejudice the operation of existing or future retail activity.

Currently however, only those allotments within the precinct with frontage to South Road are developed for retail/commercial use. With the built form being oriented towards South Road, the allotments within the western section of the precinct, which are predominantly oriented towards Wingfield Street, have remained under-developed.

This situation has resulted in some residents along Wingfield Street having an outlook over a relatively large bitumen car park and parts of the development site remaining in use for residential development.

It is considered there would be a number of benefits derived from the proposal to shift the built form away from South Road to Wingfield Street and Norrie Avenue, summarised as follows:

- it would minimise the potential future risks arising from road widening and land acquisition associated with the eventual construction of the North-South Corridor;
- it would not reduce the current vehicle access to the precinct and would not introduce any additional vehicle access points along Wingfield Street; and
- it would realise the highest and best use of the land for retail/commercial development by removing a number of existing non-conforming uses (i.e. detached dwellings) from the precinct.

It is also acknowledged that the proposed redevelopment would likely result in increased traffic movements along Norrie Avenue and Wingfield Street.

In the General Section of the Marion Council Development Plan, Principle of Development Control 1 under Design and Appearance recommends that buildings



should incorporate contemporary designs that have regard to building height, mass and proportion.

The Marion Council has identified the proposed development would present a considerable expanse of blank wall to a height of about 6.3 metres along Wingfield Street and Norrie Avenue as well as provide a changed outlook in relation to bulk and scale of the built form and minimal setbacks to boundaries.

More specifically, the proposed ALDI building would have a wall length of 63 metres along Wingfield Street and 31 metres along Norrie Avenue, and the proposed Chemist Warehouse building would have a wall length of 33 metres along Wingfield Street and 34.7 metres along the shared boundary with number 15 Wingfield Street.

It is noted the applicant has applied a number of design techniques to the proposed buildings in order to reduce their bulk and scale, and to add some interest to the visual outlook. The relevant design techniques include:

1. The use of alternating colour tones on the precast concrete walls of both buildings;
2. The use of steel screens with perforated metal panels along the western elevations of both buildings;
3. A building setback of 3.1 metres for the ALDI building and the use of medium size trees to 7 metres height within the setback; and
4. A building setback of 1.5 metres for the Chemist Warehouse building and the use of upright trees to 9 metres height within the setback.

It is considered the proposed changes to the built form and the application of the design techniques along the western or Wingfield Street elevation of the development would provide an improved visual outlook for owners/occupants of land in the locality. This is because their current outlook is over a relatively large and under-maintained car park as shown in Figures 7 and 8.

**Figure 7: Existing Northern Access Point to Car Park from Wingfield Street.**





**Figure 8: Existing Southern Access Point to Car Park from Wingfield Street.**



### **8.3 Off-Street Car Parking**

In accordance with Table Mar/2A and Principle of Development Control 7 for the Neighbourhood Centre Zone in the Marion Council Development Plan, the proposed development site is considered to be a Designated Area regarding the provision of off-street car parking. This is because the site lies within a Neighbourhood Centre Zone and within 200 metres of a bus stop for a high frequency public transit service.

This designation is confirmed by GTA Consultants in their Traffic Impact Assessment report for the applicant. It is also noted the Marion Council considers the proposal would provide a sufficient number of off-street car parks.

It is considered the proposed development would provide sufficient off-street car parking (as shown in Figure 9 below) in accordance with the guidelines in the Development Plan (Tables Mar/2 & Mar/2A).

**Figure 9: Car Parking Assessment.**

	Form of Development	Car parks Required	ALDI (1732m <sup>2</sup> )	Chemist Warehouse (1101m <sup>2</sup> )	Total Required	Proposed	Guideline Achieved
Development Plan: Table 2	Shop	5 per 100m <sup>2</sup> (where located within a centre)	87	55	142	120	YES <input type="checkbox"/> NO <input checked="" type="checkbox"/> PARTIAL <input type="checkbox"/>
Development Plan: Table 2A	All Designated Areas	Minimum of 3 per 100m <sup>2</sup> gross leasable floor area	53	33	86	120	YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> PARTIAL <input type="checkbox"/>
Development Plan: Table 2A	All Designated Areas	Maximum of 6 per 100m <sup>2</sup> gross leasable floor area	104	66	170	120	YES <input type="checkbox"/> NO <input checked="" type="checkbox"/> PARTIAL <input type="checkbox"/>

It is also considered that a significant proportion of the car parking demand generated by the Chemist Warehouse use would be short-term, high turnover parking. It is

further considered that this situation would enable the proposed car parking area to accommodate overflow parking from other land use activities within the precinct.

The Marion Council suggests that reciprocal access rights should be placed on the relevant land titles to ensure the full balance of the parking provided remains accessible to the whole of the mixed use development. It is not considered necessary for this aspect of the proposed development to be controlled due to the 120 car parks to be provided being in excess of the requirements of the Development Plan.

#### **8.4 Traffic and Movement**

The proposed development would provide for three vehicle access points to the development site which is considered to be consistent with the recommendations of the Concept Plan Map Mar/1.

It would also direct service vehicles, large and small, to the proposed access points along Norrie Avenue and Wingfield Street which would provide immediate access to the loading bays for ALDI and Chemist Warehouse respectively.

Currently, service vehicles to the existing Chemist Warehouse premises tend to access the site mainly from South Road. It is noted the Department and GTA Consultants both consider the predicted increased traffic movements from the proposed development would not adversely impact the surrounding road network.

It is considered the proposal would minimise the impacts of the likely increased traffic movements through Wingfield Street by not increasing the number of vehicle access points within that street, but instead by funnelling future traffic movements through the existing access point, which will be upgraded.

The Marion Council advises that if the existing on-street parking area along Norrie Avenue, between South Road and Wingfield Street, remains in place then it is concerned that vehicle movements for large delivery trucks to/from the proposed access point within Norrie Avenue would be compromised and could result in unsafe movements.

Council strongly advises the proponent to consult with the Council regarding the potential removal of this section of on-street parking and that this matter should be resolved prior to the issuing of Development Plan Consent.

It is noted that GTA Consultants agree that the section of on-street parking within Norrie Avenue will need to be removed in order to enable the movement of large delivery trucks from South Road into the Norrie Avenue access point.

It is considered that if the application were to be approved then the negotiation between the applicant and the Council about the removal of the on-street parking area within Norrie Avenue could more appropriately take place after the Development Plan Consent has been issued. This is because the affected section of Norrie Avenue lies outside of the boundaries of the proposed development, preventing consideration of the matter as part of this application.

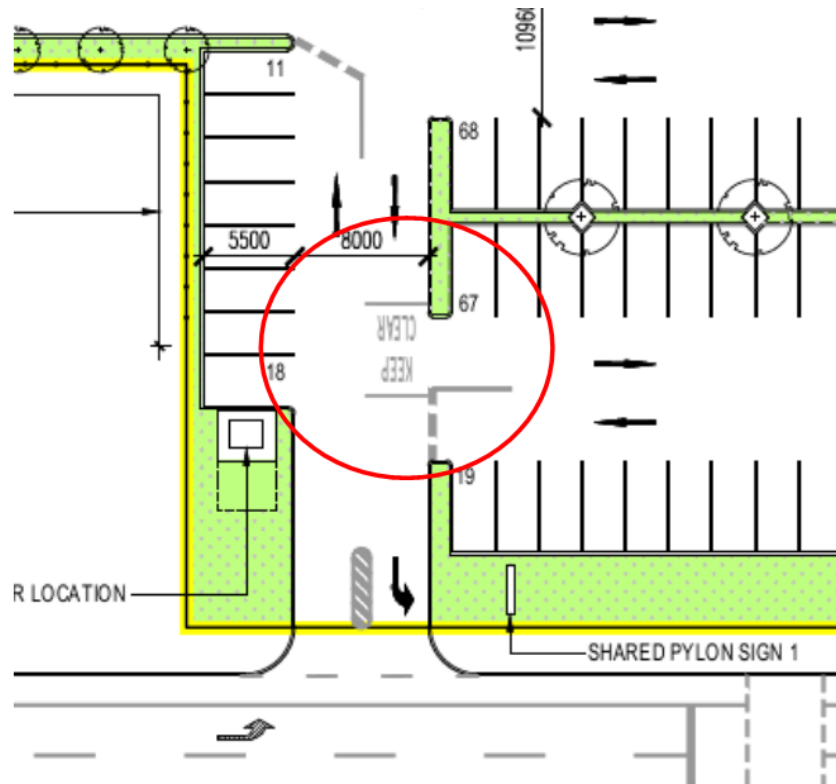
The Department recommends that the first north-south car park aisle adjacent to the South Road frontage be amended to operate as a left in and left out only. It believes that such restricted movements will avoid instances where vehicles entering the site are not impeded by vehicles attempting to turn right.

In response, GTA Consultants believe such recommendation would be impractical and would impact the car park layout and cause a loss of car parks. Moreover, it believes there would be sufficient distance within the car park from the South Road kerb to

ensure vehicles will not queue back to South Road based on the anticipated traffic volumes during the peak hour.

GTA Consultants suggest the addition of a 'Keep Clear' marking at the aisle intersection as shown in Figure 10 would assist in managing this issue.

**Figure 10: Suggested 'Keep Clear' Traffic Management Measure.**



It is considered that driver behaviour would see most vehicles driven further west into the car park in order to find a car park as close as possible to the new retail buildings.

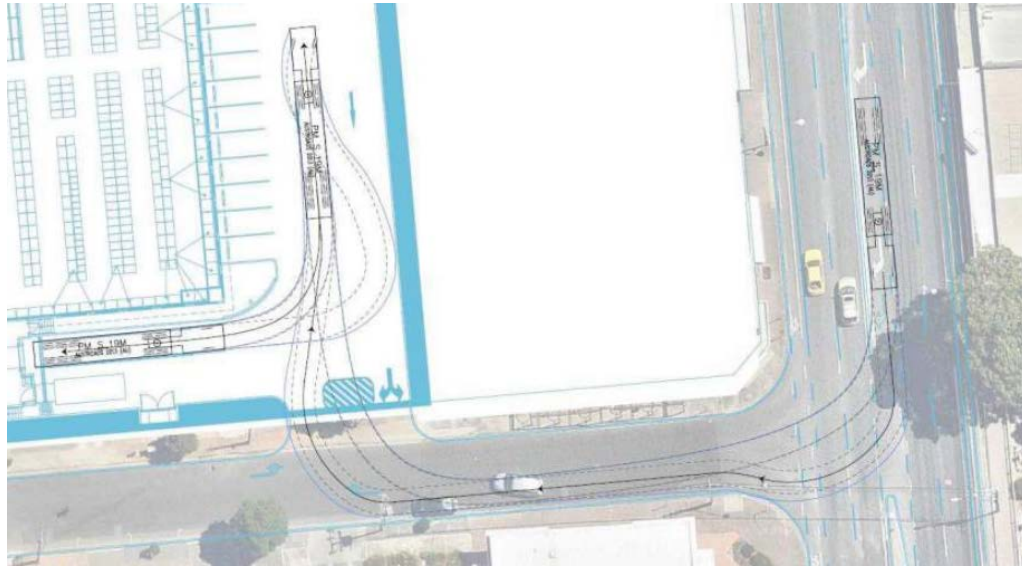
Also, in the event there is a queue of vehicles waiting to exit the site which extends across the aisle then vehicles entering the site would not be able to turn right and would continue further west into the site. In these circumstances, it is considered that no redesign of the car park or additional traffic management measures are warranted.

The Department also recommends that additional traffic management measures may be required to mitigate potential conflict at the proposed Norrie Avenue access point between delivery vehicles using the ALDI loading bay and traffic using the access point.

In response, GTA Consultants argues that the proposed loading bay design and access point are typical for many ALDI developments and no issues have arisen so far for their stores within Australia. A turn-path diagram demonstrating service vehicle manoeuvrability for delivery vehicles entering and exiting the proposed loading bay via Norrie Avenue is provided in Figure 11.

As the frequency of the large delivery trucks would be only twice daily, it is considered no additional traffic measures would be required to be implemented at the proposed Norrie Avenue access point and loading bay.

**Figure 11: Entry to ALDI Loading Bay for 19m Large Delivery Vehicle.**



On this basis the proposed site access, on-site parking and traffic management arrangements are considered acceptable and in accordance with relevant Development Plan policy.

## **8.5 Environmental Factors**

### **8.5.1 Noise Emissions**

It is considered the greatest potential for adverse noise impacts associated with the proposed development would be at the proposed loading bay for the ALDI building, which is adjacent to Norrie Avenue. This is primarily because the service vehicles for ALDI would likely be larger in size, up to 19 metres in length, and the applicant would prefer they be able to deliver twice daily to the site at any time, seven days per week.

Notwithstanding that the owners/occupiers of land adjacent to non-residential precincts should expect a higher level of activity and consequent noise, it is considered that some measures may need to be taken to minimise noise impacts at the land use interfaces along Norrie Avenue and Wingfield Street.

In this regard, Sonus advises that the proposed development would be able to comply with the *Environment Protection (Noise) Policy 2007*, subject to the implementation of acoustic treatments relating to:

- The screening of the ALDI mechanical plant;
- Specific fence heights and construction;
- Installation of acoustic absorption on the ALDI delivery fence; and
- Restricting the times for rubbish collection.

Although the proposed loading bay for the Chemist Warehouse building would be adjacent to Wingfield Street, it is considered there would be no significant noise impacts within that activity area associated with delivery vehicles. This is because the delivery vehicles for the proposed Chemist Warehouse building would be smaller in size, less than 10 metres in length, and their deliveries would be restricted to business hours only.

If the application were to be approved, it is considered that the acoustic treatments recommended by Sonus could be applied as conditions of

Development Plan Consent. In addition, it is considered the proposed restriction on the times for rubbish collection from the site could be extended to include delivery vehicles, in particular the large 19 metre long vehicles for ALDI.

## 8.6 Signage

The Marion Council Development Plan provides guidelines for advertisements or signs in its General Section. Objective 2 and Principle of Development Control 2 recommend that advertising signs should not create a hazard through driver distraction while Principle of Development Control 20 recommends that advertising signs within a Neighbourhood Centre Zone should have a maximum height of eight (8) metres and a maximum display area of 12 square metres.

Three signs proposed for the development which would exceed the maximum display area of 12 metres recommended in the Development Plan, as follows:

1. At 18 square metres for the two free standing pylon signs to be located along the South Road frontage of the site, and
2. At 21 square metres for the external graphic sign on the northern elevation of the tower element of the proposed ALDI building.

It is considered these variations to the recommended advertising sign maximum display area could be tolerated given the signs relate to the two dominant land uses within the precinct which are setback more than 50 metres from the South Road property frontage. Hence the visibility of the signs is an important design feature.

The relatively large size of the proposed external graphic sign is considered to be tolerable. This is because it would be located within a somewhat closed viewing field inside the Wingfield Street entrance to the site and opposite the Chemist Warehouse building, and oriented to the north away from the nearby residential development.

The Department has reviewed the proposed advertising signs and indicates they would achieve adequate clearances from direct sightlines to the traffic signals near the site. Consequently, it is considered the proposed signage would not cause a hazard through driver distraction.

It is considered that if the application were to be approved then the issue of illumination from any proposed advertising sign could be dealt with as an appropriate condition attached to the Development Plan Consent.

## 8.7 Landscaping

The Marion Council believes the extent of landscaping adjacent to Wingfield Street and along the South Road frontage of the site is limited and does not significantly assist in:

1. complementing the built form
2. reducing the visual impact of larger buildings
3. enhancing the appearance of the primary road frontage

The Council suggests additional ground level plantings should be incorporated adjacent to the western and southern building facades and the South Road frontage.

It is considered the extent of landscaping proposed for the development is sufficient as the Landscape Concept Plan and Plant Palette included in **Attachment 1** shows the use of ground level plantings within the building setbacks adjacent to South Road, Norrie Avenue and Wingfield Street.

Also, the planting intervals between trees are considered to be appropriate at about 5-6 metres adjacent to the Chemist Warehouse building and about 10 metres adjacent to the ALDI building. It is considered that future opportunities exist for the Council to



undertake further kerbside plantings to complement the landscaping regime provided on the development site and thereby further enhance the built form.

### **8.8 Interface**

It is considered there would be one other potential interface conflict associated with the proposed development, but it is located entirely within the Neighbourhood Centre Zone. It relates to the proposed built form along the shared boundary with number 15 Wingfield Street.

In this regard, the proposed northern wall for the Chemist Warehouse building would be 6.3 metres high and extend for nearly the entire length of the shared boundary with number 15 Wingfield Street. Most importantly, it is noted the proposed Chemist Warehouse building would be situated on the southern side of the existing dwelling.

Consequently, it is considered there would be no significant adverse impacts on the access to sunlight for the affected dwelling and its associated private open space. Also, design treatments would be undertaken in the form of landscaping and alternating colour tones to improve the appearance of the external wall.

## **9. CONCLUSION**

On balance, the application is considered to display sufficient merit to warrant support.

Overall, it is considered that the proposed development, being for the establishment of a group of shops, is a type of land use activity envisaged within the Neighbourhood Centre Zone. It is also considered the proposal would be generally consistent with the Clovelly Park Centre and Commercial Concept Plan contained in the Marion Council Development Plan.

Notwithstanding that the proposal would provide additional gross leasable floor area within the precinct, it is considered it would provide a sufficient number of off-street car parks and the likely increase in traffic movements generated by the proposal would not adversely impact on the surrounding road network.

It is considered the proposed built form and associated design articulation would improve the general appearance of the site and provide an improved outlook for adjacent owners/occupants of land.

It is also considered the proposed development would not cause a significant or adverse land use interface conflict due to the range of acoustic measures recommended to be implemented in relation to the likely activities in and around the loading bays for both retail enterprises.

Landscaping for the proposed development is considered to be sufficient and it is further considered there would be opportunities for the Marion Council to undertake additional planting within the kerb side to complement the design and appearance of the new buildings.

Advertising signage for the proposed development is considered to be appropriate in respect of its display areas and locations. It is further considered the illumination from the advertising signs would not significantly or adversely impact driver safety or residential amenity.

Pursuant to Section 35 (2) of the *Development Act 1993*, and having undertaken an assessment of the application against the relevant Development Plan, the application is NOT seriously at variance with the provisions of the plan.



## **10. RECOMMENDATION**

It is recommended that the State Commission Assessment Panel:

- 1) RESOLVE that the proposed development is NOT seriously at variance with the policies in the Development Plan.
- 2) RESOLVE that the State Commission Assessment Panel is satisfied that the proposal generally accords with the related Objectives and Principles of Development Control of the Marion Council Development Plan.
- 3) RESOLVE to grant Development Plan Consent to the proposal by Nielsen Architects for the staged demolition of all existing buildings and the staged construction of a new ALDI supermarket and Chemist Warehouse building with shared car parking for up to 120 cars, signage, fencing and landscaping at 1150-1154 & 1156-1158 South Road, 17-19 Wingfield Street, and 1-5 Norrie Avenue, Clovelly Park subject to the following conditions of consent.

## **PLANNING CONDITIONS**

1. The development granted Development Plan Consent shall be undertaken and completed in accordance with the stamped plans and documentation, except where varied by conditions below (if any).

Prepared by Nielsen Architects:

<b>Drawing Title</b>	<b>Number</b>	<b>Version</b>	<b>Date</b>
ALDI & Chemist Warehouse Clovelly Park – Location Plan.	DA01.1	H	28/11/2018
ALDI & Chemist Warehouse Clovelly Park – Contract Titles - Existing.	DA01.2	B	14/11/2018
ALDI & Chemist Warehouse Clovelly Park – Contract Titles - Proposed.	DA01.3	E	28/11/2018
ALDI & Chemist Warehouse Clovelly Park – Existing Site Plan.	DA02.1	C	28/11/2018
ALDI & Chemist Warehouse Clovelly Park – Demolition Site Plan.	DA02.2	C	28/11/2018
ALDI & Chemist Warehouse Clovelly Park – Proposed Site Plan.	DA02.3	M	22/02/2019
ALDI & Chemist Warehouse Clovelly Park – Proposed Roof Plan.	DA02.4	K	22/02/2019

ALDI & Chemist Warehouse Clovelly Park – ALDI External Elevations.	DA03.1	D	14/11/2018
ALDI & Chemist Warehouse Clovelly Park – Chemist Warehouse External Elevations.	DA03.2	H	22/02/2019
ALDI & Chemist Warehouse Clovelly Park – Streetscape Elevations.	DA03.3	H	22/02/2019
ALDI & Chemist Warehouse Clovelly Park – Sections.	DA05	E	14/11/2018
ALDI & Chemist Warehouse Clovelly Park – Signage Plan.	DA06.1	I	22/02/2019
ALDI & Chemist Warehouse Clovelly Park – Signage Details – Sheet 1.	DA06.2	E	14/11/2018
ALDI & Chemist Warehouse Clovelly Park – Signage Details – Sheet 2.	DA06.3	D	14/11/2018
ALDI & Chemist Warehouse Clovelly Park – Signage Details – Sheet 3.	DA06.4	C	14/11/2018
ALDI & Chemist Warehouse Clovelly Park – Proposed 3D Visual Renders – Sheet 1.	DA08.1	E	14/11/2018
ALDI & Chemist Warehouse Clovelly Park – Proposed 3D Visual Renders – Sheet 2.	DA08.2	G	22/02/2019
ALDI & Chemist Warehouse Clovelly Park – Proposed 3D Visual Renders – Sheet 3.	DA08.3	-	22/02/2019
Proposed ALDI & Chemist Warehouse Clovelly Park – Siteworks.	SK5	C	27/11/2018

Prepared by Outer Space:

<b>Drawing Title</b>	<b>Number</b>	<b>Version</b>	<b>Date</b>
Proposed ALDI & Chemist Warehouse Clovelly Park –	OS645_CP01D	-	22/02/2019

Landscape Concept Plan & Plant Palette.			
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2. A Traffic Management Plan for the construction period of the development shall be prepared to the satisfaction of the Department of Planning, Transport and Infrastructure and the Council prior to the commencement of construction. Such plan shall detail the types, volumes and distribution of traffic and how they will be managed. All traffic movements shall be in accordance with the Traffic Management Plan.
3. All Council, utility or state agency maintained infrastructure (i.e. roads, kerbs, drains, crossovers, footpaths, etc.) that is demolished, altered, removed or damaged during the construction of the development shall be reinstated to Council, utility or state-agency specifications. All costs associated with these works shall be met by the proponent.
4. Any obsolete crossovers/accesses shall be closed and reinstated to Council's standard kerb and gutter at the applicant's cost. Such work shall be completed prior to the operation of the development.
5. All vehicle car parks, driveways and vehicle entry and manoeuvring areas shall be designed and constructed in accordance with the relevant Australian Standards and be constructed, drained and paved with bitumen, concrete or paving bricks in accordance with sound engineering practice and appropriately line marked to the reasonable satisfaction of the Council prior to the occupation or use of the development.
6. All commercial vehicle facilities shall be designed in accordance with Australian Standard AS 2890.2-2018.
7. All car parking areas, driveways and vehicle manoeuvring areas shall be maintained at all times to the reasonable satisfaction of the Council.
8. Clear sightlines, as shown in Figure 3.3 "Minimum Sight Lines for Pedestrian Safety" in AS/NZS 2890.1:2004, shall be provided at the property line to ensure adequate visibility between vehicles leaving the site and pedestrians on the adjacent footpath.
9. Directional signs indicating the location of car parking spaces shall be provided on the subject land and maintained in a clear and legible condition at all times.
10. New pedestrian walkways on the subject land shall be adequately lit in accordance with Australian Standard AS/NZS 1158.3.1:19999 "Road Lighting Part 3.1: Pedestrian area (Category P) lighting – Performance and installation design guidelines". Such lighting shall be maintained at all times to the reasonable satisfaction of the Council.
11. The development shall be undertaken in accordance with the recommended acoustic treatments contained in the Environmental Noise Assessment report (S4217.18C4) prepared by Sonus Pty Ltd and dated July 2018.
12. The collection of refuse from the site by the refuse trucks, and the delivery of food, medical and other retail products to the site by the delivery trucks shall occur only between the hours of 9.00am to 7.00pm on a Saturday, Sunday or public holiday and between the hours of 7.00am to 7.00pm on any other day.
13. The largest vehicle permitted on-site shall be restricted to a 19 metres articulated vehicle (AS 2890.2-2018). All deliveries shall access the site via the Norrie Avenue and Wingfield Street access points only.

14. Signage on the subject site visible from the adjacent roads shall not contain any element that flashes, scrolls, moves or changes, or imitates a traffic control device.
15. All illuminated signs visible from South Road shall be limited to a low level of illumination (i.e. <150Cd/m<sup>2</sup>).
16. Landscaping shown on the plans forming part of the application shall be established prior to the operation of the development and shall be maintained and nurtured at all times with any diseased or dying plants being replaced.
17. A minimum of 50 per cent of the trees indicated to be planted on the approved plan shall be at least 1.5 metres in height at the time of planting.
18. All landscaped areas shall be separated from adjacent driveways and parking areas by a suitable kerb or non-mountable device to prevent vehicle movement thereon (incorporating ramps or crossovers to facilitate the movement of persons with a disability).
19. All loading and unloading of delivery and service vehicles associated with the subject premises shall be carried out entirely upon the subject land.
20. The development and the site shall be maintained in a serviceable condition and operated in an orderly and tidy manner at all times.
21. All hard waste and rubbish shall be stored in covered containers so as to prevent any materials entering the stormwater system either by wind or water action prior to removal and shall be kept screened from public view.
22. All external lighting of the site, including car parking areas, advertising signs and buildings shall be designed and constructed to conform with Australian Standards and must be located, directed and shielded and of such limited intensity that no nuisance or loss of amenity is caused to any person beyond the site.
23. All stormwater design and construction shall be in accordance with Australian Standards and recognised engineering best practices to ensure that stormwater does not adversely affect any adjoining property or public road.
24. A trap shall be installed as part of the site's stormwater system to prevent grease, oil, sediment, litter and other substances capable of contaminating stormwater from entering the Council's stormwater drainage system. The trap shall be regularly cleaned and maintained in good working order to the reasonable satisfaction of the Council.

#### ADVISORY NOTES

- a. This Development Plan Consent will expire after 12 months from the date of this Notification, unless final Development Approval from Council has been received within that period or this Consent has been extended by the State Commission Assessment Panel.
- b. The applicant is also advised that any act or work authorised or required by this Notification must be substantially commenced within 1 year of the final Development Approval issued by Council and substantially completed within 3 years of the date of final Development Approval issued by Council, unless that Development Approval is extended by the Council.
- c. The applicant has a right of appeal against the conditions which have been imposed on this Development Plan Consent. Such an appeal must be lodged at the Environment, Resources and Development Court within two months from the day of receiving this

notice or such longer time as the Court may allow. The applicant is asked to contact the Court if wishing to appeal. The Court is located in the Sir Samuel Way Building, Victoria Square, Adelaide (telephone number 8204 0289).

- d. The applicant is advised that in the construction of the acoustic barrier around the roof mounted mechanical services plant, a small gap may be required at the roof deck for drainage. Also, the noise level and any acoustic treatment associated with the mechanical plant shall be reviewed during the detailed design phase in the event of the final equipment selections having different sound power levels, or a different number of units is proposed to those specified within the Sonus report (S4217.18C4) dated July 2018.
- e. A Construction Environment Management Plan (CEMP) shall be prepared in consultation with the City of Marion and be implemented in accordance with current industry standards – including the Local Nuisance and Litter Control Act 2016, the EPA publications “Handbook for Pollution Avoidance on Commercial and Residential Building Sites – Second Edition” and, where applicable, “Environmental Management of On-site Remediation” – to minimise environmental harm and disturbance during construction.

The management plan should incorporate, without being limited to, the following matters:

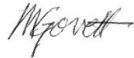
- timing, staging and methodology of the construction process and working hours;
  - control and management of construction noise, vibration, dust and mud;
  - management of infrastructure services during construction;
  - management of stormwater and groundwater during construction;
  - site security, fencing and safety and management of impacts on local amenity for residents, traffic and pedestrians;
  - disposal of construction waste, any hazardous waste and refuse in an appropriate manner according to the nature of the waste;
  - protection and cleaning of roads and pathways; and
  - overall site clean-up
- f. The applicant is advised that both the South Australian and Australian Governments have indicated clear commitments to progress a non-stop North-South Corridor for Adelaide. Construction is currently underway on the Northern Connector, Torrens Road to River Torrens and Darlington sections of the corridor, with funding committed towards the delivery of the Regency road to Pym Street section of South Road. Completion of these projects by 2022 will create a 47 kilometres continuous section of the North-South Corridor between Gawler and the River Torrens.

On 7 May 2018, the Australian Government announced its \$1.211 billion contribution towards the North-South Corridor. The Australian and South Australian Governments are working together to identify the next priority section of the corridor and are also exploring opportunities to accelerate the funding and delivery of the project.

The River Torrens to Tonsley Boulevard section is currently in the planning phase with detailed planning and design to be undertaken to determine the full scope of works and exact impacts. At this stage, the nature and timing of potential improvements to this section of South Road, in the vicinity of the subject property, are yet to be determined and any potential future land requirements are unknown. Accordingly, the Department cannot guarantee that the development will not be impacted in the future.

Once the next priority section has been identified and concept plans developed, the Department will have greater certainty of the impacts and will be able to provide reliable advice to affected land owners, local and state government agencies and the community.

Information about the 10-Year Delivery Strategy for the North-South Corridor can be found in the Scoping Report released on 18 May 2015 and available at [www.infrastructure.sa.gov.au/nsc](http://www.infrastructure.sa.gov.au/nsc) or if you would like to speak to a member of the North-South Corridor team, please email [northsouthcorrido@sa.gov.au](mailto:northsouthcorrido@sa.gov.au) or call 1300 951 145.



**Malcolm Govett**  
**PLANNING OFFICER**  
**PLANNING AND LAND USE SERVICES DIVISION**  
**DEPARTMENT OF PLANNING, TRANSPORT and INFRASTRUCTURE**



# PROPOSED ALDI & CHEMIST WAREHOUSE CLOVELLY PARK

SOUTH ROAD

CLOVELLY PARK, SOUTH AUSTRALIA



# PROPOSED ALDI & CHEMIST WAREHOUSE CLOVELLY PARK

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DA03.2	H	CHEMIST WAREHOUSE EXTERNAL ELEVATIONS	1:250
DA03.3	H	STREETSCAPE ELEVATIONS	1:350
DA05	E	SECTIONS	1:250
DA06.1	I	SIGNAGE PLAN	1:500
DA06.2	E	SIGNAGE DETAILS - SHEET 1	1:50
DA06.3	D	SIGNAGE DETAILS - SHEET 2	1:50
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DA08.1	E	PROPOSED 3D VISUAL RENDERS - SHEET 1	NTS
DA08.2	G	PROPOSED 3D VISUAL RENDERS - SHEET 2	NTS
DA08.3	-	PROPOSED 3D VISUAL RENDERS - SHEET 3	NTS



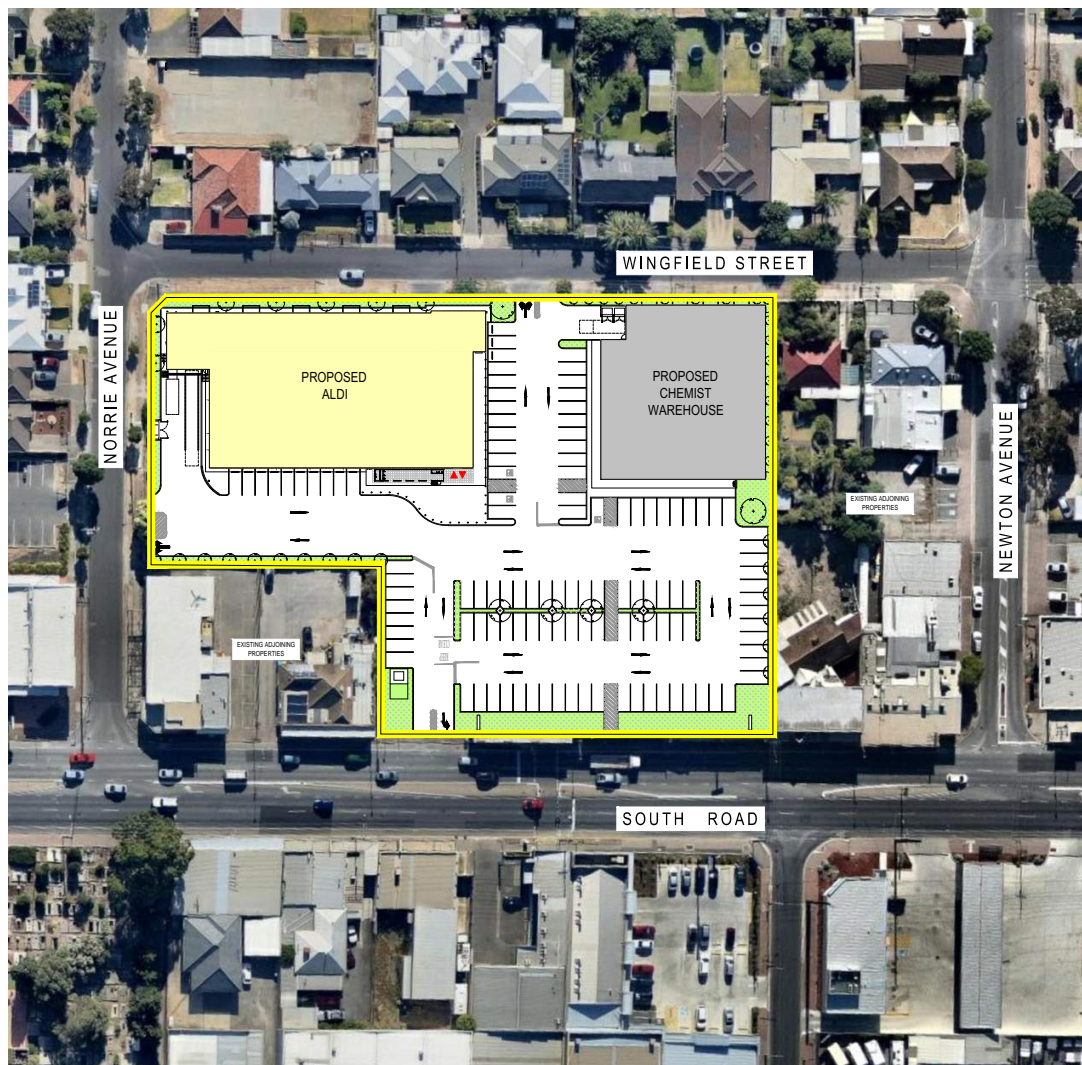
108 Mt Barker Road Stirling  
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E	01.11.18	PLANNING	DS	TB
F	12.11.18	PLANNING	DS	TB
G	14.11.18	PLANNING	DS	TB
H	28.11.18	PLANNING	DS	TB



LEGEND	
SITE BOUNDARY	
ALDI STORE	
CHEMIST WAREHOUSE	



LOCATION PLAN  
1:1000

CLIENT  
ALDI STORES &  
CHEMIST WAREHOUSE



PROJECT  
ALDI & CHEMIST WAREHOUSE CLOVELLY  
PARK  
SOUTH ROAD, CLOVELLY PARK, SA

DRAWING  
LOCATION PLAN  
PROPOSED

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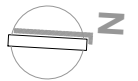
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B	14.11.18	PLANNING	DS	TB

NORRIE AVENUE

WINGFIELD STREET

NEWTON AVENUE

SOUTH ROAD



CLIENT  
ALDI STORES &  
CHEMIST WAREHOUSE



PROJECT  
ALDI & CHEMIST WAREHOUSE CLOVELLY  
PARK  
SOUTH ROAD, CLOVELLY PARK, SA

DRAWING  
CONTRACT TITLES  
EXISTING



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## LEGEND

EXISTING BOUNDARIES



CONTRACT TITLES - EXISTING  
1:500

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2052	DA01.2	DA	B





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


CLIENT		 	
ALDI STORES & CHEMIST WAREHOUSE			
PROJECT		ALDI & CHEMIST WAREHOUSE CLOVELLY PARK	
		SOUTH ROAD, CLOVELLY PARK, SA	
DRAWING		CONTRACT TITLES	
		PROPOSED	

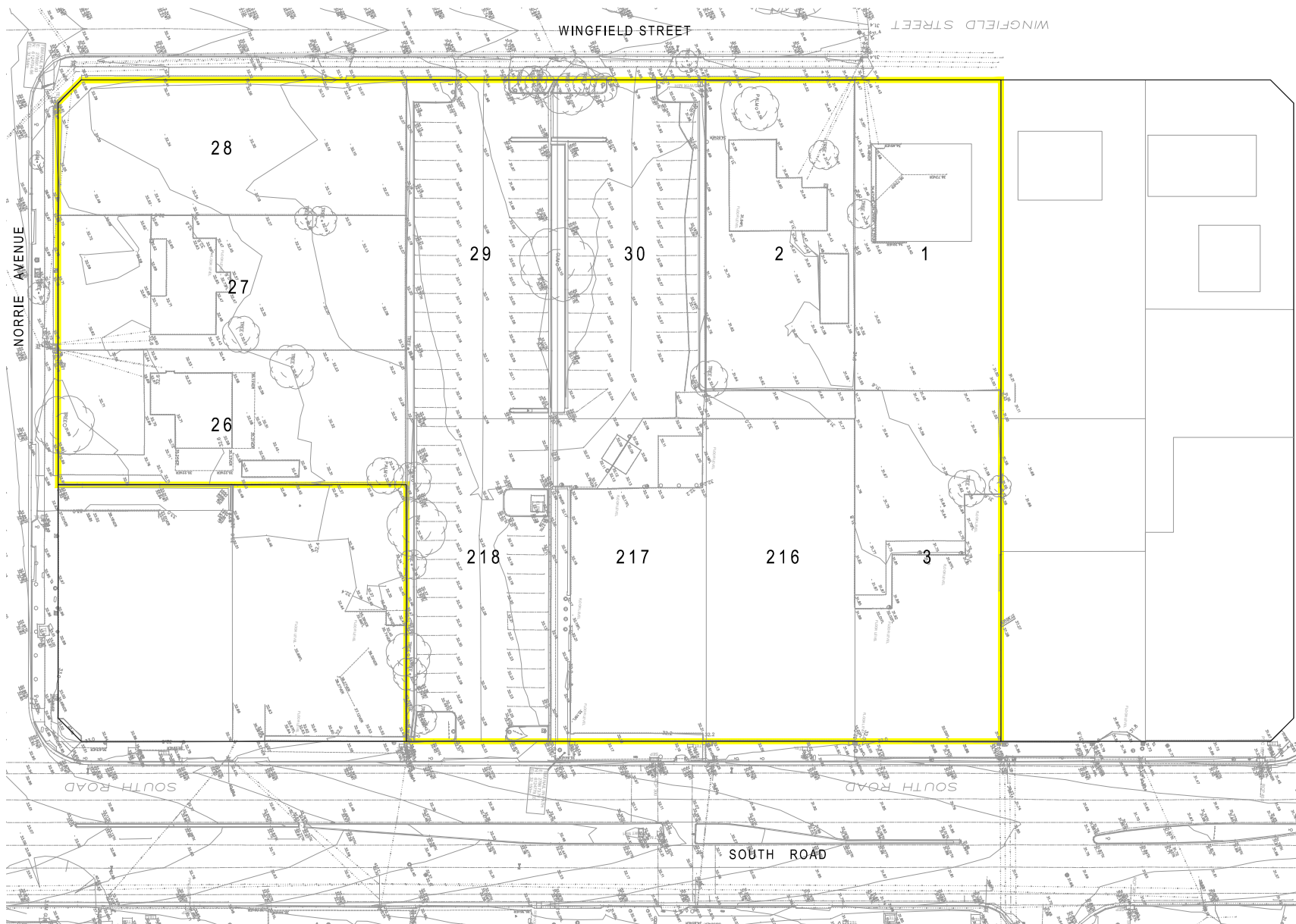

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PROJECT No	DRAWING No	STATUS	REV
2052	DA01.3	DA	E


 CONTRACT TITLES - PROPOSED  
 1:500

LEGEND	
ALDI TITLE BOUNDARY	
CHEMIST WAREHOUSE TITLE BOUNDARY	
EXISTING TITLE BOUNDARIES	





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C	28.11.18	PLANNING	DS	TB

NEWTON AVENUE



CLIENT  
ALDI STORES &  
CHEMIST WAREHOUSE




PROJECT  
ALDI & CHEMIST WAREHOUSE CLOVELLY  
PARK  
SOUTH ROAD, CLOVELLY PARK, SA

DRAWING  
EXISTING SITE PLAN

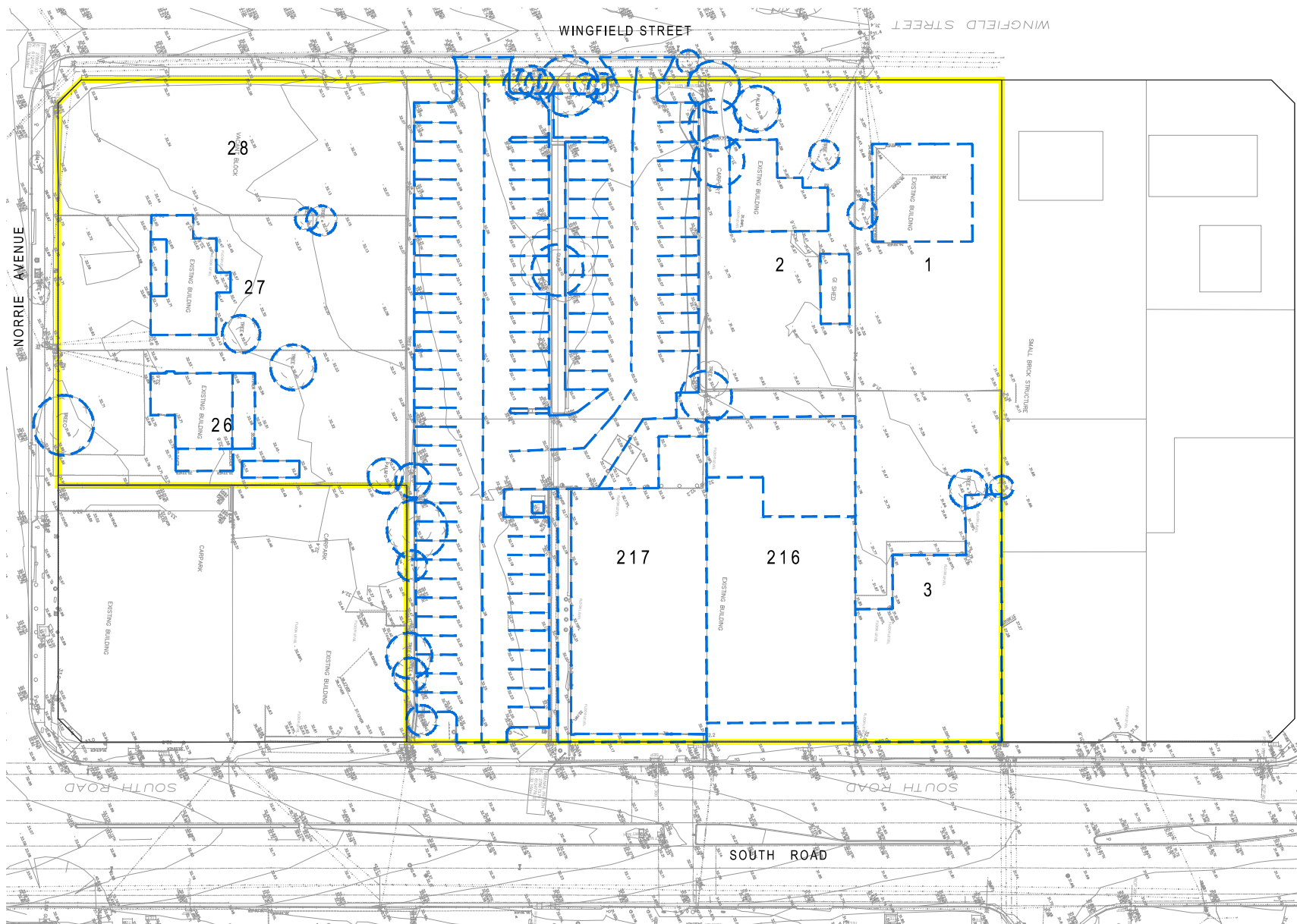
EXISTING SITE PLAN  
1:500

LEGEND	
EXISTING BOUNDARY	
EXISTING CONTOUR	— 0.00 —
EXISTING LEVEL	EX 0.00
DESIGN LEVEL	RL 0.00



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SCALE 1:500	DATE NOV 2018	DRAWN DS	CHECKED TB
PROJECT No 2052	DRAWING No DA02.1	STATUS DA	REV C



DEMOLITION SITE PLAN  
1:500

LEGEND	
EXISTING BOUNDARY	
STRUCTURES AND ITEMS TO BE DEMOLISHED	
EXISTING BOUNDARY	0.00
EXISTING LEVEL	EX 0.00
DESIGN LEVEL	RL 0.00

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C	28.11.18	PLANNING	DS	TB

NEWTON AVENUE



CLIENT  
ALDI STORES &  
CHEMIST WAREHOUSE



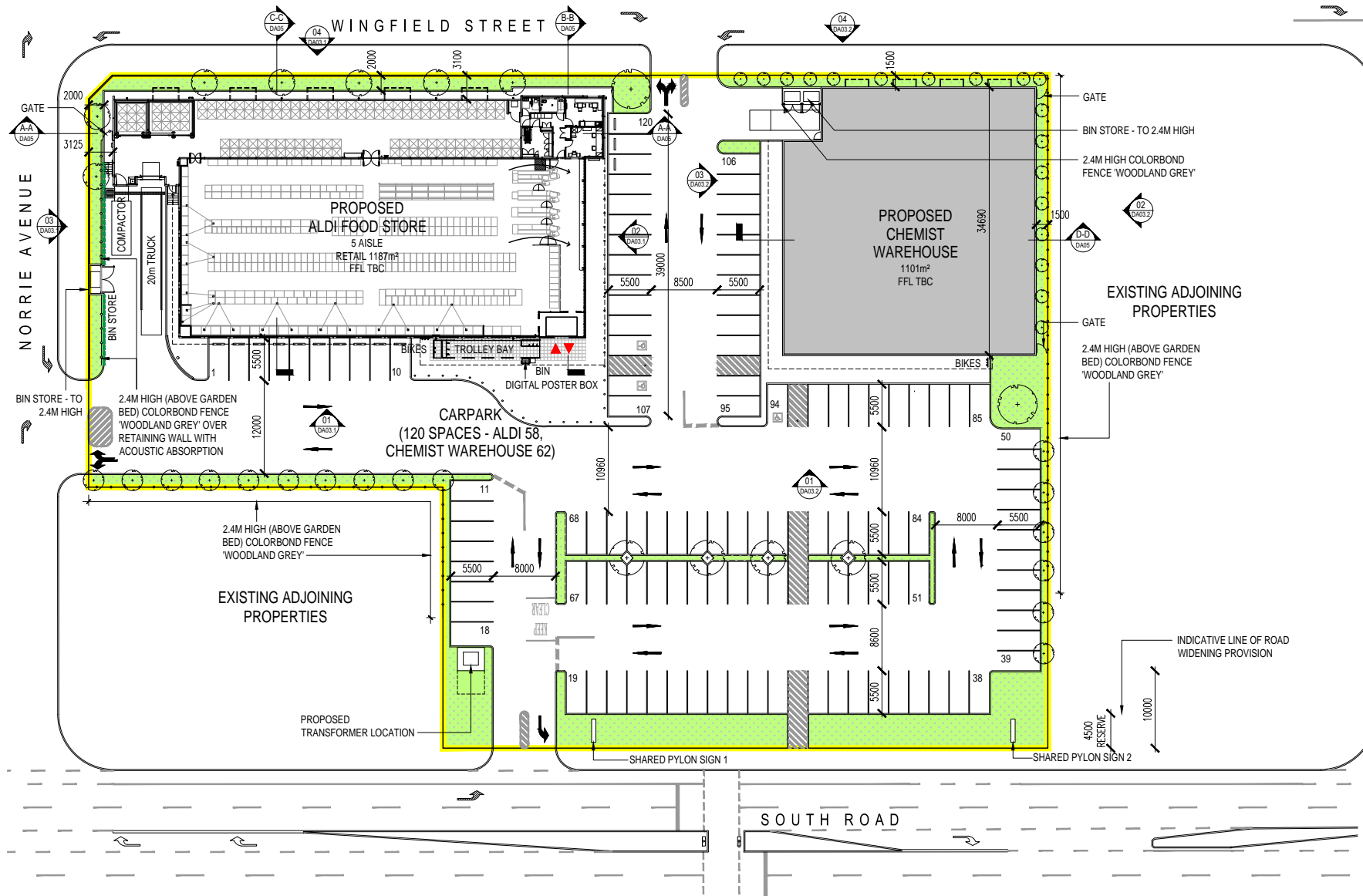
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DRAWING  
DEMOLITION SITE PLAN



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D	07.08.18	PLANNING	LT	TB
E	14.08.18	PLANNING	LT	TB
F	02.10.18	PLANNING	LT	DS
G	26.10.18	PLANNING	DS	TB
H	01.11.18	PLANNING	DS	TB
I	12.11.18	PLANNING	DS	TB
J	14.11.18	PLANNING	DS	TB
K	28.11.18	PLANNING	DS	TB
L	29.11.18	PLANNING	DS	TB
M	22.02.19	PLANNING	LT	TB

PROPOSED SITE PLAN  
1:500

DEVELOPMENT	
TOTAL SITE AREA	9207m²

### ALDI DESCRIPTION

ALDI SITE AREA	5376m²	AMENITIES NETT AREA	79m²
ALDI GROSS AREA	1732m²	REMAINING NETT AREA	490m²
ALDI NETT AREA	1677m²	BOH PALLETS	120
RETAIL NETT AREA	1187m²	CHILLER LENGTH	40m
BOH NETT AREA	411m²	SIZE OF TRUCK	20m

### CHEMIST WAREHOUSE DESCRIPTION

CHEMIST WAREHOUSE SITE AREA	3831m²
RETAIL GROSS AREA	1101m²

### CARPARKING

ALDI CARS	58
CHEMIST WAREHOUSE CARS	62

CLIENT  
ALDI STORES &  
CHEMIST WAREHOUSE

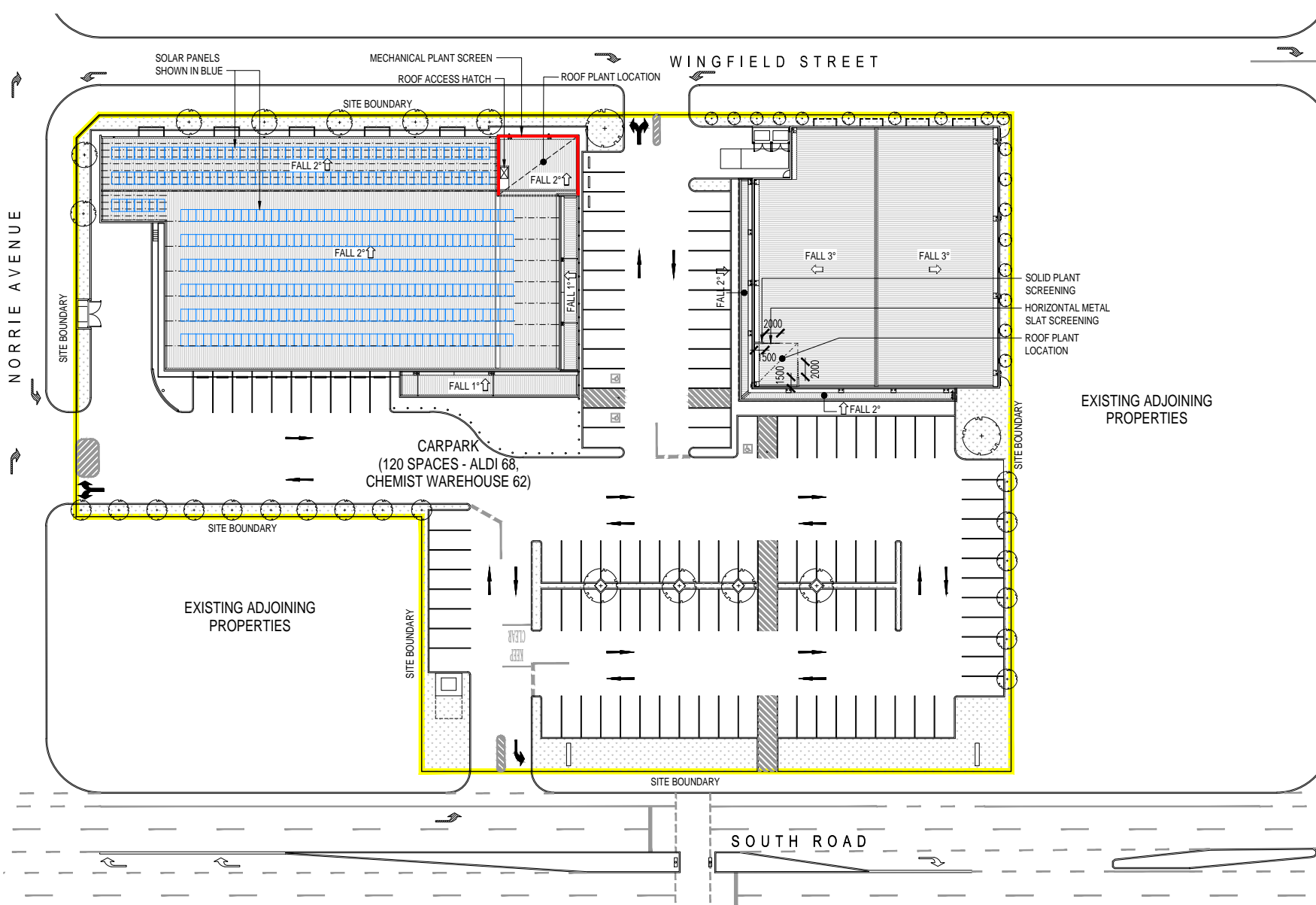


PROJECT  
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DRAWING  
PROPOSED SITE PLAN

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PROPOSED ROOF PLAN  
1:500

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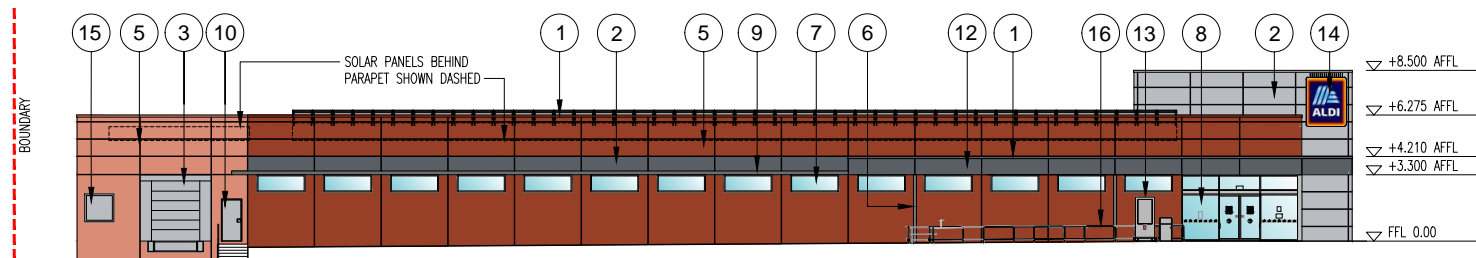
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CHEMIST WAREHOUSE

PROJECT  
ALDI & CHEMIST WAREHOUSE CLOVELLY  
PARK  
SOUTH ROAD, CLOVELLY PARK, SA

DRAWING  
PROPOSED ROOF PLAN

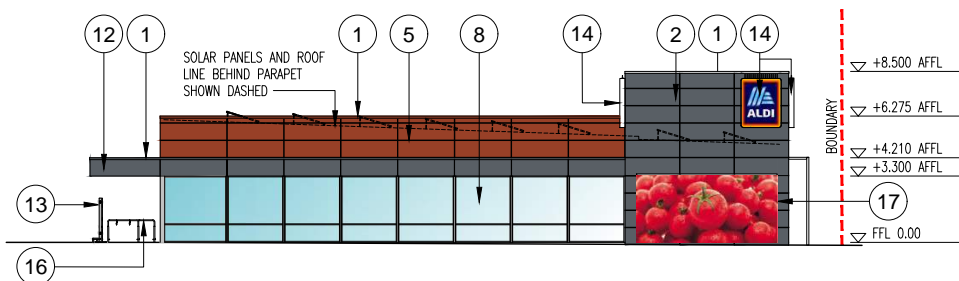
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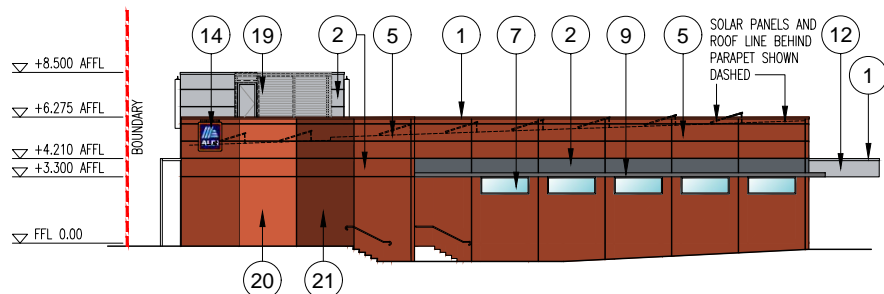
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DA02.3 1:250



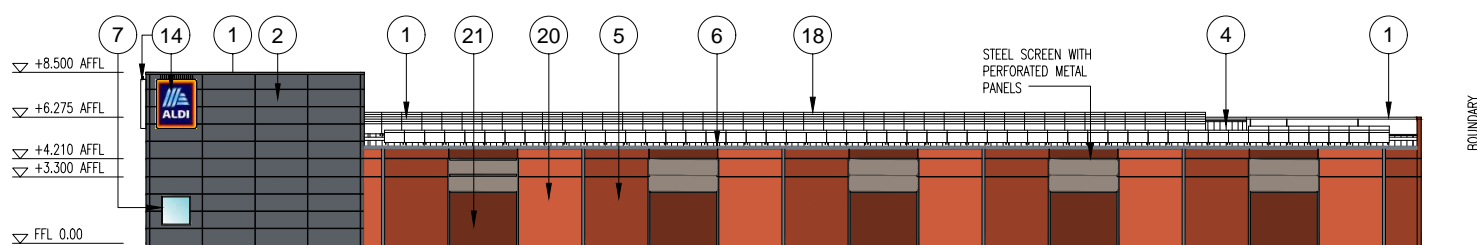
02 NORTH ELEVATION

DA02.3 1:250



03 SOUTH ELEVATION

DA02.3 1:250 PROPOSED



04 WEST ELEVATION

DA02.3 1:250 PROPOSED

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C	01.11.18	PLANNING	DS	TB
D	14.11.18	PLANNING	DS	TB

## MATERIAL SCHEDULE

- 1 - COLORBOND CAPPING, COLOUR TO MATCH ADJACENT PRECAST PANEL FINISH
- 2 - PRECAST RC PANEL, PAINT FINISH DULUX 'DRIVE TIME'
- 3 - PANEL LIFT DOOR AND COMPACTOR DOOR - COLORBOND, 'BASALT GREY'
- 4 - COLORBOND ROOF SHEETING, SURFMST
- 5 - PRECAST RC PANEL, PAINT FINISH DULUX 'FLUORESCENT FIRE'
- 6 - GUTTER AND DOWNPIPES - COLORBOND, 'BASALT GREY'
- 7 - WINDOWS - ANODISED ALUMINIUM FRAMES, NATURAL FINISH
- 8 - SHOPFRONT - ANODISED ALUMINIUM FRAMES, NATURAL FINISH
- 9 - SUNSCREEN - PAINTED STEEL FRAME - COLORBOND, 'BASALT GREY'
- 10 - DOOR & FRAME, DULUX 'OLDE PEWTER'
- 11 - BIN STORE, PRECAST CONCRETE 'FLUORESCENT FIRE'
- 12 - FIBRE CEMENT FASCIA, DULUX 'DRIVE TIME'
- 13 - POSTER BOX
- 14 - ALDI ILLUMINATED SIGNS
- 15 - COMPACTOR DOOR & FRAME - 'OLDE PEWTER'
- 16 - TROLLEY BAY
- 17 - EXTERNAL LIFESTYLE IMAGE (TOMATO)
- 18 - SOLAR PANELS
- 19 - PLANT LOUVERS - POWERCOATED 'OLDE PEWTER'
- 20 - PRECAST RC PANEL, PAINT FINISH DULUX 'ORANGE KEEPER'
- 21 - PRECAST RC PANEL, PAINT FINISH DULUX 'CENTRE SOIL'

CLIENT  
ALDI STORES &  
CHEMIST WAREHOUSE



PROJECT  
ALDI & CHEMIST WAREHOUSE CLOVELLY  
PARK  
SOUTH ROAD, CLOVELLY PARK, SA

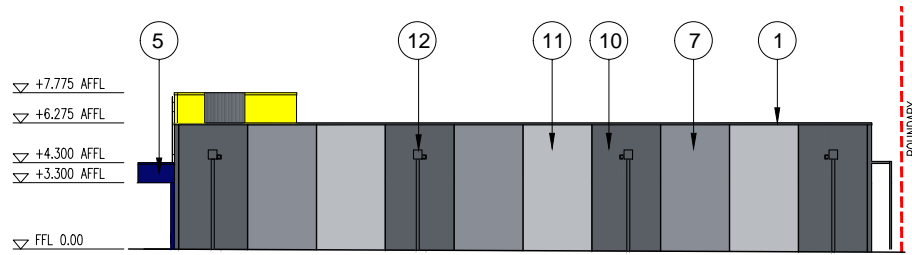
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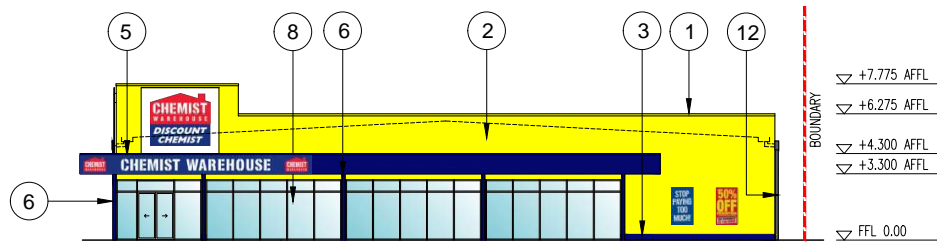
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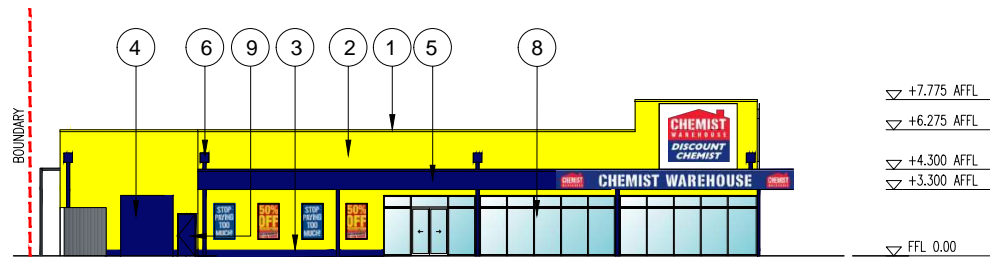




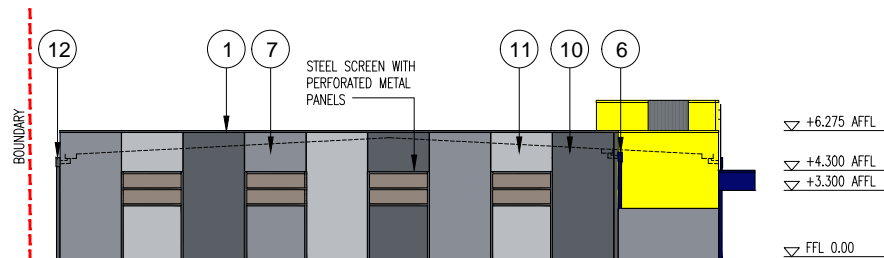
01 NORTH ELEVATION  
DA02.3 1:250



02 EAST ELEVATION  
DA02.3 1:250



03 SOUTH ELEVATION  
DA02.3 1:250



04 WEST ELEVATION  
DA02.3 1:250

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G	12.11.18	PLANNING	DS	TB
H	14.11.18	PLANNING	DS	TB
	22.02.19	PLANNING	LT	TB

## MATERIAL SCHEDULE

1 - COLORBOND CAPPING, COLOUR TO MATCH ADJACENT WALL FINISH
2 - PRECAST RC PANEL, PAINT FINISH, COLOUR CHEMIST WAREHOUSE YELLOW
3 - BOTTOM OF PRECAST PANEL BAND, PAINT FINISH, COLOUR TO MATCH CHEMIST WAREHOUSE BLUE
4 - STEEL ROLLER DOOR, PAINT FINISH, COLOUR TO MATCH CHEMIST WAREHOUSE BLUE
5 - CANOPY - LIGHT WEIGHT CLADDING, PAINTED FINISH, COLOUR TO MATCH CHEMIST WAREHOUSE BLUE
6 - RAINWATER HEADS AND DOWNPIPES - STEEL, PAINTED TO MATCH CHEMIST WAREHOUSE BLUE
7 - PRECAST RC PANEL BAND, PAINT FINISH, DULUX SIMONE WEE
8 - SHOPFRONT - FRAMES NATURAL ANODISED ALUMINIUM
9 - DOOR AND FRAME - STEEL FRAME, SOLID CORE DOOR, PAINTED FINISH, COLOUR TO MATCH CHEMIST WAREHOUSE BLUE
10 - PRECAST RC PANEL, PAINT FINISH, DULUX 'DRIVE TIME'
11 - PRECAST RC PANEL, PAINT FINISH, DULUX 'ACCORD'
12 - RAINWATER HEADS AND DOWNPIPES - STEEL, PAINTED, COLOUR GREY

CLIENT  
ALDI STORES &  
CHEMIST WAREHOUSE



PROJECT  
ALDI & CHEMIST WAREHOUSE CLOVELLY PARK  
SOUTH ROAD, CLOVELLY PARK, SA

DRAWING  
CHEMIST WAREHOUSE  
EXTERNAL ELEVATIONS

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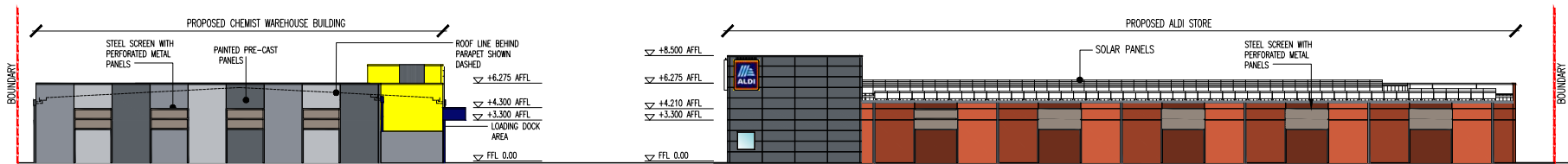
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PROJECT No	DRAWING No	STATUS	REV
2052	DA03.2	DA	H

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F	14.11.18	PLANNING	DS	TB
G	28.11.18	PLANNING	DS	TB
H	22.02.19	PLANNING	LT	TB



01 SOUTH ROAD STREETSCAPE  
DA02.3 1:350



02 WINGFIELD STREET STREETSCAPE  
DA02.3 1:350

CLIENT  
ALDI STORES &  
CHEMIST WAREHOUSE



PROJECT  
ALDI & CHEMIST WAREHOUSE CLOVELLY PARK  
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DRAWING  
STREETSCAPE ELEVATIONS

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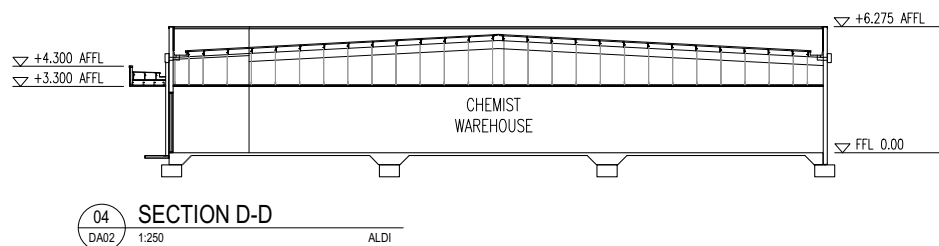
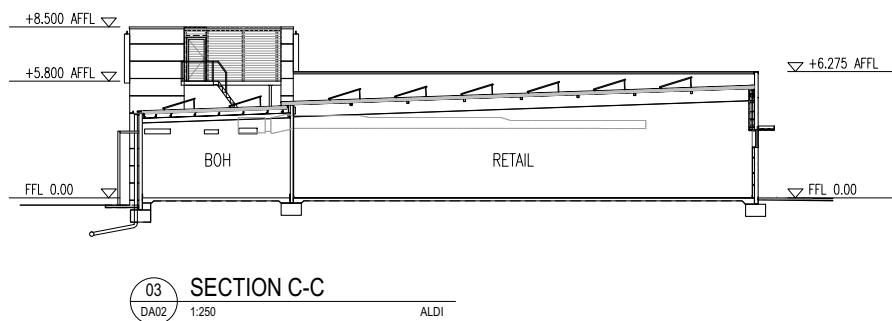
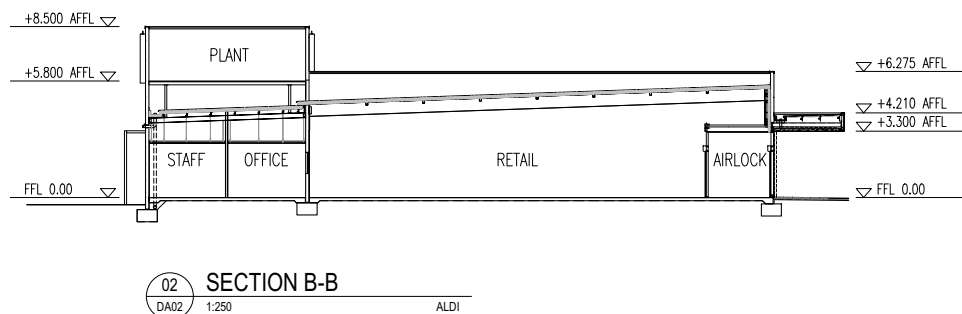
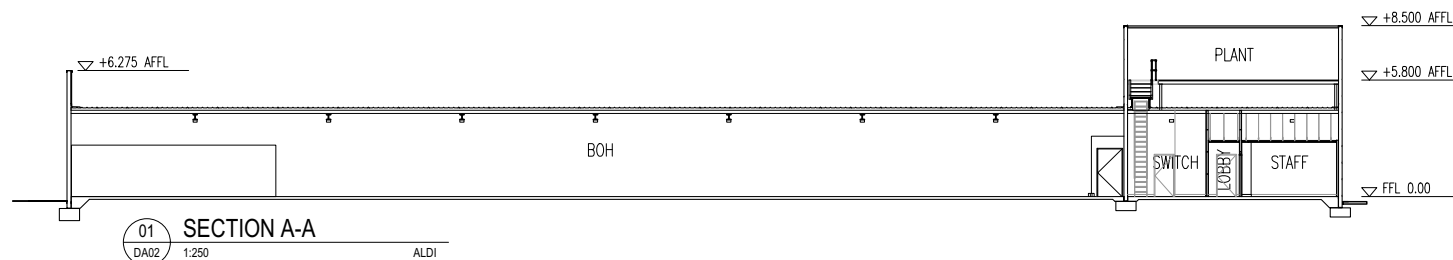
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CLIENT  
ALDI STORES &  
CHEMIST WAREHOUSE



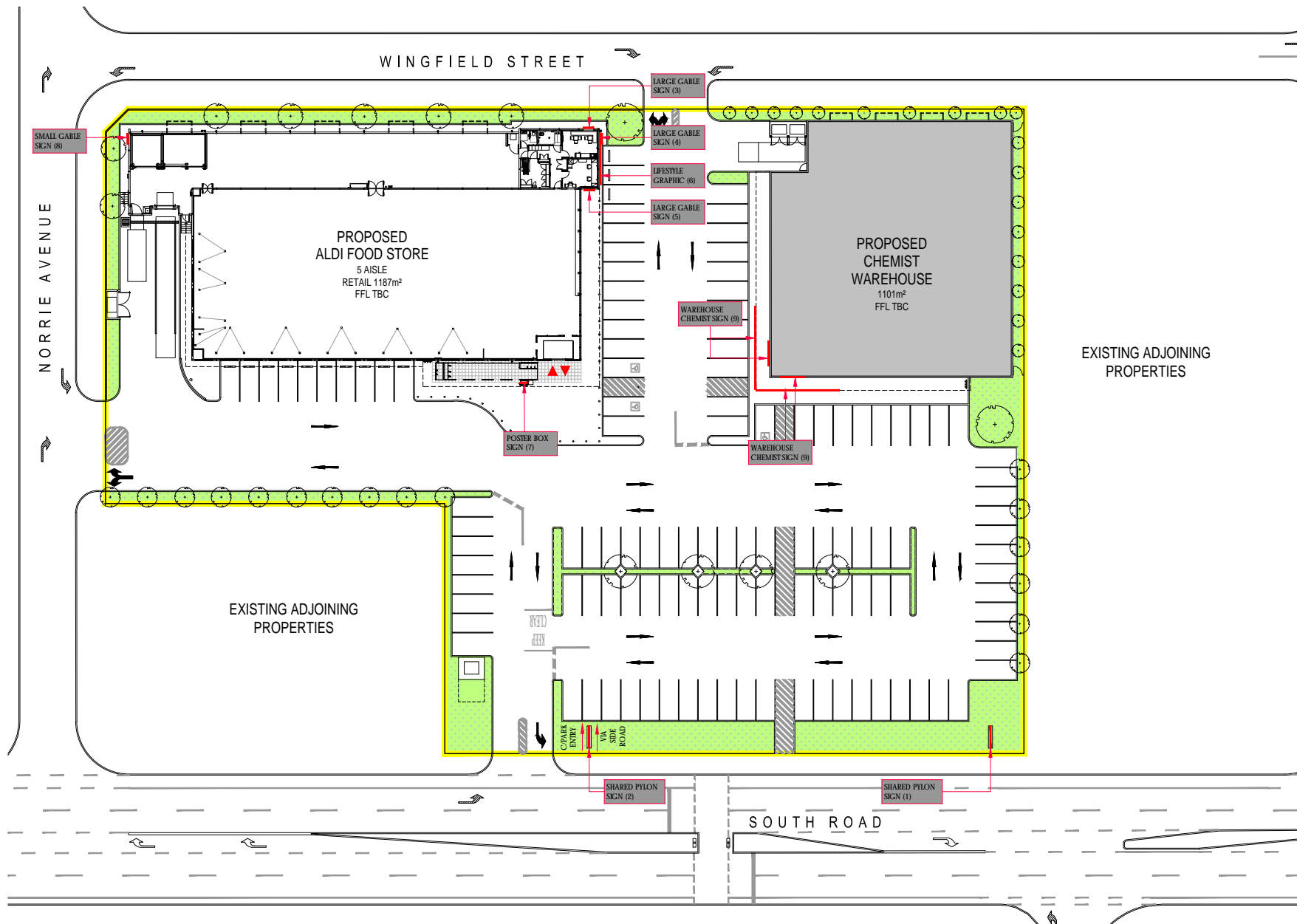
PROJECT  
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DRAWING  
SECTIONS



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2052	DA05	DA	E



## NOTES

ORIGINAL A3

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REV	DATE	DESCRIPTION	DRN	CHKD
A	20.06.18	PLANNING	DS	TB
B	25.06.18	PLANNING	DS	TB
C	19.07.18	PLANNING	DS	TB
D	26.10.18	PLANNING	DS	TB
E	01.11.18	PLANNING	DS	TB
F	12.11.18	PLANNING	DS	TB
G	14.11.18	PLANNING	DS	TB
H	28.11.18	PLANNING	DS	TB
I	29.11.18	PLANNING	DS	TB
	22.02.19	PLANNING	LT	TB

NEWTON AVENUE

EXISTING ADJOINING PROPERTIES

CLIENT  
ALDI STORES &  
CHEMIST WAREHOUSE



PROJECT  
ALDI & CHEMIST WAREHOUSE CLOVELLY PARK  
SOUTH ROAD, CLOVELLY PARK, SA

DRAWING  
SIGNAGE PLAN



1108 AM Barker Road Stirling  
South Australia 5152  
p: 08 8339 8008  
f: 08 8339 2004  
P.O. Box 691 Stirling SA 5152  
admin@nielsenarchitects.com.au  
www.nielsenarchitects.com.au

01 SIGNAGE PLAN

1:500

PROPOSED

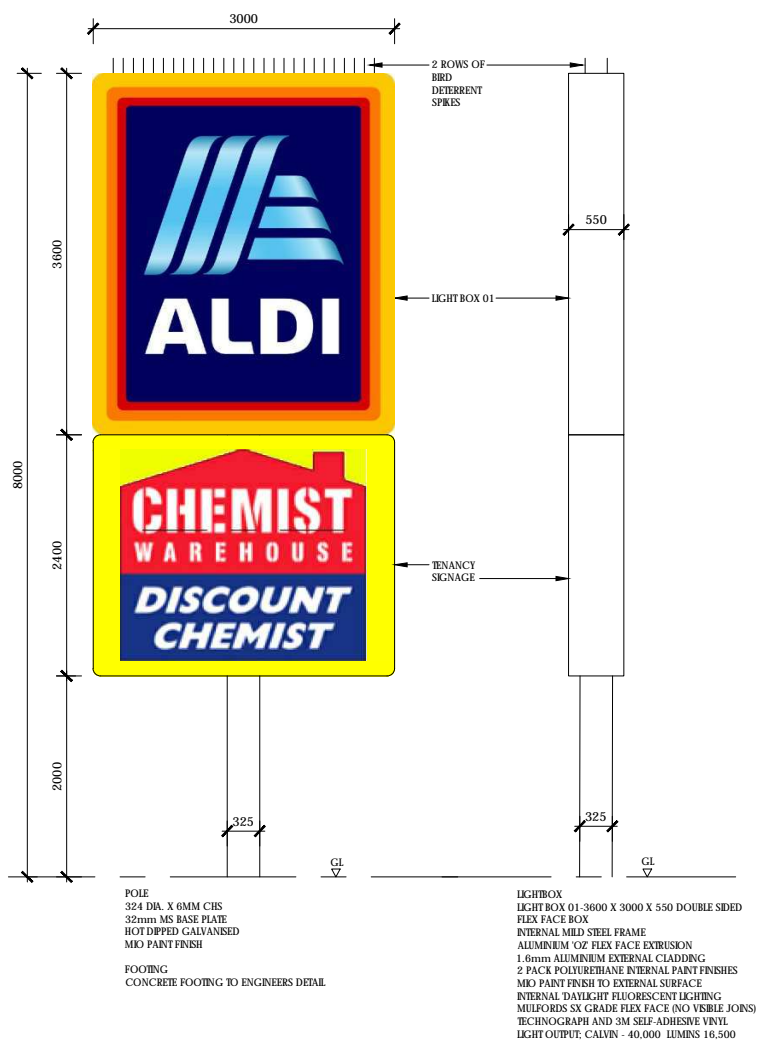
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1:500	FEB 2019	DS	TB
PROJECT No	DRAWING No	STATUS	REV
2052	DA06.1	DA	I

## NOTES

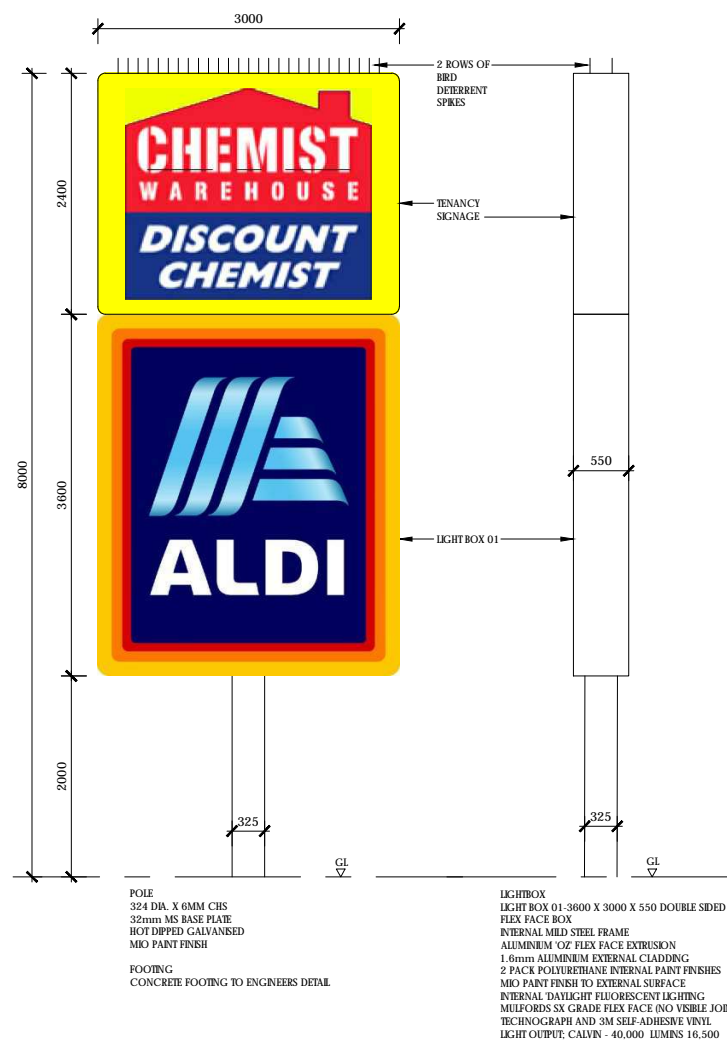
ORIGINAL A3

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REV	DATE	DESCRIPTION	DRN	CHKD
A	20.06.18	PLANNING	DS	TB
B	25.06.18	PLANNING	DS	TB
C	26.10.18	PLANNING	DS	TB
D	01.11.18	PLANNING	DS	TB
E	14.11.18	PLANNING	DS	TB



01 SHARED PYLON SIGN - NORTH FACING (1)  
1:50



02 SHARED PYLON SIGN - SOUTH FACING (1)  
1:50

CLIENT  
ALDI STORES &  
CHEMIST WAREHOUSE

PROJECT  
ALDI & CHEMIST WAREHOUSE CLOVELLY  
PARK  
SOUTH ROAD, CLOVELLY PARK, SA

DRAWING  
SIGNAGE DETAILS  
SHEET 1

1100 Sir Barker Road Stirling  
South Australia 5152  
p: 08 8339 8008  
f: 08 8339 2004  
P.O. Box 691 Stirling SA 5152  
admin@nielsenarchitects.com.au  
www.nielsenarchitects.com.au

SCALE	DATE	DRAWN	CHECKED
1:50	NOV 2018	DS	TB
PROJECT No	DRAWING No	STATUS	REV
2052	DA06.2	DA	E

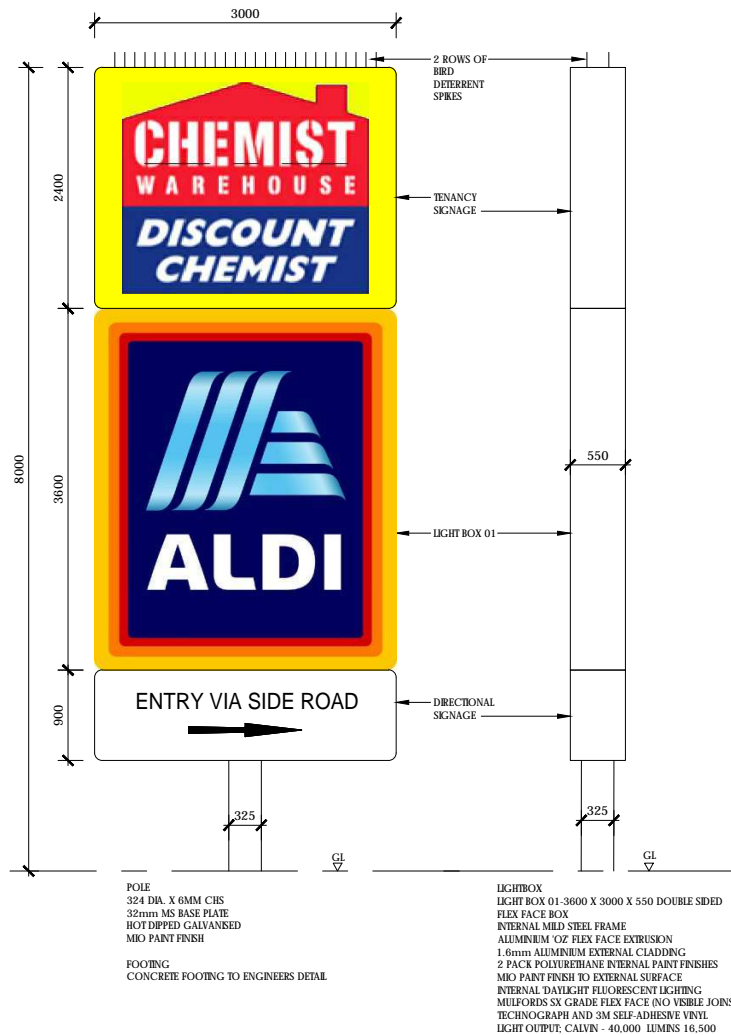


## NOTES

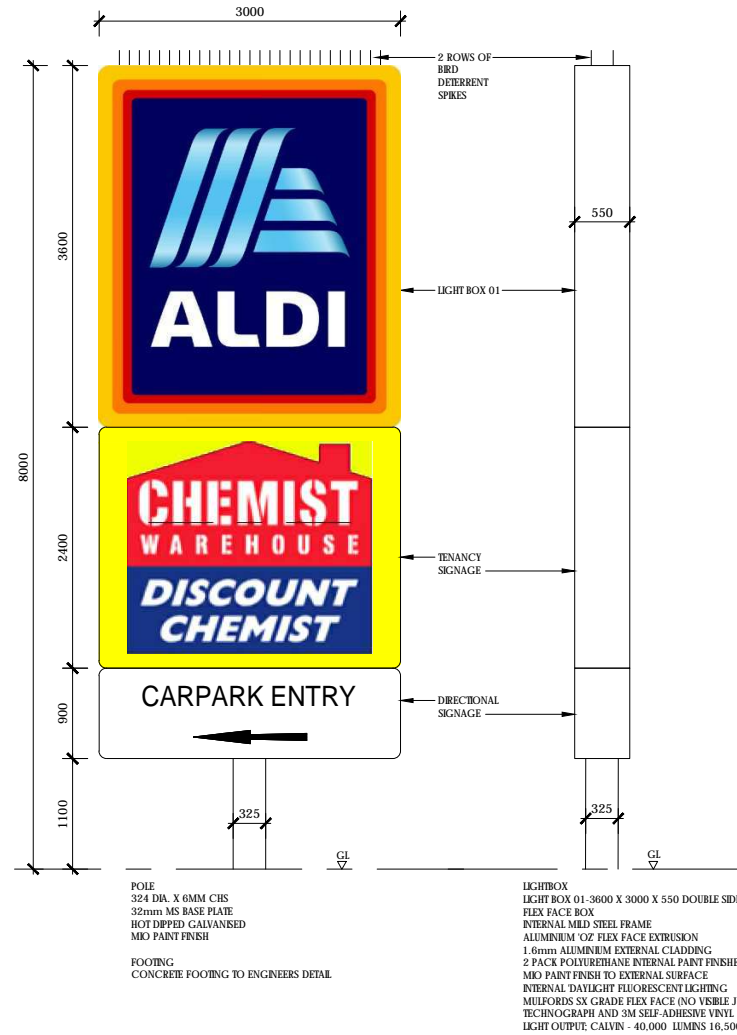
ORIGINAL A3

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REV	DATE	DESCRIPTION	DRN	CHKD
A	19.06.18	PLANNING	DS	TB
B	25.06.18	PLANNING	DS	TB
C	01.11.18	PLANNING	DS	TB
D	14.11.18	PLANNING	DS	TB



01 SHARED PYLON SIGN - NORTH FACING (2)  
1:50



02 SHARED PYLON SIGN - SOUTH FACING (2)  
1:50

CLIENT  
ALDI STORES &  
CHEMIST WAREHOUSE

PROJECT  
ALDI & CHEMIST WAREHOUSE CLOVELLY  
PARK  
SOUTH ROAD, CLOVELLY PARK, SA

DRAWING  
SIGNAGE DETAILS  
SHEET 2

1108 Wm Barker Road Stirling  
South Australia 5152  
p: 08 8339 8008  
f: 08 8339 2004  
P.O. Box 691 Stirling SA 5152  
admin@nielsenarchitects.com.au  
www.nielsenarchitects.com.au

NIELSEN ARCHITECTS

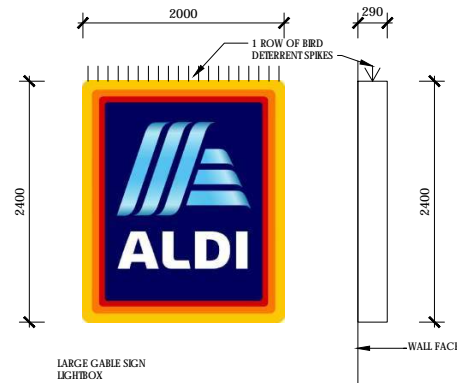
SCALE	DATE	DRAWN	CHECKED
1:50	NOV 2018	DS	TB
PROJECT No	DRAWING No	STATUS	REV
2052	DA06.3	DA	D

## NOTES

ORIGINAL A3

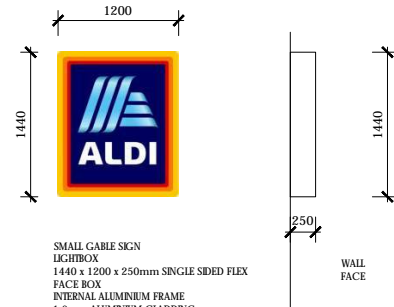
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REV	DATE	DESCRIPTION	DRN	CHKD
A	20.06.18	PLANNING	DS	TB
B	01.11.18	PLANNING	DS	TB
C	14.11.18	PLANNING	DS	TB



LARGE GABLE SIGN  
LIGHTBOX  
2000w X 2400h X 292mm SINGLE SIDED  
FLEXFACE WALL SIGN.  
DR-571 OR SIMILAR 200mm ALUM SIGNBOX  
EXTRUSION WITH 1.6mm ALUMINIUM  
CLADDING TO BACK  
PAINT FINISH EXTERNAL: DULUX OLD PEWTER  
50243 70% GLOSS IN 2 PACK POLYURETHANE  
OR POWDERCOAT F9964  
PAINT FINISH INTERNAL: FLAT WHITE  
INTERNAL ILLUMINATION: DAYLIGHT  
FLUORESCENT TUBES SPACED EVENLY  
LIGHT OUTPUT: CALVIN - 40,000  
LUMENS 16,500

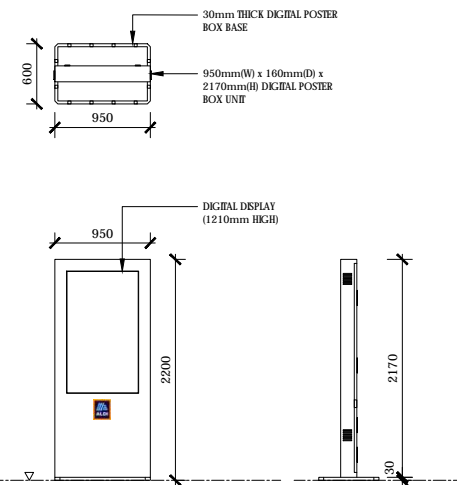
01 LARGE GABLE SIGN (3, 4 & 5)  
1:50



SMALL GABLE SIGN  
LIGHTBOX  
1440 x 1200 x 250mm SINGLE SIDED FLEX  
FACE BOX  
INTERNAL ALUMINIUM FRAME  
1.6mm ALUMINIUM CLADDING  
2 PACK POLYURETHANE INTERNAL PAINT  
FINISHES  
MULFORDS SX GRADE FLEX FACE (NO JOINS)  
TECHNOGRAPH SX 3m SELF-ADHESIVE VINYL  
INTERNAL DAYLIGHT FLUORESCENT LIGHTING  
LIGHT OUTPUT: CALVIN - 40,000 LUMENS 16,500

02 SMALL GABLE SIGN (8)  
1:50

NOTE:  
REFER TO EXTERNAL ELEVATIONS DRAWING DA03.2  
FOR CHEMIST WAREHOUSE SIGNAGE DETAILS



DIGITAL DISPLAY  
(1210mm HIGH)

03 POSTER BOX (7)  
1:50



EXTERNAL LIFESTYLE IMAGE  
7000 x 3300 x 250mm SINGLE FACED DBOND  
GRAPHIC PANEL  
INTERNAL ALUMINIUM FRAME  
50mm ALUMINIUM ANGLE COVER TRIM.  
POWDERCOAT FINISH (BLAZE)  
3mm DBOND PANEL  
APPLIED DIGITAL PRINT C/W ANTI GRAFFITI COAT  
EXTERNALLY ILLUMINATED FROM ABOVE

04 EXTERNAL LIFESTYLE GRAPHIC (6)  
1:50

CLIENT  
ALDI STORES &  
CHEMIST WAREHOUSE

PROJECT  
ALDI & CHEMIST WAREHOUSE CLOVELLY  
PARK  
SOUTH ROAD, CLOVELLY PARK, SA

DRAWING  
SIGNAGE DETAILS  
SHEET 3

NIELSEN ARCHITECTS

108 km Barker Road Stirling  
South Australia 5152  
p: 08 8339 8008  
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www.nielsenarchitects.com.au

SCALE	DATE	DRAWN	CHECKED
1:50	NOV 2018	DS	TB
PROJECT No	DRAWING No	STATUS	REV
2052	DA06.4	DA	C

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REV	DATE	DESCRIPTION	DRN	CHKD
A	05.06.18	PLANNING	DS	TB
B	19.07.18	PLANNING	DS	TB
C	07.08.18	PLANNING	LT	DS
D	01.11.18	PLANNING	DS	TB
E	14.11.18	PLANNING	DS	TB



PROPOSED 3D VISUAL RENDER  
NTS

CLIENT  
ALDI STORES &  
CHEMIST WAREHOUSE



PROJECT  
ALDI & CHEMIST WAREHOUSE CLOVELLY  
PARK  
SOUTH ROAD, CLOVELLY PARK, SA

DRAWING  
PROPOSED 3D VISUAL RENDERS  
SHEET 1

 108 Mt Barker Road Stirling  
South Australia 5152  
p 08 8339 8006  
f 08 8339 2004  
P.O.Box 691 Stirling SA 5152  
admin@nielsenarchitects.com.au  
www.nielsenarchitects.com.au

SCALE	DATE	DRAWN	CHECKED
NTS	NOV 2018	DS	TB
PROJECT No	DRAWING No	STATUS	REV
2052	DA08.1	DA	E

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REV	DATE	DESCRIPTION	DRN	CHKD
A	05.06.18	PLANNING	DS	TB
B	19.07.18	PLANNING	DS	TB
C	07.08.18	PLANNING	LT	DS
D	01.11.18	PLANNING	DS	TB
E	14.11.18	PLANNING	DS	TB
F	29.11.18	PLANNING	DS	TB
G	22.02.19	PLANNING	LT	TB



PROPOSED 3D VISUAL RENDER

NTS

CLIENT

ALDI STORES & CHEMIST WAREHOUSE

CHEMIST WAREHOUSE

DISCOUNT CHEMIST

ALDI

PROJECT

ALDI & CHEMIST WAREHOUSE CLOVELLY PARK

SOUTH ROAD, CLOVELLY PARK, SA

DRAWING

PROPOSED 3D VISUAL RENDERS

SHEET 2

NIELSEN ARCHITECTS

108 Mt Barker Road Stirling

South Australia 5152

ph 08 8339 8006

f 08 8339 8004

P.O.Box 691 Stirling SA 5152

admin@nielsenarchitects.com.au

www.nielsenarchitects.com.au

SCALE	DATE	DRAWN	CHECKED
NTS	FEB 2019	DS	TB
PROJECT No	DRAWING No	STATUS	REV
2052	DA08.2	DA	G



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REV	DATE	DESCRIPTION	DRN	CHKD
-	22.02.19	PLANNING	LT	TB



PROPOSED 3D VISUAL RENDER

NTS

CLIENT  
ALDI STORES &  
CHEMIST WAREHOUSE



PROJECT  
ALDI & CHEMIST WAREHOUSE CLOVELLY  
PARK  
SOUTH ROAD, CLOVELLY PARK, SA

DRAWING  
PROPOSED 3D VISUAL RENDERS  
SHEET 3

 **NIELSEN ARCHITECTS**  
108 Mt Barker Road Stirling  
South Australia 5152  
p: 08 8339 8006  
f: 08 8339 2004  
P.O.Box 691 Stirling SA 5152  
admin@nielsenarchitects.com.au  
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SCALE	DATE	DRAWN	CHECKED
NTS	FEB 2019	LT	TB
PROJECT No	DRAWING No	STATUS	REV
2052	DA08.3	DA	-





## LEGEND



## DESIGN INTENT

PLANTS HAVE BEEN SELECTED TO GIVE A STRONG IDENTITY TO ALL ALDI STORES WITHIN METROPOLITAN ADELAIDE, EACH SPECIES SERVING A SPECIFIC FUNCTION.

SCREENING SHRUB SPECIES ARE TO BE USED AROUND THE ELECTRICAL TRANSFORMER. STREET FRONTAGE GARDEN BEDS ARE TO BE LOW GROWING MASS PLANTED AREAS TO PROVIDE A BARRIER TO UNDESIRABLE PEDESTRIAN ENTRY.

FEATURE PLANTING WILL BE SITUATED AT ALL VEHICLE AND PEDESTRIAN ENTRIES, PROVIDING AN INSTANT IMPACT FOR SHOPPERS ENTERING THE CAR PARK. THIS WILL BE ACHIEVED THROUGH CONTRASTING COLOUR AND TEXTURES.

TREES ARE PROPOSED TO BE CLEAN TRUNKED SPECIES THAT PROVIDE SHADE TO THE CARPARK BUT ENABLE SIGHTLINES TOWARDS THE ALDI STORE FRONT AND FOR PEDESTRIAN SAFETY.

## PLANT PALETTE

### TREES



*Fraxinus angustifolia* 'Raywoodii'  
Claret Ash  
Height: 15m  
Width: 8m



*Lagerstroemia indica*  
Crape Myrtle  
Height: 7m  
Width: 5m



*Pyrus calleryana* 'Capital'  
Capital Callery Pear  
Height: 5m  
Width: 3m



*Pyrus calleryana* 'Chanticleer'  
Chanticleer Callery Pear  
Height: 11m  
Width: 6m

### SHRUBS & GROUNDCOVERS



*Cistus salvifolius* 'Brilliant'  
Pink Rock Rose  
Height: 1.5m  
Width: 1m



*Dianella caerulea* 'Little Jess'  
Little Jess  
Height: 0.4m  
Width: 0.4m



*Dianella tasmanica* 'Tas Red'  
Tas Red Flax Lily  
Height: 0.6m  
Width: 0.5m



*Dietes grandiflora*  
Butterfly Iris  
Height: 0.7m  
Width: 0.7m



*Ficinia nodosa*  
Knobby Club Rush  
Height: 1m  
Width: 0.7m



*Leucophyta brownii*  
Cushion Bush  
Height: 0.7m  
Width: 1m



*Liriope muscari*  
Blue Lily Turf  
Height: 0.5m  
Width: 0.3m



*Lomandra longifolia* 'Tanika'  
Tanika Mat Rush  
Height: 0.6m  
Width: 0.6m



*Olearia axillaris*  
Coast Daisy Bush  
Height: 1.5m  
Width: 1.5m



*Rosmarinus officinalis*  
Rosemary  
Height: 1m  
Width: 1m



*Westringia fruticosa* 'Grey Box'  
Grey Box  
Height: 0.4m  
Width: 0.4m



*Raphiolepis x delacourii*  
Cosmic White  
Height: 1.5m  
Width: 1.5m



*Viburnum tinus*  
Laurustinus  
Height: 2m  
Width: 2m



DEVELOPMENT REGULATIONS 2008  
Form of Declaration (Schedule 5 clause 2A)



Government  
of South Australia

To: State Commission Assessment Panel (SCAP)

From: Nielsen Architects

Date of Application: 28 / 08 / 2018  
Location of Proposed Development: 1150-1154 South Road, Clovelly Park  
1156-1158 South Road, Clovelly Park  
19 Wingfield Street, Clovelly Park  
1-5 Norrie Avenue, Clovelly Park

House No: \_\_\_\_\_ Lot No: \_\_\_\_\_ Street: \_\_\_\_\_

Town/Suburb: \_\_\_\_\_

Section No (full/part): \_\_\_\_\_ Hundred: \_\_\_\_\_

Volume: \_\_\_\_\_ Folio: \_\_\_\_\_ See Planning Statement Appendix 1 for CTs

Nature of Proposed Development: Staged construction of a freestanding ALDI Store (shop) and Chemist Warehouse (shop) with associated on-site signage, fencing, car parking and landscaping

I Trent Burns (Nielsen Architects) being the applicant/ a person acting on behalf of the applicant (delete the inapplicable statement) for the development described above declare that the proposed development will involve the construction of a building which would, if constructed in accordance with the plans submitted, not be contrary to the regulations prescribed for the purposes of section 86 of the Electricity Act 1996. I make this declaration under clause 2A(1) of Schedule 5 of the Development Regulations 2008.

Signed: \_\_\_\_\_

Date: 28/08/18



The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



Registrar-General

## Certificate of Title - Volume 5191 Folio 11

Parent Title(s)	CT 4282/943
Dealing(s) Creating Title	CONVERTED TITLE
Title Issued	01/06/1994
Edition	5
Edition Issued	15/01/2014



## Estate Type

FEE SIMPLE

## Registered Proprietor

GORDON KEE AIK THIA  
SHEIK LIENG LING  
OF 17 WINGFIELD STREET CLOVELLY PARK SA 5042  
AS JOINT TENANTS

## Description of Land

ALLOTMENT 1 DEPOSITED PLAN 19270  
IN THE AREA NAMED CLOVELLY PARK  
HUNDRED OF ADELAIDE

## Easements

NIL

## Schedule of Dealings

Dealing Number	Description
12055907	MORTGAGE TO NATIONAL AUSTRALIA BANK LTD.

## Notations

### Dealings Affecting Title

NIL

### Priority Notices



<b>Product</b>	Register Search Plus
<b>Date/Time</b>	04/03/2016 02:01PM
<b>Customer Reference</b>	1648055
<b>Order ID</b>	20160304007229
<b>Cost</b>	\$32.50

---

NIL

### Registrar-General's Notes

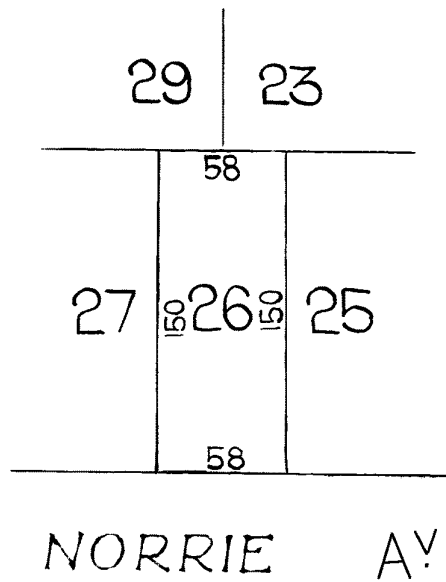
NIL

### Administrative Interests

NIL

\* Denotes the dealing has been re-lodged.





DISTANCES ARE IN FEET AND INCHES FOR METRIC CONVERSION		
1 FOOT	=	0.3048 metres
1 INCH	=	0.0254 metres

The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



Registrar-General

## Certificate of Title - Volume 5148 Folio 47

Parent Title(s)	CT 2552/15
Dealing(s) Creating Title	CONVERTED TITLE
Title Issued	08/10/1993
Edition	3
Edition Issued	15/04/2009



## Estate Type

FEE SIMPLE

## Registered Proprietor

1156-1158 STH RD CLOVELLY PARK PTY. LTD. (ACN: 135 182 231)  
OF 274 EDWARDES STREET RESERVOIR VIC 3073

## Description of Land

ALLOTMENT 216 FILED PLAN 12391  
IN THE AREA NAMED CLOVELLY PARK  
HUNDRED OF ADELAIDE

## Easements

NIL

## Schedule of Dealings

Dealing Number	Description
7651907	ENCUMBRANCE TO ABYDOS PTY. LTD. (SINGLE COPY ONLY)

## Notations

### Dealings Affecting Title

NIL

### Priority Notices

NIL

### **Notations on Plan**

NIL

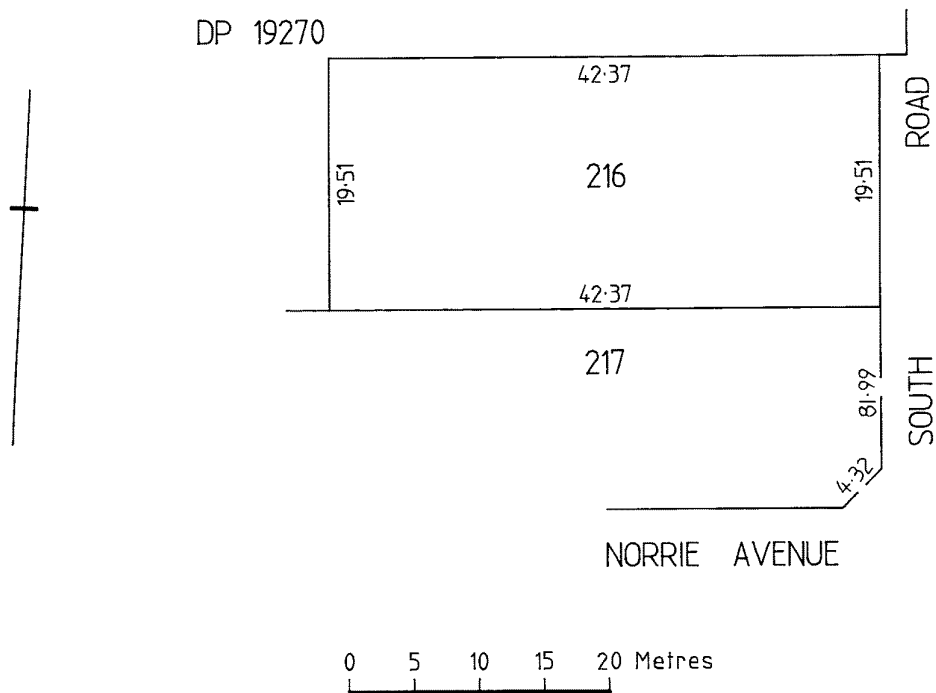
### **Registrar-General's Notes**

NIL

### **Administrative Interests**

NIL

\* Denotes the dealing has been re-lodged.



The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



Registrar-General

## Certificate of Title - Volume 5148 Folio 210

Parent Title(s)	CT 2588/47
Dealing(s) Creating Title	CONVERTED TITLE
Title Issued	08/10/1993
Edition	3
Edition Issued	15/04/2009



## Estate Type

FEE SIMPLE

## Registered Proprietor

1156-1158 STH RD CLOVELLY PARK PTY. LTD. (ACN: 135 182 231)  
OF 274 EDWARDES STREET RESERVOIR VIC 3073

## Description of Land

ALLOTMENT 29 DEPOSITED PLAN 3034  
IN THE AREA NAMED CLOVELLY PARK  
HUNDRED OF ADELAIDE

## Easements

NIL

## Schedule of Dealings

Dealing Number	Description
7651907	ENCUMBRANCE TO ABYDOS PTY. LTD. (SINGLE COPY ONLY)

## Notations

### Dealings Affecting Title

NIL

### Priority Notices

NIL



### **Notations on Plan**

NIL

### **Registrar-General's Notes**

NIL

### **Administrative Interests**

NIL

\* Denotes the dealing has been re-lodged.



DISTANCES ARE IN FEET AND INCHES  
FOR METRIC CONVERSION  
1 FOOT = 0.3048 metres  
1 INCH = 0.0254 metres

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Registrar-General

## Certificate of Title - Volume 5148 Folio 211

Parent Title(s)	CT 2588/48
Dealing(s) Creating Title	CONVERTED TITLE
Title Issued	08/10/1993
Edition	3
Edition Issued	15/04/2009



## Estate Type

FEE SIMPLE

## Registered Proprietor

1156-1158 STH RD CLOVELLY PARK PTY. LTD. (ACN: 135 182 231)  
OF 274 EDWARDES STREET RESERVOIR VIC 3073

## Description of Land

ALLOTMENT 30 DEPOSITED PLAN 3034  
IN THE AREA NAMED CLOVELLY PARK  
HUNDRED OF ADELAIDE

## Easements

NIL

## Schedule of Dealings

Dealing Number	Description
7651907	ENCUMBRANCE TO ABYDOS PTY. LTD. (SINGLE COPY ONLY)

## Notations

### Dealings Affecting Title

NIL

### Priority Notices

NIL

### **Notations on Plan**

NIL

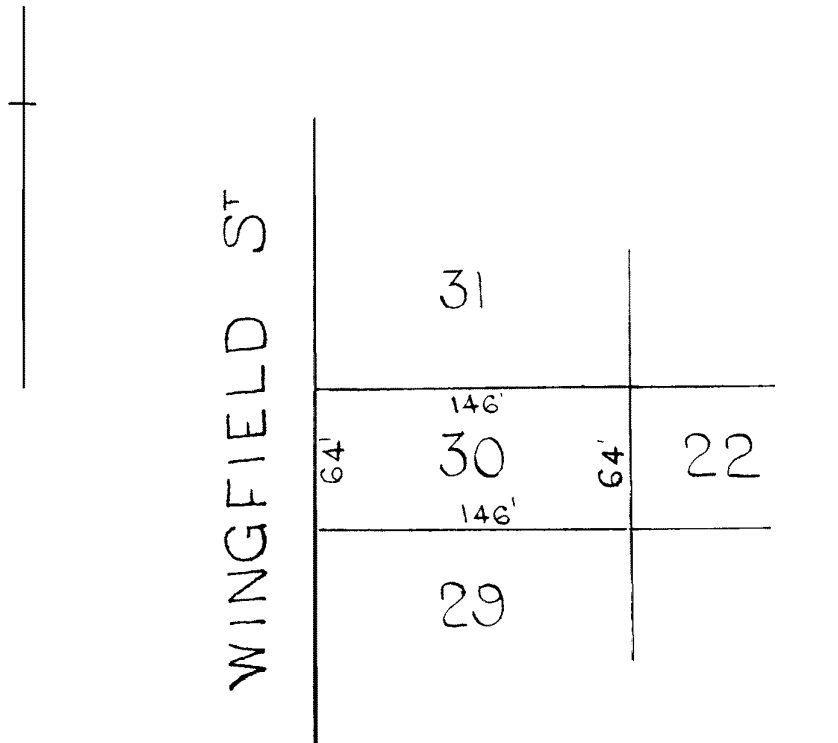
### **Registrar-General's Notes**

NIL

### **Administrative Interests**

NIL

\* Denotes the dealing has been re-lodged.



DISTANCES ARE IN FEET AND INCHES  
FOR METRIC CONVERSION

1 FOOT = 0.3048 metres  
1 INCH = 0.0254 metres



The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



Registrar-General

## Certificate of Title - Volume 5148 Folio 214

Parent Title(s)	CT 4367/503
Dealing(s) Creating Title	CONVERTED TITLE
Title Issued	08/10/1993
Edition	5
Edition Issued	12/10/2015



## Estate Type

FEE SIMPLE

## Registered Proprietor

1156-1158 STH RD CLOVELLY PARK PTY. LTD. (ACN: 135 182 231)  
OF 274 EDWARDES STREET RESERVOIR VIC 3073

## Description of Land

ALLOTMENT 218 FILED PLAN 12391  
IN THE AREA NAMED CLOVELLY PARK  
HUNDRED OF ADELAIDE

## Easements

NIL

## Schedule of Dealings

Dealing Number	Description
7651907	ENCUMBRANCE TO ABYDOS PTY. LTD. (SINGLE COPY ONLY)

## Notations

### Dealings Affecting Title

NIL

### Priority Notices

NIL



Product	Register Search Plus
Date/Time	04/03/2016 02:03PM
Customer Reference	1648055
Order ID	20160304007269
Cost	\$32.50

---

### Notations on Plan

NIL

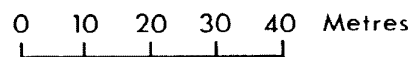
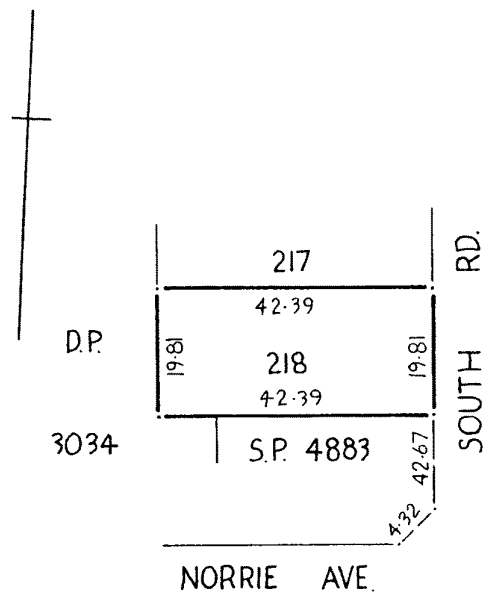
### Registrar-General's Notes

NEW EDITION CREATED DUE TO EXPIRATION OF LEASE

### Administrative Interests

NIL

\* Denotes the dealing has been re-lodged.



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Registrar-General

## Certificate of Title - Volume 5148 Folio 217

Parent Title(s)	CT 4390/792
Dealing(s) Creating Title	CONVERTED TITLE
Title Issued	08/10/1993
Edition	3
Edition Issued	15/04/2009



## Estate Type

FEE SIMPLE

## Registered Proprietor

1156-1158 STH RD CLOVELLY PARK PTY. LTD. (ACN: 135 182 231)  
OF 274 EDWARDES STREET RESERVOIR VIC 3073

## Description of Land

ALLOTMENT 217 FILED PLAN 12391  
IN THE AREA NAMED CLOVELLY PARK  
HUNDRED OF ADELAIDE

## Easements

NIL

## Schedule of Dealings

Dealing Number	Description
7651907	ENCUMBRANCE TO ABYDOS PTY. LTD. (SINGLE COPY ONLY)

## Notations

### Dealings Affecting Title

NIL

### Priority Notices

NIL



Product	Register Search Plus
Date/Time	04/03/2016 02:05PM
Customer Reference	1648055
Order ID	20160304007305
Cost	\$32.50

---

### Notations on Plan

NIL

### Registrar-General's Notes

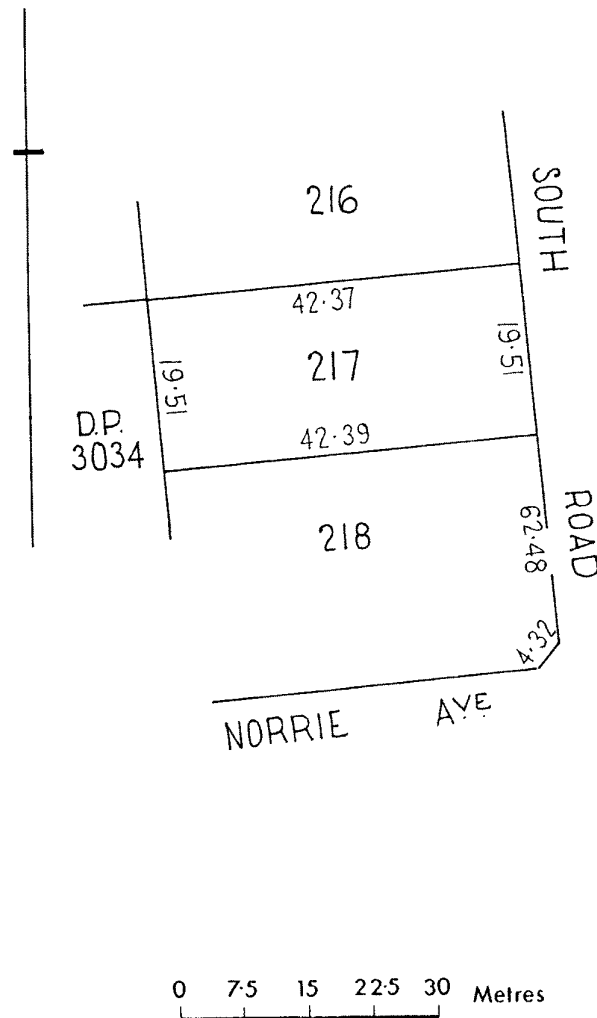
NIL

### Administrative Interests

NIL

\* Denotes the dealing has been re-lodged.





The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



Registrar-General

## Certificate of Title - Volume 5258 Folio 302

Parent Title(s) CT 2588/46  
Dealing(s) CONVERTED TITLE  
Creating Title  
Title Issued 29/03/1995  
Edition 9  
Edition Issued 19/02/2015



## Estate Type

FEE SIMPLE

## Registered Proprietor

1156-1158 STH RD CLOVELLY PARK PTY. LTD. (ACN: 135 182 231)  
OF LEVEL 15/390 ST KILDA ROAD MELBOURNE VIC 3004

## Description of Land

ALLOTMENT 28 DEPOSITED PLAN 3034  
IN THE AREA NAMED CLOVELLY PARK  
HUNDRED OF ADELAIDE

## Easements

NIL

## Schedule of Dealings

NIL

## Notations

### Dealings Affecting Title

NIL

### Priority Notices

NIL

### Notations on Plan



NIL

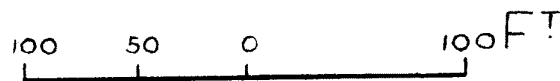
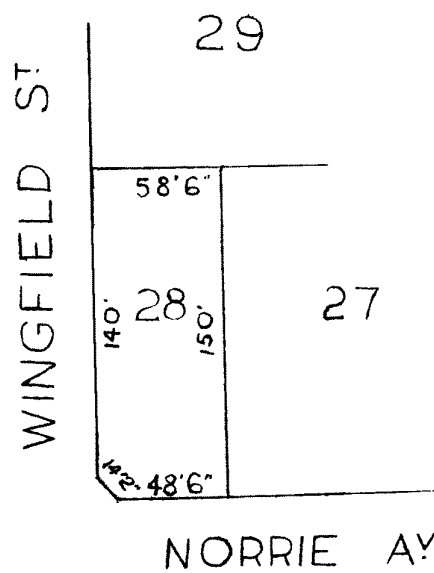
### **Registrar-General's Notes**

NIL

### **Administrative Interests**

NIL

\* Denotes the dealing has been re-lodged.



DISTANCES ARE IN FEET AND INCHES FOR METRIC CONVERSION	
1 FOOT	= 0.3048 metres
1 INCH	= 0.0254 metres

The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



Registrar-General

## Certificate of Title - Volume 5676 Folio 287

Parent Title(s) CT 2332/186  
Dealing(s) CONVERTED TITLE  
Creating Title  
Title Issued 28/07/1999  
Edition 2  
Edition Issued 09/02/2015



## Estate Type

FEE SIMPLE

## Registered Proprietor

1156-1158 STH RD CLOVELLY PARK PTY. LTD. (ACN: 135 182 231)  
OF LEVEL 15/390 ST KILDA ROAD MELBOURNE VIC 3004

## Description of Land

ALLOTMENT 27 DEPOSITED PLAN 3034  
IN THE AREA NAMED CLOVELLY PARK  
HUNDRED OF ADELAIDE

## Easements

NIL

## Schedule of Dealings

NIL

## Notations

### Dealings Affecting Title

NIL

### Priority Notices

NIL

### Notations on Plan



<b>Product</b>	Register Search Plus
<b>Date/Time</b>	04/03/2016 02:00PM
<b>Customer Reference</b>	1648055
<b>Order ID</b>	20160304007212
<b>Cost</b>	\$32.50

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NIL

### Registrar-General's Notes

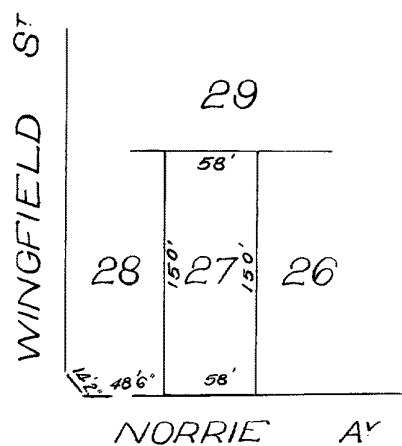
NIL

### Administrative Interests

NIL

\* Denotes the dealing has been re-lodged.





100 0 100 Feet

DISTANCES ARE IN FEET AND INCHES  
FOR METRIC CONVERSION  
1 FOOT = 0.3048 METRES  
1 INCH = 0.0254 METRES



Product	Register Search Plus
Date/Time	07/07/2016 04:22PM
Customer Reference	162878:LGA
Order ID	20160707009864
Cost	\$33.00

The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



Registrar-General

## Certificate of Title - Volume 5252 Folio 910

Parent Title(s)	CT 5103/926
Dealing(s) Creating Title	PS 7815121
Title Issued	07/03/1995
Edition	6
Edition Issued	11/02/2009



## Estate Type

FEE SIMPLE

## Registered Proprietor

1152 SOUTH ROAD CLOVELLY PARK PTY. LTD. (ACN: 133 069 539)  
OF 274 EDWARDES STREET RESERVOIR VIC 3073

## Description of Land

ALLOTMENT 3 DEPOSITED PLAN 19270  
IN THE AREA NAMED CLOVELLY PARK  
HUNDRED OF ADELAIDE

## Easements

NIL

## Schedule of Dealings

NIL

## Notations

### Dealings Affecting Title

NIL

### Priority Notices

NIL

### Notations on Plan



Product	Register Search Plus
Date/Time	07/07/2016 04:22PM
Customer Reference	162878:LGA
Order ID	20160707009864
Cost	\$33.00

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NIL

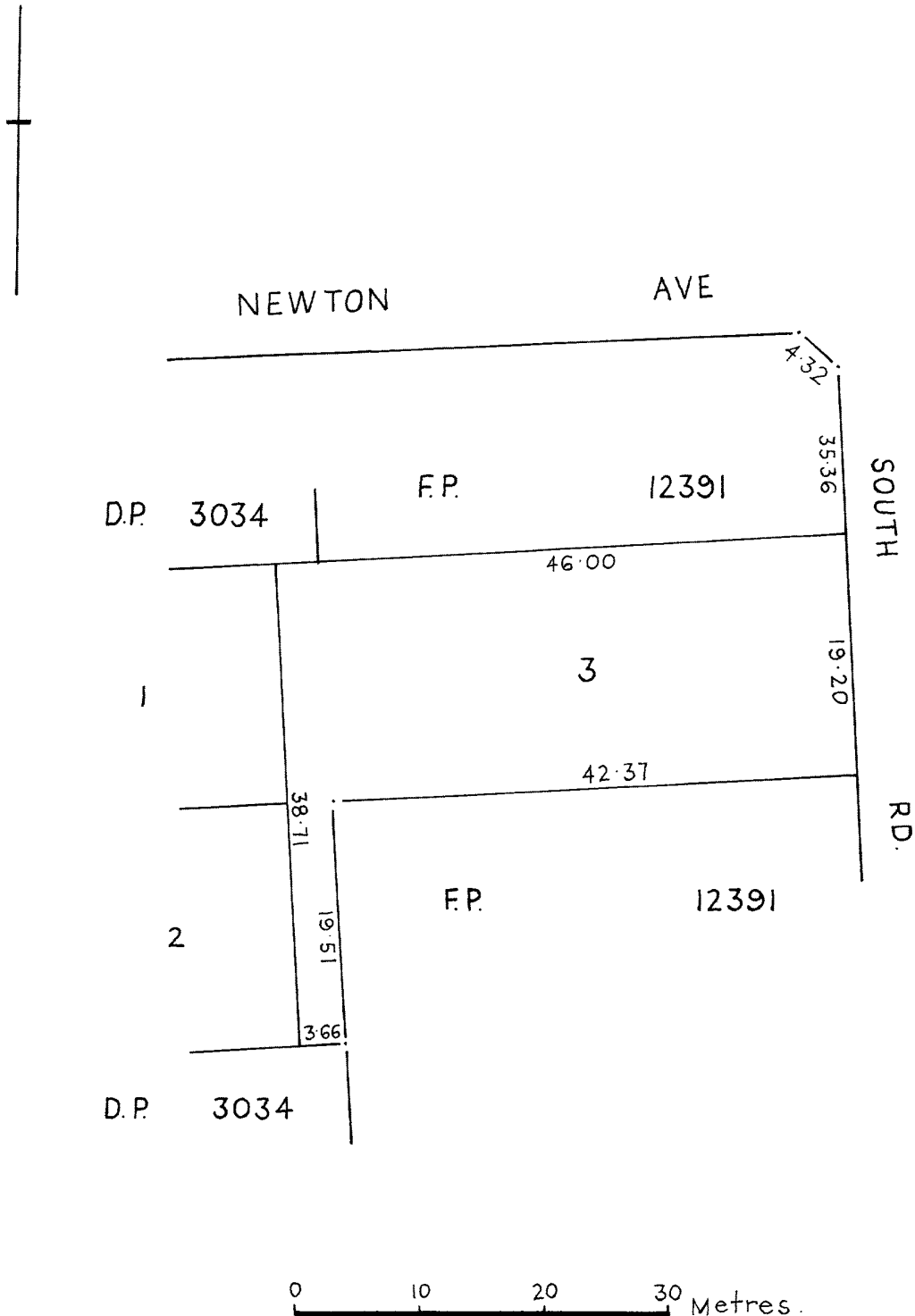
### Registrar-General's Notes

PLAN FOR LEASE PURPOSES VIDE G649/1991

### Administrative Interests

NIL

\* Denotes the dealing has been re-lodged.



The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



Registrar-General

## Certificate of Title - Volume 5450 Folio 122

Parent Title(s)	CT 4282/944
Dealing(s) Creating Title	CONVERTED TITLE
Title Issued	16/09/1997
Edition	6
Edition Issued	17/01/2015



## Estate Type

FEE SIMPLE

## Registered Proprietor

1156-1158 STH ROAD CLOVELLY PARK PTY. LTD. (ACN: 135 182 231)  
OF LEVEL 15/390 ST KILDA ROAD MELBOURNE VIC 3004

## Description of Land

ALLOTMENT 2 DEPOSITED PLAN 19270  
IN THE AREA NAMED CLOVELLY PARK  
HUNDRED OF ADELAIDE

## Easements

NIL

## Schedule of Dealings

NIL

## Notations

### Dealings Affecting Title

NIL

### Priority Notices

NIL

### Notations on Plan



Product	Register Search Plus
Date/Time	07/07/2016 02:03PM
Customer Reference	162878
Order ID	20160707007149
Cost	\$33.00

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NIL

### Registrar-General's Notes

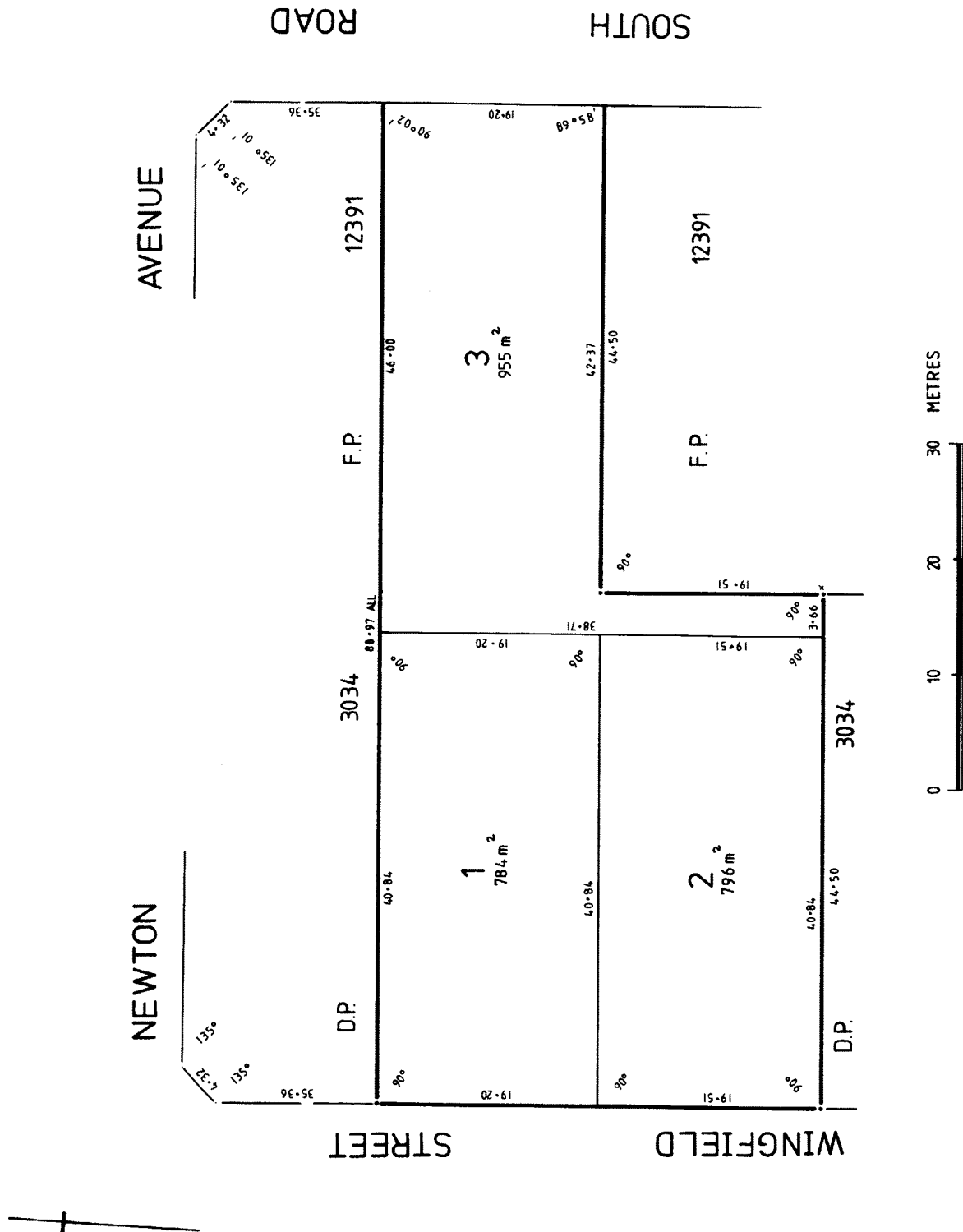
NIL

### Administrative Interests

NIL

\* Denotes the dealing has been re-lodged.





# ekistics

**ALDI & CHEMIST  
WAREHOUSE,  
CLOVELLY PARK**

Amended Planning Statement  
DA 100/E074/18  
Proposed Retail Development

Prepared for:  
**ALDI Stores &  
Chemist Warehouse**

Date:  
**December 2018**

ekistics

## Proprietary Information Statement

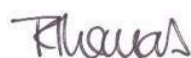
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## Document Control

Revision	Description	Author	Date
V1	Draft Planning Statement	HK/RT	19 August 2018
V2	Final Planning Statement	HK/RT	30 August 2018
V2	Amended Planning Statement	HK/RT	29 November 2018

Approved by: RT



Date: 4 December 2018

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## 1. Executive Summary

Category	Details
PROJECT	ALDI and Chemist Warehouse Clovelly Park
ADDRESS OF SITE	1150 -1154 South Road, Clovelly Park 1156-1158 South Road, Clovelly Park 17-19 Wingfield Street, Clovelly Park 1-5 Norrie Avenue, Clovelly Park
CERTIFICATES OF TITLE	Volume 5132 Folio 506 (Lot 26) Volume 5148 Folio 47 (Lot 216) Volume 5148 Folio 210 (Lot 29) Volume 5148 Folio 214 (Lot 218) Volume 5148 Folio 217 (Lot 217) Volume 5148 Folio 211 (Lot 30) Volume 5252 Folio 910 (Lot 3) Volume 5258 Folio 302 (Lot 28) Volume 5450 Folio 122 (Lot 2) Volume 5676 Folio 287 (Lot 27) Volume 5191 Folio 11 (Lot 1)
SITE AREA	ALDI Site Area – 5,451m <sup>2</sup> Chemist Warehouse Site Area – 3,756m <sup>2</sup> Total: 9,207m <sup>2</sup>
FRONTAGE	South Road – 78.03 metres Norrie Avenue – 50.13 metres Wingfield Street – 120.702 metres
DEPTH	83.9 metres (South Road to Wingfield Street)
LOCAL GOVERNMENT	City of Marion
RELEVANT AUTHORITY	State Commission Assessment Panel (SCAP) – Schedule 10, 20 (1)
DEVELOPMENT PLAN	Marion Council – Consolidated 20 February 2018
ZONING	Neighbourhood Centre Zone
POLICY AREA/PRECINCT	Nil
EXISTING USE	Retail shops and residential dwellings
PROPOSAL DESCRIPTION	Staged construction of a freestanding ALDI Store (shop) and Chemist Warehouse (shop) with associated on-site signage, fencing, car parking and landscaping
SEPARATE CONSENTS	N/A
REFERRALS	Commissioner of Highways
PUBLIC NOTIFICATION	Category 2
APPLICANT	Nielsen Architects
CONTACT PERSON	Rebecca Thomas – Ekistics Planning and Design – (08) 7231 0286
OUR REFERENCE	00263

## 2. Introduction/Background

ALDI Stores and Chemist Warehouse are seeking to construct a new ALDI Store and Chemist Warehouse (shops) adjacent South Road, Clovelly Park, within the Neighbourhood Centre Zone of Marion Council Development Plan.

The original application (DA/E074/18) was lodged on the 31<sup>st</sup> August 2018 and underwent statutory public notification and agency referrals. In response to the feedback, various design amendments have been incorporated into the proposal, together with the inclusion of an additional allotment to the north, recently acquired by Chemist Warehouse.

The amendments to the original proposal include:

- The subject site has expanded to include 17 Wingfield Street;
- The proposed Chemist Warehouse building has expanded to occupy 17 Wingfield Street and consequently the Gross Leasable Floor Area has increased by 401m<sup>2</sup>, from 700m<sup>2</sup> to 1101m<sup>2</sup>;
- The proposed Chemist Warehouse setback from the northern and western boundary has increased from 0m to 1.5 metres;
- The South Road crossover location has been revised to the south, utilising an existing crossover;
- Seven (7) additional Capital Callery Pear trees are proposed adjacent Wingfield Street, to the rear of Chemist Warehouse;
- Eight (8) additional Capital Callery Pear trees are proposed in the landscaping bed between Chemist warehouse and the northern boundary;
- Two (2) additional Crepe Myrtle trees are proposed adjacent the southern boundary, near Norrie Avenue;
- Additional glazing is proposed to the eastern elevation of the Chemist Warehouse building;
- The car parking layout has been reconfigured to accommodate the revised crossover location and three (3) additional car parking spaces are proposed (total of 120 car parking spaces);
- Pylon sign 1 moved slightly south, closer to revised South Road crossover location;
- Western elevation of the ALDI building amended to incorporate steel feature screens with perforated metal panels; and
- The Chemist Warehouse bike rack has been relocated near the northern boundary;

This planning statement provides information about the subject land and proposed development and addresses the merits of the development application against the relevant provisions of the Neighbourhood Centre Zone, as well as the most relevant 'General Section' provisions of the Marion Council Development Plan.

For the purposes of this statement, the Marion Council Development Plan (Consolidated 20 February 2018) will be referred to as the 'Development Plan', the '*Development Act 1993*' will be referred to as the 'Act' and the '*Development Regulations 2008*' will be referred to as the 'Regulations'.

The proposed development is illustrated on the amended plans and elevations prepared by Nielsen Architects as identified in Table 2.1 below.

**Table 2.1** *Drawing Schedule*

DRAWING NO	REVISION	DRAWING TITLE	SCALE
DA01.1	H	LOCATION PLAN	1:1000
DA01.2	B	CONTRACT TITLES - EXISTING	1:500
DA01.3	E	CONTRACT TITLES - PROPOSED	1:500
DA02.1	C	EXISTING SITE PLAN	1:500
DA02.2	C	DEMOLITION SITE PLAN	1:500
DA02.3	L	PROPOSED SITE PLAN	1:500
DA02.4	I	PROPOSED ROOF PLAN	1:500
DA03.1	C	ALDI EXTERNAL ELEVATIONS	1:250
DA03.2	G	CHEMIST WAREHOUSE EXTERNAL ELEVATIONS	1:250
DA03.3	G	STREETSCAPE ELEVATIONS	1:350
DA05	D	SECTIONS	1:250
DA06.1	H	SIGNAGE PLAN	1:500
DA06.2	C	SIGNAGE DETAILS - SHEET 1	1:50
DA06.3	B	SIGNAGE DETAILS - SHEET 2	1:50
DA06.4	A	SIGNAGE DETAILS - SHEET 3	1:50
DA08.1	E	PROPOSED 3D VISUAL RENDERS - SHEET 1	NTS
DA08.2	F	PROPOSED 3D VISUAL RENDERS - SHEET 2	NTS

Other supporting documentation, which also form part of the application, are appended to this report and include:

- Amended Traffic Impact Assessment prepared by GTA;
- Amended Landscape Plan prepared by Outer Space Landscape Architects;
- Amended Environmental Noise Assessment by Sonus; and
- Stormwater Management Report by Drew Rudd Engineers.

## 3. The Site and Locality

### 3.1 The Site

The subject land comprises the Certificates of Title and street addresses as identified in Table 3.1 (refer to **Appendix 1**). There are no easements listed on the Certificates of Title.

**Table 3.1** *Street addresses and Certificates of Title*

Street address	Certificate of Title reference
1150-1154 South Road, Clovelly Park	Volume 5252 Folio 910
1156-1158 South Road, Clovelly Park	Volume 5148 Folio 210
	Volume 5148 Folio 214
	Volume 5148 Folio 217
	Volume 5148 Folio 211
	Volume 5148 Folio 47
19 Wingfield Street, Clovelly Park	Volume 5450 Folio 122
17 Wingfield Street, Clovelly Park	Volume 5191 Folio 11 (Lot 1)
5 Norrie Avenue, Clovelly Park	Volume 5258 Folio 302
3 Norrie Avenue, Clovelly Park	Volume 5676 Folio 287
1 Norrie Avenue, Clovelly Park	Volume 5132 Folio 506

**Figure 3.1** *Subject site allotments*



The irregular shaped subject site has a total area of 9,207m<sup>2</sup>, and is proposed to accommodate an ALDI supermarket, Chemist Warehouse and associated car parking and landscaping. The site has a primary frontage of 78 metres to South Road, a secondary frontage of 50 metres (excluding corner cut-off) to Norrie Avenue and a rear boundary of 120 metres to Wingfield Street.

Vehicular access to the subject site is provided via one (1) dual crossover to South Road and two (2) crossovers to Wingfield Street.

The subject site currently comprises the following:

- Six (6) commercial allotments:
  - » Single storey Chemist Warehouse (shop) facing South Road;
  - » Single storey Rite Price Supermarket (shop) located behind Chemist Warehouse; and
  - » Associated ground level car parking through the middle of the site.
- Four (4) residential allotments containing single storey detached dwellings and associated outbuildings located at:
  - » 19 Wingfield Street;
  - » 17 Wingfield Street;
  - » 1 Norrie Avenue; and
  - » 3 Norrie Avenue.
- One (1) vacant allotment on the corner of Wingfield Street and Norrie Avenue.

The subject land is relatively flat with minor vegetation in the form of various trees, bushes and grasses that are scattered around the perimeter of the commercial car park and existing dwellings. None of the trees on site or street trees appear to be 'Regulated' under the Act.

A Detailed and Level Survey of the subject land has been prepared by Alexander Symonds and is included as **Appendix 6** of this report.

The images below highlight the existing development on site:

**Figure 3.2** Subject land viewed from South Road looking north-west





Figure 3.3 Subject land viewed from South Road looking south-west



Figure 3.4 Subject land viewed from South Road looking west





Figure 3.5 Subject land viewed from within the car park looking east



Figure 3.6 Subject land viewed from Wingfield Street looking east



Figure 3.7 Subject land looking north from corner of Norrie Avenue and Wingfield Street



Figure 3.8 Subject land viewed from Norrie Avenue looking north towards 1 and 3 Norrie Avenue



### 3.2 The Locality and Surrounding Development

The character of the locality is mixed in terms of land use and built-form (see **Figure 3.9** over-page).

Various commercial operations are located along South Road adjacent the subject site in the form of a petrol filling station and shop, gym, consulting room and shop. A signalised pedestrian crossing to the east of the subject site provides pedestrian connectivity across South Road. The built form of commercial development along South Road varies and includes a mix of contemporary commercial development (see **Figure 3.11**) as well as older building stock with a relatively poor streetscape amenity (see **Figure 3.12**).

To the south-east of the subject site, adjoining commercial land uses include shops (bakery, café), personal service establishment (hairdresser) a consulting room and a retail showroom. Adjoining the subject site to the north, a consulting room fronts South Road and a single storey detached dwelling faces Wingfield Street.

The locality plan (**Figure 3.9**) displays the existing land use across the subject site and surrounding allotments. The adjoining detached dwelling to the north (15 Wingfield Street) will be the last remaining dwelling within the Neighbourhood Centre Zone bounded by South Road, Norrie Avenue, Wingfield Street and Newton Avenue, should the proposal be approved.

Residential development is located on the opposite side of Wingfield Street, in the form of single-storey detached, semi-detached and group dwellings.

Signage associated with the range of commercial development within the locality includes freestanding signs (Chemist Warehouse, F45 indoor Gym, Petro Filling Station) as well as flat wall façade signage and window displays.

South Road is a DPTI controlled primary arterial road with a median strip separating the dual lane north-bound and south-bound traffic. The median strip also serves to restrict vehicle access to the site from South Road to left-in, left out movements.

The site is located approximately 26 metres from the nearby public transport bus stop along South Road, to the north of the site form (see **Figure 3.9** over-page).

The locality also includes the State Heritage listed St Mary's Anglian Church and associated cemetery fronting South Road, approximately 56m to the south-east of the subject land.



Figure 3.9 Locality Plan

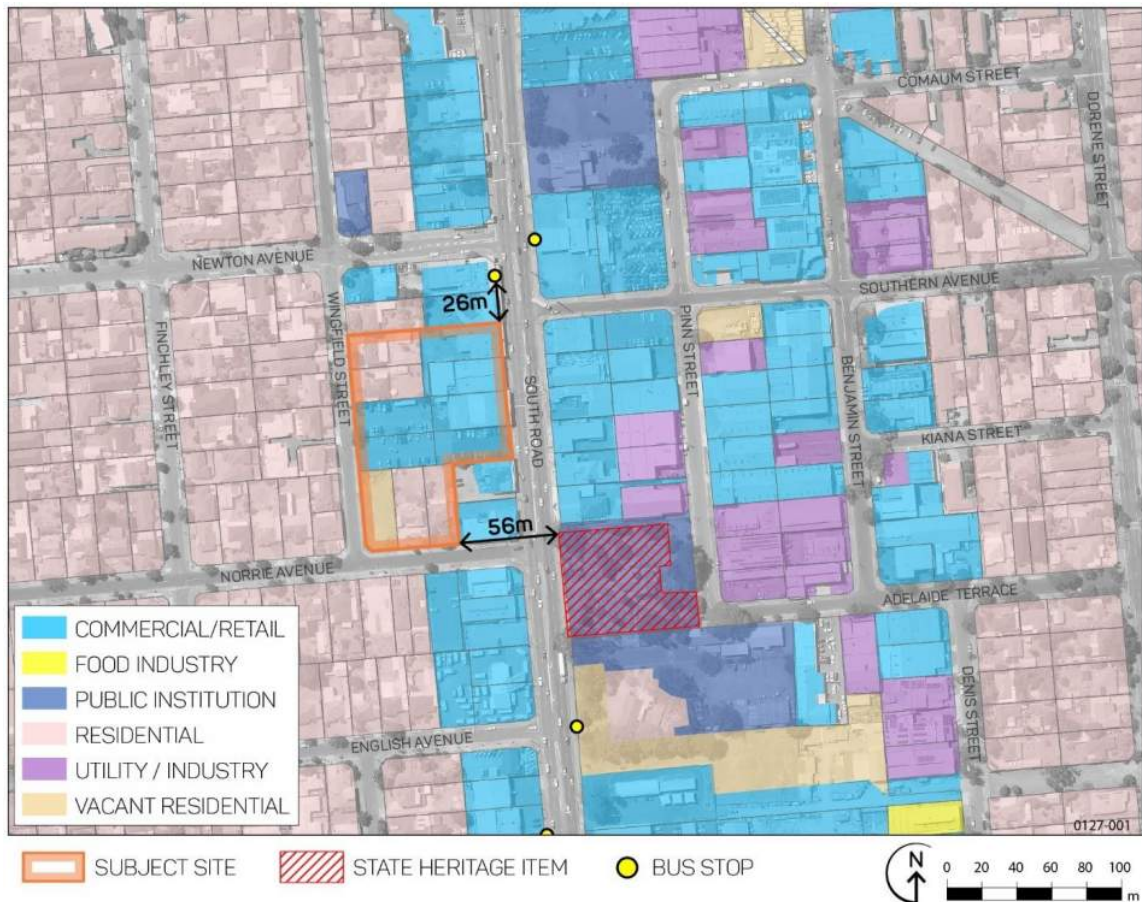


Figure 3.10 Adjoining South Road commercial development to the south-east of the subject site



Figure 3.11 Adjacent contemporary commercial development along South Road, looking east



Figure 3.12 Adjacent older commercial development along South Road, looking south-east



Figure 3.13 South Road signalised pedestrian crossing and median strip, looking south





**Figure 3.14** Interface between adjoining retail showroom to the south-east of the subject site and 1 Norrie Avenue (subject site), looking north



**Figure 3.15** Adjacent residential development along Wingfield Street, facing south-west





**Figure 3.16** Adjoining property to the north – 15 Wingfield Street (white pillar fence) and recently aquired property (17 Wingfield) to the south



**Figure 3.17** Wingfield Street facing north, residential development to the west and subject site to the east



**Figure 3.18** Adjacent residential and commercial development along Norrie Avenue, facing south, from Wingfield Street through the vacant allotment (subject site).



## 4. Proposed Development

### 4.1 Land Use

The proposed development involves the demolition of the existing commercial and residential buildings on site and in their place the following is proposed:

- a new ALDI supermarket to be constructed on the south-western portion of the land adjacent the corner of Norrie Avenue and Wingfield Street;
- a new Chemist Warehouse building to be constructed on the north-western portion of the land abutting Wingfield Street; and
- a substantial shared car-park established between and in front of the ALDI Store and Chemist Warehouse to provide convenient parking for customers with vehicular access provided from South Road, Wingfield Street and Norrie Avenue.

The ALDI Store will have a nett floor area of 1,677m<sup>2</sup>, of which 1,187m<sup>2</sup> will be retail floor space, 411m<sup>2</sup> will be 'back of house' functions and 79m<sup>2</sup> will be for staff amenities. The Chemist Warehouse will have a gross retail floor area of 1101m<sup>2</sup>. The combined retail area of the ALDI store and Chemist Warehouse is 2,988m<sup>2</sup>.

A separate land division application will be lodged shortly to consolidate the existing titles and realign a boundary to accommodate the two proposed commercial operations. Appropriate easements and rights of way to facilitate the integrated vehicular access, manoeuvring areas and car parking. Importantly, a development deed has been prepared between ALDI and Chemist Warehouse which will ensure that vehicular access and car parking will be managed in a coordinated manner across the site.

A centralised 120 space car park will be established to service the ALDI Store and Chemist Warehouse with roughly half allocated to each store (albeit they will appear on a single consolidated car park). This will provide convenient parking near the entrances to the shops as well as ensuring that vehicles can safely access the site from South Road, Norrie Avenue and Wingfield Street.

The proposed development (including the ALDI Store and Chemist Warehouse) meets the definition of 'shop' in accordance with Schedule 1 of the *Development Regulations, 2008*:

**shop** means—

- (a) premises used primarily for the sale by retail, rental or display of goods, foodstuffs, merchandise or materials; or
- (b) a restaurant; or
- (c) a bulky goods outlet or a retail showroom; or
- (d) a personal service establishment,

but does not include—

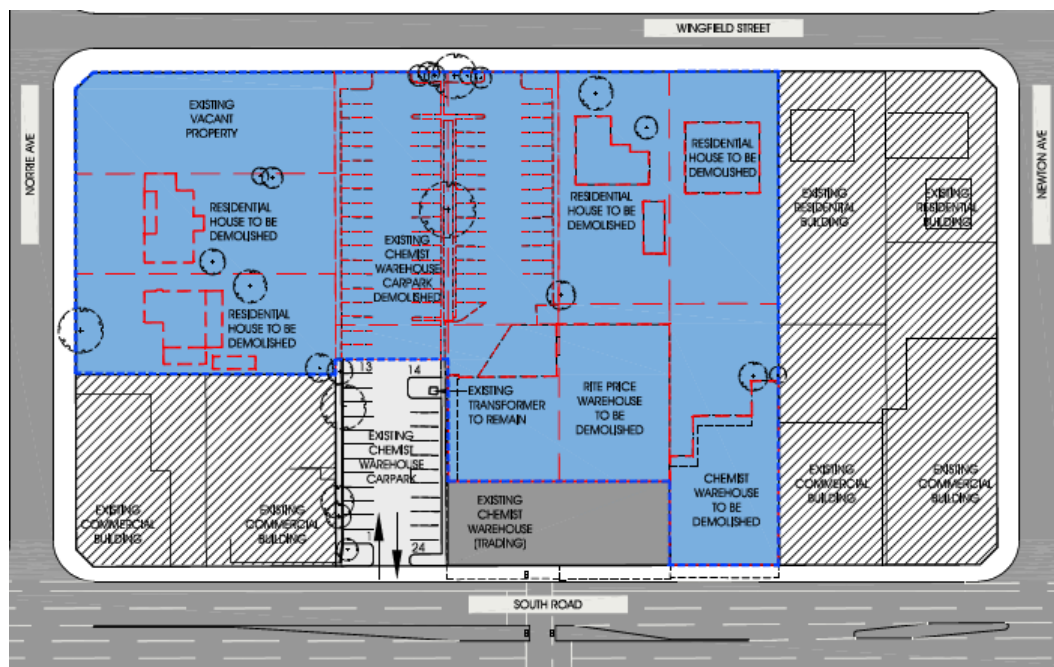
- (e) a hotel; or

- (f) a motor repair station; or
- (g) a petrol filling station; or
- (h) a plant nursery where there is no sale by retail; or
- (i) a timber yard; or
- (j) service trade premises; or
- (k) service industry;

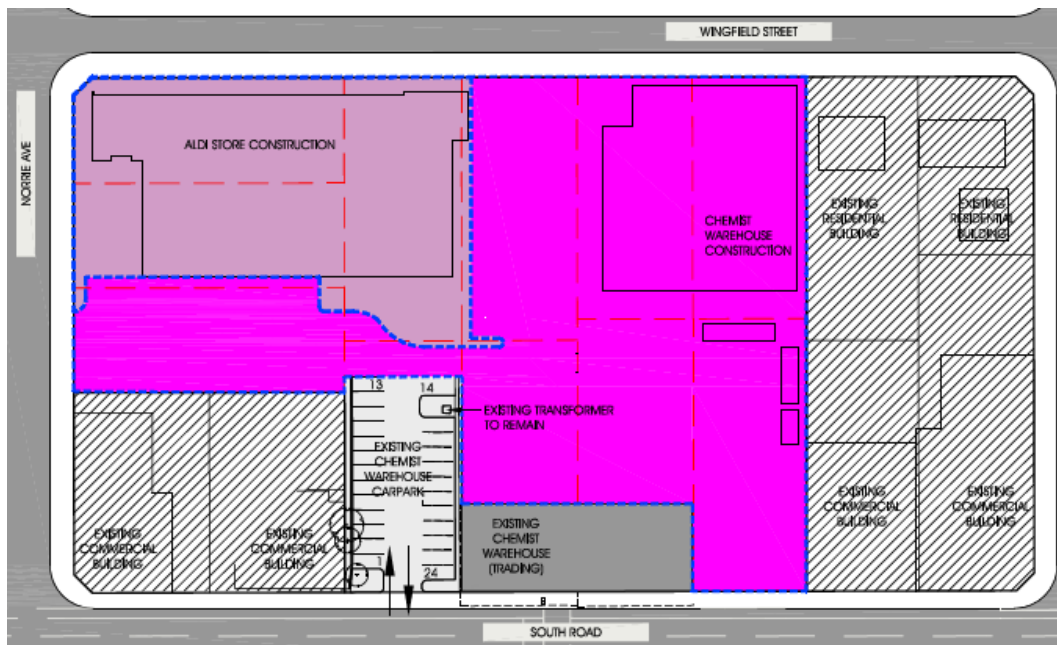
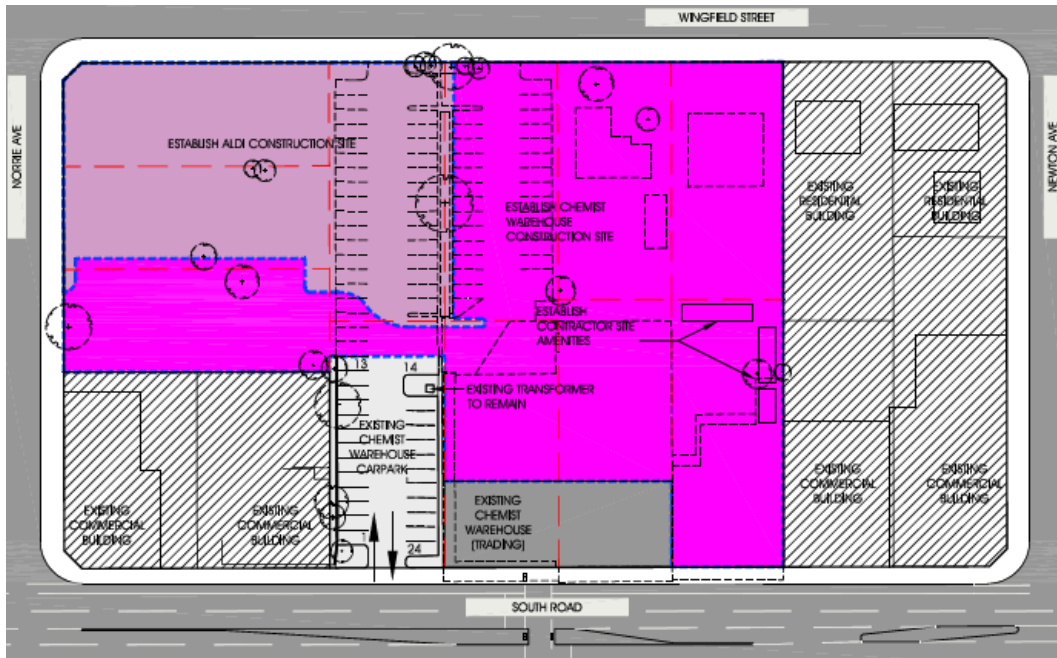
## 4.1.1 Staging and Operative Period

The development is intended to be staged as follows:

- Stage 1 – Demolition of residential dwellings, Rite Price Supermarket and northern portion of existing Chemist Warehouse. Reduced floor area of Chemist Warehouse to continue trading with adjacent (existing) 24 carparks accessible.

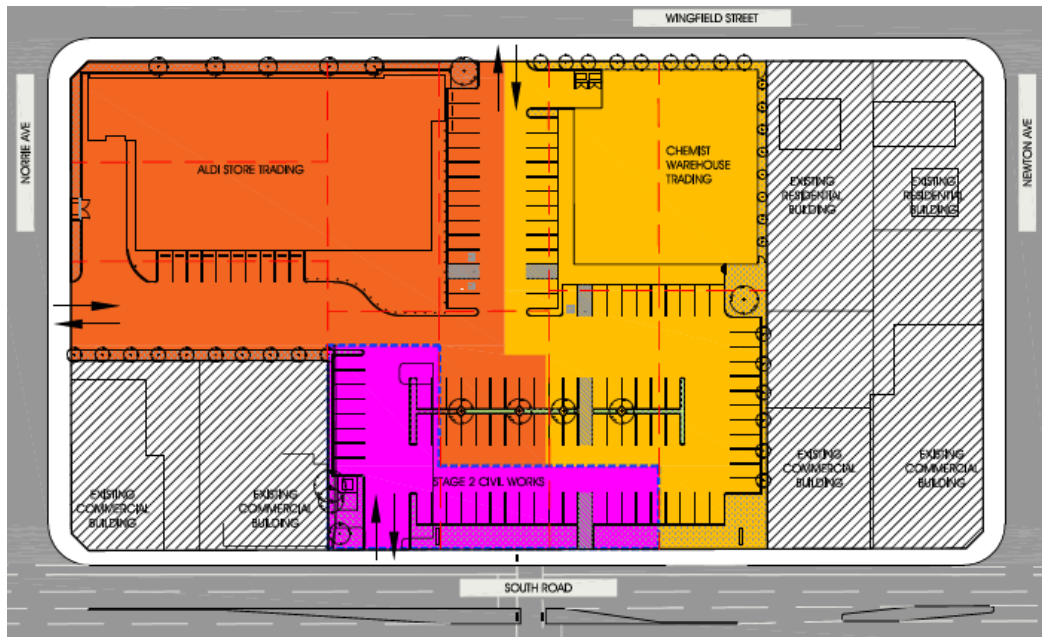


- Stage 2 – Construction of new Chemist Warehouse and ALDI buildings (all construction works contained behind still trading Chemist Warehouse and remaining 24 space carpark).

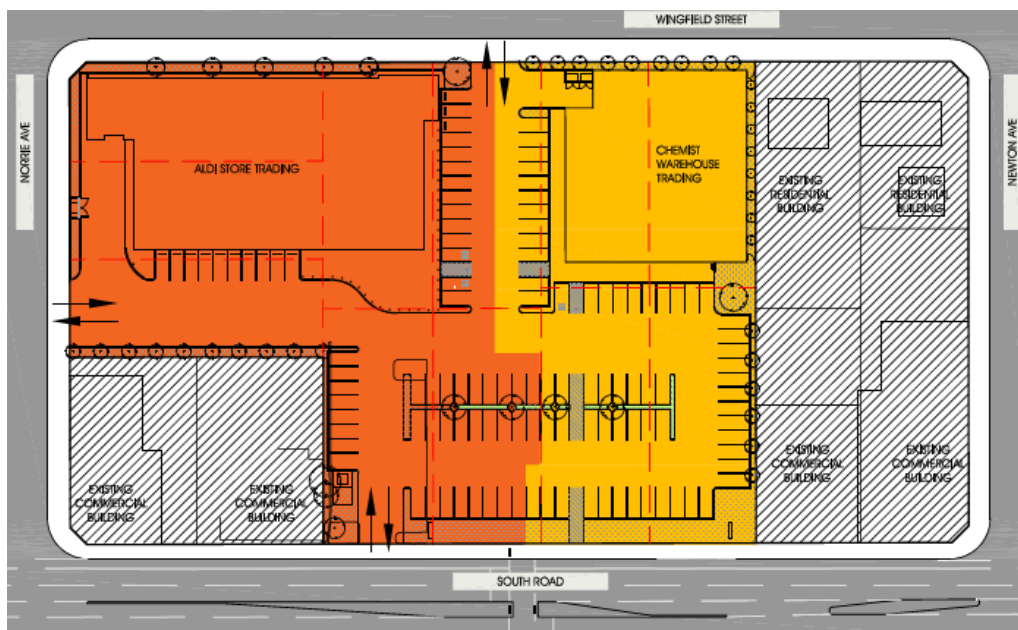




- Stage 4 – Remaining Chemist Warehouse and carpark demolished. New car park configuration under construction. New Chemist Warehouse store trading and ALDI available to trade.



- Stage 5 – Balance of carpark constructed, remaining civil works and landscaping completed.



For procedural convenience, we request that should Development Plan Consent be granted, it reflects the intended staging of the development, and in particular, enables operational continuity of Chemist Warehouse during construction.

#### 4.1.2 Operational Aspects

ALDI stores are in many ways similar to typical supermarkets in that they offer traditional grocery products in a familiar aisle-by-aisle format. However, there are a number of key differences that distinguish ALDI from other supermarket operations. These differences include:

- Predominately exclusive ALDI label branded products;
- 'Hard discount' food and grocery model;
- Simplified, consistent supply chain, building development, internal layout, merchandising, store operations and marketing;
- All delivery and logistics undertaken by ALDI employees, with only two 20 metre truck deliveries per 24 hours and one daily bakery delivery via a small rigid vehicle;
- Low swept exhaust delivery vehicles (ALDI trucks are to European standards with the exhaust discharge at wheel level on the Prime Mover, i.e. there is no exhaust discharge above the vehicle cabin);
- Regulated product range of approximately 1,300 items (compared with typical full-line supermarkets which offer between 20,000-30,000 items); and
- Considerably smaller retail floor plate of 1,732m<sup>2</sup> (compared with full-time supermarkets which are typically between 3,000m<sup>2</sup> to 4,000m<sup>2</sup>).

Of particular note is that ALDI stores, of which there are in excess of 500 across Australia in Victoria, NSW, ACT, QLD, WA and SA successfully operate in close proximity to other major supermarket chains (i.e. Coles and Woolworths), with direct competition in approximately 80% of the existing locations.

Unlike most supermarket operations, ALDI own, operate and control all of its supply and logistics via its purpose-built distribution centre located in Regency Park. This streamlined system of operation enables ALDI to position itself as a discount supermarket quite differently to its competitors. Accordingly, a dedicated loading bay will be situated on the southern side of the building which will provide for ALDI's dedicated delivery by 19.0 metre long semi-trailers.

A typical ALDI Store is expected to employ 25 full-time equivalent (FTE) employees with 6-10 employees present on site at any one time.

The hours of operation for the supermarket will be established and potentially varied in response to customer demands, however will be in accordance with any limitations set out by the Shop Trading Hours Act, 1977. The store is unlikely to operate with extended hours, with likely opening times no earlier than 7:00am and likely closing times no later than 9:00pm, other than in limited seasonal peaks such as Christmas and Easter.

## 4.2 Built Form and Site Works

The proposal seeks the staged construction of two single-storey buildings to be used as shops. A detailed description of each element of the proposed development is provided below.



#### 4.2.1 Proposed ALDI Store

The proposed ALDI Store will be setback approximately 3.1 metres from Wingfield Street and Norrie Avenue, and 52 metres from South Road. A 2.0 metre wide landscaping buffer will be provided to Wingfield Street and Norrie Avenue and a 4.5m wide landscaping buffer is proposed to South Road in addition to internal pockets of landscaping throughout the car park.

The proposed supermarket will have a maximum height of approximately 8.5 metres, measured from finished floor level, while the majority of the building will have a height of 6.2 metres above finished floor level. The highest portion of the building will be the 'tower' element which will be located on the north-west elevation of the building, near Wingfield Street. The tower element is a key design feature of ALDI Stores and is intended to create visual interest, clearly identifying the entrance of the store for customers and also enclose the roof top plant / equipment. As noted on the elevations, the tower will be a different colour from the balance of the Store and will feature a graphic image which will further assist to create visual interest.

The main entrance to the ALDI Store will be located on the eastern facade of the building while the main shop-front for the store will face north, towards the internal car park. A canopy will wrap around the front and a portion of the eastern side of the building to provide shelter for customers and create additional visual interest.

The eastern elevation (facing the main car parking area), will feature glass automatic doors at the entrance to the Store. High level windows will be installed on the eastern and southern elevations whilst large floor to canopy windows are proposed to the northern elevation.

The western elevation includes five (5) feature steel screens that project from the building to add depth and articulation to the western façade.

The loading dock and associated facilities will be located near the south-western corner of the building, near the intersection of Norrie Avenue and Wingfield Street.

The proposed store will be primarily constructed of the following materials and colours:

- Precast concrete panel walls to all sides of the building – painted 'Fluorescent Fire';
- Precast concrete panels for the tower element – painted Dulux 'Drive Time';
- Fibre cement fascia – painted Dulux 'Drive Time';
- Windows and shopfront – anodised aluminium frames, natural finish;
- Colorbond® capping – colour to match adjacent precast panel finish;
- Downpipes and gutters – Colorbond® 'Basalt Grey';
- Roof – Colorbond® sheeting – 'Surfmist'; and
- Sunscreen – Painted steel frame, Colorbond® 'Basalt Grey'.

Full details of the proposed external materials and finishes are contained on the elevation plans prepared by Nielsen Architects (**Appendix 2**).

Solar panels will be installed on the roof of the ALDI Store and will, generally, be located behind the parapet. Consequently, the majority of the solar panels won't be visible from surrounding land. The exception is along a portion of the western elevation where some views of the solar panels may be visible from Wingfield Street.

#### 4.2.2 *Proposed Chemist Warehouse*

As outlined previously, the proposed development also includes a new Chemist Warehouse building to be located in the north-western portion of the site near Wingfield Street and the adjoining northern allotment.

The setback to the northern and western boundaries has been amended and increased to 1.5 metres.

The building height is similar to the proposed ALDI store as the majority of the building will have a height of 6.2 metres above finished floor level with the exception of the raised parapet feature in the south-east corner of the building.

The parapet element complements the ALDI tower and creates visual interest while also clearly identifying the entrance of the store for customers. The roof top plant equipment is also discretely housed behind, screened on all sides.

The entrance to the Chemist Warehouse will be located on the eastern and southern facade of the building towards the internal car park. A canopy will wrap around a portion of the eastern and southern side of the building to provide shelter for customers and assist with identifying the building entry.

The southern and eastern elevation (facing the car parking area), will feature windows and glass automatic doors at the entrances to the building. The loading dock and associated facilities will be located on near the south-western corner of the building, near Wingfield Street.

The proposed shop will be primarily constructed of the following materials and colours:

- Precast concrete panel walls to the eastern and southern sides of the building as well as the tower element – painted 'Chemist Warehouse Yellow';
- Precast concrete panel walls to the northern and western sides of the building in alternating grey tones;
- Light weight canopy cladding – painted 'Chemist Warehouse Blue';
- Windows and shopfront – anodised aluminium frames, natural finish;
- Colorbond® capping – colour to match Colorbond® 'Woodland Grey';
- Roof – Zincolume.

The proposed Chemist Warehouse store hours of operation are as follows:

Monday - Wednesday, and Friday: 7:30am - 8:00pm

Thursday: 7:30am - 9:00pm

Saturday: 8:00am - 6:00pm

Sunday: 10:00am - 6:00pm

### 4.3 Transport, Parking and Access

GTA Consultants have undertaken a detailed traffic and parking assessment of the proposal, including the amended crossover location and car parking layout, to confirm that the proposed access/egress, vehicle manoeuvring and parking arrangements are feasible, safe and achieve the relevant Australian Standards (refer to **Appendix 3**). GTA's report sets out an assessment of the anticipated traffic implications of the proposed development, including:

- Existing traffic and parking conditions surrounding the site;
- Parking demand likely to be generated by the proposed development;
- Suitability of the proposed parking in terms of supply (quantum) and layout;
- Traffic generation characteristics of the proposed development;
- Proposed access arrangements for the site; and
- Traffic impact of the development proposal on the surrounding road network.

#### 4.3.1 Access/Egress

Vehicular access to the site will occur via three (3) access points as discussed within the GTA report and described below:

##### 1. South Road Access (amended)

The existing dual access point to South Road will be re-installed in its current location. This access point will continue to facilitate left turn ingress and left turn egress only for light vehicles.

##### 2. Norrie Avenue Access

A new access point has been proposed on Norrie Avenue adjacent the proposed ALDI Store. The access will be located approximately 40 metres west of the South Road/Norrie Avenue intersection and facilitate both light vehicles and heavy vehicles up to a 19.0 metre Semi Trailers. The access point will also facilitate un-restricted turning movements. Given the access proximity to South Road, a 19.0 metre Semi Trailer will be able to store prior to turning into the site without queuing back to South Road.

##### 3. Wingfield Street Access

The two existing access points on Wingfield Street will be consolidated into a single access with un-restricted turning movements. The access will facilitate both light vehicles and trucks up to a 10 metre delivery truck for the proposed Chemist Warehouse store.

#### 4.3.2 Heavy Vehicle Manoeuvring

The siting of the buildings means that heavy vehicle movements will be managed safely and appropriately on-site with semi-trailers entering and exiting the land in a forward direction from Norrie Avenue and Wingfield Street. In addition, heavy vehicle movements will be separated from the South Road crossover, being the primary access point of the car park, which also assists to reduce the potential for conflicts between large trucks and passenger vehicles.

More specifically, ALDI heavy vehicles will perform a right-hand turn from South Road to Norrie Avenue and enter the site in a forward direction from Norrie Avenue. The existing on-street parking will need to be removed on Norrie Avenue for an approximately length of 35 metres from the intersection with South Road (subject to negotiation with Council) to assist the manoeuvring of heavy vehicles. The ALDI trucks will then undertake a reversing manoeuvre into the loading dock near the rear of the site. Once the loading/unloading process has been completed, the trucks will exit the site in a forward direction back to Norrie Avenue. The Chemist Warehouse loading vehicles will access the loading area via Wingfield Street and similarly exit in a forward direction.

#### 4.3.3 *Parking*

A total of 120 car parking spaces are proposed for the site to be shared amongst the ALDI Store and the Chemist Warehouse. The parking layout has been designed in accordance with the relevant Australian Standards and will be provided with lighting throughout.

Three disabled parking spaces are provided, two adjacent the ALDI store entrance and one adjacent the Chemist Warehouse entrance to provide convenient access to people with a disability.

While a proposed future land division would create a technical separation of the two car parking areas, in reality, the 120 spaces will function as one integrated car park accessible for all who enter the site for whatever purpose. Appropriate reciprocal access rights would encumber the land to ensure the full balance of the parking provided remains accessible for the whole of the mixed-use development.

#### 4.3.4 *Deliveries*

Two separate loading areas for delivers are proposed, one for each of the respective shops.

The ALDI loading area is proposed to be located at the south-western corner of the site, adjacent Norrie Avenue, at the end of the ALDI building. The ALDI loading dock will be provided in accordance with ALDI's standard detail and seeks 24 hour delivery access with two main deliveries made per day by 19.0 metre long semi-trailers. A third delivery, via a small rigid vehicle, will deliver bakery products once a day. All delivery vehicles will enter and exit the site in a forward direction from Norrie Avenue as mentioned previously.

ALDI oversees and undertakes all its own deliveries and therefore has full control of the operations and logistics, which enables the business to minimise the impact of deliveries on customers and nearby residents. A copy of ALDI's loading procedures are attached (*Appendix 4*).

The Chemist Warehouse loading area is adjacent Wingfield Street, at the western end of the building. Deliveries to the Chemist Warehouse store will occur by trucks and vans up to 10.0 metres long during trading hours. A maximum of two deliveries will be made per day and delivery vehicles will exit Wingfield Street in a forward direction.

#### 4.3.5 Pedestrian and Cyclist Accessibility

The subject land is well served by pedestrian infrastructure with footpaths located on both sides of Wingfield Street, Norrie Avenue and South Road. The subject land is also well serviced by public transport with Bus Stop 20 on South Road providing services up to 15 minutes in frequency.

A signalised pedestrian crossing to the east of the subject site provides pedestrian connectivity across South Road. Designated bike lanes are present on both sides of South Road, adjacent the subject site.

The proposal includes the provision of six (6) bicycle spaces across three (3) bike rails. Two (2) bike rails are located near the ALDI Store entrance and the other near the Chemist Warehouse entrance.

#### 4.4 Landscaping

A variety of landscaping is proposed in association with the development. A landscape plan, prepared by Outerspace Landscape Architects, is included in **Appendix 5**. A total of 46 medium sized trees will be planted across the site, including Claret Ash, Crepe Myrtle, Capital Callery Pear and Chanticleer Callery Pear trees.

Two (2) Claret Ash trees are proposed within the car park. Seven (7) Crepe Myrtle trees will be planted adjacent the ALDI store near and 17 Capital Callery Pear trees will be planted near the Chemist Warehouse building. Four (4) Chanticleer Callery Pear trees will be planted within the car park, parallel to South Road. 16 Capital Callery Pear trees will be planted near the northern and eastern boundary of the proposed car park area.

The trees will be complemented by landscape beds located around the site which will be densely planted with a range of shrubs and groundcovers. In particular, landscaping strips will be established along the road frontages including the 4.5 metre wide landscaping strip adjacent South Road and the 2.0 metre wide landscaping buffer to Wingfield Street and Norrie Avenue to soften the appearance of the built form and the car park.

#### 4.5 Signage

The application proposes to erect two (2), double sided 8 metre high internally illuminated pylon signs adjacent South Road— one near South Road crossover and another near the north-eastern corner. The shared pylon signs will feature an ALDI advertisement 3.0 metres by 3.6 and a Chemist Warehouse advertisement 2.4 metres by 3.0 metres as well as minor directional signage.

The sign near the main vehicular entrance will also feature the ALDI logo as well as a triangular ‘opening hours sign’ below.

Three internally illuminated gable signs measuring 2.0 metres by 2.4 metres will be attached to the ALDI tower. In addition, an internally illuminated ‘lifestyle graphic’, measuring 7 metres by 3.3 metres, will be located on the northern elevation of the tower element to provide additional visual interest to the building as viewed internally from the car park area.

A digital display freestanding poster box measuring 2.2 metres in height and 0.95 metres in width will be located near the entrance to the ALDI Store.

Three (3) flat wall Chemist Warehouse signs will be placed on the eastern, southern and western façade as detailed on the Elevation Plans.

Specific details of each proposed sign are outlined in the Nielsen Architects drawings located in **Appendix 2**.

#### 4.6 Stormwater Management

A Stormwater Management Plan prepared by Drew Rudd Engineers (**Appendix 2**) confirms that stormwater collected on the subject site currently discharges to South Road, Norrie Avenue and Wingfield Street.

Drew Rudd Engineers have reviewed the proposed development, liaised with Council and DPTI and prepared a stormwater management strategy that will result in the following outcomes:

- *Reduces the post development 5 and 100-year ARI flows to the existing 5 and 100-year ARI flows from the site.*
- *Satisfies DPTI requirements to limit 100-year ARI post development flows to existing conditions flow rates discharging to South Road.*
- *Satisfies Council's requirements to limit 5-year ARI post development discharges to the kerb to less than 20 L/s.*
- *Reduces the risk of gross pollutants, sediment, nutrients and oil discharging from the site by using grated inlet pits throughout the site, installing Rocla First Defence sediment and oil removal devices and providing a vegetated basin to treat stormwater runoff from the Chemist Warehouse roof and car park areas.*

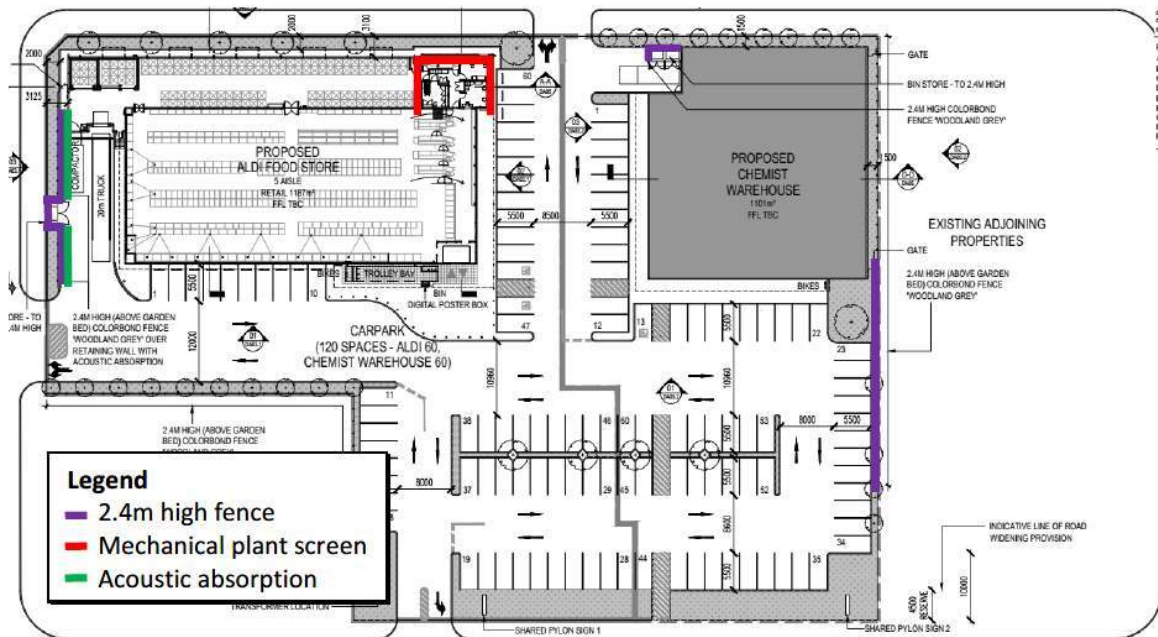
#### 4.7 Fencing

Following a detailed acoustic assessment of the proposed development, Sonus Engineers have provided an acoustic report that includes recommended fencing (**Appendix 7**).

Sonus have advised that 2.4m high "Colorbond" fencing as shown in **PURPLE** (see **Figure 4.1**) should be constructed such that they are sealed airtight at all junctions, including at the ground and at the building wall.



Figure 4.1 Recommended fencing treatment (C/-Sonus)



Consequently, a 2.4m high Colorbond® fence to is proposed near the southern boundary adjacent the ALDI Store loading dock, to the western boundary abutting Chemist Warehouse loading area as well as the northern boundary adjacent the car park.

#### 4.8 Waste collection

Waste collection will be managed within the respective loading and service bays for each building.

ALDI's recycling and rubbish will be stored within a compactor and bin enclosure located in the loading dock which will be screened from view. The compactor and bins will be emptied regularly by vehicles up to Medium Rigid Vehicle class, typically 10.5 metres or less in length. The collection of the ALDI bin is undertaken by a third-party contractor who will attend the Store on average three times per week to collect the contents of the bin.

ALDI's organic waste comprising fruit, vegetables and meat nearing the products best before date will be collected and re-distributed every day by ALDI's partner companies – Foodbank and SecondBite.

The Chemist Warehouse building does not generate perishable (organic) waste and therefore there will be no odour associated with the dedicated refuse bay.

Waste collection on-site will be restricted to between the hours of 9am and 7pm on a Sunday or public holiday, and 7am and 7pm on any other day.

## 5. Procedural Requirements

### 5.1 Relevant Authority

The relevant authority to determine the development application is the State Commission Assessment Panel (SCAP), with referral being made to the City of Marion. A request made under Schedule 10, Part 20 of the *Development Regulations*, 2008 was accepted by the State Coordinator-General by letter dated 9 July 2018.

### 5.2 Nature of Development

As outlined in Section 4.1, it is considered that the staged proposal includes demolition of the existing buildings on site and construction of a new supermarket (ALDI Store) and shop (Chemist Warehouse) with associated car parking, signage, fencing and landscaping.

The 'procedural matters' section of the Neighbourhood Centre Zone within the Development Plan list the following:

#### **Non-complying Development**

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of Development	Exceptions
Advertisement and/or advertising hoarding	<p>Except where the advertisement and/or advertising hoarding achieves any one of the following:</p> <ul style="list-style-type: none"> <li>(a) it is attached to a building or structure where the height of the advertisement does not exceed the height of the roof of the walls or parapet of the building or structure by more than 2 metres</li> <li>(b) it is freestanding and has a height not exceeding 8 metres</li> <li>(c) it is located on a side or rear wall facing and not within 50 metres of an abutting residential zone.</li> </ul>

The proposed signage achieves exceptions (a) and (b) listed above as the proposed façade signage does not project above the building roofline and the pylon signs do not exceed 8 metres in height. Therefore, as at least one of the three non-complying exceptions is satisfied, the proposed signage is a 'merit' form of development.

The remaining aspects of the proposed development are neither complying nor non-complying and, therefore the application must be assessed on its merits against the relevant provisions of the Development Plan.

### 5.3 Public Notification

The 'procedural matters' section of the Neighbourhood Centre Zone defers to the Schedule 9 of the Regulations in regard to categories of public notification.

Schedule 9, Part 2 (19) of the Regulations assigns any kind of development (excluding non-complying development) within the Neighbourhood Centre Zone, where the site is adjacent land in a different zone, as a Category 2 form of development. As the subject site is located within the Neighbourhood Centre Zone and

adjacent the Residential Zone and Commercial Zone (see **Figure 6.1**) the proposal is a subject to **Category 2** public notification.

#### 5.4 Agency Referrals

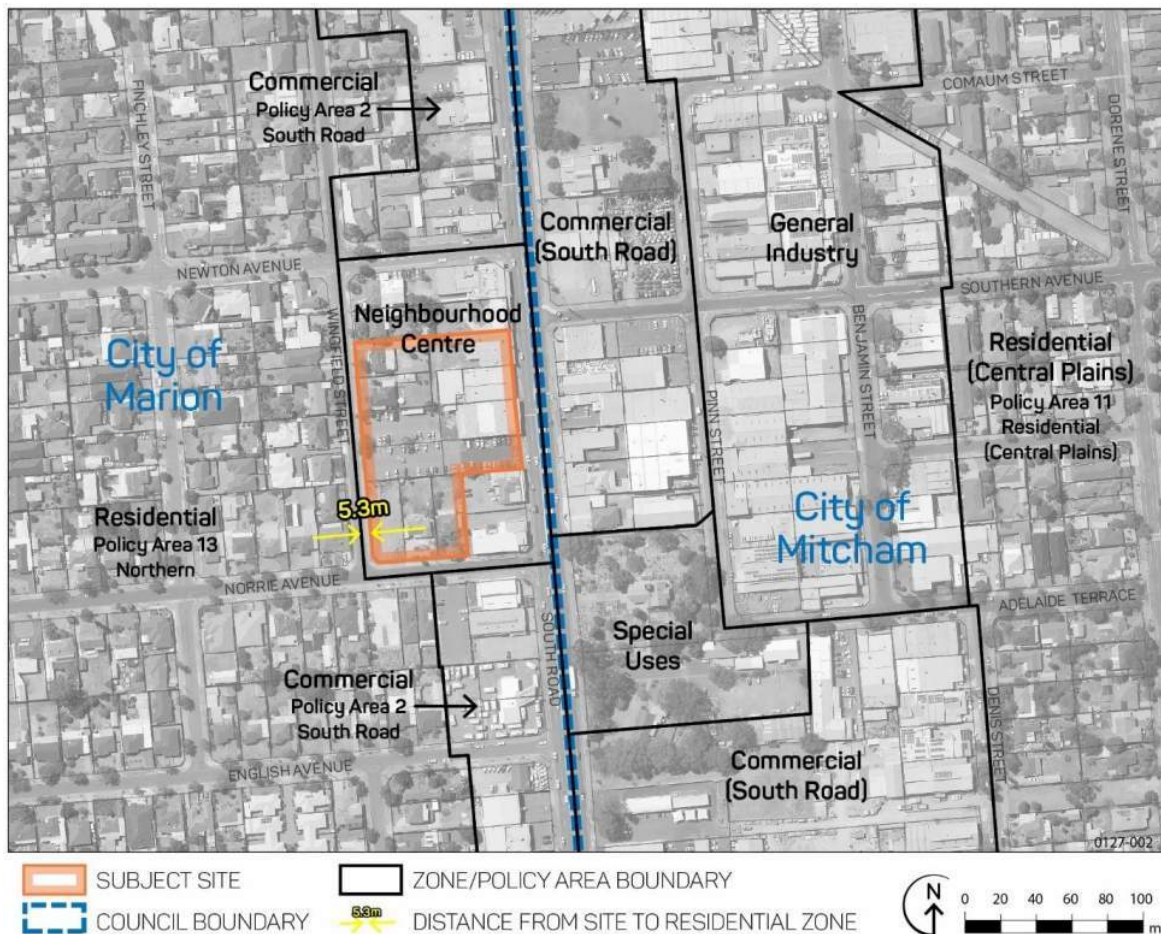
Given that the State Commission Assessment Panel (SCAP) is the relevant Planning Authority, it is understood that the proposal will be referred to the City of Marion for a period of 6 weeks. In addition, a 4 week referral period will be required to the Commissioner of Highways in accordance with clause 3 of the Table in Schedule 8 of the Regulations as the proposal will alter an existing access (remove), create a new access to an arterial road (South Road) and noting the 10 metre road widening requirement to South Road.

## 6. Development Plan Assessment

### 6.1 Overview

The following section provides an assessment of the proposal against the Marion Council Development Plan (consolidated 20 February 2018). The Zoning of the land and locality is identified in **Figure 6.1** below.

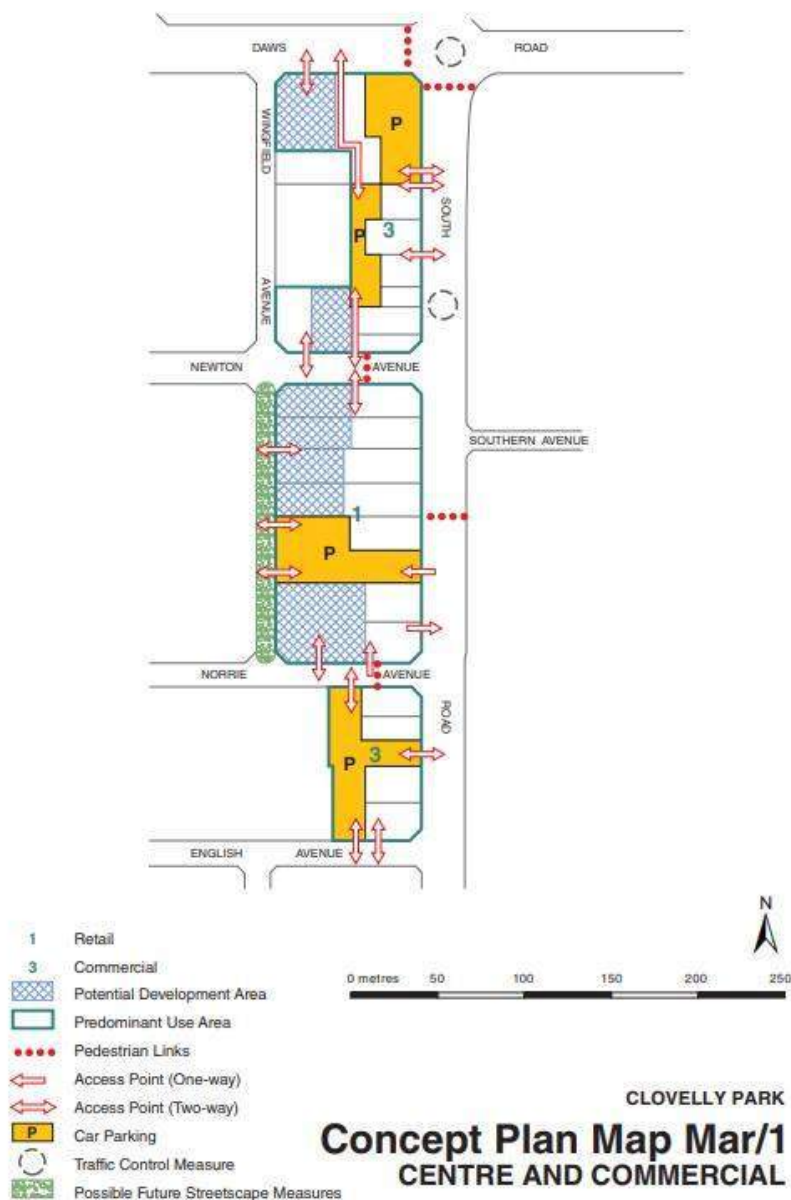
**Figure 6.1** Zoning



While the Neighbourhood Centre Zone does not have any Policy Areas or Precincts, the Zone does refer to Concept Plan Map Mar/1 – Clovelly Park Centre and Commercial. As can be seen on **Figure 6.2**, the subject land is situated within an area intended for retail development which extends from Norrie Avenue to Newton Avenue, bound by South Road and Wingfield Street.

Concept Plan Map Mar/1 also earmarks the corner of Norrie Avenue and Wingfield Street as a 'Potential Development Area' which is essentially where the proposed ALDI building will be located. The Concept Plan Map Mar/1 is discussed further in section 6.2.1 of this report.

**Figure 6.2** Concept Plan Map Mar/1 Centre and Commercial



For convenience, this assessment has been grouped under a series of headings which reflect the key relevant planning ‘themes’ from the Development Plan. The following provides an assessment of the proposal against relevant Development Plan Objectives (OBJ) and Principles of Development Control (PDC).

## 6.2 Land Use

The Neighbourhood Centre Zone aims to provide a *“a range of facilities to meet the shopping, community, business, and recreational needs of the surrounding neighbourhood”* (Zone OBJ 1). The Zone is intended as the *“main focus of business and community life outside a district centre, and provides for the more frequent and regularly recurring needs of a community”* (Zone OBJ 2).

Retail development including shops and supermarkets are both listed as envisaged uses in the Zone (Zone PDC 1).

Concept Plan Map Mar/1 - Centre and Commercial in the Marion Development Plan provides guidance on the preferred layout of the Centre and identifies the subject site location as one to be developed:

- For ‘retail’ land uses (Area 1);
- Consolidated on-site car parking with access from South Road, Wingfield Ave and Norrie Ave; and
- Development across the front (*‘Predominant Use Area’*) and to the rear of the site (*‘Potential Development Area’*).

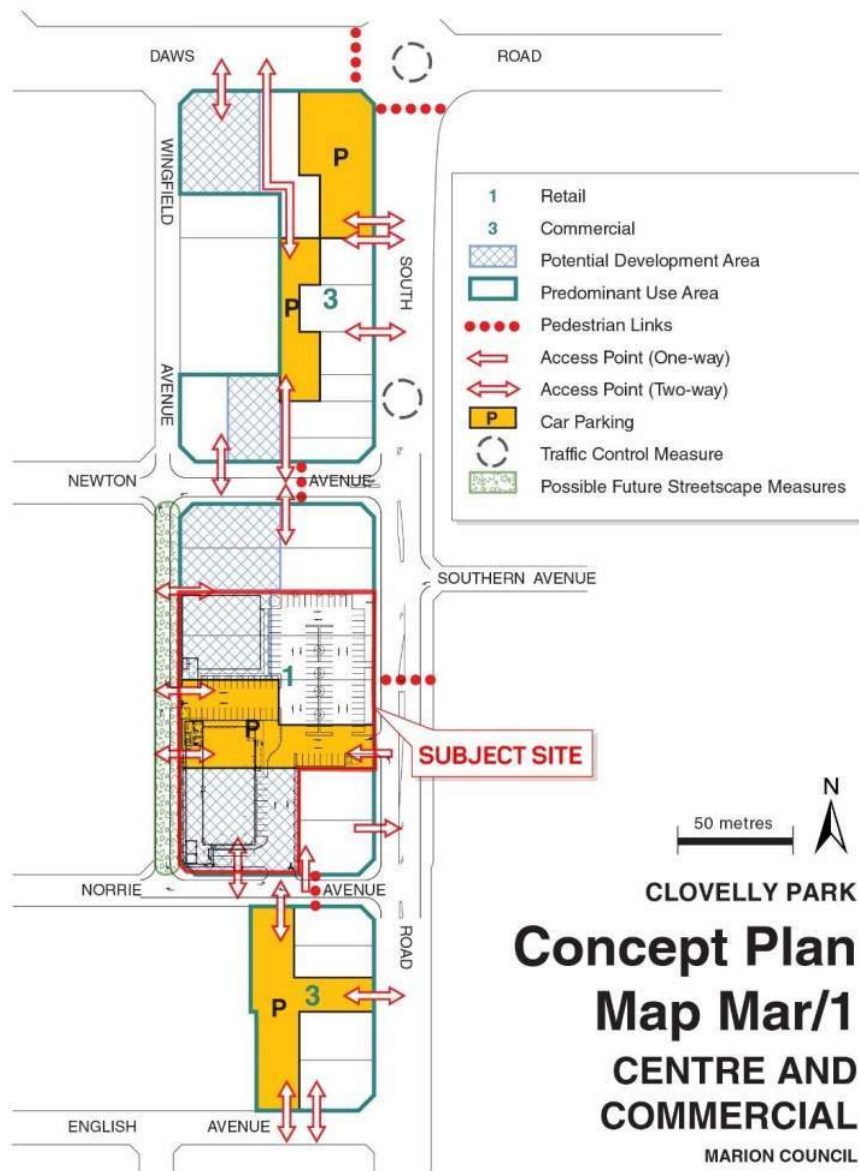
We have overlayed the proposed site area and layout on this Concept Plan over page which illustrates the close alignment of the application with the aims of the Concept Plan.

In particular, we note that the proposal:

1. The proposal is for a supermarket and a shop, both envisaged uses, which will contribute to meeting the local, everyday, convenience based shopping needs of surrounding residential community and passers-by;
2. Incorporates a modified dual access/egress point from South Road located to the south of the pedestrian crossing as indicated on the Concept Plan;
3. Incorporates a consolidated, shared parking arrangement, between South Road and Wingfield Street with access/egress to both Norrie Avenue and Wingfield Street as suggested in the Concept Plan; and
4. The proposed buildings are sited on land identified as ‘potential development area’ on the Concept Plan.



Figure 6.3 Neighbourhood Centre Concept Plan overlayed with proposal



The Zone policies referenced above and the proposal's clear alignment with these policies is reinforced by the relevant Centre's policy in the General Section of the Development Plan which encourages centres to offer convenience, vibrancy, safety through pedestrian movement connectivity and efficient layouts.

#### General Section – Centres and Retail Development

**OBJ 2** Centres that ensure rational, economic and convenient provision of goods and services and provide:



(a) a focus for community life

(b) safe, permeable, pleasant and accessible walking and cycling environments.

**PDC 1** Development within centres should:

(a) integrate facilities within the zone

(b) allow for the multiple use of facilities and the sharing of utility spaces

(c) allow for the staging of development within the centre

(d) be integrated with public and community transport...

We note that Zone PDC 5 suggests limitation to the extent of retail floor area that should be established in the various Neighbourhood Centres within Marion as follows (our emphasis):

**Zone PDC 5** The maximum gross leasable retail floor areas in the following centres should generally be in the order of the amounts shown in the table below:

<b>Location of Neighbourhood Centre</b>	<b>Area (square metres)</b>
Marion Road - Ascot Park	2000
Marion Road - Marion/Mitchell Park	2500
Marion Road - Park Holme	4500
Marion Road - South Plympton	1200
<b><u>South Road - Clovelly Park</u></b>	<b><u>2000</u></b>

The proposed ALDI Supermarket and Chemist Warehouse have a combined gross leasable floor area of 2,833m<sup>2</sup>.

The existing retail buildings which will be demolished to make way for the new development currently account for approximately 2,100m<sup>2</sup> of gross leasable floor area. Therefore, the proposal presents an increase of approximately 733m<sup>2</sup> additional retail floor area. In the context of a new, integrated site redevelopment, which replaces a compromised 'mini' supermarket with a larger, yet still modest sized supermarket, this additional floor area is considered nominal.

We also note that the Clovelly Park Neighbourhood Centre is approx. 14,500m<sup>2</sup> in size (being the block between South Road, Wingfield Street, Norrie and Newton Avenues). Therefore, the Development Plan's guide for retail floor area in this location suggests that only 13.8% of the entire Zone should be allocated to retail floor area.

This allocated percentage of retail area does not correspond with the intent of the Zone or the Concept Plan Map Mar/1 which suggests a small area of consolidated parking in the centre of the Zone with the land surrounding occupied by 'predominant' and 'potential' development. While some of this could be for other complimentary uses (consulting rooms, personal services and residential development), it remains questionable that the floor area guide and the Concept Plan for Clovelly Park are informed by any valid retail analysis or demand study (noting that Marion Council have not undertaken a review of their centres policy for many years).

Given the small amount of retail floor area which will remain in the Zone once the buildings on the subject site area demolished (equating to approx. 350m<sup>2</sup>) and the resulting moderate increase in retail floor area that will

result from the development, it is considered that the proposal will not compromise the intent of the Neighbourhood Centre Zone or the viability of any other Centres within the region.

The application is appropriately aligned with the relevant provisions of the Marion Development Plan as they relate to land use.

### 6.3 Built Form

The Neighbourhood Centre Zone offers limited guidance on the built form appearance expected and matters of site layout and land use arrangements have been addressed in the previous section.

The 'Centres and Retail Development' and 'Design and Appearance' General Sections of the Development Plan offer the following relevant built form policies:

#### ***General Section – Centres and Retail Development***

***PDC 4*** *A single architectural theme should be established within centres through:*

- (a) constructing additions or other buildings in a style complementary to the existing shopping complex*
- (b) renovating the existing shopping complex to complement new additions and other buildings within the centre*
- (c) employing a signage theme.*

#### ***General Section – Design and Appearance***

***PDC 1*** *Buildings should reflect the desired character of the locality while incorporating contemporary designs that have regard to the following:*

- (a) building height, mass and proportion*
- (b) external materials, patterns, colours and decorative elements*
- (c) roof form and pitch*
- (d) façade articulation and detailing*
- (e) verandas, eaves, parapets and window screens.*

***PDC 14*** *Buildings, landscaping, paving and signage should have a coordinated appearance that maintains and enhances the visual attractiveness of the locality.*

***PDC 15*** *Buildings should be designed and sited to avoid extensive areas of uninterrupted walling facing areas exposed to public view.*

The supermarket and shop buildings present a contemporary and complimentary architectural design which is relatively consistent with the character and scale of development in the locality. The appearance of the development in conjunction with the landscaping, will enhance the subject site, as illustrated below.

Figure 6.4 Existing Site as viewed from South Road, facing north-west



Figure 6.5 Proposed Development from South Road (artist impression)



The typically single storey form with low pitched roofs behind raised parapets and shop front glazing is retained however the new buildings present a unified, streamlined appearance compared to the patchy amalgam of built form elements currently on the site. Canopies are incorporated into the building design to define the building entrances albeit they are setback from the street edge.

The building scale is modest and consistent with commercially functioning buildings, with a height of 6.27m for the majority of the structures. A small area of raised parapet on the south-eastern corner of the Chemist building is proposed to define the entrance and conceal roof top plant equipment. The ALDI similarly has an extended 'tower' rising to 8.5m in its north-west corner opposite Wingfield Ave. In addition to providing variation and interest in the building form, this element also houses back of house functions and encloses the plant equipment servicing the supermarket. This integrated approach to plant screening achieves the following policy.

#### ***General Section – Design and Appearance***

**PDC 4** *Structures located on the roofs of buildings to house plant and equipment should be screened from view and should form an integral part of the building design in relation to external finishes, shaping and colours.*

A range of materials are proposed which, in conjunction with the architectural design, will present a vibrant and robust built form appearance. Extensive areas of uninterrupted walling have been avoided, for the most part by

using a variety of window openings, colours, feature concrete panels, signage and graphics within the building façades.

In relation to the front boundary setback, the need for a consolidated, accessible carpark together with the 10m wide road widening requirement has informed the site layout and built form setback as proposed.

We note the Development Plan calls for setbacks to reflect others in the locality while also acknowledging the impact siting has on function and appearance.

#### General Section – Centres and Retail Development

**PDC 21** *Except in areas where a new character is desired, the setback of buildings from public roads should:*

*(a) be similar to, or compatible with, setbacks of buildings on adjoining land and other buildings in the locality*

*(b) contribute positively to the function, appearance and/or desired character of the locality.*  
*Consolidated*

**PDC 24** *All setbacks from the road frontage should be additional to the road widening setback established under the Metropolitan Adelaide Road Widening Plan Act 1972.*

The traditional strip shop alignment of buildings constructed to the street edge is still prevalent in the immediate area, however newer developments are adopting a substantially greater front setback, both to accommodate future road widening and acquisition of land along South Road for the impending arterial road upgrade as well as the desire to present conveniently accessible car parking. The recently constructed commercial development immediately opposite the subject site on the corner of South Road and Southern Ave is an example of this, as illustrated in **Figure 6.6**.

**Figure 6.6** *Figure Setback variations – South Road looking south*



In this context, the proposed setback form South Road is considered acceptable.

Both the supermarket and shop have sited the loading and services areas to minimise public visibility while also ensuring convenient and safe vehicle manoeuvring. Both bays are also screened by fencing and landscaping. As such, the proposal achieves the following design provision.

## **General Section – Centres and Retail Development**

**PDC 20** Outdoor storage, loading and service areas should be:

- (a) screened from public view by a combination of built form, solid fencing and/or landscaping
- (b) conveniently located and designed to enable the manoeuvring of service and delivery vehicles
- (c) sited away from sensitive land uses.

## **6.4 Signage**

The Neighbourhood Centre and adjoining Commercial Zones currently incorporate a wide range of signage, an established character element of South Road for most of its length. The diversity of land uses and desire for visual exposure on this high frequency transport route has resulted in a streetscape comprising freestanding pylon signs, parapet advertisement, roof top signs, projecting verandah signs (see **Figure 6.7** and **Figure 6.8**).

**Figure 6.7** Existing Site Signage





Figure 6.8 Existing signage in the locality



The Development Plan calls for retail development to present a consistent signage theme which enhances the appearance of site and building/s and does not disfiguring streetscapes. Other key policies are as follows:

#### **General Section – Advertisement**

- PDC 1** *The location, siting, design, materials, size, and shape of advertisements and/or advertising hoardings should be:*
- (a) consistent with the predominant character of the urban or rural landscape*
  - (b) in harmony with any buildings or sites of historic significance or heritage value in the area*
  - (c) co-ordinated with and complement the architectural form and design of the building they are to be located on.*
- PDC 3** *Buildings occupied by a number of tenants should exhibit co-ordinated and complementary advertisements and/or advertising hoardings to identify the tenants and their type of business.*
- PDC 7** *Advertisements and/or advertising hoardings attached to buildings should not be sited on the roof or higher than the walls of a building, unless the advertisement or advertising hoarding is appropriately designed to form an integrated and complementary extension of the existing building.*
- PDC 18** *Freestanding advertisements and/or advertising hoardings should be:*
- (a) limited to only one primary advertisement per site or complex*
  - (b) of a scale and size in keeping with the desired character of the locality and compatible with the development on the site.*

Further, free standing signs in Neighbourhood Centre Zones should not exceed 8m in height with a 6m<sup>2</sup> display area on both sides of the sign (Advertisements PDC 20).



The application proposes to erect two, eight (8) metre pylon signs and a number of façade signage panels on the building as previously outlined.

In relation to the proposed signage we note that:

- The proposal, which incorporates two commercial developments over ten (10) separate allotments, proposes two pylon signs to a maximum height of 8m as per the advertising height guidelines;
- The pylon signs and modest building façade signs are consistent with the predominate character of the commercial locality;
- The façade signage is integrated into the building design and does not extend above the parapet lines;
- The pylon signs incorporate the branding of all proposed tenants on the site; and
- The face of the pylon signs exceeds that suggested (6m<sup>2</sup> recommended and 18m<sup>2</sup>) however in the context of the land size and the other signage in proximity to the site, this is considered acceptable.

Visually evident signage is a reality of commercial sites and critically important for businesses which rely heavily on passing vehicle trade and the ability of customers to clearly identify the business on approach and make a decision within ample time to enter the site.

In considering the proposed development and the policies related to advertising, the proposed site signage sufficiently achieves the intent of the Marion Development Plan.

## 6.5 Transport, Access and Parking

The Development Plan contains numerous provisions which seek to ensure that traffic can move efficiently and safely while also ensuring that an appropriate amount of car parking is provided to meet the demands generated by various developments.

As mentioned previously, GTA traffic consultants have undertaken a detailed traffic assessment of the proposed development (**Appendix 3**).

### 6.5.1 Car Parking

GTA note that the subject site is within 200 metres of a high frequency public bus service and therefore the site is within a 'Designated Area' under the Development Plan. Zone PDC 7 seeks on-site vehicle parking in accordance with Table Mar/2A – Off Street Vehicle Parking Requirements for Designated Areas, being a minimum rate of 3 spaces per 100m<sup>2</sup> of GLFA and maximum rate of 6 spaces per 100m<sup>2</sup> GLFA.

GTA provided the following car park assessment for the proposed development, based on the Development Plan rates:

**Table 6.1** Development Plan Car Parking Rates (GTA)

Use	Size (sq. m)	Parking Rate		Parking Requirement	
		Minimum	Maximum	Minimum	Maximum
ALDI Store	1,677	3 spaces per 100 sq. m	6 spaces per 100 sq. m	51	101
Chemist Warehouse	1,100	3 spaces per 100 sq. m	6 spaces per 100 sq. m	33	66
<b>Total</b>	<b>2,777</b>			<b>84</b>	<b>167</b>

Therefore, the proposed development generates a minimum and maximum development plan requirement of 84 and 167 spaces respectively.

The proposed development will provide a total of 120 car parks across the site, which equates to a parking rate of 4.3 spaces per 100m<sup>2</sup> which meets the Development Plan requirements.

GTA confirm that the proposed car parking layout has also been designed in accordance with the relevant Australian Standard (AS/NZS2890.1.2004), thereby achieving the intent of PDC 35, Transport and Access.

**PDC 35** Development should be consistent with Australian Standard AS: 2890 - Parking facilities.

Three disabled parking spaces are provided, two adjacent the ALDI store entrance and one adjacent the Chemist Warehouse entrance, to provide convenient access to people with a disability, in accordance with PDC 32, Transport and Access.

**PDC 32** Development should be sited and designed to provide convenient access for people with a disability.

The proposal includes the provision of six (6) bicycle spaces across three (3) bike rails. Two bike rails are located near the ALDI Store entrance and the other near the Chemist Warehouse entrance. Whilst the proposed bicycle parking spaces is below the recommended 13 spaces stated within Table Mar/5 of the Development Plan, GTA consider the Development Plan bicycle parking rate to be notably high. GTA conclude that the proposed six (6) bicycle spaces is adequate for this form of development. Notwithstanding, additional bike rails may be provided on a demand basis.

### 6.5.2 Access & Loading Arrangements

As mentioned previously in section 4.3.2 of this report, access to the site will be via South Road, Norrie Avenue and Wingfield Street, all of which are all-weather public roads consistent with PDC 22, Transport and Access.

**PDC 22** Development should have direct access from an all-weather public road.

The two existing crossovers to Wingfield Street will be consolidated to one (1) crossover and a new crossover is proposed to Norrie Avenue. The existing South Road crossover will be modified however the current location of the dual crossover will remain.

There is no increase in the number of access points proposed to South Road (arterial road) and the proposed South Road access will service both the ALDI Store and Chemist Warehouse, consistent with the intent of PDC 25, Transport and Access.

**PDC 25** *The number of vehicle access points onto arterial roads shown on Overlay Maps - Transport should be minimised and, where possible, access points should be:*

*(a) limited to local roads (including rear lane access)*

*(b) shared between developments.*

The South Road access is for customer vehicles only. Heavy vehicles will access the site from Norrie Avenue servicing the ALDI building and medium rigid vehicles via Wingfield Street servicing Chemist Warehouse. Vehicle access to South Road is restricted to left-in, left-on movements only and vehicles will exit the site to South Road (arterial road) in a forward direction, consistent with PDC 27, Transport and Access.

**PDC 27** *Development with access from arterial roads or roads as shown on Overlay Maps – Transport should be sited to avoid the need for vehicles to reverse onto or from the road.*

The loading facility for the ALDI Store will accommodate truck access up to a 19.0 metre Semi Trailer with access via South Road and Norrie Avenue. The existing on-street parking will need to be removed on Norrie Avenue for an approximately length of 35 metres from the intersection with South Road (subject to negotiation with Council) to assist the manoeuvring of heavy vehicles. The loading facility for the Chemist Warehouse will accommodate trucks up to a 10.0 metre rigid truck. All vehicles will enter and exit the site in a forward direction and the proposal achieves the intent of PDC 14, Transport and Access.

**PDC 14** *Development should provide for the on-site loading, unloading and turning of all traffic likely to be generated.*

In relation to the traffic impact and the capacity of the existing road network to cater for vehicles generated by the proposal, GTA noted the following:

*The additional traffic generated by the ALDI Development won't adversely impact on the surrounding road network. While there will be increases in the average delays and queue lengths, these will generally be marginal. The right turn lane into Norrie Avenue will experience a more significant increase in the average delay (29 seconds) with a 95th Percentile Queue of 22.8 metres (~3 vehicles). Notwithstanding, this will remain within the capacity of the lane.*

*The additional right turns from Norrie Avenue will experience a large delay with a slight increase in queue length. However, existing traffic turning right onto South Road already experience long delays. In practice, most vehicles will turn left and seek alternative routes such as Daws Road to link back to the south. The proposed access point on Norrie Avenue will generate minimal queues and delays, and as such won't adversely impact on South Road.*

*Norrie Avenue/Wingfield Street and Newton Avenue/Wingfield Street [intersection] will also experience an increase in traffic. Notwithstanding, given the existing volumes for these intersections are low, the intersections will continue to perform well post development.*

Based on GTA's assessment and conclusions, the proposed development satisfies the relevant provisions of the Development Plan relating to movement, transport and car parking. In particular, GTA has identified that sufficient on-site parking spaces will be provided to achieve the guidelines contained within the Development Plan.

## 6.6 Landscaping

Outerspace Landscape Architects has prepared a Landscape Plan (**Appendix 5**). This Landscape Plan proposes a range of plants which have been selected to give a strong identity to ALDI Stores within Metropolitan Adelaide, with each species serving a specific function.

As mentioned previously, the proposed landscaping is orientated towards the street frontages to soften the appearance of the buildings and car park. In particular, the proposed landscaping buffer to the three road frontages is primarily 2.0 metres wide to Norrie Avenue and Wingfield Street adjacent the ALDI Store and 1.5 metres to Wingfield Street adjacent Chemist Warehouse. The landscaping buffer to South Road is 4.5 metres wide, and the proposal is generally consistent with the quantitative guideline within PDC 3, Landscaping Fencing and Walls.

**PDC 3** *Landscaped areas along road frontages should have a width of not less than 2 metres and be protected from damage by vehicles and pedestrians.*

While some established vegetation will be removed to accommodate the proposed crossover, a total of 46 medium trees will be planted across the site in addition to screening shrubs and ground covers. The 15 proposed medium trees adjacent Wingfield Street will assist to soften the visual impact of the proposed built form when viewed from the adjacent Residential Zone. Feature planting is sited at all vehicle and pedestrian entries, providing an instant impact for shoppers entering the car park through contrasting colour and texture. The proposed landscaping will ultimately provide a coordinated mix of vegetation that enhances the appearance and amenity of the site and aligns with PDC 1 & 2 Landscaping, Fencing and Walls.

**PDC 1** *Development should incorporate open space and landscaping in order to:*

- (a) complement built form and reduce the visual impact of larger buildings (eg taller and broader plantings against taller and bulkier building components)*
- (b) enhance the appearance of road frontages*
- (c) screen service yards, loading areas and outdoor storage areas*
- (d) minimise maintenance and watering requirements*
- (e) enhance and define outdoor spaces, including car parking areas*
- (f) provide shade and shelter*

- (g) assist in climate control within buildings*
- (h) maintain privacy*
- (i) maximise stormwater re-use*
- (j) complement existing native vegetation*
- (k) contribute to the viability of ecosystems and species*
- (l) promote water and biodiversity conservation.*

**PDC 2** *Landscaping should:*

- (a) include the planting of locally indigenous species where appropriate*
- (b) be oriented towards the street frontage*
- (c) result in the appropriate clearance from powerlines and other infrastructure being maintained.*

In addition, the proposed landscaping within the car park will assist to reduce heat loads in summer by providing shade to hard stand areas, in accordance with PDC 40, Transport and Access

**PDC 40** *To assist with stormwater detention and reduce heat loads in summer, outdoor vehicle parking areas should include landscaping.*

Further, it is noted that the proposed landscaping includes clean trunk tree species that will maintain view-lines to entrances and exits as well as allowing clear views to areas where people may gather. The proposed landscaping is generally consistent with the intent of PDC 1, Crime Prevention.

**PDC 1** *Development should be designed to maximise surveillance of public spaces through the incorporation of clear lines of sight, appropriate lighting and the use of visible permeable barriers wherever practicable.*

For these reasons, the proposed development and the associated landscaping including 46 medium sized trees satisfies the relevant provisions of the Development Plan.

## 6.7 Stormwater

The Development Plan seeks to ensure that stormwater is managed appropriately to protect water quality, protect stormwater from pollution sources, reduce runoff and peak flows and prevent the risk of downstream flooding (OBJ 6, Natural resources).

In addition, the following provisions are considered most relevant to the assessment of stormwater management.

**Natural Resources**

**PDC 8** *Water discharged from a development site should:*

*(a) be of a physical, chemical and biological condition equivalent to or better than its pre-developed state*

*(b) not exceed the rate of discharge from the site as it existed in pre-development conditions.*

**PDC 9** *Development should include stormwater management systems to protect it from damage during a minimum of a 1-in-100 year average return interval flood.*

**PDC 10** *Development should have adequate provision to control any stormwater over-flow runoff from the site and should be sited and designed to improve the quality of stormwater and minimise pollutant transfer to receiving waters.*

**PDC 12** *Development should include stormwater management systems to minimise the discharge of sediment, suspended solids, organic matter, nutrients, bacteria, litter and other contaminants to the stormwater system.*

**PDC 14** *Stormwater management systems should:*

*(a) maximise the potential for stormwater harvesting and reuse, either on-site or as close as practicable to the source*

*(b) utilise, but not be limited to, one or more of the following harvesting methods:*

*(i) the collection of roof water in tanks*

*(ii) the discharge to open space, landscaping or garden areas, including strips adjacent to car parks*

*(iii) the incorporation of detention and retention facilities*

*(iv) aquifer recharge.*

**PDC 15** *Where it is not practicable to detain or dispose of stormwater on site, only clean stormwater runoff should enter the public stormwater drainage system*

As mentioned previously, Drew Rudd Engineers have provided a Stormwater Management Plan contained within **Appendix 8**. Drew Rudd Engineers liaised with Council and DPTI who requested the following:

- the 100-year Average Recurrence Interval (ARI) post development flow rate discharging to South Road does not exceed the existing conditions 100-year ARI flow rate.
- the design should include rain gardens in addition to gross pollutant, sediment and oil removal to help achieve EPA requirements.
- No stormwater discharge to the kerb is to exceed 20 L/s.

The proposed stormwater management plan includes the following key components:

- *A vegetated basin will be provided in the north west corner of the Chemist Warehouse car parking area to provide detention storage and stormwater quality improvement for stormwater runoff from the proposed car park and Chemist Warehouse building roof. Stormwater runoff from the vegetated basin will discharge to 35 m of DN750 mm pipe, which will provide detention storage prior to discharging to the kerb on Wingfield Street.*
- *Stormwater runoff from the Aldi building roof will discharge to an underground detention basin in the Aldi carpark. Stormwater runoff from the pathway on the western side of the Aldi building will discharge*



*to the landscaped area along the western boundary. This will promote infiltration of stormwater in the vegetated area.*

- *Rocla First Defense - High Capacity separators will be provided prior to discharging to the existing side entry pit on South Road and to the kerb on Wingfield Street, to treat the shared driveway and car parking areas. The First Defense - High Capacity separator has internal components designed to remove and retain gross debris, total suspended solids and hydrocarbons.*

In summary, Drew Rudd Engineers confirm that the proposed stormwater management strategy will:

- *Reduces the post development 5 and 100-year ARI flows to the existing 5 and 100-year ARI flows from the site.*
- *Satisfies DPTI requirements to limit 100-year ARI post development flows to existing conditions flow rates discharging to South Road.*
- *Satisfies Council's requirements to limit 5-year ARI post development discharges to the kerb to less than 20 L/s.*
- *Reduces the risk of gross pollutants, sediment, nutrients and oil discharging from the site by using grated inlet pits throughout the site, installing Rocla First Defense sediment and oil removal devices and providing a vegetated basin to treat stormwater runoff from the Chemist Warehouse roof and car park areas.*

Given the above, the proposed management of stormwater satisfies the relevant provisions of the Development Plan.

## 6.8 Interface between Land Uses

There are a number of relevant provisions in the Development Plan which seek to address interface considerations. OBJ 1 and OBJ 2, Interface Between Land Uses broadly seek to “*protect community health and amenity from adverse impacts of development*” (OBJ 2) and ensure that development is “*located and designed to minimise adverse impact and conflict between land uses*” (OBJ 1).

More specifically, PDC 6, Interface Between Land Uses seeks to minimise noise impacts between non-residential and residential land uses.

**PDC 6** *Non-residential development on land abutting a residential zone should be designed to minimise noise impacts to achieve adequate levels of compatibility between existing and proposed uses.*

The direction provided by PDC 6 is reinforced by the PDC 7, which references the desire for development that emits noise to include noise attenuation measures, with reference to the Environmental Protection (Noise) Policy.

**PDC 7** *Development that emits noise (other than music noise) should include noise attenuation measures that achieve the relevant Environment Protection (Noise) Policy criteria when assessed at the nearest existing noise sensitive premises.*

This Policy is based on the World Health Organisation Guidelines to prevent annoyance, sleep disturbance and unreasonable interference on the amenity of an area.

It is noted that the noise transfer from the subject site to adjacent residential properties would already occur given the existing site layout and uses which includes commercial development (shops and associated car park) similar to the proposed. Further, the site would also be currently serviced by delivery vehicles and waste collection trucks.

Notwithstanding, the potential transfer of noise resulting from the proposed development has been reviewed and will be minimised by:

- Facing the main entrance to the ALDI Store and Chemist Warehouse towards the customer car park rather than towards the residential area;
- Maintaining typical shopping hours for supermarkets and shops;
- Enclosing the plant equipment within the tower element of the ALDI Store and behind a screened roof parapet on the Chemist Warehouse building; and
- Implementing the noise attenuation measures recommended by the acoustic consultants.

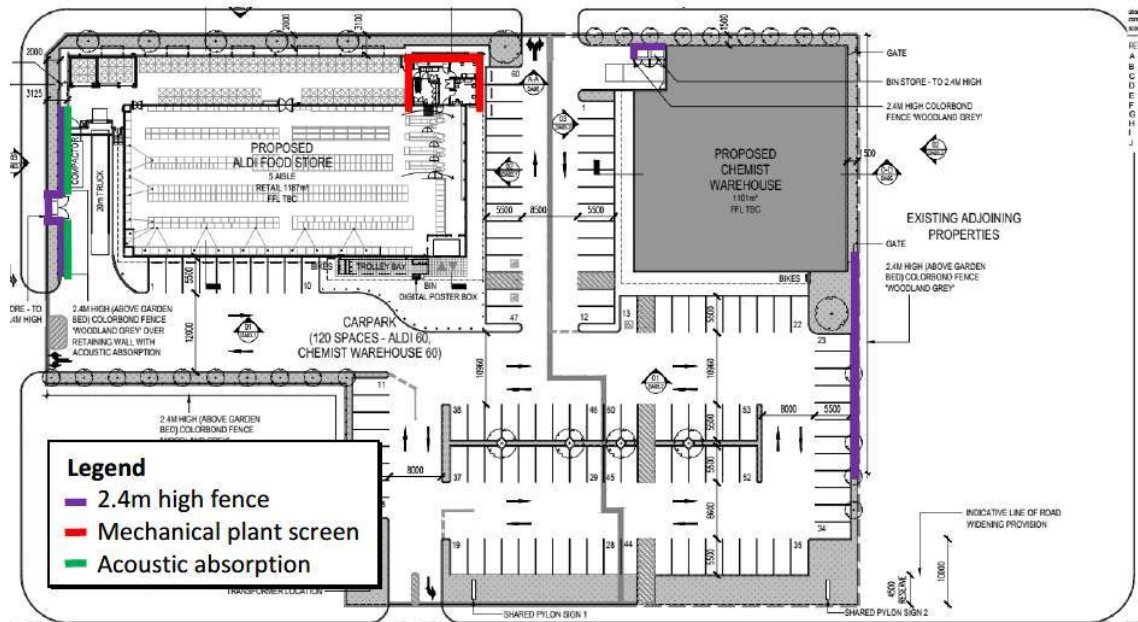
In order to confirm that the proposed development satisfies the requirements of the Development Plan, Sonus have prepared an Environmental Noise Assessment (**Appendix 7**). The acoustic assessment summarises the prediction of noise from the proposed ALDI Store and Chemist Warehouse, compares the predictions with the relevant criteria and provides indicative recommendations for acoustic treatment to ensure that the noise from the proposal does not detrimentally affect the amenity of the locality.

Sonus have reviewed the proposal against the Environment Protection (Noise) Policy 2007 and have considered potential noise generating activities on the site including noise from rubbish collection, car park activity and vehicle movements, the mechanical plant operation and delivery activities.

Sonus have advised that subject to the following acoustic treatments, the proposed development will satisfy the requirements of the Environment Protection (Noise) Policy 2007:

- Restrict the hours of rubbish collection from the site to the hours of Division 3 of the Environment Protection (Noise) Policy 2007. That is, only between the hours of 9am and 7pm on a Sunday or public holiday, and 7am and 7pm on any other day;
- Specific fence heights and constructions including installation of acoustic absorption:
  - » Construct 2.4m high “Colorbond” fences as shown in **PURPLE** (see **Figure 6.9**) such that they are sealed airtight at all junctions, including at the ground and at the building wall;
  - » Install 50mm thick acoustic insulation with a minimum density of 32 kg/m<sup>3</sup> to the ALDI loading area fence for the extent shown in **GREEN** (see **Figure 6.9**). The insulation should extend for the full practicable height of the fence and be should be installed as indicated within the Sonus report; and
  - » Screening of ALDI mechanical plant for the extent shown in **RED** (see **Figure 6.9**).

Figure 6.9 Site plan and recommended acoustic treatments (c/-Sonus image)



Following the above, Sonus conclude that “the facility has been designed to minimise adverse impact and conflict between land uses, avoid unreasonable interference on amenity, and will not detrimentally affect the locality by way of noise, thereby achieving the relevant provisions of the Development Plan related to environmental noise”.

In addition, the proposed development will also ensure that car park lighting achieves appropriate Australian Standards and minimises spill or glare towards residential areas. As mentioned previously, sufficient on-site car parking will be provided, in accordance with the Development Plan car parking rate, to limit car park spill in adjoining streets.

Accordingly, the proposed development therefore aligns with the intent of PDC 1, Interface Between Land Uses.

**PDC 1** Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:

- (a) the emission of effluent, odour, smoke, fumes, dust or other airborne pollutants
- (b) noise
- (c) vibration
- (d) electrical interference
- (e) light spill
- (f) glare
- (g) hours of operation
- (h) traffic impacts.

The proposed Chemist Warehouse building is setback 1.5 metres from the western boundary. 17 Capital Callery Pear trees and ground covers are proposed to the northern and western boundary of the Chemist Warehouse building to soften the built form as viewed from the adjacent properties.

A 2.0 metre wide landscaped buffer including seven (7) Crepe Myrtle trees interspersed with shrubs and ground covers is proposed to the west and south of the ALDI building, which will similarly soften the interface with the adjacent Residential Zone. In addition to the landscaping buffer, the ALDI Store is setback 3.1 metres from the western and southern boundaries, further reducing the scale of the building as viewed from adjacent residential dwellings separated by Wingfield Street and Norrie Avenue.

Consideration has been given to the impact of the proposed development on the adjoining detached dwelling to the north of the site at 15 Wingfield Street. The amended design includes a 1.5m setback between the northern boundary and the proposed Chemist Warehouse building, which was originally sited on the adjoining boundary. In addition, a row of eight (8) Capital Callery Pear trees are proposed within the northern landscaping bed, to soften the Chemist Warehouse built form as viewed from the adjoining residential property and the streetscape.

It is noted that the residential property to the north is located within the Neighbourhood Centre Zone and should the proposed development be approved and constructed, this dwelling will be the only residential land use remaining within the Neighbourhood Centre Zone bounded by Newton Avenue, Wingfield Street, Norrie Avenue and South Road.

Further, Zone PDC 4 states that “*dwellings should be located only behind or above non-residential uses on the same allotment*” indicating the preferred ground floor land use facing the street is non-residential development and consequently street fronting ground level dwellings are not encouraged (albeit we acknowledge this dwelling has been present for many years).

The visual impact of the wall as viewed from the adjoining northern allotment is softened by the intentional choice of paint colour: ‘Woodland Grey’ (rather than the standard Chemist Warehouse branding colours of yellow and blue). Further, a strip of ‘Shale Grey’, a lighter colour, is proposed to break up the bulk of the wall consistent with PDC 15, Design and Appearance.

**PDC 15** *Buildings should be designed and sited to avoid extensive areas of uninterrupted walling facing areas exposed to public view.*

Additionally, the proposed 6.2 metre high Chemist Warehouse boundary wall will have no overshadowing impact on the adjoining northern allotment and the side wall with no openings, will maintain privacy between the proposed building and adjoining allotment, consistent with the intent of PDC 3, Interface Between Land Uses:

**PDC 3** *Development adjacent to a Residential Zone should be designed to minimise overlooking and overshadowing of adjacent dwellings and private open space.*

In the context of the existing buildings on site, there are existing examples of commercial buildings abutting residential allotments such as the Rite Price building that abuts the rear (eastern) boundary of 19 Wingfield Street (see **Figure 6.10**). The proposed Chemist Warehouse building will have no overshadowing impact and arguably less of a visual impact to the adjoining residential allotment to the north (1.5 metre side boundary setback) than the existing Rite Price building abutting an eastern (rear) boundary of a residential allotment.

**Figure 6.10** Rite Price building abutting a residential allotment



Finally, the adjoining residential allotment is earmarked as a 'potential development area' on Concept Plan Map Mar/1 and therefore it is reasonable to assume that the site may be developed in the future to enjoy one of the many non-residential land uses envisaged within the Zone.

In assessing the amenity-related impacts of a development, it is worthwhile considering the contemplated land uses for the locality. For example, residential properties abutting commercial zones and activities and close to main roads, will typically be exposed to greater amenity-related impacts created by commercial built form, greater volumes of noise and the like when compared with residential properties situated within the heart of a residential zone.

Whilst the preservation of high levels of amenity is of importance, it is also necessary to consider existing and anticipated land use activities within the locality when determining what is an acceptable amenity level for a locality.

This view is consistent with the approach adopted by the Environmental Resources and Development Court, as considered in the matter of *Wilkins v City of Unley* [ERDC No. 524 of 2000]:

*If people choose to live at or near the boundary between a residential zone and a business zone, they must expect some noise, traffic, overshadowing and the like which would not be appropriate further into the residential zone. Likewise, the businesses must expect some residentially based activities which may annoy.*

In considering the potential interface issues which may arise from the proposal, it is our view that, subject to certain treatments, the development will satisfy the relevant provisions of the Development Plan.

## 7. Conclusion

This development application seeks to construct an ALDI Store and Chemist Warehouse within the Neighbourhood Centre Zone of the Marion Council Development Plan. Given the location of the subject site and ability to provide convenient access for customers and delivery trucks, the site is well suited for its intended use for retail development.

Following an inspection of the subject land and locality, a review of the proposed plans and associated documentation as well as a detailed assessment of the proposed development against the relevant provisions of the Marion Council Development Plan, we have formed the opinion that the proposed development represents appropriate and orderly development that deserves favourable consideration for approval. More specifically:

- The proposal is consistent with the land uses envisaged within the Neighbourhood Zone;
- New investment will lift the amenity of the area and improve what is currently a relatively dilapidated site;
- The supply of car parking spaces will satisfy the anticipated demand generated by the proposed development and will exceed the minimum standards set out in the Development Plan;
- The ALDI Store will provide a range of groceries and other shopping products which will serve the needs of the local and broader community while also providing competition to other supermarkets which will help to drive down prices;
- The building features a sleek design which, combined with the proposed materials and finishes, will provide an enhanced and contemporary retail development across the co-ordinated site and improve the streetscape amenity to South Road, Norrie Avenue and Wingfield Street;
- The building setback to South Road will accommodate future road widening, the irregular shape of the allotment and the desire for car parking to be easily visible to passing traffic along South Road;



- The proposed landscaping will improve the amenity of the locality and soften views of the building from surrounding streets and adjacent properties;
- Projected traffic generation and distribution will not unreasonably impact on the function and/or capacity of the adjacent road networks;
- Vehicle access, egress and circulation movements will be appropriately managed through the provision of dedicated loading docks for delivery trucks; and
- The proposed advertising signage will clearly and concisely identify the intended use, while also achieving consistency with the architectural style of the building and complementing the character of existing signage along South Road and within the broader locality.

The proposed development is aligned with the most relevant provisions of the Development Plan and warrants Development Plan Consent, subject to reasonable and relevant conditions.



# Proposed ALDI Store South Road, Clovelly Park Transport Impact Assessment


**Client //** ALDI Australia  
**Office //** SA  
**Reference //** S117000  
**Date //** 29/08/2018

Proposed ALDI Store  
South Road, Clovelly Park  
Transport Impact Assessment

Issue: A 29/08/2018

Client: ALDI Australia  
Reference: S117000  
GTA Consultants Office: SA

Quality Record

Issue	Date	Description	Prepared By	Checked By	Approved By	Signed
A	29/08/18	Final	Richard Frimpong	Ian Bishop	Paul Morris	

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# 1. Introduction

## 1.1 Background

A development comprising an ALDI Store and Chemist Warehouse is proposed on South Road in Clovelly Park, and located on a site between Norrie Avenue, South Road and Wingfield Street. GTA Consultants (GTA) has been commissioned to undertake a transport impact assessment of the proposed development.

## 1.2 Purpose of this Report

This report sets out an assessment of the anticipated transport implications of the proposed development, including consideration of the following:

- i existing traffic and parking conditions surrounding the site
- ii parking demand likely to be generated by the proposed development
- iii suitability of the proposed parking in terms of supply (quantum) and layout
- iv traffic generation characteristics of the proposed development
- v proposed access arrangements for the site
- vi transport impact of the development proposal on the surrounding road network.

## 1.3 References

In preparing this report, reference has been made to the following:

- o City of Marion Development Plan (consolidated 20 February 2018)
- o Australian Standard/ New Zealand Standard, Parking Facilities, Part 1: Off-Street Car Parking AS/NZS 2890.1:2004
- o Australian Standard, Parking Facilities, Part 2: Off-Street Commercial Vehicle Facilities AS 2890.2:2002
- o Australian Standard / New Zealand Standard, Parking Facilities, Part 6: Off-Street Parking for People with Disabilities AS/NZS 2890.6:2009
- o plans for the proposed development prepared by Nielsen Architects
- o traffic and car parking surveys undertaken by GTA Consultants as referenced in the context of this report
- o various technical data as referenced in this report
- o an inspection of the site and its surrounds
- o other documents as nominated.



## 2. Existing Conditions

### 2.1 Subject Site

The subject site is located in Clovelly Park on South Road, to the north of Norrie Avenue, and extends to Wingfield Street. The properties on the corner of South Road and Norrie Avenue are not included as part of this proposal. The site of approximately 8,400 sq. m has frontages of approximately 78 metres to South Road, 50 metres to Norrie Avenue and 101 metres to Wingfield Street.

The site is located within a Neighbourhood Centre zone and is currently occupied by a mix of residential and commercial uses including Chemist Warehouse and Rite Price. The surrounding properties include residential and commercial land uses.

The location of the subject site and the surrounding environs is shown in Figure 2.1.

**Figure 2.1: Subject Site and its Environs**



(PhotoMap courtesy of NearMap Pty Ltd)

#### 2.1.1 Adjoining Roads

##### South Road

South Road is an arterial road managed and maintained by the Department for Planning, Transport and Infrastructure (DPTI). The road is aligned in an approximate north to south direction and configured with dual carriageways with two (2) lanes in each direction and a central raised median. The carriageways are approximately 7.9 metres wide and set within a 24.2 metre wide road reserve (adjacent the site).

South Road carries approximately 44,700 vehicles per day<sup>1</sup> and has a posted speed limit of 60km/h.

#### Norrie Avenue

Norrie Avenue is a local road managed and maintained by the City of Marion. The road is aligned in an approximate east to west direction, with a two-way carriageway width of approximately 8.0 metres wide, set within a 15.4 metre wide road reserve.

Parking controls are located on the southern side of Norrie Avenue to restrict parking to 1 hour between 8am and 5pm (on any day).

Norrie Avenue carries approximately 840 vehicles per day<sup>2</sup> and is subject to the default urban speed limit of 50km/h.

#### Wingfield Street

Wingfield Street is a local road managed and maintained by the City of Marion. The road is aligned in an approximate north to south direction, with a two-way carriageway width of approximately 7.0 metres wide, set within a 12.4 metre wide road reserve.

Parking controls are located on the eastern side of Wingfield Street with No Stopping 8am to 5.30pm Monday to Friday and 8am to Noon Saturdays. One Hour parking applies on the western side 8.30am to 4.30pm Monday to Friday.

Wingfield Street carries approximately 1070 vehicles per day<sup>2</sup> and is subject to the default urban speed limit of 50km/h.

#### Newton Avenue

Newton Avenue is a local road managed and maintained by the City of Marion. The road is aligned in an approximate east to west direction and configured a single carriageway with one lane in each direction. The carriageway is approximately 11.1 metres wide and set within an 18.3 metre wide road reserve. Time limit parking control are applied on various sections of Newton Avenue.

Newton Avenue carries approximately 740 vehicles per day<sup>2</sup> and is subject to the default urban speed limit of 50km/h.

### 2.1.2 Surrounding Intersections

The following intersections currently exist in the vicinity of the site:

- Norrie Avenue/South Road (unsignalised)
- Norrie Avenue/Wingfield Street (unsignalised)
- Newton Avenue/Wingfield Street (unsignalised)
- Newton Avenue/South Road (unsignalised)
- Southern Avenue/South Road (unsignalised).

### 2.1.3 Traffic Volumes

GTA Consultants undertook traffic movement counts at the following intersections:

- Norrie Avenue/South Road
- Norrie Avenue/Wingfield Street
- Wingfield Street/ Southern Existing Access

<sup>1</sup> Based on data collected by the Department of Planning, Transport and Infrastructure (DPTI) available on LocationSA Map Viewer.

<sup>2</sup> Based on the peak hour traffic counts undertaken by GTA on 5 July 2018 and assuming a peak-to-daily ratio of 10%.

- Wingfield Street/ Northern Existing Access
- Newton Avenue/Wingfield Street
- Newton Avenue/South Road
- South Road/Existing Access

The traffic movement counts were undertaken during the following periods:

- Saturday 30 June 2018, 10:00am – 1:00pm
- Thursday 5 July 2018, 4:30pm – 6:30pm

The weekday PM peak hour and the Saturday peak hour traffic volumes are shown in Figure 2.2 and Figure 2.3 respectively.

**Figure 2.2: Existing Weekday PM Peak Hour Traffic Volumes (4:45pm – 5:45pm)**

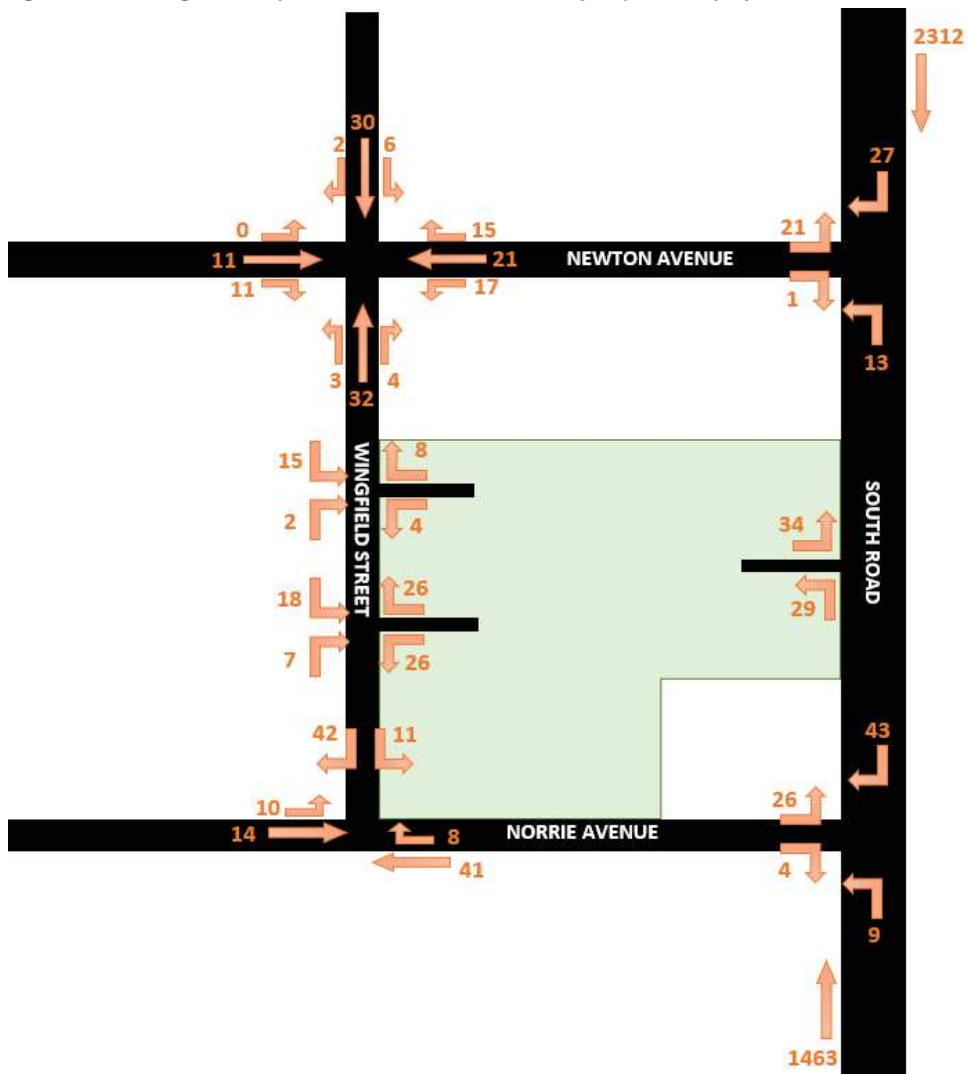
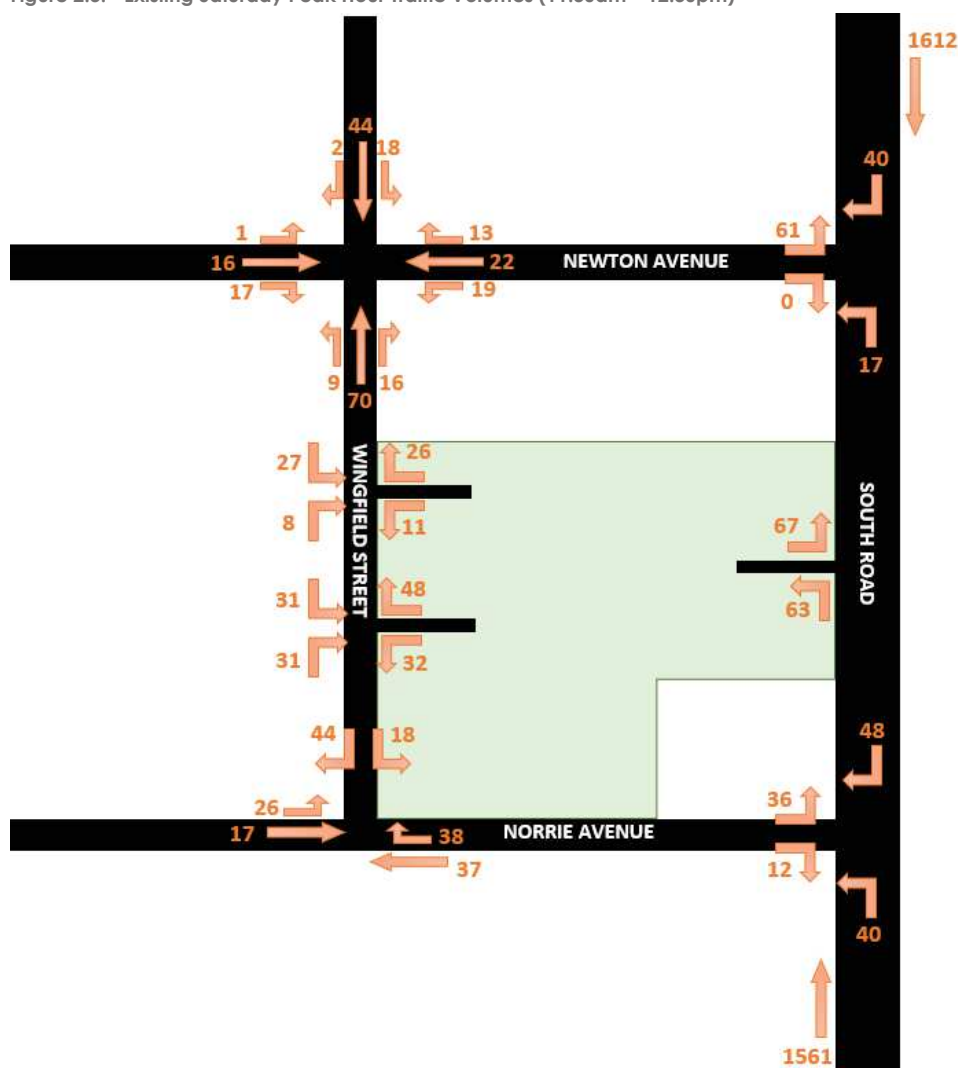


Figure 2.3: Existing Saturday Peak Hour Traffic Volumes (11:30am – 12:30pm)



#### 2.1.4 Intersection Operation

The operation of the following critical intersections below has been assessed using *SIDRA INTERSECTION*<sup>3</sup>, a computer based modelling package which calculates intersection performance.

- South Road/Newton Avenue – Table 2.1
- South Road/Chemist Warehouse Access – Table 2.2
- South Road/Norrie Avenue – Table 2.3

The commonly used measure of intersection performance is referred to as the *Degree of Saturation (DOS)*. The DOS represents the flow-to-capacity ratio for the most critical movement on each leg of the intersection. For signalised intersections, a DOS of around 0.95 has been

<sup>3</sup> Program used under license from Akcelik & Associates Pty Ltd.

typically considered the 'ideal' limit, beyond which queues and delays increase disproportionately<sup>4</sup>.

The results have been discussed below:

#### South Road/Newton Avenue

The intersection of South Road and Newton Road has been assessed during the Weekday PM Peak and Saturday Peak as per Table 2.1.

**Table 2.1: South Road/Newton Avenue – Existing Conditions**

Approach	Movement	Weekday PM Peak			Saturday Peak Hour		
		Level of Service (LOS)	Average Delay (s)	95 <sup>th</sup> Percentile Queue (m)	Level of Service (LOS)	Average Delay (s)	95 <sup>th</sup> Percentile Queue (m)
South Rd (South App)	Left	A	5.6	0.0	A	5.6	0.0
	Through	A	0.1	0.0	A	0.1	0.0
South Rd (North App)	Through	A	0.1	0.0	A	0.1	0.0
	Right	C	24.8	2.8	D	31.7	5.2
Newton Ave (West App)	Left	B	10.7	1.0	B	11.6	3.2
	Right	F	4032.5	14.6	F	4364.3	15.7

Based on the above, the existing intersection generally operates suitably during the Weekday PM Peak and Saturday Peak, with the exception of vehicles turning right out of Newton Avenue. Notwithstanding, while long delays are common for vehicles turning right from a minor road onto a major road, only one vehicle carried out this manoeuvre over an hour period for both the Weekday PM Peak and Saturday Peak.

#### South Road/Chemist Warehouse Access

The intersection of South Road and the Chemist Warehouse Access has been assessed during the Weekday PM Peak and Saturday Peak as per Table 2.2.

**Table 2.2: South Road/Chemist Warehouse Intersection – Existing Conditions**

Approach	Movement	Weekday PM Peak			Saturday Peak Hour		
		Level of Service (LOS)	Average Delay (s)	95 <sup>th</sup> Percentile Queue (m)	Level of Service (LOS)	Average Delay (s)	95 <sup>th</sup> Percentile Queue (m)
South Rd (South App)	Left	A	5.6	0.0	A	5.6	0.0
	Through	A	0.1	0.0	A	0.1	0.0
South Rd (North App)	Through	A	0.1	0.0	A	0.1	0.0
Site Access (West App)	Left	B	10.7	1.6	B	11.3	3.4

Based on the above, the existing intersection generally operates suitably during the Weekday PM Peak and Saturday Peak, generally with a Level of Service (LOS) of A, low queuing and average delays.

<sup>4</sup> SIDRA INTERSECTION adopts the following criteria for Level of Service assessment:

		Intersection Degree of Saturation (X)	
		Unsignalised Intersection	Signalised Intersection
A	Excellent	<=0.50	<=0.60
B	Very Good	0.50-0.70	0.60-0.75
C	Good	0.70-0.80	0.75-0.90
D	Acceptable	0.80-0.90	0.90-0.95
E	Poor	0.90-1.00	0.95-1.00
F	Very Poor	>=1.0	>=1.0

## South Road/Norrie Avenue

The intersection of South Road and Norrie Avenue has been assessed during the Weekday PM Peak and Saturday Peak as per Table 2.3.

**Table 2.3: South Road/Norrie Avenue Intersection – Existing Conditions**

Approach	Movement	Weekday PM Peak			Saturday Peak Hour		
		Level of Service (LOS)	Average Delay (s)	95 <sup>th</sup> Percentile Queue (m)	Level of Service (LOS)	Average Delay (s)	95 <sup>th</sup> Percentile Queue (m)
South Rd (South App)	Left	A	5.6	0.0	A	5.6	0.0
	Through	A	0.1	0.0	A	0.1	0.0
South Rd (North App)	Through	A	0.1	0.0	A	0.1	0.0
	Right	C	24.0	4.5	D	29.5	4.8
Norrie Ave (West App)	Left	B	10.6	1.2	B	11.1	1.8
	Right	F	1214.9	15.8	F	528.2	19.8

Based on the above, the existing intersection operates suitably during both the Weekday PM Peak and the Saturday Peak. Similar to the South Road/Newton Avenue Intersection, the right turn out from Newton Avenue operates with a larger average delay and 95<sup>th</sup> percentile queue. Notwithstanding, this is not uncommon for vehicles turning right from a minor road onto a major road. It is further emphasised the number of right turn movements during both periods were low at 4 movements and 12 movements respectively.

## 2.1.5 Accident Statistics

A review of the reported accident casualty history for the roads and intersections adjoining the subject site has been sourced from the DPTI between 2012 and 2016. A summary of the accidents for the last available five-year period is presented in Table 2.4.

**Table 2.4: Crash Statistic Summary**

Location		No. of Crashes	Type of Crash	Total Casualties
South Road	Midblock (between Newton Avenue and Southern Avenue)	3	1 x Rear End 1 x Side Swipe 1 x Right Angle	0
	Midblock (between Southern Avenue and Norrie Avenue)	7	6 x Rear End 1 x Side Swipe	2
	T-junction (Norrie Avenue and South Road)	8	3 x Rear End 1 x Hit Fixed Object 3 x Right Angle 1 x Other	2
	T-junction (Newton Avenue and South Road)	4	4 x Right Turn	3
	T-junction (Southern Avenue and South Road)	1	1 x Right Angle	0
Newton Avenue	Newton Avenue and Wingfield Street Intersection	2	2 x Right Angle	0
Norrie Avenue	Midblock (Between Wingfield Street and South Road)	1	1 x Right Angle	0



Based on the above, the most common type of crashes at the intersections were right angle, while on the mid-block sections rear end collisions were the most common, which is typical of other intersections and midblock sections.

## 2.2 Car Parking

### 2.2.1 Supply

GTA Consultants recorded a total of 81 car parking spaces within the existing site for the Chemist Warehouse car park.

### 2.2.2 Demand

Parking demand surveys were undertaken within Chemist Warehouse/Rite Price car park during the following periods:

- Saturday, 30 June 2018 (between 10:00 and 13:00)
- Thursday, 5 July 2018 (between 16:30 and 18:30)

The key results are summarised in Table 2.5.

**Table 2.5: Existing Parking Demand**

Location	Supply	Date	Average Parking Demand		Peak Parking Demand	
			Volume	Occupancy (%)	Volume	Occupancy (%)
Chemist Warehouse Car Park	81	Saturday (30 June 18)	49	61%	59 11:30	73%
		Thursday (5 July 18)	38	47%	54 17:00	67%

The existing Chemist Warehouse and Rite Price contains a combined floor area of 1,236 sq. m, which equates to a peak parking rate of 4.8 spaces per 100 sq. m during the Saturday Peak and 4.4 spaces per 100 sq. m respectively.

## 2.3 Sustainable Transport Infrastructure

### 2.3.1 Public Transport

The site is located within close proximity to Bus Stop 20 on South Road, which provides services at up to 15 minutes in frequency. Table 2.6 considers the list of services available from South Road.

**Table 2.6: Road Based Public Transport Provision**

Route No.	Road Description	Nearest Stop
719	Flinders University to City	130 m
720/720H	Old Reynella Interchange to City	130 m
721/721F/1721	Noarlunga Interchange to City	130 m
722/1722	Noarlunga Interchange to City	130 m
723F	Colonnades Centre Interchange to City	130 m

### 2.3.2 Pedestrian Infrastructure

Footpaths are located on both sides of South Road, Norrie Avenue and Wingfield Street. There is also a pedestrian activated crossing (PAC) located on South Road, which enables a safe crossing location to/from the eastern side of South Road.

### 2.3.3 Cycle Infrastructure

There are bicycle lanes on both sides of South Road, which operate exclusively between 7:30am – 9:00am and 4:30pm – 6:00pm, Monday to Friday. There is no cycling infrastructure on Norrie Avenue and Wingfield Street.

### 3. Development Proposal

3

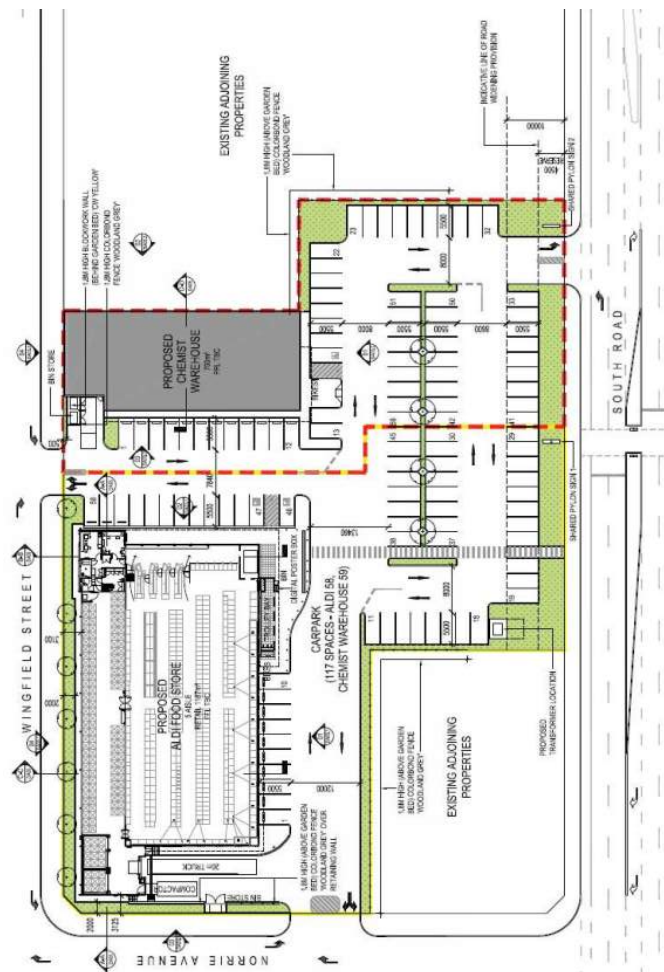
#### 3.1 Land Uses

The proposed development includes;

- ALDI Supermarket of approximately 1,677 sq. m gross leasable floor area (GLFA)
- Chemist Warehouse of 700 sq. m GLFA
- Car parking for 117 vehicles within the site.
- Access points located on South Road, Norrie Avenue and Wingfield Street.
- Loading facilities for the ALDI Store and Chemist Warehouse.

Figure 3.1 presents the proposed site layout.

Figure 3.1: Proposed Layout Plan



## 4. Car Parking

### 4.1 Development Plan Car Parking Requirements

Car Parking rates for the proposed development have been sourced from Table Mar/2A of the Marion Council Development Plan.

The site is located within a Neighbourhood Centre Zone and is within 200 metres of a road along which a bus service operates at high frequency. On this basis, the site is a Designated Area for the purposes of assessing the development car parking requirements.

The most applicable rates for the retail component of the development is a non-residential development as follows:

*Non-Residential Development*

<i>Minimum Parking Requirement</i>	<i>3 spaces per 100 square metres of gross leasable floor area</i>
<i>Maximum Parking Requirement</i>	<i>6 spaces per 100 square metres of gross leasable floor area</i>

Based on the above rates, Table 4.1 has been prepared to summarise the development plan parking requirements of the site.

**Table 4.1: Development Plan Parking Requirements**

Use	Size (sq. m)	Parking Rate		Parking Requirement	
		Minimum	Maximum	Minimum	Maximum
ALDI Store	1,677	3 spaces per 100 sq. m	6 spaces per 100 sq. m	51	101
Chemist Warehouse	700	3 spaces per 100 sq. m	6 spaces per 100 sq. m	21	42
<b>Total</b>	<b>2,377</b>			<b>72</b>	<b>143</b>

Therefore, the proposed development generates a minimum and maximum development plan requirement of 72 and 143 spaces respectively.

### 4.2 Adequacy of Parking Supply

The provision of 117 car parking spaces equates to a parking rate of 4.9 spaces per 100 sq. m. This meets the Development Plan Car Parking requirements. Furthermore, the typical parking rate for ALDI stores throughout Metropolitan Adelaide and Australia is 4.1 spaces per 100sq.m GLFA based on existing parking surveys undertaken by GTA. It is also noted the existing Chemist Warehouse generates a parking rate of 4.8 spaces per 100 sq. m which occurred on the Saturday. Both rates are below the 4.9 spaces per 100 sq.m provided by the proposed development.

## 5. Parking Layout and Access

### 5.1 Car Parking Layout

The car parking layout has been designed in accordance with AS/NZS2890.1:2004. The parking spaces are suitable for User Class 3A, short term, high turnover car parking. As such, parking spaces will be 2.6 metres wide and 5.5 metres long.

The car parking aisles will be at least 7.8 metres wide, with widths up to 12.0 metres provided within the car park to accommodate truck movements. These dimensions will meet or exceed the minimum requirements of AS/NZS2890.1:2004.

Three disabled parking spaces are located along the store frontages and meet the dimensions of 'Australian / New Zealand Standards for Off-Street Parking Facilities for People with Disabilities' (2009, henceforth referred to as AS/NZS2890.6).

Further to the above, the grades within the parking area will conform to the following requirements (as per AS/NZS2890.6 and AS2890.2):

- Maximum grade of 1 in 20 (5%) across nature strip
- Maximum grade of 1 in 40 (2.5%) across any footpath
- Maximum grade of 1 in 20 (5%) for 15 metres into the site (where commercial vehicles use the driveway, i.e. northern driveway)
- A maximum grade of 1 in 6.5 (15.4%) along commercial vehicles circulation roads, the maximum grade shall be 1 in 8 (12.5%) where reverse manoeuvres are required
- A maximum grade of 1 in 20 (5%) measured parallel to the angle of parking
- Maximum grade of 1 in 16 (6.25%) measured in any other direction to the angle of parking.

### 5.2 Access

Three (3) access points are proposed for the development.

#### 5.2.1 South Road Access

The existing access point will be relocated approximately 50 metres north (to the north of the Pedestrian Actuated Crossing (PAC) to maximise separation from the pedestrian crossing. This will continue to facilitate left turn ingress and left turn egress only. The access point will be located outside of the 6.0 metre prohibited zone of the median nose, which assists to mitigate conflict to the Southern Avenue intersection. The access point will facilitate light vehicle traffic only.

#### 5.2.2 Norrie Avenue Access

A new access point has been proposed on Norrie Avenue adjacent to the proposed ALDI Store. The access will be located approximately 40 metres west of the South Road/Norrie Avenue intersection and facilitate both light vehicles and heavy vehicles up to a 19.0 metre Semi Trailer. The access point will also facilitate un-restricted turning movements. Given the access proximity to South Road, a 19.0 metre Semi Trailer will be able to store prior to turning into the site without queuing back to South Road.

### 5.2.3 Wingfield Street Access

The two existing access points on Wingfield Street will be consolidated into a single access with un-restricted turning movements. The access will facilitate both light vehicles and trucks up to a 10 metre delivery truck for the proposed Chemist Warehouse store.



## 6. Sustainable Transport Infrastructure

### 6.1 Bicycle End of Trip Facilities

As discussed in Section 4.1, the proposed development is located within a designated area as it is located within a Neighbourhood Centre Zone and is located close to a high frequency bus stop. Therefore the bicycle parking rates have been sourced from Table Mar/5 of the Marion Council Development Plan. The most applicable rates to the development is as follows:

<i>Shop</i>	
<i>Employee</i>	<i>1 for every 300 sq. m of Gross Leasable Floor Area</i>
<i>Shopper</i>	<i>1 for every 600 sq. m of Gross Leasable Floor Area</i>

Based on the above, the ALDI Store of 1,677 sq. m will generate nine (9) spaces including six (6) bicycle spaces for staff and three (3) bicycle spaces for shoppers. The Chemist Warehouse will generate four (4) spaces including one space for shoppers.

While some provision would be required for visitor bicycle parking, GTA considers the above rate high, especially given staff will usually store their bicycle internally. Therefore, the provision of four (2) bicycle spaces (2 rails) for the ALDI Store and two (2) spaces (1 rail) for the Chemist Warehouse is considered appropriate. Additional bicycle parking can be provided on a demand basis.

### 6.2 Walking and Cycling Network

The site is well integrated with South Road with pedestrian connectivity being provided to both ALDI and Chemist Warehouse. The site is also located adjacent an existing Pedestrian Activated Crossing, which enables pedestrians to link to the eastern side of South Road.

### 6.3 Public Transport

As discussed in Section 2.3.1, the site is located close to bus stop 20, which provides high frequency bus services to the City, Noarlunga and Flinders University.

## 7. Loading Facilities

### 7.1 Development Plan Requirements

Principle of Development Control (PDC) 14 in the 'Movement Systems' section of the Marion Council Development Plan sets out the requirements for loading applicable to the proposed development. PDC 14 is as follows:

*14 "Development should provide for the on-site loading, unloading and turning of all traffic likely to be generated."*

### 7.2 Proposed Loading Arrangements

#### 7.2.1 ALDI

A loading area is proposed at the south of the site adjacent to Norrie Avenue. Whilst 19.0 metre vehicles will be used for deliveries, swept path assessments for 20.0 metre Semi Trailers have been used for future proofing of the site if these vehicles are approved for use in the future.

Entry will occur via a right turn into Norrie Avenue from South Road followed by a right turn into the site as shown in Figure 7.1. The existing on-street parking will be removed on Norrie Avenue between South Road and the Jaycar access to assist the turning into the site. Exit will occur via a left turn from the site onto Norrie Avenue followed by a left turn back onto South Road as per Figure 7.2. The loading dock will be provided in accordance with ALDI's standard detail. A bin store and compactor will also be located in the loading dock area.

The loading dock will also provide access for other vehicles including waste collection, compactor collection and bread deliveries. These are all carried out by vehicles up to Heavy Rigid Vehicle class, typically 10.5 metres or less in length.

**Figure 7.1: 20m Semi Trailer – Entry – ALDI**

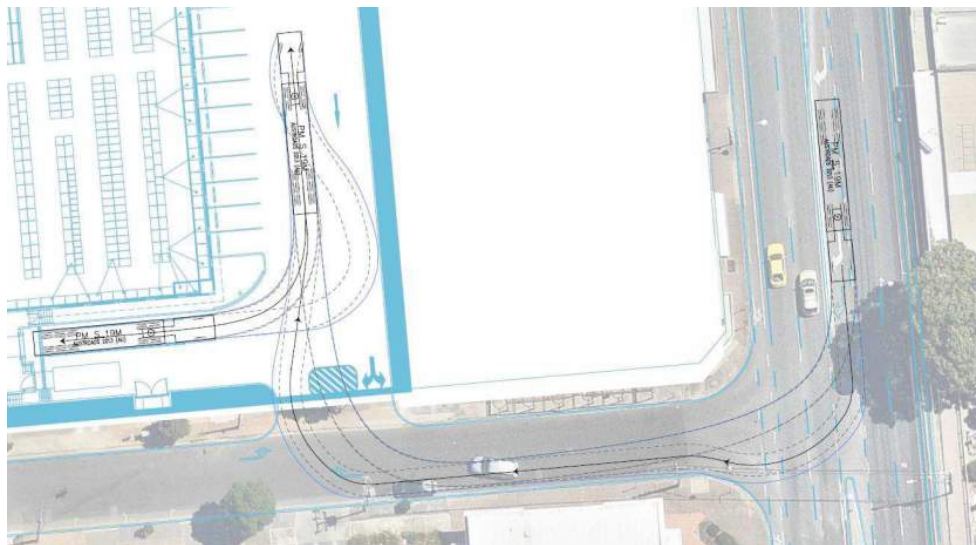
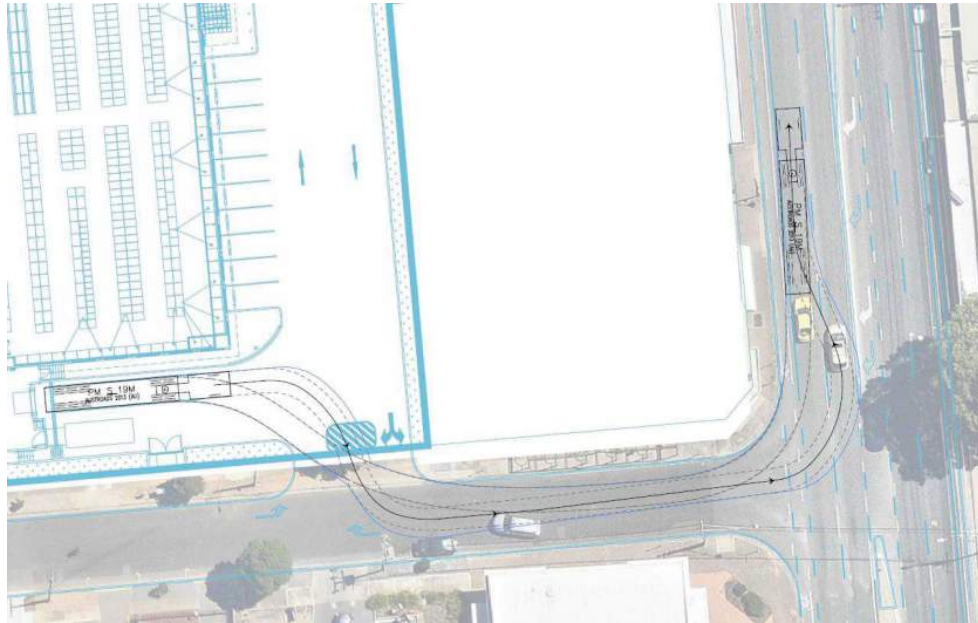


Figure 7.2: 20m Semi Trailer – Exit – ALDI



## 7.2.2 Chemist Warehouse

The loading area for Chemist Warehouse is proposed at the west of the site adjacent Wingfield Street. Deliveries will occur by trucks up to 10 metres long, with entry movements occurring via a right turn from Wingfield Street as shown in Figure 7.3. Exit movements will occur via a left turn movement from the site onto Wingfield as shown in Figure 7.4.

Figure 7.3: 10m Truck – Entry – Chemist Warehouse

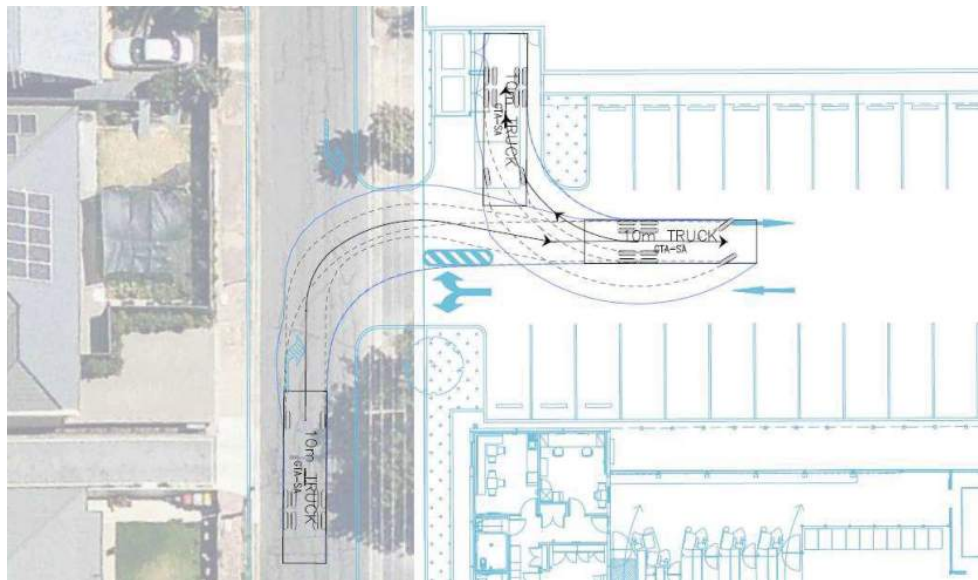
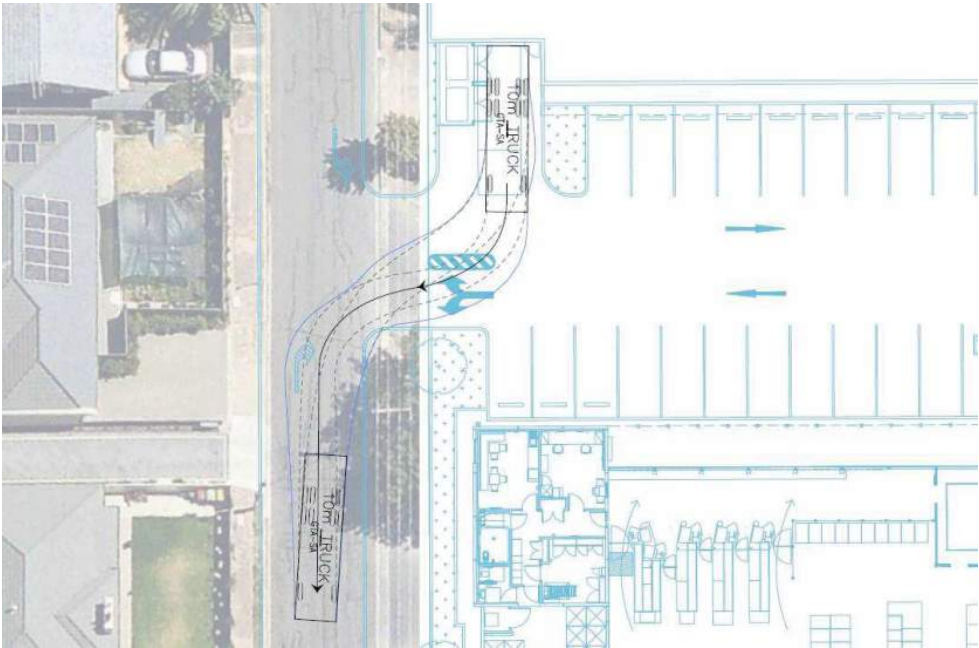


Figure 7.4: 10m Truck – Exit – Chemist Warehouse



## 8. Traffic Impact Assessment

### 8.1 Empirical Traffic Rates

The traffic assessment is based on consideration of empirical (or observed) traffic generation rates for ALDI stores in Victoria and South Australia.

#### 8.1.1 ALDI Store

Table 8.1 presents the results of traffic generation surveys undertaken by GTA at standalone ALDI stores.

**Table 8.1: Traffic Generation Surveys – ALDI Stores**

Location	Gross Leasable Floor Area (sq.m)	Date	Traffic Generation (trips per 100sq.m)	
			PM Peak Hour	Saturday Peak Hour
Sunbury	1,274	17/02/2006	13.5	
Hampton Park	1,291	17/02/2006	14.6	
Carrum Downs	1,284	24/02/2006	13.2	
Rosebud	1,454	24/02/2006	10.7	
Ferntree Gully	1,274	15&31/07/2010	27.5	25.7
Pakenham	1,382	15&31/07/2010	12.0	13.5
<b>AVERAGE</b>			<b>15.3</b>	<b>19.6</b>

Based on the traffic generation survey results the following traffic generation rates have been adopted:

Weekday (PM Peak Hour): 15.3 trips per 100sq.m gross leasable floor area

Weekend (Saturday Peak) 19.6 trips per 100sq. m gross leasable floor area

Given the above, the proposed ALDI store of 1,677 sq. m gross leasable floor area would generate approximately 257 and 328 vehicle trips in the PM peak hour and Saturday peak respectively.

#### Link Trip Discount

GTA undertook 162 questionnaire surveys at six existing ALDI stores located in Victoria. The survey questions involved travel patterns including mode of travel, typical origin and destination and linking of trips. The results of the questionnaire survey with regards to passing trade from linked trips are presented in Table 8.2.

**Table 8.2: ALDI Travel Patterns Questionnaire Survey – Link Trips**

Site	Customer Surveys		
	Passing Trade	Total	Percentage
Belmont	7	30	23%
Carrum Downs	8	26	31%
Ferntree Gully	12	40	30%
Hampton Park	16	38	42%
Rosebud	2	5	40%
Sunbury	2	23	9%
<b>AVERAGE</b>	<b>47</b>	<b>162</b>	<b>29%</b>

The results of the table above indicate approximately 30% of all customers surveyed were identified as passing trade.

Based on the results of the questionnaire surveys a 30% discount factor for passing trade is considered reasonable for the development. Application of this discount factor results in an overall trip generation for the site of 180 and 230 new vehicle trips onto the road network in the PM peak and Saturday peak respectively.

The concept of linked trips is also supported by *Austrroads Guide to Traffic Management Part 12* where it is recognised that traffic generated by (or attracted to) a development will be composed of the following:

- new trips that will not be made on the network if the development does not proceed
- existing trips between an origin and destination that divert a significant distance to visit the development
- existing trips that use the roads immediately abutting the development and break the journey to use the development.

### 8.1.2 Proposed Chemist Warehouse

Traffic generation for the Chemist Warehouse has been sourced from empirical turning count movements undertaken during the Thursday PM Peak and Saturday Peak respectively.

The following entry and exit movements were recorded during the Weekday PM Peak and Saturday PM Peak in Table 8.3 below.

**Table 8.3: Chemist Warehouse Volumes**

	Entry	Exit	Total
Thursday Weekday PM Peak	71	86	<b>169</b>
Saturday Weekend PM Peak	160	184	<b>344</b>

While the above movements did incorporate traffic associated with Rite Price, as a conservative assessment, the above numbers were adopted for the Chemist Warehouse trip generation. While an exact 50:50 distribution was not recorded during the survey, a 50:50 inbound: outbound split was considered appropriate as discussed in Section 8.1.3.

### 8.1.3 Traffic Generation

Given the traffic volume associated with the Chemist Warehouse is existing; the traffic generated by the site will only be additional trips associate with the ALDI Store. This equates to an additional 180 and 230 trips onto the network during the Weekday PM Peak Hour and the Saturday Peak Hour respectively. The site will generate an additional is 257 and 328 trips during the Weekday PM Peak and Saturday Peak respectively.

### 8.1.4 Distribution and Assignment

The directional distribution and assignment of traffic generated by the proposed development will be influenced by a number of factors, including the:

- i configuration of the road network in the immediate vicinity of the site
- i existing operation of intersections providing access between the local and arterial road network
- ii distribution of households in the vicinity of the site
- iii likely distribution of employee's residences in relation to the site
- iv configuration of access points to the site.



Given the change to the access configuration for the proposed development, the distribution of traffic entering the site will be influenced by the proposed access on Norrie Avenue. This will alleviate traffic being generated onto the Wingfield Street access point. Furthermore, while the traffic entering from the South Road's north and south approaches were considered to be the same, a much higher volume of vehicles exiting to the north was considered.

This was influenced by the difficulty of vehicles undertaking a right turn out onto South Road under existing conditions and was reflected in the existing survey data. This condition is not unusual for right turn movements from local roads onto arterial roads in other locations. As such, only a very small proportion of vehicles exiting onto South Road via a right turn were assumed based on existing conditions.

Much of the catchment for this store is expected to be to the north on South Road with limited residential to the west and south of the site. This store will be located midway between ALDI stores at Marion Shopping Centre and Hawthorn. This will influence the catchment of customers primarily to the areas to the north on South Road (residential areas to the east and west of South Road). There is only a relatively small catchment of residential directly to the west.

Having consideration to the above, the traffic volume associated with the proposed ALDI and Chemist Warehouse is shown Figure 8.1 and Figure 8.2 respectively.

The diagrams below do not consider the additional traffic generated into the site and onto the road network, but rather the combined traffic associated with ALDI Store and Chemist Warehouse.

Figure 8.1: Combined ALDI and Chemist Warehouse Traffic – Weekday PM Peak

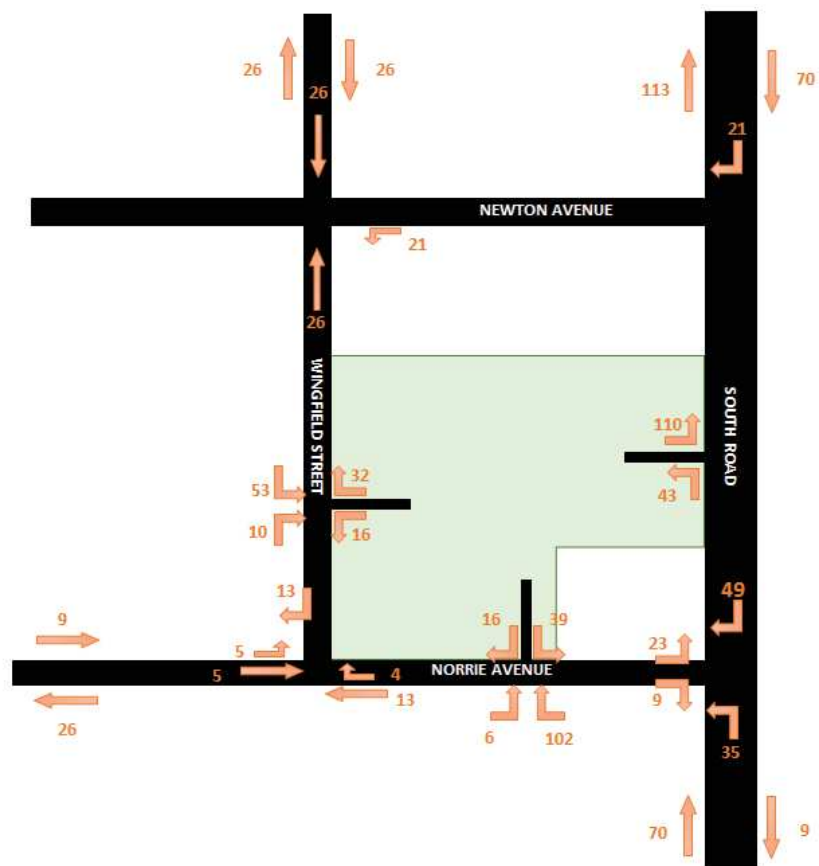
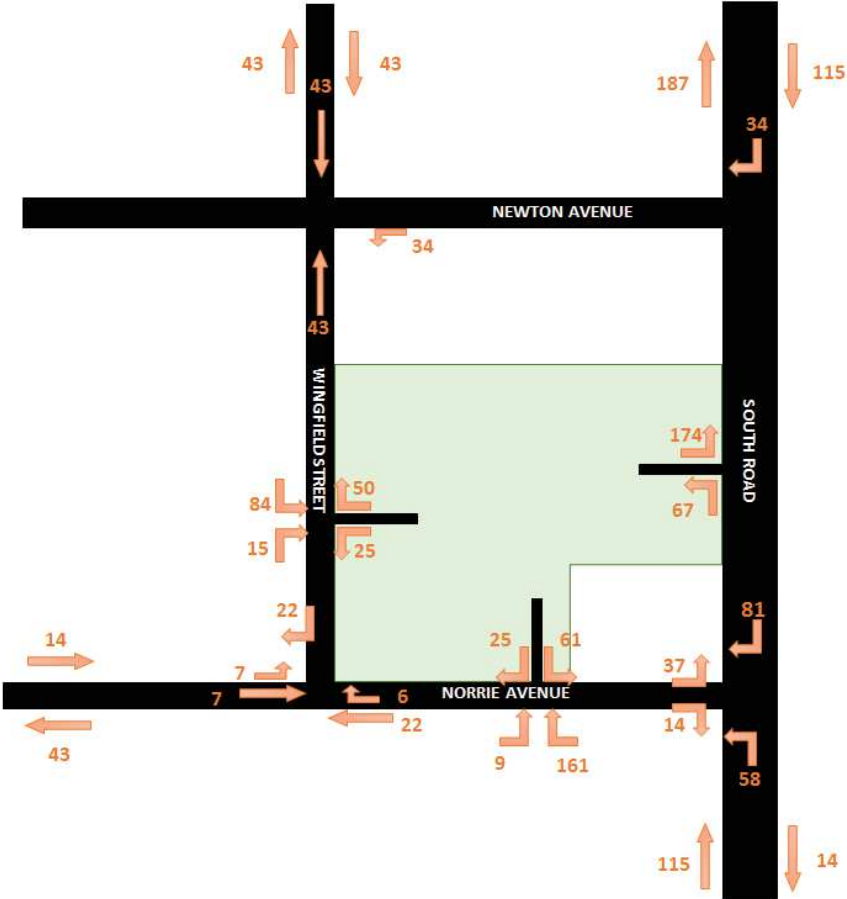


Figure 8.2: Combined ALDI and Chemist Warehouse Traffic – Saturday Peak Hour



Based on above, Figure 8.3 and Figure 8.4 considers the anticipated post development traffic during the Weekday PM Peak Hour and the Saturday Peak Hour.

Figure 8.3: Post Development – Weekday PM Peak Hour

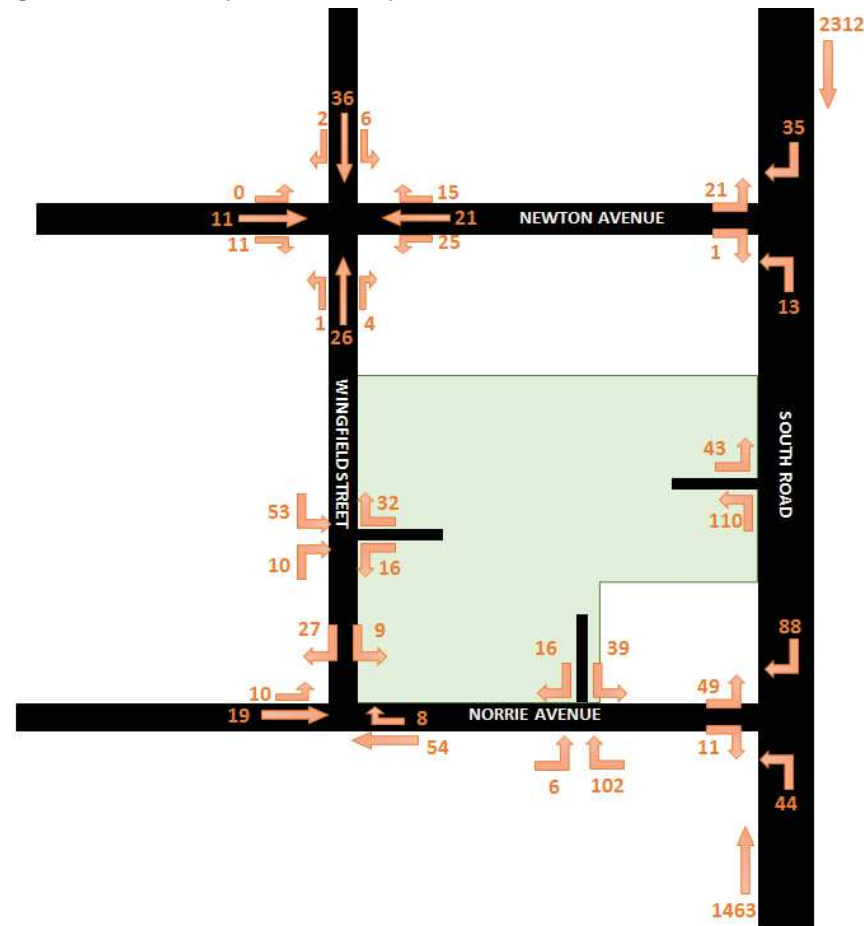
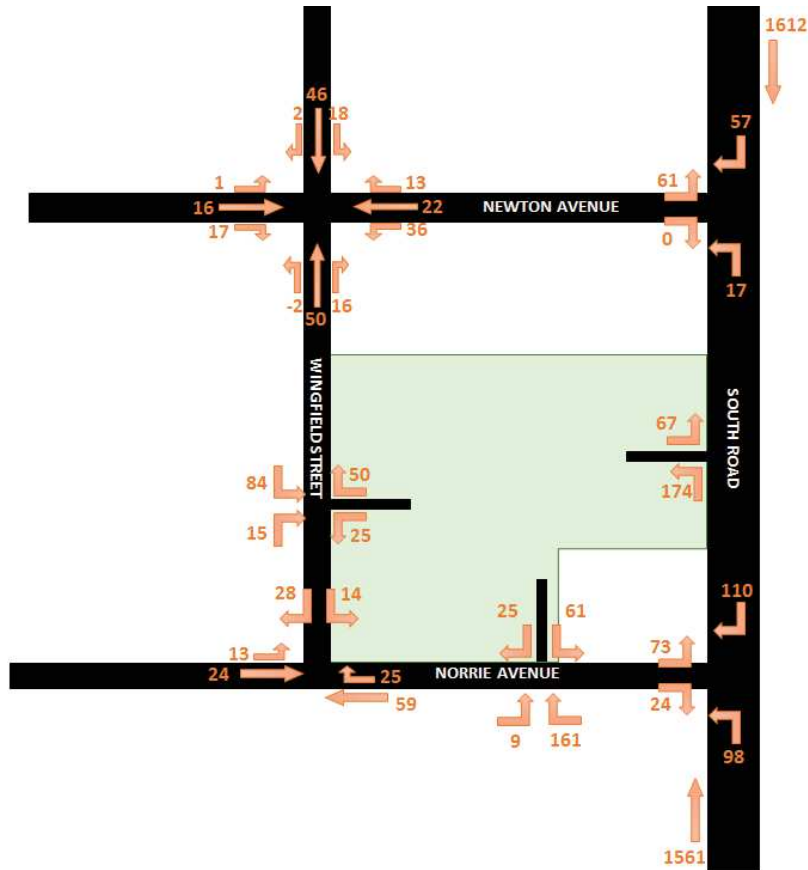


Figure 8.4: Post Development Volume – Saturday Peak Hour



## 8.2 SIDRA Intersection Analysis

The operation of the following intersections has been assessed using SIDRA INTERSECTION, a computer-based modelling package which calculates intersection performance. All movement summaries have been shown in Appendix A.

- South Road/Newton Avenue Intersection – Table 8.4
- South Road/Proposed Access – Table 8.5\*
- South Road/Norrie Avenue Intersection – Table 8.6
- Norrie Avenue/Proposed Access – Table 8.7

The above intersections were considered critical to model during the Weekday PM Peak and the Saturday Peak considering their close proximity to South Road. While the local road intersections are likely to see a slight change in traffic distribution, these intersections weren't assessed given their current suitable level of operation and available capacity.

### South Road/Newton Avenue Intersection

The results of the SIDRA analysis on the intersection of South Road and Newton Avenue is summarised in Table 8.4 during the Weekday PM Peak and Saturday Peak for post development conditions.

**Table 8.4: South Road/Newton Avenue – Post Development**

Approach	Movement	Weekday PM Peak			Saturday Peak Hour		
		Level of Service (LOS)	Average Delay (s)	95 <sup>th</sup> Percentile Queue (m)	Level of Service (LOS)	Average Delay (s)	95 <sup>th</sup> Percentile Queue (m)
South Rd (South App)	Left	A	5.6	0.0	A	5.6	0.0
	Through	A	0.1	0.0	A	0.1	0.0
South Rd (North App)	Through	A	0.1	0.0	A	0.1	0.0
	Right	D	25.7	3.7	D	34.4	7.8
Newton Ave (West App)	Left	B	10.7	1.0	B	11.6	3.2
	Right	F	4030.4	14.6	F	4354.3	15.7

Based on the above, the intersection will continue to operate similar to existing conditions for both Weekday PM Peak and the Saturday Peak. The right turn into Newton Avenue will experience a marginal increase in delay and queuing, notwithstanding, this will still be within acceptable limits assuming poor level of service for right turns onto South Road will continue.

### South Road/Revised Access

The results of the SIDRA analysis on the intersection of South Road and proposed access is summarised in Table 8.5 during the Weekday PM Peak and Saturday Peak for post development conditions.

**Table 8.5: South Road/Proposed Site Access – Post Development**

Approach	Movement	Weekday PM Peak			Saturday Peak Hour		
		Level of Service (LOS)	Average Delay (s)	95 <sup>th</sup> Percentile Queue (m)	Level of Service (LOS)	Average Delay (s)	95 <sup>th</sup> Percentile Queue (m)
South Rd (South App)	Left	A	5.6	0.0	A	5.6	0.0
	Through	A	0.1	0.0	A	0.1	0.0
South Rd (North App)	Through	A	0.1	0.0	A	0.1	0.0
	Right						
Newton Ave (West App)	Left	B	10.2	1.9	B	10.5	3.1
	Right						

Despite the relocation of the access point to the north of the PAC, the intersection will continue to operate at the same LOS, with minimal changes to the average delay and 95<sup>th</sup> percentile queue length for both the Weekday PM Peak and the Saturday Peak.



### South Road/Norrie Avenue Intersection

The results of the SIDRA analysis on the intersection of South Road and Norrie Avenue is summarised in Table 8.6 during the Weekday PM Peak and Saturday Peak for post development conditions.

**Table 8.6: South Road/Norrie Avenue – Post Development**

Approach	Movement	Weekday PM Peak			Saturday Peak Hour		
		Level of Service (LOS)	Average Delay (s)	95 <sup>th</sup> Percentile Queue (m)	Level of Service (LOS)	Average Delay (s)	95 <sup>th</sup> Percentile Queue (m)
South Rd (South App)	Left	A	5.6	0.0	A	5.6	0.0
	Through	A	0.1	0.0	A	0.1	0.0
South Rd (North App)	Through	A	0.1	0.0	A	0.1	0.0
	Right	D	30.8	11.2	F	58.3	22.8
Norrie Ave (West App)	Left	B	10.5	2.3	B	10.9	3.6
	Right	F	518.2	17.8	F	293.3	22.4

Based on the above, the intersection will operate similar to existing conditions with some increases to the average delay and 95<sup>th</sup> percentile queue length on a couple of approaches. During the Weekday Peak, the right turn into Norrie Avenue would experience a 5.7 metre and 6.8 second increase in the 95<sup>th</sup> percentile queue length (less than one vehicle) and average delay respectively. The right turn out will experience an increase in queue length by 2 metres which is marginal. During the Saturday Peak, the right turn in will experience a 29 second increase in the average delay and an 11.7 metre increase in the 95<sup>th</sup> percentile queue. While this is a more notable increase, and the approach will experience a drop of Level of Service (LOS) to F, this delay is considered acceptable, with the 95<sup>th</sup> percentile queue being within the storage capacity of the right turn lane. The degree of Saturation for the Saturday Peak was 0.779 for the right turn indicating it will operate within the maximum capacity. Therefore, there will be no adverse impacts from the right turn lane into the site. The right turn onto South Road will experience a 3 metre increase in the 95<sup>th</sup> percentile queue length which is marginal.

### Norrie Avenue/Site Access Intersection

The results of the SIDRA analysis on the intersection of Norrie Avenue and the proposed access is summarised in Table 8.7 during the Weekday PM Peak and Saturday Peak for post development conditions.

**Table 8.7: Norrie Avenue/Site Access – Post Development**

Approach	Movement	Weekday PM Peak			Saturday Peak Hour		
		Level of Service (LOS)	Average Delay (s)	95 <sup>th</sup> Percentile Queue (m)	Level of Service (LOS)	Average Delay (s)	95 <sup>th</sup> Percentile Queue (m)
Norrie Ave (East App)	Through	A	0.1	2.6	A	0.1	4.4
	Right	A	5.6	2.6	A	5.6	4.4
Site Access (North App)	Left	A	5.6	1.1	A	5.6	1.8
	Right	A	6.1	1.1	A	6.5	1.8
Norrie Ave (West App)	Left	A	5.5	0.0	A	5.5	0.0
	Through	A	0.0	0.0	A	0.0	0.0

Based on the above, the intersection would operate at a LOS of A with no notable queuing or delay. This is concurrent during the Weekday PM Peak and Saturday Peak Hour. In particular, the right turn into the site from Norrie Avenue experiences a 95<sup>th</sup> percentile queue length of 4.4 metres (less than a vehicle). Therefore, the provision of this access will not result in queuing back

to South Road. As such, South Road won't be adversely impacted by the provision of this access point.

### 8.3 Traffic Impact

Therefore, the additional traffic generated by the ALDI Development won't adversely impact on the surrounding road network. While there will be increases in the average delays and queue lengths, these will generally be marginal. The right turn lane into Norrie Avenue will experience a more significant increase in the average delay (29 seconds) with a 95<sup>th</sup> Percentile Queue of 22.8 metres (~3 vehicles). Notwithstanding, this will remain within the capacity of the lane.

The additional right turns from Norrie Avenue will experience a large delay with a slight increase in queue length. However, existing traffic turning right onto South Road already experience long delays. In practice, most vehicles will turn left and seek alternative routes such as Daws Road to link back to the south. The proposed access point on Norrie Avenue will generate minimal queues and delays, and as such won't adversely impact on South Road.

Norrie Avenue/Wingfield Street and Newton Avenue/Wingfield Street will also experience an increase in traffic. Notwithstanding, given the existing volumes for these intersections are low, the intersections will continue to perform well post development.

## 9. Conclusion

9

Based on the analysis and discussions presented within this report, the following conclusions are made:

- i The proposed supply of 117 car parking spaces (4.9 spaces per 100 sq. m) meets the Development Plan requirement. The parking provision also exceeds the empirical rate for ALDI Stores and the existing Chemist Warehouse.
- ii The proposed car parking layout is consistent with dimensional requirements set out in the Australian/New Zealand Standard for Off-Street Car Parking (AS2890.1:2004) and the Australian Standard for Parking for People with Disabilities (AS2890.6:2009).
- iii Three (3) access points have been proposed on South Road, Norrie Avenue and Wingfield Street respectively. All access points will operate suitably.
- iv GTA considers the provision of six (6) bicycle parking spaces to be appropriate including four (4) spaces for the ALDI Store and two (2) spaces for Chemist Warehouse. If there is additional demand, additional spaces can be provided.
- v The loading facility for the ALDI Store will accommodate truck access up to a 19.0 metre Semi Trailer with access via South Road and Norrie Avenue. The on-street parking on Norrie Avenue between South Road and the Jaycar access will be removed.
- vi The loading facility for the Chemist Warehouse will accommodate trucks up to a 10.0 metre rigid truck. They will access the site via Wingfield Street.
- vii The existing Chemist Warehouse and Rite Price generate a Weekday PM Peak and Saturday Peak volume of 169 and 344 trips. As a conservative approach, this was considered to reflect the proposed volumes associated with Chemist Warehouse, although GTA considers a slight reduction in traffic.
- viii The site will generate an additional 257 and 328 trips during the Weekday PM Peak and Saturday Peak respectively. This volume is purely associated with ALDI traffic. Given the proximity of other ALDI Stores at Marion and Hawthorn, it is anticipated that the majority of the catchment will be to/from the north.
- ix An analysis of the additional traffic generated by the proposed development during the Weekday PM Peak and Saturday Peak indicates the road network will experience some increases in queuing and delays, although marginal in most cases. The right turn into Norrie Avenue from South Road will experience an increase in delay by 29 seconds, although the 95<sup>th</sup> percentile queue length will remain within the current physical capacity of the lane.

## Appendix A

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### SIDRA Intersection results



## Site Access with South Road

### Thursday PM Peak – Existing Conditions

#### MOVEMENT SUMMARY

Site: 101 [South Road\_Site Access\_Weekday PM Peak\_Existing Conditions]

Thursday PM Peak - Existing Conditions  
Giveaway / Yield (Two-Way)

Movement Performance - Vehicles											
Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn sat	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: South Road											
1	L2	31	0.0	0.415	5.8	LOS A	0.0	0.0	0.00	0.02	58.1
2	T1	1540	4.1	0.415	0.1	LOS A	0.0	0.0	0.00	0.01	59.8
Approach		1571	4.0	0.415	0.2	NA	0.0	0.0	0.00	0.01	59.7
North: South Road											
8	T1	2434	2.8	0.635	0.1	LOS A	0.0	0.0	0.00	0.00	59.7
Approach		2434	2.8	0.635	0.1	NA	0.0	0.0	0.00	0.00	59.7
West: Site Access											
10	L2	36	0.0	0.066	10.7	LOS B	0.2	1.6	0.61	0.82	49.9
Approach		36	0.0	0.066	10.7	LOS B	0.2	1.6	0.61	0.82	49.9
All Vehicles		4040	3.2	0.635	0.2	NA	0.2	1.6	0.01	0.01	59.6

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).  
Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

### Thursday PM Peak – Post Development

#### MOVEMENT SUMMARY

Site: 101 [South Road\_Site Access\_Weekday PM Peak\_Post Development]

Thursday PM Peak - Post Development  
Giveaway / Yield (Two-Way)

Movement Performance - Vehicles											
Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn sat	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: South Road											
1	L2	116	0.0	0.438	5.6	LOS A	0.0	0.0	0.00	0.08	57.5
2	T1	1540	4.1	0.438	0.1	LOS A	0.0	0.0	0.00	0.04	59.5
Approach		1656	3.8	0.438	0.5	NA	0.0	0.0	0.00	0.04	59.4
North: South Road											
8	T1	2434	2.8	0.635	0.1	LOS A	0.0	0.0	0.00	0.00	59.7
Approach		2434	2.8	0.635	0.1	NA	0.0	0.0	0.00	0.00	59.7
West: Site Access											
10	L2	45	0.0	0.078	10.2	LOS B	0.3	1.9	0.58	0.80	50.2
Approach		45	0.0	0.078	10.2	LOS B	0.3	1.9	0.58	0.80	50.2
All Vehicles		4135	3.2	0.635	0.4	NA	0.3	1.9	0.01	0.03	59.4

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).  
Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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Proposed ALDI Store, South Road, Clovelly Park



## Saturday Peak – Existing Conditions

### MOVEMENT SUMMARY

Site: 101 [South Road\_Site Access\_Saturday Peak\_Existing Conditions]

Saturday Peak - Existing Conditions  
Giveaway / Yield (Two-Way)

Movement Performance - Vehicles											
Mov ID	QD Mov	Demand Flows Total veh/h	HV %	Deq. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: South Road											
1	L2	66	0.0	0.447	5.6	LOS A	0.0	0.0	0.00	0.05	57.8
2	T1	1643	2.5	0.447	0.1	LOS A	0.0	0.0	0.00	0.02	59.7
Approach		1709	2.4	0.447	0.3	NA	0.0	0.0	0.00	0.02	59.6
North: South Road											
8	T1	1697	2.4	0.442	0.1	LOS A	0.0	0.0	0.00	0.00	59.9
Approach		1697	2.4	0.442	0.1	NA	0.0	0.0	0.00	0.00	59.9
West: Site Access											
10	L2	71	0.0	0.136	11.3	LOS B	0.5	3.4	0.65	0.85	49.5
Approach		71	0.0	0.136	11.3	LOS B	0.5	3.4	0.65	0.85	49.5
All Vehicles		3477	2.3	0.447	0.4	NA	0.5	3.4	0.01	0.03	59.5

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Apkelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

## Saturday Peak – Post Development

### MOVEMENT SUMMARY

Site: 101 [South Road\_Site Access\_Saturday Peak\_Post Development]

Saturday Peak - Post Development  
Giveaway / Yield (Two-Way)

Movement Performance - Vehicles											
Mov ID	QD Mov	Demand Flows Total veh/h	HV %	Deq. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: South Road											
1	L2	183	0.0	0.479	5.6	LOS A	0.0	0.0	0.00	0.12	57.2
2	T1	1643	2.5	0.479	0.1	LOS A	0.0	0.0	0.00	0.05	59.4
Approach		1826	2.2	0.479	0.6	NA	0.0	0.0	0.00	0.06	59.1
North: South Road											
8	T1	1697	2.4	0.442	0.1	LOS A	0.0	0.0	0.00	0.00	59.9
Approach		1697	2.4	0.442	0.1	NA	0.0	0.0	0.00	0.00	59.9
West: Site Access											
10	L2	71	0.0	0.124	10.5	LOS B	0.4	3.1	0.60	0.83	50.0
Approach		71	0.0	0.124	10.5	LOS B	0.4	3.1	0.60	0.83	50.0
All Vehicles		3594	2.3	0.479	0.6	NA	0.4	3.1	0.01	0.05	59.3

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Apkelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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Transport Impact Assessment // Issue: A  
Proposed ALDI Store, South Road, Clovelly Park





## South Road and Newton Avenue Intersection

Thursday PM Peak – Existing Conditions

### MOVEMENT SUMMARY

▽ Site: 101 [South Road\_Newton Avenue\_Weekday PM Peak\_Existing Conditions]

Thursday PM Peak - Existing Conditions  
Giveaway / Yield (Two-Way)

Movement Performance - Vehicles											
Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: South Road											
1	L2	14	0.0	0.410	5.6	LOS A	0.0	0.0	0.00	0.01	58.2
2	T1	1540	4.1	0.410	0.1	LOS A	0.0	0.0	0.00	0.01	59.8
Approach		1554	4.1	0.410	0.1	NA	0.0	0.0	0.00	0.01	59.8
North: South Road											
8	T1	2434	2.8	0.635	0.1	LOS A	0.0	0.0	0.00	0.00	59.7
9	R2	28	0.0	0.151	24.8	LOS C	0.4	2.8	0.89	0.95	41.8
Approach		2462	2.7	0.635	0.4	NA	0.4	2.8	0.01	0.01	59.4
West: Newton Road											
10	L2	22	0.0	0.041	10.7	LOS B	0.1	1.0	0.61	0.79	49.9
12	R2	1	0.0	1.000	4032.5	LOS F	2.1	14.6	1.00	1.02	0.9
Approach		23	0.0	1.000	193.5	LOS F	2.1	14.6	0.63	0.80	14.3
All Vehicles		4039	3.2	1.000	1.4	NA	2.1	14.6	0.01	0.01	58.5

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Thursday PM Peak – Post Development

### MOVEMENT SUMMARY

▽ Site: 101 [South Road\_Newton Avenue\_Weekday PM Peak\_Post Development]

Thursday PM Peak - Post Development  
Giveaway / Yield (Two-Way)

Movement Performance - Vehicles											
Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: South Road											
1	L2	14	0.0	0.410	5.6	LOS A	0.0	0.0	0.00	0.01	58.2
2	T1	1540	4.1	0.410	0.1	LOS A	0.0	0.0	0.00	0.01	59.8
Approach		1554	4.1	0.410	0.1	NA	0.0	0.0	0.00	0.01	59.8
North: South Road											
8	T1	2434	2.8	0.635	0.1	LOS A	0.0	0.0	0.00	0.00	59.7
9	R2	37	0.0	0.195	25.7	LOS D	0.5	3.7	0.89	0.96	41.3
Approach		2471	2.7	0.635	0.5	NA	0.5	3.7	0.01	0.01	59.3
West: Newton Road											
10	L2	22	0.0	0.041	10.7	LOS B	0.1	1.0	0.61	0.79	49.9
12	R2	1	0.0	1.000	4030.4	LOS F	2.1	14.6	1.00	1.02	0.9
Approach		23	0.0	1.000	193.4	LOS F	2.1	14.6	0.63	0.80	14.3
All Vehicles		4047	3.2	1.000	1.5	NA	2.1	14.6	0.01	0.02	58.4

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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## Saturday Peak – Existing Conditions

### MOVEMENT SUMMARY

▽ Site: 101 [South Road\_Newton Avenue\_Saturday Peak\_Existing Conditions]

Saturday Peak - Existing Conditions  
Giveway / Yield (Two-Way)

Movement Performance - Vehicles											
Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg Satn vic	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: South Road											
1	L2	18	0.0	0.434	5.6	LOS A	0.0	0.0	0.00	0.01	58.1
2	T1	1643	2.5	0.434	0.1	LOS A	0.0	0.0	0.00	0.01	59.8
Approach		1661	2.5	0.434	0.1	NA	0.0	0.0	0.00	0.01	59.8
North: South Road											
8	T1	1697	2.4	0.442	0.1	LOS A	0.0	0.0	0.00	0.00	59.9
9	R2	42	0.0	0.270	31.7	LOS D	0.7	5.2	0.92	0.99	38.7
Approach		1739	2.3	0.442	0.8	NA	0.7	5.2	0.02	0.02	59.1
West: Newton Road											
10	L2	64	0.0	0.129	11.6	LOS B	0.5	3.2	0.66	0.86	49.3
12	R2	1	0.0	1.000	4364.3	LOS F	2.2	15.7	1.00	1.02	0.8
Approach		65	0.0	1.000	81.8	LOS F	2.2	15.7	0.66	0.86	25.3
All Vehicles		3465	2.3	1.000	2.0	NA	2.2	15.7	0.02	0.03	58.0

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

## Saturday Peak – Post Development

### MOVEMENT SUMMARY

▽ Site: 101 [South Road\_Newton Avenue\_Saturday Peak\_Post Development]

Saturday Peak - Post Development  
Giveway / Yield (Two-Way)

Movement Performance - Vehicles											
Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg Satn vic	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: South Road											
1	L2	18	0.0	0.434	5.6	LOS A	0.0	0.0	0.00	0.01	58.1
2	T1	1643	2.5	0.434	0.1	LOS A	0.0	0.0	0.00	0.01	59.8
Approach		1661	2.5	0.434	0.1	NA	0.0	0.0	0.00	0.01	59.8
North: South Road											
8	T1	1697	2.4	0.442	0.1	LOS A	0.0	0.0	0.00	0.00	59.9
9	R2	60	0.0	0.385	34.4	LOS D	1.1	7.8	0.93	1.01	37.6
Approach		1757	2.3	0.442	1.2	NA	1.1	7.8	0.03	0.03	58.7
West: Newton Road											
10	L2	64	0.0	0.129	11.6	LOS B	0.5	3.2	0.66	0.86	49.3
12	R2	1	0.0	1.000	4354.3	LOS F	2.2	15.7	1.00	1.02	0.8
Approach		65	0.0	1.000	81.6	LOS F	2.2	15.7	0.66	0.86	25.3
All Vehicles		3483	2.3	1.000	2.2	NA	2.2	15.7	0.03	0.04	57.8

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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## South Road and Norrie Avenue Intersection

Thursday PM Peak – Existing Conditions

### MOVEMENT SUMMARY

Site: 101 [South Road\_Norrie Avenue\_Weekday PM Peak\_Existing Conditions]

Thursday PM Peak - Existing Conditions  
Giveaway / Yield (Two-Way)

Movement Performance - Vehicles											
Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Dep. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: South Road											
1	L2	9	0.0	0.409	5.6	LOS A	0.0	0.0	0.00	0.01	58.2
2	T1	1540	4.1	0.409	0.1	LOS A	0.0	0.0	0.00	0.00	59.8
Approach		1549	4.1	0.409	0.1	NA	0.0	0.0	0.00	0.00	59.8
North: South Road											
8	T1	2434	2.8	0.635	0.1	LOS A	0.0	0.0	0.00	0.00	59.7
9	R2	45	0.0	0.215	24.0	LOS C	0.6	4.5	0.88	0.96	42.1
Approach		2479	2.7	0.635	0.6	NA	0.6	4.5	0.02	0.02	59.3
West: Norrie Avenue											
10	L2	27	0.0	0.050	10.6	LOS B	0.2	1.2	0.60	0.79	49.9
12	R2	4	0.0	1.000	1214.9	LOS F	2.3	15.8	1.00	1.05	2.9
Approach		32	0.0	1.000	171.2	LOS F	2.3	15.8	0.65	0.82	15.6
All Vehicles		4060	3.2	1.000	1.7	NA	2.3	15.8	0.01	0.02	58.2

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Thursday PM Peak – Post Development

### MOVEMENT SUMMARY

Site: 101 [South Road\_Norrie Avenue\_Weekday PM Peak\_Post Development]

Thursday PM Peak - Post Development  
Giveaway / Yield (Two-Way)

Movement Performance - Vehicles											
Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Dep. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: South Road											
1	L2	46	0.0	0.419	5.6	LOS A	0.0	0.0	0.00	0.03	58.0
2	T1	1540	4.1	0.419	0.1	LOS A	0.0	0.0	0.00	0.02	59.7
Approach		1586	4.0	0.419	0.2	NA	0.0	0.0	0.00	0.02	59.7
North: South Road											
8	T1	2434	2.8	0.635	0.1	LOS A	0.0	0.0	0.00	0.00	59.7
9	R2	93	0.0	0.469	30.8	LOS D	1.6	11.2	0.92	1.03	39.1
Approach		2526	2.7	0.635	1.3	NA	1.6	11.2	0.03	0.04	58.6
West: Norrie Avenue											
10	L2	52	0.0	0.091	10.5	LOS B	0.3	2.3	0.60	0.82	50.0
12	R2	12	0.0	1.000	518.2	LOS F	2.5	17.8	1.00	1.10	6.3
Approach		63	0.0	1.000	103.6	LOS F	2.5	17.8	0.67	0.87	22.0
All Vehicles		4176	3.1	1.000	2.4	NA	2.5	17.8	0.03	0.04	57.5

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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## Saturday Peak – Existing Conditions

### MOVEMENT SUMMARY

▽ Site: 101 [South Road\_Norrie Avenue\_Saturday Peak\_Existing Conditions]

Saturday Peak - Existing Conditions  
Giveaway / Yield (Two-Way)

Movement Performance - Vehicles											
Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: South Road											
1	L2	42	0.0	0.441	5.6	LOS A	0.0	0.0	0.00	0.03	58.0
2	T1	1643	2.5	0.441	0.1	LOS A	0.0	0.0	0.00	0.01	59.7
Approach		1685	2.4	0.441	0.2	NA	0.0	0.0	0.00	0.01	59.7
North: South Road											
8	T1	1697	4.0	0.446	0.1	LOS A	0.0	0.0	0.00	0.00	59.9
9	R2	40	0.0	0.239	29.5	LOS D	0.7	4.8	0.91	0.98	39.6
Approach		1737	3.9	0.446	0.7	NA	0.7	4.8	0.02	0.02	59.2
West: Norrie Avenue											
10	L2	38	0.0	0.072	11.1	LOS B	0.3	1.8	0.62	0.83	49.6
12	R2	13	0.0	1.000	528.2	LOS F	2.8	19.8	1.00	1.11	6.2
Approach		51	0.0	1.000	140.4	LOS F	2.8	19.8	0.72	0.90	16.0
All Vehicles		3473	3.1	1.000	2.5	NA	2.8	19.8	0.02	0.03	57.5

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

## Saturday Peak – Post Development

### MOVEMENT SUMMARY

▽ Site: 101 [South Road\_Norrie Avenue\_Saturday Peak\_Post Development]

Saturday Peak - Post Development  
Giveaway / Yield (Two-Way)

Movement Performance - Vehicles											
Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: South Road											
1	L2	103	0.0	0.457	5.6	LOS A	0.0	0.0	0.00	0.07	57.6
2	T1	1643	2.5	0.457	0.1	LOS A	0.0	0.0	0.00	0.03	59.6
Approach		1746	2.4	0.457	0.4	NA	0.0	0.0	0.00	0.04	59.4
North: South Road											
8	T1	1697	4.0	0.446	0.1	LOS A	0.0	0.0	0.00	0.00	59.9
9	R2	116	0.0	0.779	58.3	LOS F	3.3	22.8	0.98	1.18	30.2
Approach		1813	3.7	0.779	3.8	NA	3.3	22.8	0.06	0.08	56.3
West: Norrie Avenue											
10	L2	77	0.0	0.140	10.9	LOS B	0.5	3.6	0.62	0.84	49.7
12	R2	25	0.0	1.000	293.3	LOS F	3.2	22.4	1.00	1.18	10.3
Approach		102	0.0	1.000	80.8	LOS F	3.2	22.4	0.72	0.92	25.5
All Vehicles		3661	3.0	1.000	4.3	NA	3.3	22.8	0.05	0.08	55.8

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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## Site Access with Norrie Avenue

Thursday PM Peak – Post Development

### MOVEMENT SUMMARY

Site: 101 [Norrie Avenue\_Site Access\_Weekday PM Peak\_Post Development]

Thursday PM Peak - Post Development  
Giveaway / Yield (Two-Way)

Movement Performance - Vehicles											
Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
East: Norrie Avenue											
5	T1	32	0.0	0.078	0.1	LOS A	0.4	2.6	0.10	0.43	55.7
6	R2	107	0.0	0.078	5.6	LOS A	0.4	2.6	0.10	0.43	54.1
Approach		139	0.0	0.078	4.3	NA	0.4	2.6	0.10	0.43	54.5
North: Site Access											
7	L2	41	0.0	0.041	5.6	LOS A	0.2	1.1	0.07	0.56	53.4
9	R2	17	0.0	0.041	6.1	LOS A	0.2	1.1	0.07	0.56	53.3
Approach		58	0.0	0.041	5.7	LOS A	0.2	1.1	0.07	0.56	53.4
West: Norrie Avenue											
10	L2	6	0.0	0.015	5.5	LOS A	0.0	0.0	0.00	0.13	57.3
11	T1	23	0.0	0.015	0.0	LOS A	0.0	0.0	0.00	0.13	58.8
Approach		29	0.0	0.015	1.2	NA	0.0	0.0	0.00	0.13	58.5
All Vehicles		226	0.0	0.078	4.3	NA	0.4	2.6	0.08	0.42	54.7

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Saturday Peak – Post Development

### MOVEMENT SUMMARY

Site: 101 [Norrie Avenue\_Site Access\_Saturday Peak\_Post Development]

Saturday Peak - Post Development  
Giveaway / Yield (Two-Way)

Movement Performance - Vehicles											
Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
East: Norrie Avenue											
5	T1	49	4.3	0.123	0.1	LOS A	0.6	4.4	0.13	0.43	55.6
6	R2	169	0.0	0.123	5.6	LOS A	0.6	4.4	0.13	0.43	54.0
Approach		219	1.0	0.123	4.4	NA	0.6	4.4	0.13	0.43	54.4
North: Site Access											
7	L2	64	0.0	0.066	5.6	LOS A	0.3	1.8	0.09	0.56	53.4
9	R2	26	0.0	0.066	6.5	LOS A	0.3	1.8	0.09	0.56	53.2
Approach		91	0.0	0.066	5.9	LOS A	0.3	1.8	0.09	0.56	53.3
West: Norrie Avenue											
10	L2	9	0.0	0.022	5.5	LOS A	0.0	0.0	0.00	0.13	57.2
11	T1	33	3.2	0.022	0.0	LOS A	0.0	0.0	0.00	0.13	58.8
Approach		42	2.5	0.022	1.2	NA	0.0	0.0	0.00	0.13	58.4
All Vehicles		352	0.9	0.123	4.4	NA	0.6	4.4	0.10	0.43	54.5

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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# ALDI Clovelly Park

Environmental Noise Assessment

S4217.18C4

July 2018

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ALDI Clovelly Park  
Environmental Noise Assessment  
S4217.18C4  
July 2018

sonus.

**Document Title** : ALDI Clovelly Park  
Environmental Noise Assessment

**Document Reference** : S4217.18C4

**Date** : July 2018

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## INTRODUCTION

An environmental noise assessment has been made of the proposed ALDI supermarket and Chemist Warehouse located on South Road, Clovelly Park.

The closest noise sensitive receivers to the proposed development are the residences located immediately north of the site, and the residences located to the south and west of the site, on the opposite side of Norrie Avenue and Wingfield Street respectively. The locations of the closest residences and the proposed development are shown in Appendix A.

The assessment considers noise levels at noise sensitive locations from activity at the proposed facility. Specifically, the following noise sources have been considered:

- General car park activity and vehicle movements;
- Delivery activity;
- Operation of mechanical plant servicing the facility; and,
- Rubbish collection.

The assessment has been based on:

- *Neilson Architects'* drawings for "ALDI CLOVELLY PARK SOUTH ROAD CLOVELLY PARK, SA" (project number "2052") dated June 2018 including "DA01.1" revision "C", "DA02.3" revision "C", "DA02.4" revision "C", "DA03.1" revision "A", "DA03.2" revision "B", "DA03.3" revision "B", "DA08.1" revision "B", and "DA08.2" revision "B";
- Continuous noise measurements conducted at the subject site from 11 to 18 July 2018;
- The understanding that:
  - Trade will not occur at the site before 7:00am or after 10:00pm;
  - Delivery trucks will access the Aldi site at any time during the day or night;
  - All delivery trucks accessing the Aldi site will have down swept (low level discharge) exhausts, attenuated compressed air release and a noise level equivalent to that measured at the ALDI Derrimut distribution centre (Victoria, 2014), and,
  - The Aldi store's refrigeration units will operate at all times during the day and night, however air conditioning, fans and the compactor will not operate before 7:00am or after 10:00pm.
  - Chemist Warehouse deliveries will be via manually unloaded vans, and will only occur during trading hours;

- Implementation of all measures described in the ALDI SA, "Delivery & Loading Procedures" (Appendix D) to minimise noise. The procedure includes measures such as turning off the refrigeration and reversing beepers prior to entering the site, minimising truck movements on site, and turning off the truck during unloading.

This assessment summarises the prediction of noise from the proposed ALDI and Chemist Warehouse, compares the predictions with the relevant criteria, and provides indicative recommendations for acoustic treatment to ensure that the noise from the proposal does not detrimentally affect the amenity of the locality.

## EXISTING ACOUSTIC ENVIRONMENT

The development is well considered from an environmental noise perspective with the carpark located adjacent to a major road corridor such that vehicle movements within the carpark have minimal adverse impact. Additionally, store buildings have been located between many of the residences and the delivery areas. This provides shielding between the existing residences and the noise sources with the highest potential for an adverse impact.

To determine the existing noise environment at the residences, the noise level has been measured continuously over a 24 hour period at an equivalent setback distance from Norrie Avenue and South Road as the residences south of the site. The noise logging location is shown in Appendix A.

The noise logging was conducted from 11 to 18 July 2018 and collected continuous 15 minute measurements during the logging period. The results of the logging are provided in Appendix B, showing the background ( $L_{90}$ ), average ( $L_{eq}$ ) and maximum ( $L_{max}$ ) noise levels during both the day time and night time assessment periods.

The results of the noise logging are typical of a residential area, dominated by distant traffic and intermittent increases in the noise level due to local vehicles on Norrie Avenue. Background noise levels during the proposed operating hours were no less than 47 dB(A) and 35dB(A) during the day (7am to 10pm) and night (10pm to 7am) periods respectively.

## CRITERIA

### Development Plan

The proposed site and residences immediately north of the site are located within a Local Centre Zone of the Marion Council Development Plan<sup>1</sup> (the Development Plan), while the other surrounding noise sensitive receivers are located within a Residential Zone of the Development Plan. The Development Plan has been reviewed and the following provisions are considered relevant to the noise assessment.

### *General Section – Interface Between Land Uses*

#### OBJECTIVES

- 1 *Development located and designed to minimise adverse impact and conflict between land uses.*
- 2 *Protect community health and amenity from adverse impacts of development.*
- 3 *Protect desired land uses from the encroachment of incompatible development.*

#### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 *Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:*  
....  
*(b) Noise*  
...
- 2 *Development should be sited and designed to minimise negative impact on existing and potential future land uses desired in the locality.*  
...
- 6 *Non-residential development on land abutting a residential zone should be designed to minimise noise impacts to achieve adequate levels of compatibility between existing and proposed uses.*

#### Noise Generating Activities

- 7 *Development that emits noise (other than music noise) should include noise attenuation measures that achieve the relevant “Environment Protection (Noise) Policy” criteria when assessed at the nearest noise sensitive premises.*
- 8 *Development with the potential to emit significant noise (e.g. industry) should incorporate noise attenuation measures that prevent noise from causing unreasonable interference with the amenity of noise sensitive premises.*

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<sup>1</sup> Consolidated 20 February 2018.



### **Environment Protection (Noise) Policy 2007**

Principle of Development Control 7 from the Development Plan references the Environment Protection (Noise) Policy 2007, which provides goal noise levels to be achieved at residences from general activity at a site and specific provisions for other activity such as rubbish collection.

The Policy is based on the World Health Organisation Guidelines to prevent annoyance, sleep disturbance and unreasonable interference on the amenity of an area. Therefore, compliance with the Policy is considered to be sufficient to satisfy all provisions of the Development Plan relating to environmental noise.

#### General Activity

The Policy provides goal noise levels to be achieved at residences based on the principally promoted land use of the Development Plan Zones in which the noise source (the development) and the noise receivers (the residences) are located. Based on the land uses and the “development” nature of the project, the following goal noise levels are provided by the Policy to be achieved at residences:

- Within the Local Centre Zone (immediately North of the site):
  - An average ( $L_{eq}$ ) 57 dB(A) during the day (7am to 10pm);
  - An average ( $L_{eq}$ ) 50 dB(A) at night (before 7am or after 10pm); and,
  - A maximum ( $L_{max}$ ) 60 dB(A) at night (before 7am or after 10pm).
- Within the Residential Zone (south and west of the site):
  - An average ( $L_{eq}$ ) 52 dB(A) during the day (7am to 10pm);
  - An average ( $L_{eq}$ ) 45 dB(A) at night (before 7am or after 10pm); and,
  - A maximum ( $L_{max}$ ) 60 dB(A) at night (before 7am or after 10pm).

When measuring or predicting noise levels for comparison with the goal noise levels of the Policy, penalties may be applied for each characteristic of tone, impulse, low frequency and modulation of the noise source, however this must be considered within the context the existing acoustic environment. In this circumstance, a penalty has not been applied to the site given that there is significant noise from existing traffic on South Road generating higher noise levels than vehicle movement at the site.

Rubbish Collection

The Policy deals with rubbish collection by limiting the collection hours to the least sensitive period of the day. Division 3 of the Policy requires rubbish collection to only occur between the hours of 9am and 7pm on Sunday or public holiday, and between 7am and 7pm on any other day, except where it can be shown that the maximum ( $L_{max}$ ) noise level from such activity is less than 60 dB(A).

## ASSESSMENT

### General Activity

#### Carpark Activity and Vehicle Movements

The noise from car park activity and vehicle movements on the site has been predicted at the nearby residences, based on noise measurements conducted at similar sites. These measurements account for the noise associated with:

- car park activity such as people talking as they vacate or approach their vehicles, the opening and closing of vehicle doors, vehicles starting, vehicles idling, and vehicles moving into and accelerating away from their park position; and,
- typical vehicle movement through a carpark.

Based on the predictions, the daytime requirements of the Policy will be achieved at all noise sensitive locations in the vicinity of the development with the following acoustic treatments:

- Construct 2.4m high “Colorbond” fences as shown in **PURPLE** in Figure 1 such that they are sealed airtight at all junctions, including at the ground and at the building wall; and,

#### Mechanical Plant

As is typical at the development application stage, the proposed cool room and air conditioning plant units have not yet been designed or selected. The assessment of the mechanical plant has therefore been based on previous noise measurements and procured data at similar facilities. The subsequent overall sound power level data for the following mechanical plant components are summarised in Appendix C:

- 3 x Daikin RX(Y)Q18TY1A air conditioning units; and,
- 3 x condenser units;

Based on the mechanical plant outlined above, the following acoustic treatments likely to be required in order to achieve the requirements of the Policy:

- Locate all mechanical plant within the proposed areas and construct the proposed barriers for the extent shown in **RED** in Figure 1 around the roof mounted mechanical plant to a minimum height of 1.0m above the tallest unit. The barrier should be constructed from a solid material such as sheet steel “Colorbond” or similar, and such that it is air tight at all joints. It is noted that a small gap may be required at the roof deck for drainage.

The noise level and any acoustic treatment associated with mechanical plant should be reviewed during the detailed design phase, should the final equipment selections have different sound power levels or should a different number of units be proposed to those specified within this report.

#### Deliveries

The noise from deliveries to the Aldi supermarket has been predicted based on specific noise measurements of ALDI trucks for each of the following processes, conducted in accordance with the ALDI SA "Delivery & Loading Procedures" (refer Appendix D) which include turning off the truck refrigeration equipment and reversing beepers:

- driving into a site;
- reversing into a loading dock and stopping the engine;
- unloading, and;
- starting the engine, accelerating and driving out of the site.

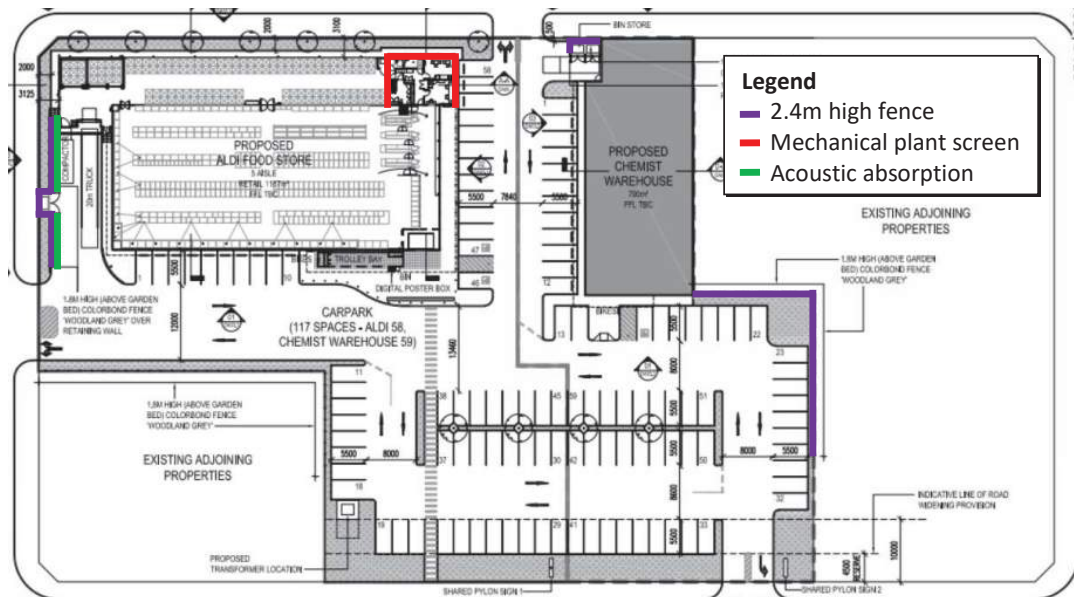
The noise from deliveries to Chemist Warehouse has been predicted based on previous noise measurements at a similar facility including:

- Delivery vans driving into a site, reversing into loading areas, being manually unloaded, and driving out of the site; and,
- Operation of refrigeration units on delivery vehicles.

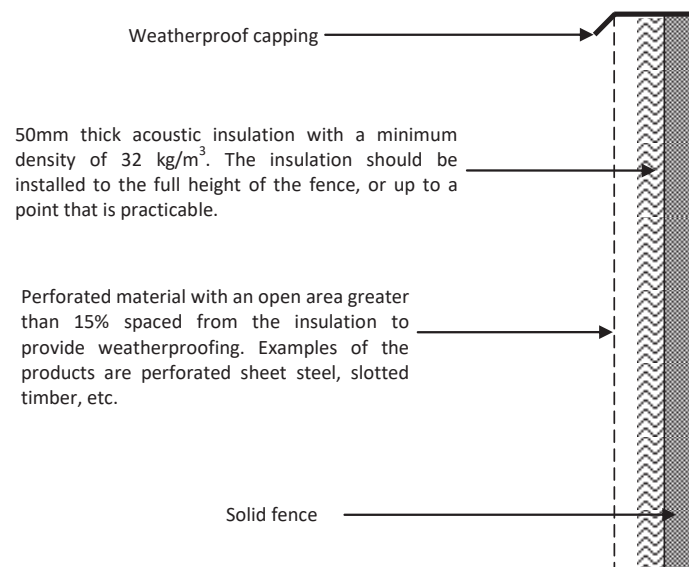
The predictions of noise from deliveries have been based on a single ALDI delivery in any 15-minute period of the day or night, and a single Chemist Warehouse delivery in any 15-minute period of the day. In order to achieve the requirements of the Policy, it is recommended that the following acoustic treatment be incorporated into the site:

- Construct 2.4m high "Colorbond" fences as shown in **PURPLE** in Figure 1 such that they are sealed airtight at all junctions, including at the ground and at the building wall; and,
- Install 50mm thick acoustic insulation with a minimum density of 32 kg/m<sup>3</sup> to the ALDI loading area fence for the extent shown in Figure 1 as **GREEN**. The insulation should extend for the full practicable height of the fence and be should be installed in accordance with Figure 2. Other proprietary materials such as "Pyrotek Reapor" can be used in lieu of the detailed construction.

**Figure 1: Site plan and recommended acoustic treatments.**



**Figure 2: Fence absorption construction detail.**



#### Compactor

The noise from compactor operation has been determined based on predictions provided in the Marshall Day Acoustics report "LT001 2010139 addendum ALDI Geelong West revised site plan" prepared for ALDI Geelong West.

Based on the predictions and the inclusion of the acoustic treatments for deliveries, the noise from compactor operation will achieve the daytime criteria of the Policy with no additional acoustic treatment.

#### Combined Noise Levels

The predicted noise level from all noise sources at the facility operating concurrently has been determined and with the inclusion of the acoustic treatments detailed in this report will achieve the relevant requirements of the *Environment Protection (Noise) Policy 2007* during the day and night.

Additionally, the maximum noise levels from ALDI deliveries during the night have been predicted to achieve the 60 dB(A) criterion of the Policy.

#### **Rubbish Collection**

To ensure there is not unreasonable interference from noise from rubbish collection, it is recommended that the hours of rubbish collection from the site be restricted to the hours of Division 3 of the *Environment Protection (Noise) Policy 2007*. That is, only between the hours of 9:00am and 7:00pm on a Sunday or public holiday, and 7:00am and 7:00pm on any other day.

## CONCLUSION

An environmental noise assessment has been made of the proposed ALDI supermarket and Chemist Warehouse located on South Road, Clovelly Park.

The assessment considers noise at nearby residences from car park activity, vehicle movements, delivery activity, mechanical plant, and rubbish collection within the context of the surrounding acoustic environment to ensure the proposal does not adversely impact on the amenity of the locality.

The predicted noise levels from the development will achieve the relevant requirements of the *Environment Protection (Noise) Policy 2007* subject to the treatments in this report, comprising;

- Specific fence heights and constructions;
- Installation of acoustic absorption on the ALDI delivery fence;
- Screening of ALDI mechanical plant; and,
- Restricting the times for rubbish collection.

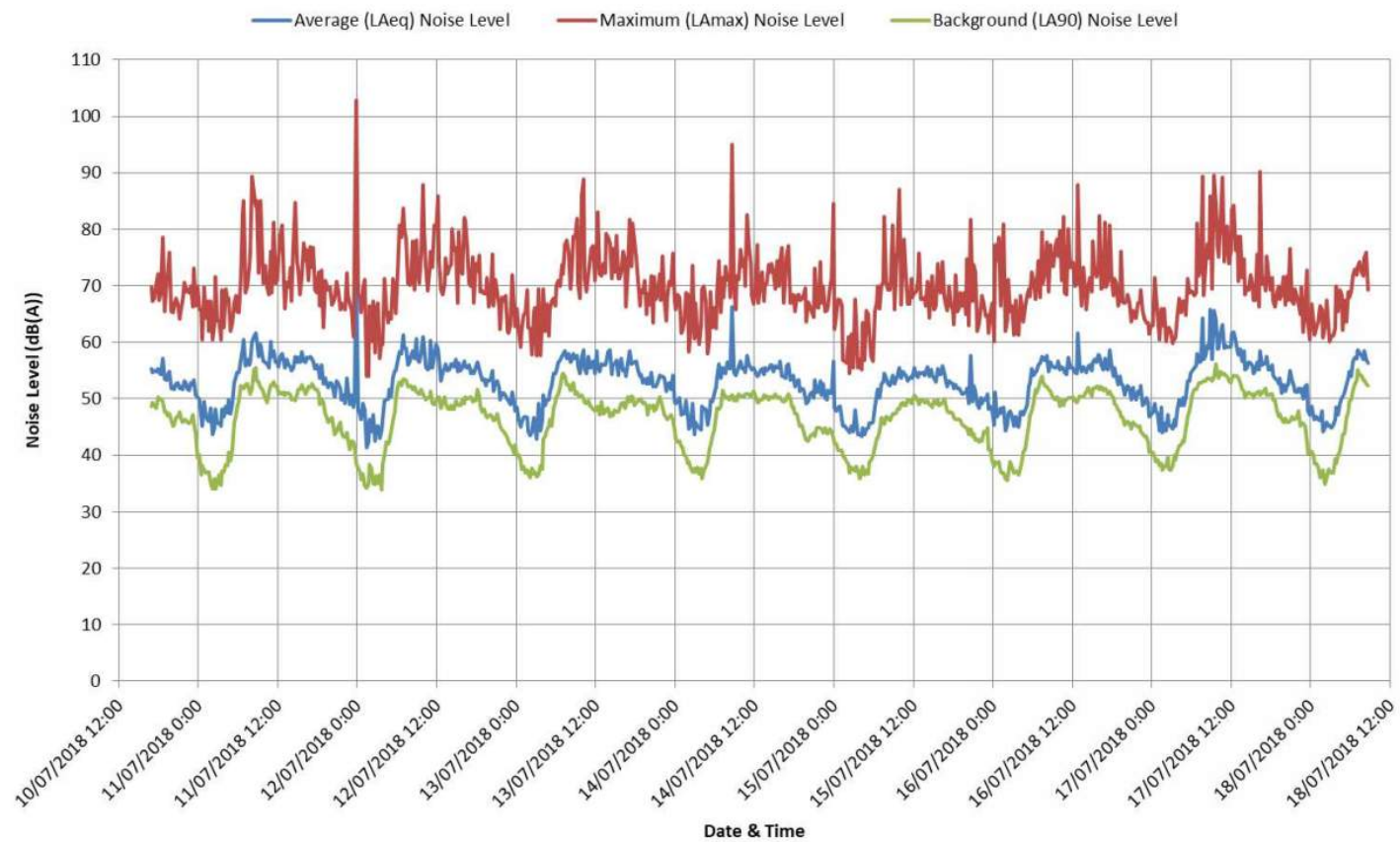
It is therefore considered that the facility has been designed to *minimise adverse impact and conflict between land uses, avoid unreasonable interference on amenity, and will not detrimentally affect the locality by way of noise*, thereby achieving the relevant provisions of the Development Plan related to environmental noise.



**APPENDIX A: Development Site Locality.**



**APPENDIX B: Existing Noise Levels 10 to 18 July 2018.**



**APPENDIX C: Noise Source Sound Power Level Data.**

Equipment/Activity		Sound Power Level
<b>Carpark Activity and Vehicle Movements</b>	Car movement	82 dB(A)
	General carpark activity	77 dB(A)
<b>Mechanical Plant</b>	Condenser	86 dB(A)
	Daikin RX(Y)Q18TY1A	70 dB(A)
<b>Deliveries</b>	Van movement	94 dB(A)
	Van unloading	86 dB(A)
	Truck forward	98 dB(A)
	Truck reverse	97 dB(A)
	Refrigeration unit	91 dB(A)
<b>Compactor</b>	Compactor	92 dB(A)

APPENDIX D: ALDI Delivery & Loading Procedures

**ALDI SOUTH AUSTRALIA**



**DELIVERY & LOADING PROCEDURES**

**ALDI DELIVERY & LOADING PROCEDURES**

- ALDI own, operate and control all of its supply and logistics via its purpose built Distribution Centre. South Australia's Distribution Centre is under construction in Regency Park.
- ALDI's supply chain and logistics operates on a palletised system. All products are delivered to our Distribution Centre on pallets. The same product is then loaded onto delivery trucks; delivered to the store and in many cases the same pallet is then located on the retail floor. This streamlined system of operation enables ALDI to position itself as a discount supermarket quite differently to its competitors.
- With all ALDI supplies delivered directly to our Distribution Centre, ALDI then undertake consolidated store deliveries to each store via its own fleet of trucks/trailers and ALDI employed drivers.
- ALDI Stores do not undertake "night fill" or store replenishment outside retail trading hours therefore the stores close and staff leave very shortly after retail close. Deliveries are however enabled to undertaken throughout a full 24 hour period to any store given the Distribution Centre operates 24/7 and our own truck and drivers are able to access and unload to a store without the store being open or trading.
- With the dedicated ALDI supply chain direct from our Distribution Centre to a store, only a maximum of two ALDI deliveries are undertaken within a 24 period to each store. The only exception is one bread delivery from a bakery supplier, which is undertaken in a small rigid truck, once per day.
- The ALDI truck movement, on any site, typically takes between 1 to 2 minutes, with the truck moving onto the site, stopping, reversing and engaging with the purpose built loading dock.
- The prime mover is always turned off during unloading.
- Given the palletised system and dedicated dock connection the total delivery period is on average 15 minutes from the moment the truck is docked. The unloading is undertaken by only one person being the ALDI truck driver.
- All products are unloaded from within the trailer directly inside the building using a manually operated pallet jack. (No forklifts are used and no external activity occurs outside the truck). The trailers are sealed and connected to the building via a dedicated dock leveller and dock curtain.
- The ALDI prime movers and trailers are purpose built to ALDI specifications.
  - All trailers are built with a reversing camera which is connected to a driver display in the cabin of the prime mover.
  - All trailers are also able to be controlled from within the cabin to turn-off the reversing beepers (if required) and the refrigeration units (if required).
- All ALDI loading docks are fitted with motion sensors and automatic lighting to ensure that night time deliveries benefit from appropriate surveillance including safe reversing manoeuvres without reversing beepers (where required).
- All drivers are briefed and aware of each site and any site specific circumstances or restrictions that are applicable for that store.
- If an acoustic assessment requires any specific noise management then either or both the refrigeration and reversing beepers can be turned off including any requirement to do so whilst transiting past any noise sensitive areas.
- As a result of the above, where other retailers cannot, ALDI consistently can comply with any prescribed operational or acoustic requirements of either the Environmental Protection Authority and any relevant local statutory requirements when performing night time deliveries.
- Coupled with the above where additional on-site acoustic mitigation measures are required/recommended ALDI will construct these to further alleviate and mitigate any potential noise interface issues i.e. Acoustic screens, fences and/or gates.

27 November 2018

Mr Nigel Uren  
ALDI Stores  
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Regency Park SA 5010  
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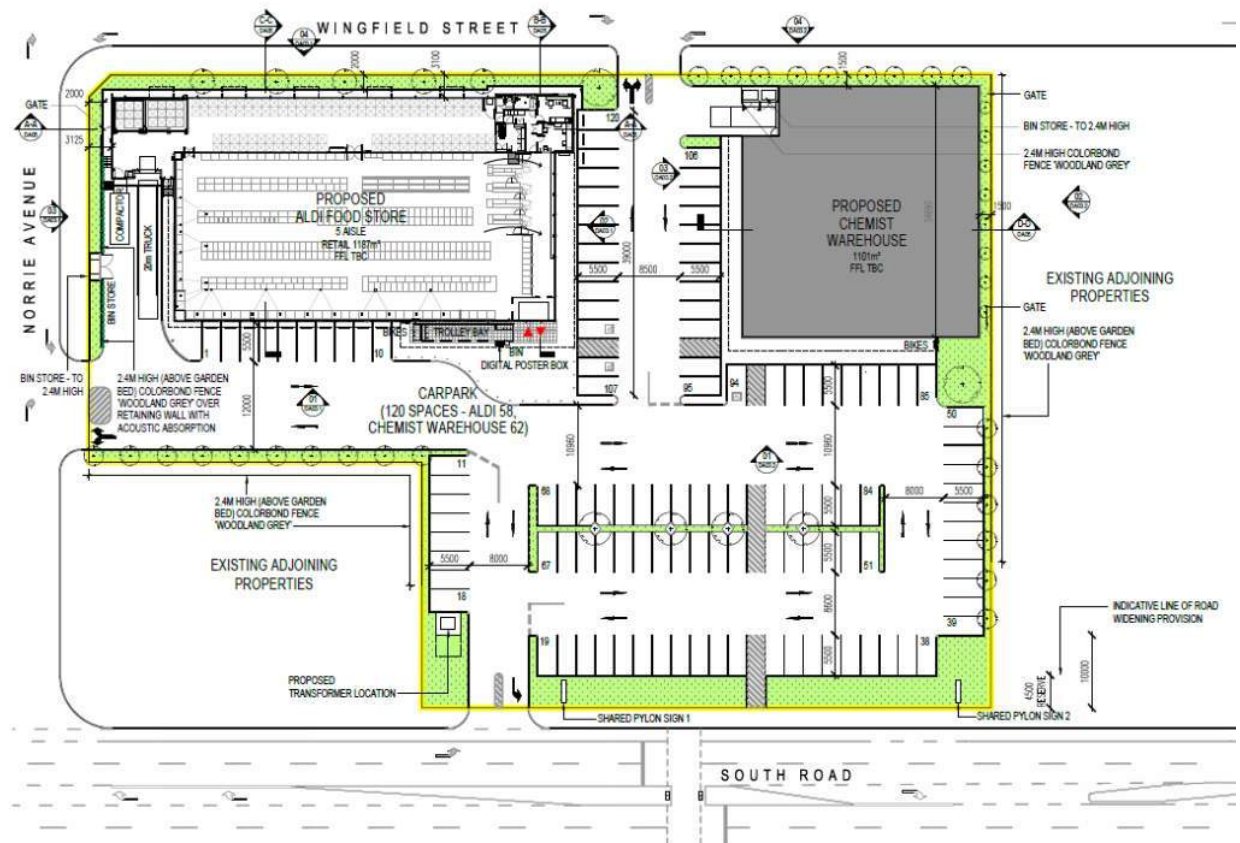
Our ref: ALDI Clovelly Park  
SMP  
Revision: 4

## ALDI and Chemist Warehouse, Clovelly Park - Stormwater Management Plan

### 1 Introduction

ALDI commissioned Drew Rudd Engineers to prepare a Stormwater Management Plan (SMP) for a proposed development on South Road, Clovelly Park. The proposed development is located between Norrie Avenue to the south, Wingfield Street to the west and South Road to the east and will consist of a new ALDI store and car parking facilities and a new Chemist Warehouse store and car parking facilities. The boundary (red dashed line) between the Aldi and Chemist Warehouse sites is shown in Image 1.

This stormwater management plan summarises how stormwater will be managed for the proposed development, as shown in Image 1 and Figure 1, Appendix A.



**Image 1 – Proposed Development**



## 2 Existing Stormwater System and Council Requirements

### 2.1 Existing Stormwater System

There is an existing single Side Entry Pit (SEP) on the western side of South Road, immediately to the south of the pedestrian crossing. The invert level of the existing SEP is 31.36 m AHD and the watertable level is 31.85 m AHD.

It is difficult to determine how much of the existing site discharges to South Road. The site was visited on the 7 June 2018 to assess the existing catchment discharging to South Road. There are four known existing points of discharge to South Road. The existing catchment area discharging to South Road is shown in the blue outline in Image 2. It is estimated that approximately 1,260 m<sup>2</sup> of roof area discharges to South Road. The remainder of the site discharges to Norrie Avenue and Wingfield Street. Stormwater runoff discharges to the kerb, as there is no existing underground stormwater system in Norrie Avenue and Wingfield Street.



**Image 2 – Existing Development Catchment Discharging to South Road**

## 2.2 DPTI and Council Stormwater Management Requirements

DPTI have requested that the 100-year Average Recurrence Interval (ARI) post development flow rate discharging to South Road does not exceed the existing conditions 100-year ARI flow rate. The future South Road upgrade will require a 10 m wide strip of land along the boundary with South Road. DPTI have requested that no infrastructure is provided within the 10 m wide strip. The 10 m wide strip of land is shown in Image 1 and Figure 1, Appendix A.

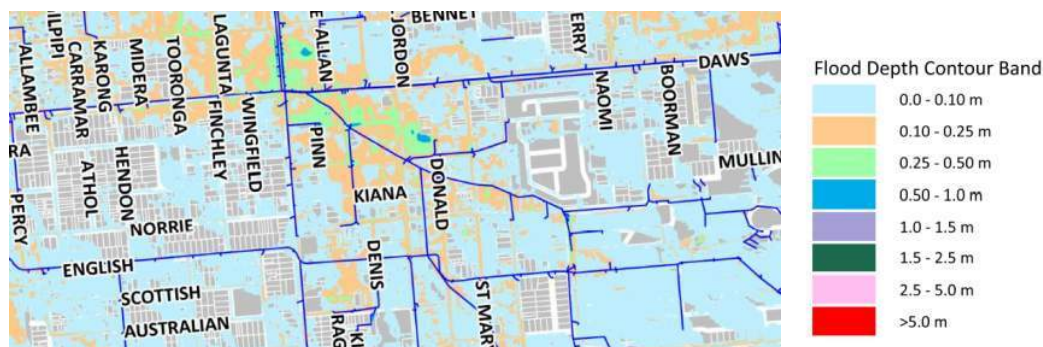
Council's stormwater requirements for industrial and commercial developments is shown in Appendix B. Council also requested that:

- The design should include rain gardens in addition to gross pollutant, sediment and oil removal to help achieve EPA requirements.
- No stormwater discharge to the kerb is to exceed 20 L/s.

## 3 Proposed Stormwater Management Strategy

### 3.1 Adopted Floor Levels

Preliminary floodplain mapping of the Sturt River catchment, prepared by Southfront, shows that the southern residential portion of the site is inundated in a 100-year ARI storm event. The 100-year ARI flood inundation map is shown in Image 3. Image 3 shows that there is 0 to 0.1 m of inundation over the southern residential portion of the Aldi site. This area of flooding corresponds to the southern end of the proposed Aldi building. The existing surface level at the southern end of the proposed Aldi building is 32.60 m AHD. Assuming a flood depth of 100 mm and a freeboard of 300 mm, this equates to a minimum floor level of 33.00 m AHD. Therefore, a minimum floor level of 33.00 m AHD has been adopted for the proposed Aldi building.



**Image 3 – Existing Conditions 100-year ARI Floodplain Mapping**

### 3.2 Stormwater Management Strategy – Aldi Site

The proposed stormwater management strategy is shown in Figure 1, Appendix A and includes:

- Stormwater runoff from the Aldi building roof will discharge to an underground detention basin in the Aldi carpark. The underground detention storage will reduce peak 5 and 100-year ARI flow rates from the Aldi roof area. A high early discharge arrangement will be used for the underground detention storage. The High early discharge pit will be located at the north west corner of the building with overflows from the high early discharge weir being directed to the



underground detention storages at the north east and east ends of the building. Stormwater will be gravity fed to the kerb on Wingfield Street.

- Stormwater runoff from the pathway on the western side of the Aldi building will discharge to the landscaped area along the western boundary. This will promote infiltration of stormwater in the vegetated area.
- Three low points will be provided in the proposed Aldi carpark. The low points will provide detention storage on the surface of the car park. A Rocla First Defense – High Capacity separator will be provided prior to discharging to the existing side entry pit on South Road. The First Defense - High Capacity separator has internal components designed to remove and retain gross debris, total suspended solids and hydrocarbons.
- The proposed driveway and car parking area between the proposed Aldi and Chemist Warehouse buildings will discharge to a single side entry pit in the Chemist Warehouse car park before discharging to a Rocla First Defense – High Capacity separator. Stormwater will discharge to the Wingfield Street kerb.

### 3.3 Stormwater Management Strategy – Chemist Warehouse Site

A vegetated basin will be provided in the north west corner of the Chemist Warehouse car parking area to provide detention storage and stormwater quality improvement for stormwater runoff from the proposed car park and building roof. Stormwater runoff from the vegetated basin will discharge to 35 m of DN750 mm pipe, which will provide detention storage prior to discharging to the kerb on Wingfield Street. Subsoil drainage pipes can be provided in the vegetated basin to reduce the risk of waterlogging.

## 4 Hydrology and Hydraulics

### 4.1 Existing Conditions Flow Rates

The proposed Aldi site area is approximately 5,452 m<sup>2</sup> and the proposed Chemist Warehouse site area is approximately 3,755 m<sup>2</sup>. Both sites have a mix of existing residential and commercial areas. A summary of the existing residential and commercial areas is shown in Table 1. The residential areas on each site are shaded green in Image 4.

**Table 1 – Existing Aldi and Chemist Warehouse Residential and Commercial Areas**

Site	Existing Residential Area (m2)	Existing Commercial Area (m2)
Aldi	2,427	3,025
Chemist Warehouse	1,581	2,174

Council's existing conditions flow rate calculation requirements include:

- Any Industrial/Commercial Redevelopments in Existing Residential Zones must conform to:
  - 5 Year ARI      Runoff Coefficient      0.25
  - 100 Year ARI      Runoff Coefficient      0.45

- Any Industrial/Commercial Redevelopments in Existing Industrial/Commercial Zones must conform to:
  - 5 Year ARI              Runoff Coefficient              0.65
  - 100 Year ARI          Runoff Coefficient              0.85

The 5 and 100-year ARI, 10 minute storm duration rainfall intensities are 59.1 mm/h and 125 mm/h, respectively.

The Rational Method was used to calculate the 5 and 100-year ARI existing conditions flows for each site, which are shown in Table 2.

**Table 2 – Existing Aldi and Chemist Warehouse Sites 5 and 100-year ARI Flow Rates**

Site	Existing 5-year ARI Flow Rate (L/s)	Existing 100-year ARI Flow Rate (L/s)
Aldi	42	127
Chemist Warehouse	30	89
Total	72	216



**Image 4 – Existing Residential and Commercial Areas**

## 4.2 Post Development Catchment Plan

Catchments for each inlet pit are shown in Figure 1, Appendix A.

A summary of the post development catchment areas and percentage impervious fractions is shown in Table 3. The catchment IDs are shown in Figure 1, Appendix A.

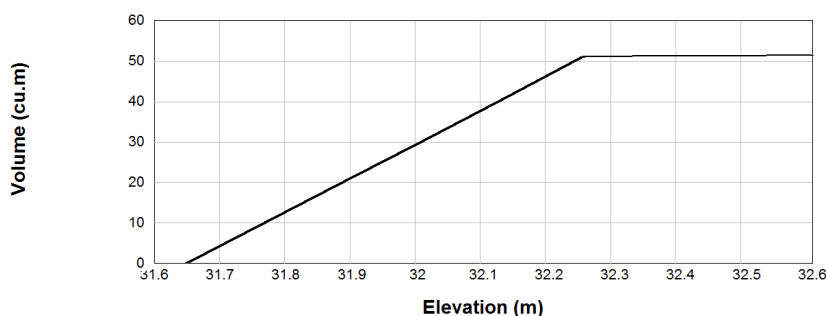
**Table 3 –Post Development Catchment Areas and % Impervious**

Catchment ID	Total Area (m <sup>2</sup> )	Pervious Area (m <sup>2</sup> )	% Impervious
cAldiRoof	1922	0	100%
cChemBas	1919	312	84%
cChemBas2	55	55	0%
cChemRoof	1233	0	100%
cGIP1	931	158	83%
cGIP2	722	91	87%
cGIP3	785	72	91%
cGIP4	303	39	87%
cSEP1	760	0	100%
cWing	263	61	77%
cWing3	248	157	37%
cSouth	77	0	100%

## 4.3 DRAINS Modelling

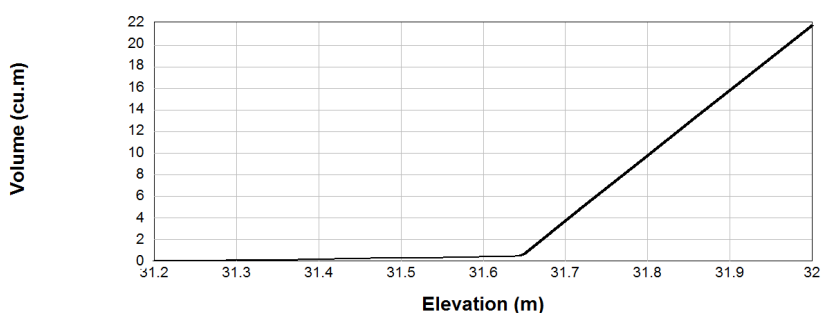
A DRAINS model (hydrologic and hydraulic modelling software) was established to calculate the 5 and 100-year ARI post development peak discharges from the site. The following data was input and assumptions made to establish the post development DRAINS model:

- The post development site land use is shown in Table 3.
- Paved and grassed area depression storages equal 1 mm and 10 mm respectively.
- Soil type equals 2 (moderate infiltration rates) to account for rainfall losses in landscaped areas.
- Antecedent Moisture Condition (AMC) equals 2.
- The Aldi underground detention basin elevation versus storage relationship is shown in Image 5.



**Image 5 – Aldi Underground Basin Elevation Versus Storage Relationship**

- The Chemist Warehouse vegetated basin elevation versus storage relationship is shown in Image 6. The elevation versus storage relationship includes ponding in the car park up to the overflow level of 32.25 m AHD.



**Image 6 – Chemist Warehouse Vegetated basin Elevation Versus Storage Relationship**

- Stormwater runoff from the loading dock will be pumped to GIP 3, shown in Figure 1, Appendix A at a rate of 4 L/s. Temporary ponding can be expected in the loading dock in storm events exceeding a 5-year ARI.
- The Aldi roofwater collection system will discharge to a high early discharge pit with a DN65 mm orifice and a DN150 mm outlet pipe at an invert level of 31.65 m AHD. The weir level in the high early discharge pit has been set at 32.45 m AHD. The weir length is 900 mm, which is the internal dimension of the high early discharge pit.
- Chemist Warehouse vegetated basin has a DN150 mm outlet pipe with a DN100 mm orifice at an invert level of 31.20 m AHD. The basin will overflow to the area on the northern side of the Chemist Warehouse building at a level of 31.95 m AHD. Overflows from the vegetated basin will be captured by the grated inlet pit at the upstream end of the DN750 mm detention pipe. The DN750 mm detention pipe will have a DN115 mm orifice at an invert level of 31.1 m AHD. A DN150 mm pipe will discharge to the kerb on Wingfield Street. Overflows from the western end of the DN750 mm detention pipe will be via a 1.5 m wide weir at the top of the retaining wall set at a level of 31.90 m AHD. The top of the retaining wall on the north and west sides of the Chemist Warehouse building will be set at a level of 32.10 m AHD to reduce the risk of water overflowing onto the properties to the north.
- GIP1 has a DN125 mm orifice with a DN225 mm outlet pipe at an invert level of 31.42 m AHD.

- GIP2 has a DN140 mm orifice with a DN225 mm outlet pipe at an invert level of 31.49 m AHD.
- GIP3 has a DN120 mm orifice with a DN225 mm outlet pipe at an invert level of 31.65 m AHD.

#### 4.3.1 Post Development DRAINS Modelling Results

The post development DRAINS model was simulated for a range of storm durations for the 5 and 100-year ARI storm events, using Bureau of Meteorology, 2016 Intensity Frequency Duration data and temporal patterns. The post development 5 and 100-year ARI flow rates from the ALDI and Chemist Warehouse sites are shown in Table 4. The results in Table 4 show that:

- No 5-year ARI discharge to the kerb is in excess of 20 L/s.
- The 5-year ARI post development flow rate is equal to the existing 5-year ARI allowable flow rate.
- The 100-year ARI post development flow rate is 89 L/s less than the existing 100-year ARI allowable flow rate.

**Table 4 – 5 and 100-year ARI Post Development DRAINS Modelling Results**

Point of Discharge	5-year ARI Flow (L/s)	100-year ARI Flow (L/s)
South Road Existing SEP (Aldi)	25	24
Entrance off South Road (Aldi)	2	4
Wingfield Street 1 (Aldi)	8	8 (Pipe) + 13 (Overland) = 21
Wingfield Street 2 (Shared)	14 (Pipe) + 4 (Overland) = 18	24 (Pipe) + 15 (Overland) = 39
Wingfield Street 3 (Aldi)	2	4
Wingfield Street 4 (Chemist Warehouse)	17	24 (Pipe) + 32 (Overland) = 56
Total =	72	148

The DRAINS model layout and 5 and 100-year ARI post development DRAINS modelling results are shown in Appendix C.

The existing conditions 100-year ARI peak flow rate to South Road was calculated to be 61 L/s. The post development DRAINS modelling results, in Table 4, show that the peak 100-year ARI flow rate to South Road is 28 L/s.

A summary of the depth of ponding in the underground detention basins, Chemist Warehouse vegetated basin, Aldi car park and loading dock for the 5 and 100-year ARI storm events is shown in Table 5. The 100-year ARI extent of ponding in the Aldi car park is shown in Figure 1, Appendix A.

**Table 5 – 5 and 100-year ARI Post Development Ponding Levels in Basins and Car Parks**

Location	Surface Level (m AHD)	5-year ARI Ponding Level (m AHD)	100-year ARI Ponding Level (m AHD)
Aldi Underground Basin	32.60 (overflow level)	31.72	32.62
Chemist Warehouse Basin	31.95 (overflow level)	31.90	32.02
DN750 mm Detention Pipe	31.90 (overflow level)	31.56	31.96
GIP1	32.35	32.38	32.46
GIP2	32.32	32.42	32.47
GIP3	32.50	32.57	32.65
GIP4	31.75	31.45	31.88

#### 4.4 Stormwater Quality Improvement

Grated inlet pits will be used throughout the site to reduce the risk of gross pollutants entering the underground stormwater pipe system and increasing the risk of blocking orifices.. Rocla First Defense – High Capacity separators will be provided prior to discharging to the existing side entry pit on South Road and to the kerb on Wingfield Street, to treat the shared driveway and car parking areas. The First Defense - High Capacity separator has internal components designed to remove and retain gross debris, total suspended solids and hydrocarbons.

The Chemist Warehouse vegetated basin will treat stormwater runoff from the roof and surrounding car park area.

A MUSIC model was established to simulate the proposed stormwater quality improvement strategy. MUSIC nodes provided by Rocla were used for the First Defense treatment devices. The MUSIC model was simulated using Adelaide, 6-minute rainfall data. The MUSIC model layout and treatment train effectiveness are shown in Appendix D.

The MUSIC model results compared against standard stormwater quality targets are shown in Table 6.

**Table 6 – MUSIC Modelling Results Compared to Standard Targets**

Pollutant	Standard % Reduction Target	Actual % Reduction
Total Suspended Solids (TSS)	80	78.4
Total Phosphorus (TP)	60	46.2
Total Nitrogen (TN)	45	22.4

The MUSIC modelling results show that the TSS target reduction is close to the target, however the TP and TN target reductions have not been achieved. Constraints placed on the site by future road widening works associated with the South Road upgrade limit opportunities to meet the TP and TN target

reductions. We consider that the stormwater quality improvement measures provided in the proposed stormwater management strategy, considering the site constraints, are adequate.

#### 4.4.1 Vegetation Selection

The Chemist Warehouse vegetated basin should be planted extensively; at a density of 5 plants/m<sup>2</sup>, depending on the growth form. Shrubs and trees should be planted at a density of < 1 plant/m<sup>2</sup>. A higher density of planting should be provided at locations where stormwater enters the vegetated basin. Rock ballast will also be required at inflow points to the vegetated basin to reduce the risk of erosion. Some typical plant species that are high in nitrogen removal are shown in Image 7. Mulch is not recommended as it has a tendency to float and block outlets. Subsoil drains will be provided in the vegetated basin to reduce the risk of waterlogging.

Objective	Effective
Nitrogen removal	<ul style="list-style-type: none"> <li>• <i>Baumea juncea</i></li> <li>• <i>Baumea rubiginosa</i></li> <li>• <i>Carex appressa</i></li> <li>• <i>Carex tereticaulis</i></li> <li>• <i>Ficinia nodosa</i></li> <li>• <i>Goodenia ovata</i></li> <li>• <i>Juncus amabilis</i></li> <li>• <i>Juncus flavidus</i></li> <li>• <i>Juncus pallidus</i></li> <li>• <i>Juncus subsecundus</i></li> <li>• <i>Melaleuca ericifolia</i></li> <li>• <i>Melaleuca incana</i></li> <li>• <i>Melaleuca lateritia</i></li> </ul>

**Image 7 – High Nitrogen Removal Plant Species (CRC for Water Sensitive Cities)**

## 5 Maintenance

The following inspection and maintenance measures are recommended to maintain the integrity of the proposed stormwater system:

- Inspection of sediment, gross pollutant and oil build up in the Rocla First Defense units, every 3 months for the first year to determine the cleaning frequency. Cleaning and maintenance should also be undertaken in accordance with the manufacturer's specifications.
- Inspection of all pits to check that orifices are not blocked. Pits should be inspected every 3 months or after rainfall events totalling 20 mm or more to ensure that there are no blockages or sediment build up restricting the performance of the orifices.
- Inspection and removal of gross pollutants at grated inlet pits.
- Inspections to check for an excess of sediment, erosion or boggy conditions in the vegetated basin. Excess sediment should be removed to as close to original design levels as possible and



erosion should be repaired by filling with sandy loam material and rock ballast if erosion continues to be a high risk.

- Regular pruning and weeding to remove any foreign species and any diseased plantings, to promote new growth.
- Monitor vegetation closely during the first year to ensure plants are becoming established and have sufficient water. Some irrigation may be required to establish new plants. Dead plants should be replaced with new plants.

Routine maintenance inspections should be undertaken every month and/or after rainfall events totalling 20 mm or more.

## **6 Conclusion**

A stormwater management strategy has been developed for the proposed ALDI and Chemist Warehouse developments. The stormwater management strategy:

- Reduces the post development 5 and 100-year ARI flows to the existing 5 and 100-year ARI flows from the site.
- Satisfies DPTI requirements to limit 100-year ARI post development flows to existing conditions flow rates discharging to South Road.
- Satisfies Council's requirements to limit 5-year ARI post development discharges to the kerb to less than 20 L/s.
- Reduces the risk of gross pollutants, sediment, nutrients and oil discharging from the site by using grated inlet pits throughout the site, installing Rocla First Defense sediment and oil removal devices and providing a vegetated basin to treat stormwater runoff from the Chemist Warehouse roof and car park areas.

If you have any queries regarding this report please contact the undersigned on 0422 150 775.

Yours faithfully

Drew Rudd Engineers



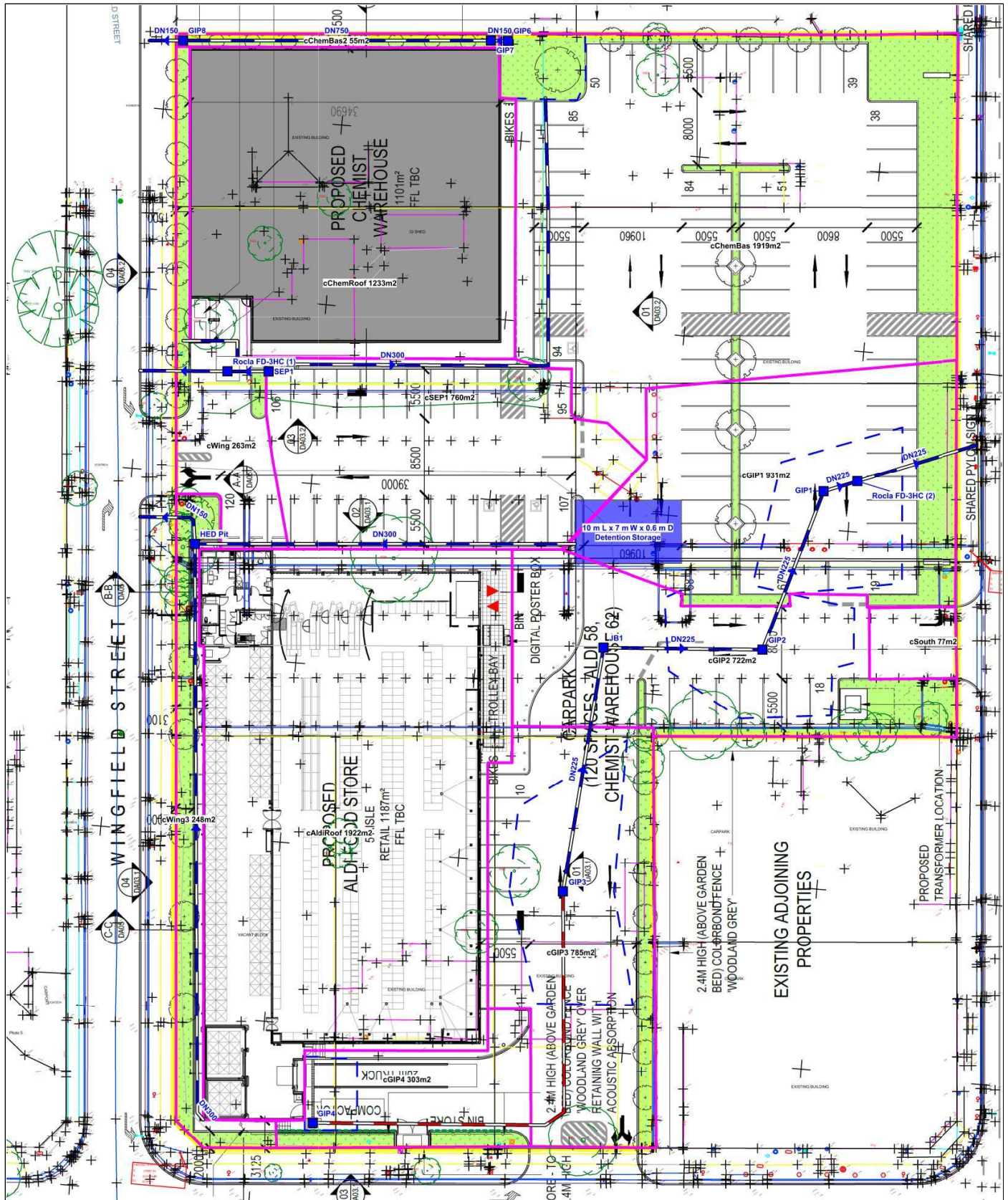
**Dean Nobbs**

Senior Civil Engineer  
0422 150 775

# Appendix A – Figures

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Figure 1 – Catchment Plan  
Site Works Plan



LEGEND

Proposed SW Pit Proposed Rising Main Proposed SW Pipe Catchments 100yr Extent of Ponding Aldi Roof Detention

1:400  
Map projection: Universal Transverse Mercator  
Horizontal datum: Geocentric Datum of Australia 1994  
Grid: Map Grid of Australia, Zone 54



Aldi  
Clovelly Park  
Catchment Plan

job no. ADL0192  
rev no. E

Figure 01

27 | November 2018





## **Appendix B – Council Requirements**

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### *Guidelines for Stormwater Requirements – Industrial and Commercial Developments*

# CITY OF MARION INFRASTRUCTURE DEPARTMENT

## Guidelines for Stormwater Requirements – Commercial/Industrial Development

Revision Date : 15 December 2006

Stormwater drainage calculations are to be carried out by a qualified engineering consultant as per the guidelines given in Australian Rainfall and Runoff –Volume 1  
The following methods may be adopted :

Rational Method      Time-Area Runoff Method      Modelling Programs such as ILSAX or DRAINS

For Development North of Seacombe Road, a detention/retention system must be provided to cater for the balance between the proposed discharge from the commercial/industrial development and the allowable discharge using Council's coefficients as per Note 1 below.

**IMPORTANT NOTE :** The Detention System must incorporate an above ground or below ground Detention Tank. The Car Park can only be used as a secondary form of detention

**REFERENCE :** This guideline is to be read in conjunction with the Marion (City) Development plan Consolidated 10 November 2005 and Council's Brochure – "Stormwater Detention"

### INFORMATION TO BE SUPPLIED WITH DEVELOPMENT APPLICATION INCLUDE A SITE PLAN AND STORMWATER CALCULATIONS

#### **SECTION A : SITE PLAN**

- (a) A Site Plan to a scale of 1:100 or 1:200 indicating floor plans, pervious and impervious areas.
  - (b) Provide as a minimum the following Reduced levels on the site plan :
    - Floor Level of the proposed Buildings.
    - Design Surface Level across the front property boundary.
    - Design Surface Level of the driveway/s at the front property boundary.
    - Invert Level of the Kerb and Water Table.
  - (c) Details of the surface flow path and details of pipes, channels and sumps with respective sizes, grades, invert and surface levels.
  - (d) Locations of the discharge points at the kerb and water table
  - (e) If approval is received to connect to existing DTEI/Council Side Entry Pits or Pipes, indicate the location and a detail drawing of the connection point.
  - (f) Locations of the detention/retention tanks including above ground or below ground & dimensions.
  - (g) Locations of the proposed orifice size for the detention/retention system and its relative location to the detention tank (with respect to height above or below ground)
  - (h) Locations of the interception device (gross pollutant trap) for oil, grease, litter, sediment etc
- If Developer proposes any Secondary Surface Storage in a Carpark Area, include the following on the site plan:**
- (i) Provide Reduced Levels on the perimeter of the carpark
  - (j) Maximum Ponding Depth at the low point of the carpark

**IMPORTANT NOTE :** Failure to provide all information may delay the processing of the application

## SECTION B : STORMWATER CALCULATIONS

It is the responsibility of the engineering consultant to provide legible and detailed calculations. Each formulae, along with all constants, variables and substitutions used must be clearly written and defined. Council officers should not be required to decipher or interpret shorthand calculations. If these are presented, the consulting engineer will be requested to resubmit the calculations.

The following detailed calculations are to be forwarded:

- (a) Allowable Council Discharge Calculation using Council's coefficients for the proposed development for both the 5 Year ARI and 100 Year ARI
- (b) Post Site Development Discharge Calculations as per Australian Rainfall and Runoff –Vol 1, for the proposed development for both the 5 Year ARI and 100 Year ARI for various storm durations
- (c) Determination of the detention/retention system including above ground or below ground, make, model, size, dimensions and capacity to cater for the balance of the discharge to remain on site
- (d) Determination of the orifice size for the detention/retention system
- (e) Details of the interception device (gross pollutant trap) for oil, grease, litter, sediment etc
- (f) Details of the discharge points at the kerb and water table including location and flow rate at each
- (g) Type of jointing method when discharging to existing DTEI/Council Side Entry Pits or Pipes
- (h) For secondary surface storage in a carpark, please provide prism calculations necessary for determining the potential surface storage in the carpark
- (i) The following parameters as per the numbered points are to be used in the calculations:

### 1. Allowable Council Stormwater Discharge

To cater for Council's existing drainage infrastructure, Stormwater runoff must be controlled from the total site area to the flows determined using the following coefficients.

#### 1.1 Site Time of Concentration

To determine the allowable discharge rate from the site, a storm duration equal to the travel time from the furthestmost point on the site to the adjacent road or existing Council Drainage System is to be used consistently throughout the calculations.

(NOTE: It is reasonable to assume that for a standard site :- Time of Concentration = 10 minutes. If a different figure is to be used, please state the reasoning for the variation to the 10 minutes.)

#### 1.2 Determine the Allowable Council Stormwater discharge for the entire site area using the following coefficients as per the Zone required

##### 1.2.1 Any Industrial/Commercial Redevelopments in **Existing Residential Zones must conform to:**

- 5 Year ARI          Runoff Coefficient          0.25
- 100 Year ARI      Runoff Coefficient          0.45

#### **OR**

##### 1.2.2 Any Industrial/Commercial Redevelopments in **Existing Industrial/Commercial Zones must conform to:**

- 5 Year ARI          Runoff Coefficient          0.65
- 100 Year ARI      Runoff Coefficient          0.85



## 2. Post Site Development Discharge

### 2.1 Site Time of Concentration

As per 1.1 above

### 2.2 Post Site Development Coefficients

In determining the actual post site development discharge from the site, the following coefficients are to be used:

Roof Areas	$C_{5R} = 0.9$	$C_{100R} = 1.0$
Impervious Ground Service	$C_{5I} = 0.75$	$C_{100I} = 0.9$
Pervious Area	$C_{5P} = 0.1$	$C_{100P} = 0.12$

Where Modelling Programs (i.e. ILSAX/ DRAINS) are used, the following percentages are to be used to determine the allowable site discharge.

Directly Connected	80%
Supplementary Paved	10%
Pervious Area	10%

### 2.3 Determining Volume of Detention

The actual post site development discharge is now compared with the allowable site discharge as determined by the Council's runoff coefficients in Part 1 above. Where actual site discharge for the new development exceeds the allowable discharge, the difference must be contained/managed using a stormwater retention/detention installation.

The following checks need to be undertaken for detention systems:

2.3.1 Various storm durations (between 10 and 360 minutes as required) for both the 5 year and 100 year ARI's shall be checked and compared to the 1 in 5 year value and the 1 in 100 year value calculated in Part 1 above. This is to determine and confirm the maximum required retention/detention volumes. The results are to be presented in a Table.

2.3.2 Inflow/outflow hydrographs shall be used to determine the necessary volume of retention/detention. Hydrographs should also indicate site outflows that reflect the use of submersible pumps, restrictor orifices or any other device or system (including time delays on automatic pumps) commonly used in stormwater retention/detention. The hydrographs shall be presented as Figures.

The design must cater for repeat storm events. A sealed retention system that does not allow stormwater to escape or discharge will eventually become full. Stormwater runoff from subsequent storm events will only bypass the system and consequently the site discharge will cease to comply with Council requirements.

#### 2.3.3 Sites greater than 1 hectare

For sites greater than 1 ha the peak outflow from the detention device needs to be checked to ensure that the outflow after 90 minutes for the critical storm duration used to size the storage is not greater than the flow that would arise from a 90 minute storm event based on an undetained catchment with a runoff coefficient of 0.25 in Residential Zones or 0.65 in Industrial/Commercial Zones.

### 3. Details of Detention Systems

#### 3.1 Final Size of Detention Tank required

The Final size of the Detention Tank depends on its location and the volume of water entering the tank from specific run off areas. Please indicate

- where the volume of water is entering the tank from the Proposed Site Drainage System,
- the calculations to determine that volume of water,
- if the detention tank is above ground or below ground.

#### 3.2 Orifice Size Required for Above Ground Detention Tank or from Car Park Sump to the Kerb

Please indicate

- the derived orifice head in metres, used in the calculations,
- the calculation to determine the outflow of water from the tank or sump to ensure Note 4 below is not exceeded.

Note: Outflow =  $\{0.6 \times (\text{area of proposed orifice m}^2) \times [(2 \times 9.81 \times \text{head in metres})^{1/2}]\} \times 1000$

#### 3.3 Pump Size for below ground detention tank

Please indicate

- the make model and number of pumps,
- the outflow from the detention tank in litres/sec,
- if a time delay device is required to ensure that maximum allowable point discharge rates are not exceeded as described in Note 4 below,
- and show calculations to derive how full the tank must be prior to the pump starting operation.

### 4. Discharge from the site to the Kerb and Watertable

shall not exceed 20 litres/sec at any one point for a 1 in 5 year storm event (ie if it is greater than 20 l/s then there must be 2 or more discharge points). Please show calculations of the

- the total flow to each discharge point such that it does not exceed 20 L/s at any point in time during or after the storm duration,
- the size and type of pipe from the property boundary to the kerb and watertable.

### 5. Connections to Council's Drainage System

In certain circumstances, the Council may allow a stormwater connection into the existing underground drainage system in the street where provided. For information on the Council's underground drainage infrastructure, please contact the Council's City Assets Department. Approval to connect must be sought from the Council and specific details of the connection must be provided.

### 6. Connections to Department for Transport, Energy & Infrastructure (DTEI) Drainage System

Approval from DTEI's Metropolitan Region ph. 8226 8222 is to be obtained for any stormwater connection to an existing Side Entry Pit on the following Main Arterial Roads :

Cross Road	Lonsdale Road	Majors Rd	Ocean Boulevard	South Road
Daws Road	Main South Road	Morphett Rd	Reynella Bypass	Southern Expwy

**7. Interception Device (Gross Pollutant Trap)**

Runoff from impervious ground surfaces is to be directed to a stormwater treatment system capable of removing litter, sediment, grease, oil and other substances are capable of contaminating stormwater prior to discharging to the Council drainage system. The treatment system may consist of interception devices and is to be regularly cleaned and maintained by the property owner for the life of the development.

**8. Soil Erosion and Sediment Control Structures**

Soil Erosion and Sediment Control Structures should be implemented on the site in accordance with the EPA Code of practice for the building and construction industry.

**EXAMPLE OF STORMWATER CALCULATIONS**

In order for Council to check the calculations quickly please provide the calculations in the following headings and format showing all formulas and variables:

**Covering Page**

Site Stormwater Management Calculations

For the proposed Development

At – (insert Address including Street Number and Lot Number)

Development Application Number –

Prepared for – (insert Name of Developer and Company)

Prepared by – (insert Name of Consultant and Company)

**Calculation Pages**

It is the responsibility of the engineering consultant to provide legible and detailed calculations. Each formula, along with all constants, variables and substitutions used must be clearly written and defined. Council officers should not be required to decipher or interpret shorthand calculations. If these are presented, the consulting engineer will be requested to resubmit the calculations.

**1. Allowable Council Stormwater Discharge (ACS)**

**1.1 Site Time of Concentration**

State Time of Concentration – 10 minutes OR reasons if other than 10 minutes

**1.2 Determine the Allowable Council Stormwater (ACS) discharge for the entire site area using the following coefficients as per the Zone required**

State Council Zoning of Development site – Residential, Commercial, Industrial, Other (state)

State the relevant Council Run Off coefficients – 1 in 5 & 1 in 100 (0.25 & 0.45 or 0.65 & 0.85)

Methodology Used – (Rational Method, Time-Area Runoff Method, Modelling Programs such as ILSAX or DRAINS)

Total Site Areas – indicate dimensions used and the total for each of the following in m<sup>2</sup>:

Total Area of Allotment =

$$Q_5(ACS) = FCIA = m^3/sec =$$

Where  $F = 1/360$ ,  $C = (0.25 \text{ or } 0.65)$ ,  $I = \text{Intensity mm/hr}$ ,  $A = \text{area hectare}$

$$Q_{100}(ACS) = FCIA = m^3/sec =$$

Where  $F = 1/360$ ,  $C = (0.45 \text{ or } 0.85)$ ,  $I = \text{Intensity mm/hr}$ ,  $A = \text{area hectare}$

ACS = Allowable Council Stormwater Discharge

## 2. Post Site Development Discharge (PSD)

### 2.1 Site Time of Concentration

State (figure as per 1.1 above)

### 2.2 Post Site Development Coefficients

#### Sub Area Names (for example)

$A_{R1}$  - Roof Existing Building =

$A_{R2}$  - Roof Proposed Building =

$A_{I1}$  - Impervious Rear CarPark =

$A_{I2}$  - Impervious Front CarPark =

$A_P$  - Pervious Areas =

#### Runoff Coefficients

$C_{5R} = 0.9$        $C_{100R} = 1.0$

$C_{5I} = 0.75$        $C_{100I} = 0.9$

$C_{5P} = 0.1$        $C_{100P} = 0.12$

$$Q_5(PSD) = F (RoofC_{5R}A_R + ImperviousC_{5I}A_I + PerviousC_{5P}A_P) I$$

State all variables

$$Q_{100}(PSD) = F (RoofC_{100R}A_R + ImperviousC_{100I}A_I + PerviousC_{100P}A_P) I$$

State all variables

PSD = Post Site Development Discharge

### 2.3 Determining Volume of Detention

2.3.1 Various storm durations – Show calculations and present a summary of the results in a Table.

2.3.2 Inflow/outflow hydrographs - The hydrographs shall be presented as Graphs and Figures.

2.3.3 Sites greater than 1 hectare – Show calculation if required

## 3. Details of Detention Systems

Show required information and calculations under the following headings

### 3.1 Final Size of Detention Tank required

### 3.2 Orifice Size Required for Above Ground Detention Tank or from Car Park Sump to the Kerb

### 3.3 Pump Size for below ground detention tank

**4. Discharge from the site to the Kerb and Watertable** - Show required information and calculations

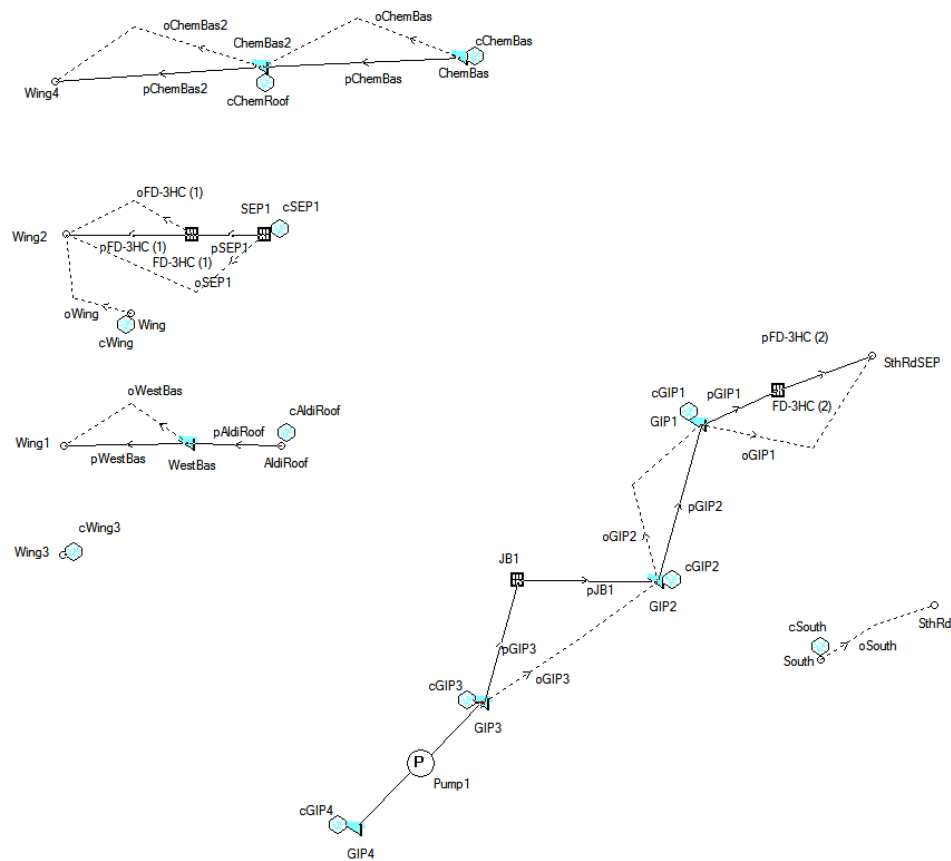
**5. Connections to Council's Drainage System** - Complete if required

**6. Connections to DTEI Drainage System** - Attach approval letter

**7.Interception Device (Gross Pollutant Trap)** – Provide details of proposed make, model, size

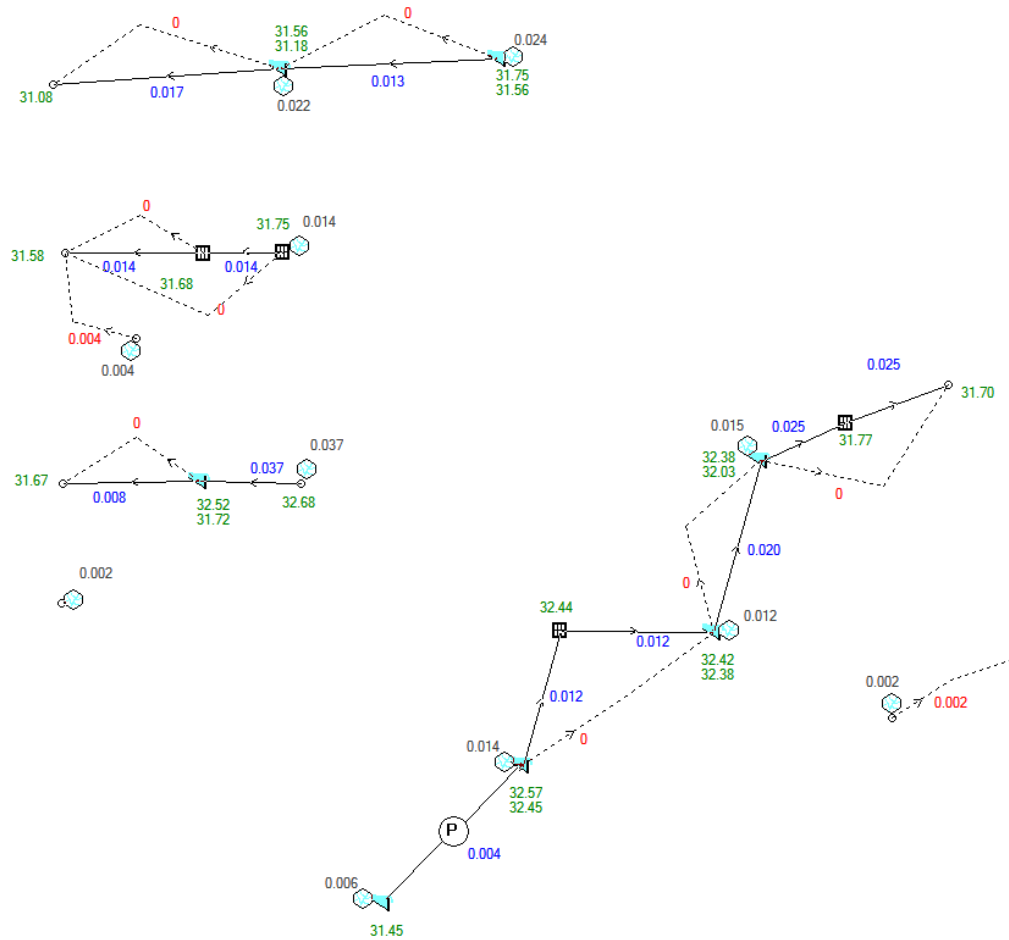
**8.Soil Erosion and Sediment Control Structures** – Provide details of measures to be undertaken

## Appendix C – DRAINS Model Results



Post Development DRAINS Model Layout (above)

ALDI Clovelly Park Development



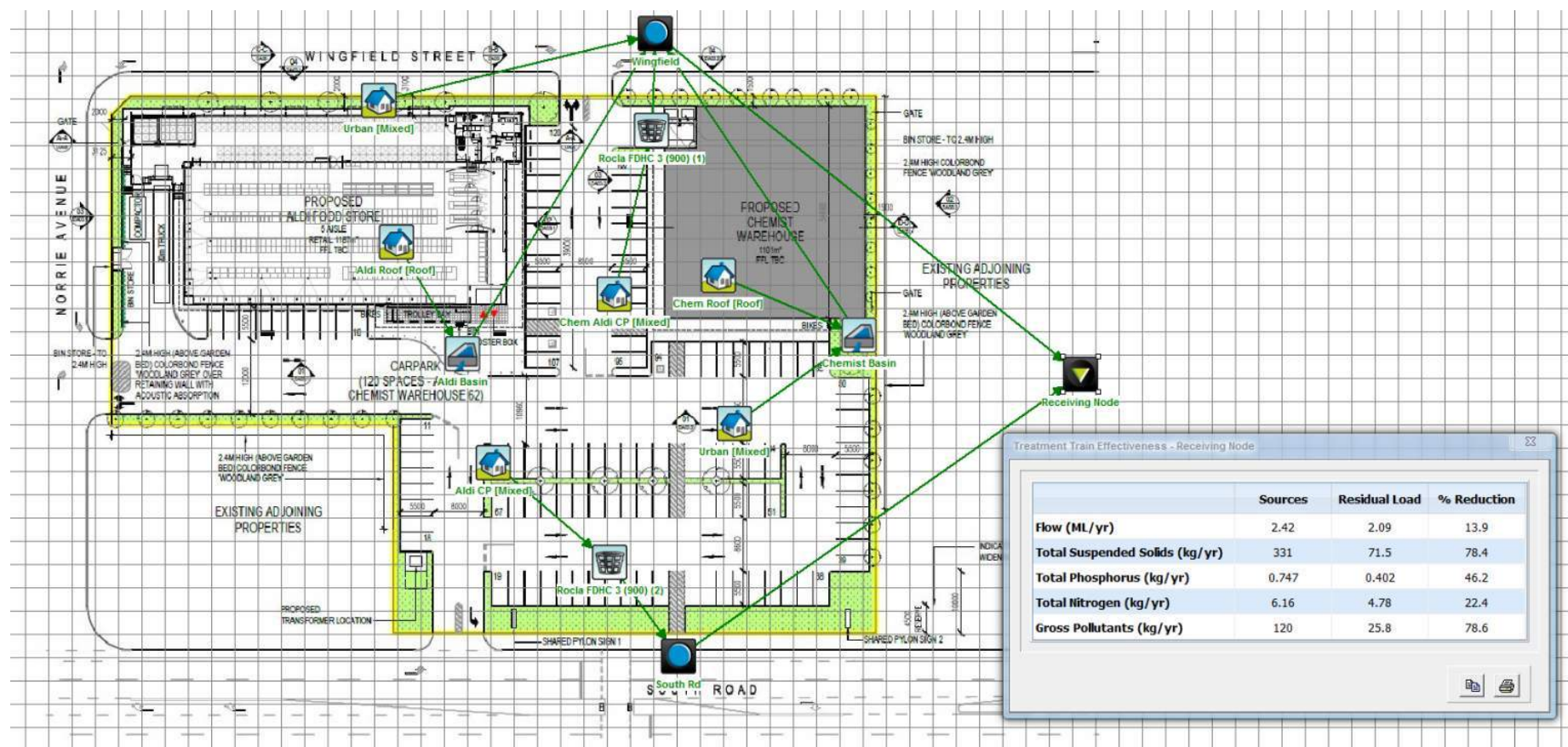
## Post Development 5-year ARI DRAINS Model Results (above)

ALDI Clovelly Park Development



## ALDI Clovelly Park Development

## Appendix D – MUSIC Model Results



In reply please quote 2019/00327, Process ID: 558117  
Enquiries to Marc Hryciuk  
Telephone (08) 7109 7877  
E-mail dpti.luc@sa.gov.au



Government of South Australia

Department of Planning,  
Transport and Infrastructure

POLICY, STRATEGY AND  
PROGRAM DEVELOPMENT

Transport Assessment and  
Policy Reform

GPO Box 1533  
Adelaide SA 5001

ABN 92 366 288 135

14 February 2019

State Commission Assessment Panel  
C/- Mr Malcolm Govett  
Department of Planning, Transport and Infrastructure  
GPO Box 1815  
ADELAIDE SA 5001

Dear Mr Govett

#### SCHEDULE 8 - REFERRAL RESPONSE

<b>Development No.</b>	100/E074/18 Amended Plan – Proposed Site Plan, Project No. 2052, Drawing No. DA02.3, Rev. L dated 29 November 2018
<b>Applicant</b>	Nielsen Architects
<b>Location</b>	1150-1158 South Road, Clovelly Park
<b>Proposal</b>	Construction of two shops, associated car parking, signage and landscaping

I refer to the above development application forwarded to the Commissioner of Highways (CoH) in accordance with Section 37 of the *Development Act 1993*. The proposed development involves development adjacent a main road as described above.

The following response is provided in accordance with Section 37(4)(b) of the *Development Act 1993* and Schedule 8 of the *Development Regulations 2008*.

#### PROPOSAL

The subject application is for the redevelopment of the site as two shops (an ALDI Supermarket and Chemist Warehouse) along with shared parking, signage and landscaping. It is proposed that the site will have access to South Road, Wingfield Street and Norrie Avenue. The Department of Planning, Transport and Infrastructure (DPTI) has previously provided advice on the subject proposal and requested modifications to the access arrangements on South Road. The subject plans have been provided to address the issues raised in DPTI's previous advice dated 29 October 2018.

#### CONSIDERATION

The subject site abuts South Road, an arterial road under the care, control and management of the CoH as well as Wingfield Street and Norrie Avenue which are local roads under the care, control and management of the City of Marion. The adjacent section of South Road forms part of the strategically important National Land Transport Network and is classified as a Major Traffic Route, Primary Freight Route, Major Cycling Route and High Frequency Public Transport Corridor under DPTI's 'A Functional Hierarchy for South Australia's Land Transport Network'. At this location South Road carries approximately 44,700 vehicles per day (6.5% commercial vehicles) and has a posted speed limit of 60 km/h.

## Road Widening

Whilst the subject site is not currently affected by any requirements under the Metropolitan Adelaide Road Widening Plan, both the South Australian and Australian Governments have indicated clear commitments to progress a non-stop North-South Corridor for Adelaide. Construction is currently underway on the Northern Connector, Torrens Road to River Torrens and Darlington sections of the corridor, with funding committed towards the delivery of the Regency Road to Pym Street section of South Road. Completion of these projects by 2022 will create a 47-kilometre continuous section of the North-South Corridor between Gawler and the River Torrens.

On 7 May 2018, the Australian Government announced its \$1.211 billion contribution towards the North-South Corridor. The Australian and South Australian Governments are working together to identify the next priority section of the corridor and are also exploring opportunities to accelerate the funding and delivery of the project.

The River Torrens to Tonsley Boulevard section is currently in the planning phase with detailed planning and design to be undertaken to determine the full scope of works and exact impacts. At this stage, the nature and timing of potential improvements to this section of South Road, in the vicinity of the subject property, are yet to be determined and any potential future land requirements are unknown. Accordingly, DPTI cannot guarantee that the development will not be impacted in the future.

Once the next priority section has been identified and concept plans developed, DPTI will have greater certainty of the impacts and will be able to provide reliable advice to affected land owners, local and state government agencies and the community.

## Access and Road Safety

It is proposed that the subject site be accessed via three two-way access points, one on each of the abutting roads. The South Road access will be located adjacent to the southern end of the site at the location of an existing access serving the site. This is supported as it maximises the separation between the access and the South Road/Southern Avenue junction as well as the adjacent pedestrian actuated crossing.

However as noted in DPTI's previous advice, it is suggested that the connection to the first north-south aisle (i.e. adjacent to the South Road frontage) be modified to operate on a left turn in and left turn out basis only, to ensure that vehicles entering the site are not impeded by vehicles attempting to turn right. This will require minor modifications to the car park design.

With respect to the other two access points, DPTI does not object to the location or operation of these. However, it is noted that the majority of service vehicle movements will be via the Norrie Avenue junction and that the ALDI loading bay is located immediately adjacent to this access. Given the potential for movements associated with the loading bay to interfere with traffic utilising the Norrie Avenue access, appropriate traffic management arrangements will need to be put in place to ameliorate this issue.

DPTI has reviewed the traffic impact assessment undertaken by GTA Consultants and is generally satisfied that the development will not adversely impact on the adjacent road network provided that the above modification to the car park design is undertaken.

It is noted that the plans show some landscaping along the South Road frontage of the site and adjacent to the access points. Whilst DPTI does not object to this, clear sightlines as shown in Figure 3.3 'Minimum Sight Lines for Pedestrian Safety' in *AS/NZS 2890.1:2004*,



should be provided at the property line to ensure adequate visibility between vehicles leaving the site and pedestrians on the adjacent footpath.

### **Signage and Lighting**

Illuminated signage is proposed on this site. DPTI has released the *'Advertising Signs - Assessment Guidelines for Road Safety'* to assist with the review and assessment of advertising signs abutting the arterial road network. DPTI has reviewed the proposed signage against the above guidelines and advised that:

- Some of the signs are within a device restriction area as defined in the Guide;
- The signs would be viewable to traffic travelling along the abutting roads;
- The signs achieve adequate clearances from direct sightlines to the adjacent pedestrian activated traffic signals; and
- The luminance level of the signs are undefined.

The application does not indicate if the car parking areas will be lit at night. DPTI does not object to these areas being illuminated provided that any lighting is appropriately located and/or shielded in order to minimise the potential for driver distraction or discomfort.

### **ADVICE**

The planning authority is advised to attach the following conditions to any approval:

1. The access points shall be in general accordance with Nielsen Architects Proposed Site Plan, Project No. 2052, Drawing No. DA02.3, Rev. L dated 29 November 2018.
2. Any obsolete crossovers/accesses shall be closed and reinstated to Council's standard kerb and gutter at the applicant's cost. This work shall be completed prior to operation of the development.
3. All vehicles shall enter and exit the site in a forward direction.
4. All off-street car parking shall be designed in accordance with *AS/NZS 2890.1:2004* and *AS/NZS 2890.6:2009*. All commercial vehicle facilities shall be designed in accordance with *AS 2890.2-2018*.
5. The largest vehicle permitted on-site shall be restricted to a 19-metres articulated vehicle (*AS 2890.2-2018*). All deliveries shall access the site via the Norrie Avenue and Wingfield Street access points only.
6. Clear sightlines, as shown in Figure 3.3 'Minimum Sight Lines for Pedestrian Safety' in *AS/NZS 2890.1:2004*, shall be provided at the property line to ensure adequate visibility between vehicles leaving the site and pedestrians on the adjacent footpath.
7. A Traffic Management Plan for the construction period of the development shall be produced to the satisfaction of DPTI and Council prior to the commencement of construction. This plan shall detail the types, volumes and distributions of traffic and how they will be managed. All traffic movements shall be in accordance with this plan.
8. Signage on this site visible from the adjacent roads shall not contain any element that flashes, scrolls, moves or changes, or imitates a traffic control device.

9. All illuminated signs visible from South Road shall be limited to a low level of illumination (i.e. < 150Cd/m<sup>2</sup>).
10. Any floodlighting associated with the site shall be positioned and/or shielded so as to not produce glare or create a distraction for passing motorists on the abutting roads.
11. Stormwater run-off shall be collected on-site and discharged without jeopardising the integrity and safety of the adjacent road network. Any alterations to the transport corridor drainage infrastructure required to facilitate this shall be at the applicant's cost.

The following note provides important information for the benefit of the applicant and is required to be included in any approval:

- i. Both the South Australian and Australian Governments have indicated clear commitments to progress a non-stop North-South Corridor for Adelaide. Construction is currently underway on the Northern Connector, Torrens Road to River Torrens and Darlington sections of the corridor, with funding committed towards the delivery of the Regency Road to Pym Street section of South Road. Completion of these projects by 2022 will create a 47-kilometres continuous section of the North-South Corridor between Gawler and the River Torrens.

On 7 May 2018, the Australian Government announced its \$1.211 billion contribution towards the North-South Corridor. The Australian and South Australian Governments are working together to identify the next priority section of the corridor and are also exploring opportunities to accelerate the funding and delivery of the project.

The River Torrens to Tonsley Boulevard section is currently in the planning phase with detailed planning and design to be undertaken to determine the full scope of works and exact impacts. At this stage, the nature and timing of potential improvements to this section of South Road, in the vicinity of the subject property, are yet to be determined and any potential future land requirements are unknown. Accordingly, DPTI cannot guarantee that the development will not be impacted in the future.

Once the next priority section has been identified and concept plans developed, DPTI will have greater certainty of the impacts and will be able to provide reliable advice to affected land owners, local and state government agencies and the community.

Information about the 10-Year Delivery Strategy for the North-South Corridor can be found in the Scoping Report released on 18 May 2015 and available at [www.infrastructure.sa.gov.au/nsc/10yds](http://www.infrastructure.sa.gov.au/nsc/10yds). Further information on the North-South Corridor can be obtained at [www.infrastructure.sa.gov.au/nsc](http://www.infrastructure.sa.gov.au/nsc) or if you would like to speak to a member of the North-South Corridor team, please email [northsouthcorridor@sa.gov.au](mailto:northsouthcorridor@sa.gov.au) or call 1300 951 145.

Yours sincerely



**MANAGER, TRANSPORT ASSESSMENT AND POLICY REFORM**  
for **COMMISSIONER OF HIGHWAYS**

A copy of the decision notification form should be forwarded to [dpti.developmentapplications@sa.gov.au](mailto:dpti.developmentapplications@sa.gov.au)

20 February 2019

PO Box 21, Oaklands Park  
South Australia 5046

245 Sturt Road, Sturt  
South Australia 5047

**T** (08) 8375 6600

**F** (08) 8375 6699

**E** [council@marion.sa.gov.au](mailto:council@marion.sa.gov.au)

Mr Malcolm Govett  
Planning Officer  
c/- State Commission Assessment Panel  
GPO Box 1815  
ADELAIDE 5001

*Via email: [Malcolm.Govett@sa.gov.au](mailto:Malcolm.Govett@sa.gov.au)*

Dear Mr Govett

**RE:- Development Application 100/2018/1665  
Staged construction of an ALDI Store (shop) and Chemist Warehouse  
(shop) with associated on-site signage, fencing, carparking and  
landscaping at 1150-1152 South Road CLOVELLY PARK 5042.**

Please find enclosed the above described application which is referred to by Schedule 10 of the Regulations under the Development Act 1993, as one which is to be determined by the Commission.

Council provided comments on 8 November in relation to the proposed development.

Whilst Council is generally supportive of the proposal as, when assessed against the applicable Development Plan provisions it achieves relative compliance, there are several elements of the proposal which Council believes require attention prior to any decision being made. Council does however acknowledge the positive changes made in relation to the amended design.

Council originally made comments in relation to the proposed bulk, scale, orientation of the built form and the poor relationship with the surrounding street network. Council provides the following comments in relation to the amended plans;

### **Built Form**

The application proposes two separate buildings and the architecture is reflective of other recently constructed ALDI and Chemist Warehouse buildings in terms of design, bulk/scale and presentation.

The ALDI building has incorporated further colour treatments to the southern and western façade to improve the visual appearance of the built form when viewed from the immediate street network. Additionally, large perforated metal panels have been incorporated to the southern façade to provide further visual interest and variation.

The location of the proposed Chemist Warehouse building has been altered to provide minimum 1.5 metre setbacks to both the northern and western side boundaries.

The City of Marion acknowledges it is part of Kaurna land and recognises the Kaurna people  
as the traditional and continuing custodians of the land.



[facebook.com/CityofMarion](https://facebook.com/CityofMarion)



[twitter.com/CityofMarion](https://twitter.com/CityofMarion)



[youtube.com/CityofMarion](https://youtube.com/CityofMarion)

[marion.sa.gov.au](https://marion.sa.gov.au)



The provision of a 1.5 metre setback will provide greater separation to the adjacent residential property, this notwithstanding however, despite the incorporation of alternate façade colours, the built form will present a considerable expanse of blank wall to height of 6.25 metres.

Whilst the comprehensive redevelopment of the site will generally result in an improve amenity outcome by providing modern buildings and car parking, existing occupants along Wingfield Street and Norrie Avenue will experience a changed outlook, particularly in relation to the bulk/scale of the built form and minimal setbacks to boundaries.

## **Traffic, Access & Car parking**

### Car parking

A total of 120 spaces are to be provided as part of the proposed application, which exceeds the minimum requirements specified within the Development Plan.

The SCAP should be satisfied appropriate reciprocal access rights are placed on the relevant titles to ensure the full balance of the parking provided remains accessible for the whole of the mixed-use development.

### Traffic & Access

The proposal will, overall, result in an increase in traffic movements throughout the local road network and an increase of delivery trucks and heavy vehicles, particularly along Norrie Avenue and Wingfield Street.

The SCAP should be satisfied the likely increase in vehicles are appropriate, can be accommodated by the existing road network, and will not result in unreasonable noise or amenity impacts on occupants of allotments within the immediate locality.

Additionally, the following comments have been provided by Council Officers;

Development Engineer

1. *It should be a condition of approval that “the on-street parking on Norrie Ave between South Road and the Jaycar access be removed” – this would need to occur following consultation*
2. *Drainage Levels nominated in the Stormwater Management Plan contradict the levels shown on Siteworks Plan SK5 RevC. This should be rectified.*

Technical Officer - Traffic & Design

- *Council would require the developer to undertake consultation with the adjacent business (Jaycar) regarding the requirement to remove the existing timed one hour (1P) parking operational between the hours of 8am to 5pm and replace with “No Standing” in its place on the southern side of Norrie Avenue, between South Road and the Jaycar access.*

*For Councils records, a copy of the consultation notification and consultation outcomes would be required.*

The SCAP should be satisfied the request for the removal of on-street parking along the southern side of Norrie Avenue has been resolved prior to the issuing of Development Plan Consent. Should on-street parking remain, Council has concerns vehicular movements for large/heavy trucks may be compromised and could potentially result in unsafe vehicular movements. It is strongly advised the applicant consult with Council regarding the potential removal of on-street parking.

## **Interface between land uses**

When considering the impacts of the proposed development, regard should be given to the proposed built form and likely intensity of the use (hours of operation, noise, traffic etc).

In this regard, the use considerably changes both the built form appearance and likely intensity of use in relation to activities occurring on site and potential traffic generation. Traffic impacts have been discussed above.

the redesigned site layout and introduction of heavy vehicles required to service ALDI, adjacent occupants will experience new and additional noise impacts.

An Environmental Noise Assessment has been prepared by the applicant which concludes the development has been designed to minimise adverse impacts and conflict between land uses, avoid unreasonable interference on amenity, and will not detrimentally affect the locality by way of noise, subject to the following treatments, comprising;

- Specific fence heights and constructions (i.e. fence heights);
- Installation of acoustic absorption on the ALDI delivery fence;
- Screening of ALDI mechanical plant; and,
- Restricting the times for rubbish collection.

In relation to the to the above mentioned noise/acoustic treatments, the SCAP should be satisfied these are considered to adequately reduce and mitigate potential noise impacts on the wider locality, and that any treatments are in place and in operation prior to occupation of the premises. Furthermore, it is recommended SCAP attach a condition of consent limiting/controlling proposed delivery hours as to ensure no unreasonable impacts on adjacent allotments occur.

In relation to waste, the SCAP should be satisfied the disposal of waste on site, and the subsequent collection of waste, does not determinately affect the amenity of adjacent or nearby occupants in relation to odour and noise. Additionally, it should collection of waste is undertaken in accordance with *Environment Protection (Noise) Policy 2007*, or subsequent legislation.

## **Hours of Operation**

The proposed hours of operation for both uses are considered appropriate and reflect contemporary trading hours. Additionally, the hours are somewhat reflective of the existing uses located on the site. The SCAP should however be satisfied that the hours proposed are within other legislative Acts/ requirements.

## **Stormwater Disposal**

Council's Designer - Civil Engineering has reviewed the proposed stormwater disposal methods and advised the stormwater calculations and discharge points look acceptable, although approval to connect into the pits along South Road will have to come from DPTI.

## **Landscaping**

General landscaping throughout the site has been provided. The extent of landscaping proposed, particularly adjacent the western façade of the ALDI building and along the South Road frontage, is limited and does not, in Council's opinion, significantly assist in complementing the built form and reduce the visual impact of larger buildings or enhance the appearance of the primary road frontage. Council suggests additional ground level plantings should be incorporated adjacent the southern and western ALDI

façades, whilst further small scale planting should be planted adjacent the South Road frontage, to assist in providing an improved streetscape outcome and increase the overall attractiveness of the locality.

### **Proposed Conditions**

Should the proposed areas of concern be satisfactorily addressed, it is recommended the following conditions be attached to any consent;

1. The development shall be constructed and maintained in accordance with the plans and details submitted with and forming part of Development Application No. 100/E074/18, except when varied by the following conditions of consent;
2. The development shall operate in accordance with the trading hours prescribed by the *Shop Trading Hours Act, 1977* or related legislation.
3. All areas nominated as landscaping or garden areas on the approved plans shall be planted and maintained with a suitable mix and density of native trees, shrubs and groundcovers prior to the occupation of the premises to the reasonable satisfaction of the Council.
4. Landscaping shall be maintained so as to not obstruct the views of drivers or pedestrians entering or exiting the site, to the reasonable satisfaction of Council.
5. A minimum of 50% of the trees indicated to be planted on the approved plan shall be at least 1.5 metres in height at the time of planting.
6. All existing vegetation nominated to be retained and all new vegetation to be planted shall be nurtured and maintained in good health and condition at all times with any diseased or dying plants being replaced, to the reasonable satisfaction of the Council.
7. All landscaped areas shall be separated from adjacent driveways and parking areas by a suitable kerb or non-mountable device to prevent vehicle movement thereon (incorporating ramps or crossovers to facilitate the movement of persons with a disability).
8. All industrial and commercial vehicles visiting the site shall enter and exit the land in a forward direction.
9. All loading and unloading of vehicles associated with the subject premises shall be carried out entirely upon the subject land.
10. Driveways, car parking spaces, manoeuvring areas and landscaping areas shall not be used for the storage or display of any goods, materials or waste at any time.
11. All car parking areas, driveways and vehicle manoeuvring areas must be constructed, sealed and drained in accordance with recognised engineering practices prior to the occupation of the tenancy or the use of the development herein approved.
12. The driveways, parking areas and vehicle manoeuvring areas must be maintained in a good condition at all times.
13. All car parking spaces shall be linemarked or delineated in a distinctive fashion prior to occupation of the premises, with the marking maintained in a clear and visible condition at all times.

14. Directional signs indicating the location of car parking spaces must be provided on the subject land and maintained in a clear and legible condition at all times.
15. All waste and other rubbish shall be stored in a manner so that it does not create insanitary conditions, unreasonable nuisance or pollution to the environment to the reasonable satisfaction of the Council.
16. All waste and other rubbish shall be screened from public view and be kept in a tidy manner and condition at all times, to the reasonable satisfaction of Council.
17. All hard waste must be stored on-site in such a manner so as to prevent any materials entering the stormwater system either by wind or water action.
18. New pedestrian walkways on the subject site shall be adequately lit in accordance with Australian / New Zealand Standard AS/NZS 1158.3.1:1999 "Road Lighting Part 3.1: Pedestrian area (Category P) lighting - Performance and installation design guidelines". Such lighting shall be maintained at all times, to the reasonable satisfaction of the Council.
19. All external lighting of the site, including car parking areas and buildings, shall be located, directed, shielded and of an intensity not exceeding lighting in adjacent public streets, so as not to cause nuisance or loss of amenity to any person beyond the site to the reasonable satisfaction of the Council.
20. All stormwater from buildings and paved areas shall be disposed of in accordance with the approved plans and details prior to the occupation of the premises to the reasonable satisfaction of the Council.

*(Further information on the applicable coefficients can be obtained within Council's Development Plan).*

21. A trap shall be installed as part of the site's stormwater system to prevent grease, oil, sediment, litter and other substances capable of contaminating stormwater from entering the Council's stormwater drainage system. The trap shall be regularly cleaned and maintained in good working order to the reasonable satisfaction of the Council.

Could you please confirm also whether Council is to be the relevant authority in terms of assessment under the Building Rules, or whether the Commission wishes to supply an alternative arrangement.

Please contact me if further information is required regarding this application.

Yours sincerely



Alex Wright  
**Acting Team Leader - Planning**

Phone: 8375.6668  
Email Address: marionds@marion.sa.gov.au

South Australian  
DEVELOPMENT ACT, 1993  
REPRESENTATION ON APPLICATION – CATEGORY 2

Applicant: Nielsen Architects  
Development Number: 100/E074/18  
Nature of Development: Demolition of all existing buildings and construction of a new ALDI supermarket and Chemist Warehouse building with shared car parking for up to 117 cars.  
Zone / Policy Area: Neighbourhood Centre Zone  
Subject Land: 1150-1154 & 1156-1158 South Road, Clovelly Park and 19 Wingfield Street, Clovelly Park, and 1, 3, and 5 Norrie Avenue, Clovelly Park.  
Contact Officer: Malcolm Govett  
Phone Number: 7109 7094  
Close Date: 4 October 2018

RECEIVED 26 SEP 2018

My Name: RITA FURLANI My phone number: 0407720313

Primary method(s) of contact: Email: furlanirita@bigpond.com  
Postal Address: 15 WINGFIELD ST Postcode: 5042  
CLOVELLY PARK

**You may be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.**

My interests are:  
(please tick one)

- ☒ owner of local property  
☐ occupier of local property  
☐ a representative of a company/other organisation affected by the proposal  
☐ a private citizen

The address of the property affected is:

15 WINGFIELD ST CLOVELLY PARK Postcode 5042

My interests are:  
(please tick one)

- ☐ I support the development  
☐ I support the development with some concerns  
☒ I oppose the development

The specific aspects of the application to which I make comment on are: THE SIZE AND  
LOCATION OF PROPOSED SUPERMARKET.  
DEMOLITION OF 19 WINGFIELD ST.

I:

(please  
tick one)

- ☒ wish to be heard in support of my submission  
☐ do not wish to be heard in support of my submission  
(Please tick one)

By:

(please  
tick one)

- ☒ appearing personally  
☐ being represented by the following person  
(Please tick one)

Signature:

Date:

Rita Furlani  
23/09/2018



South Australian  
DEVELOPMENT ACT, 1993  
REPRESENTATION ON APPLICATION – CATEGORY 2

**Applicant:** Nielsen Architects  
**Development Number:** 100/E074/18  
**Nature of Development:** Demolition of all existing buildings and staged construction of a new ALDI supermarket and Chemist Warehouse building with shared car parking for up to 120 cars, signage, fencing and landscaping.  
**Zone / Policy Area:** Neighbourhood Centre Zone  
**Subject Land:** 1150-1154 & 1156-1158 South Road, Clovelly Park and 17-19 Wingfield Street, Clovelly Park, and 1, 3, and 5 Norrie Avenue, Clovelly Park.  
**Contact Officer:** Malcolm Govett  
**Phone Number:** 7109 7094  
**Close Date:** 4 February 2019

**My Name:** RITA FURLANI **My phone number:** 0407720313

**Primary method(s) of contact:** Email: furlanirita@bigpond.com  
Postal Address: 15 WINGFIELD ST Postcode: 5042  
CLOVELLY PARK

**You may be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.**

**My interests are:**  
(please tick one)

- ☐ owner of local property  
☒ occupier of local property  
☐ a representative of a company/other organisation affected by the proposal  
☐ a private citizen

**The address of the property affected is:**

15 WINGFIELD ST CLOVELLY PARK Postcode 5042

**My interests are:**  
(please tick one)

- ☐ I support the development  
☐ I support the development with some concerns  
☒ I oppose the development

**The specific aspects of the application to which I make comment on are:**

OVERALL DEVELOPMENT IMPACT ON MY  
QUALITY OF LIFE

**I:** ☒ wish to be heard in support of my submission  
(please tick one) ☐ do not wish to be heard in support of my submission  
(Please tick one)

**By:** ☒ appearing personally  
(please tick one) ☐ being represented by the following person  
(Please tick one)

**Signature:** Rita Furlani

**Date:** 20/01/2019

**RECEIVED**

22 JAN 2019

South Australian  
DEVELOPMENT ACT, 1993  
REPRESENTATION ON APPLICATION – CATEGORY 2

Applicant: Nielsen Architects  
Development Number: 100/E074/18  
Nature of Development: Demolition of all existing buildings and construction of a new ALDI supermarket and Chemist Warehouse building with shared car parking for up to 117 cars.  
Zone / Policy Area: Neighbourhood Centre Zone  
Subject Land: 1150-1154 & 1156-1158 South Road, Clovelly Park and 19 Wingfield Street, Clovelly Park, and 1, 3, and 5 Norrie Avenue, Clovelly Park.  
Contact Officer: Malcolm Govett  
Phone Number: 7109 7094  
Close Date: 4 October 2018

My Name: MICHAEL GANNON My phone number: 0412 972 247

Primary method(s) of contact:

Email:

MICHAEL@GANNONGROUP.COM.AU.

Postal Address:

Postcode:

**You may be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.**

My interests are:  
(please tick one)



owner of local property



occupier of local property



a representative of a company/other organisation affected by the proposal



a private citizen

The address of the property affected is:

1166 SOUTH ROAD, CLOVELLY PARK.

Postcode

My interests are:  
(please tick one)



I support the development



I support the development with some concerns



I oppose the development

The specific aspects of the application to which I make comment on are:

DIVIDING FENCE

THE DIVIDING FENCE BETWEEN MY PROPERTY AND THE APPLICANT  
COMPLIES WITH THE FENCING ACT. AT 1.8m HIGH.  
I DO NOT WANT TO BE CHARGED FOR ANY AMOUNT OF THE NEW 2.4m  
HIGH FENCE.

I:



wish to be heard in support of my submission

(please  
tick one)



do not wish to be heard in support of my submission

(Please tick one)

By:



appearing personally

(please  
tick one)



being represented by the following person

(Please tick one)

Signature:

Date:

28/9/18



South Australian  
DEVELOPMENT ACT, 1993  
REPRESENTATION ON APPLICATION – CATEGORY 2

Applicant: Nielsen Architects  
Development Number: 100/E074/18  
Nature of Development: Demolition of all existing buildings and construction of a new ALDI supermarket and Chemist Warehouse building with shared car parking for up to 117 cars.  
Zone / Policy Area: Neighbourhood Centre Zone  
Subject Land: 1150-1154 & 1156-1158 South Road, Clovelly Park and 19 Wingfield Street, Clovelly Park, and 1, 3, and 5 Norrie Avenue, Clovelly Park.  
Contact Officer: Malcolm Govett  
Phone Number: 7109 7094  
Close Date: 4 October 2018

RECEIVED 02 OCT 2018

My Name: ELISABETH KANSY My phone number: 82765248  
OR. 0417 565 575.  
Primary method(s) of contact: Email: kansy@bigpond.com  
Postal Address: 8 Norrie Avenue Postcode: 5042  
Clovelly Pk SA

You may be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.

My interests are:  
(please tick one)

- ☒ owner of local property  
☐ occupier of local property  
☐ a representative of a company/other organisation affected by the proposal  
☐ a private citizen

The address of the property affected is:

8 NORRIE AVENUE, CLOVELLY PARK Postcode 5042

My interests are:  
(please tick one)

- ☐ I support the development  
☒ I support the development with some concerns \* See a Hatched please.  
☐ I oppose the development

The specific aspects of the application to which I make comment on are:

PLEASE SEE ANNEXURE ATTACHED.

I:

(please  
tick one)

- ☒ wish to be heard in support of my submission  
☐ do not wish to be heard in support of my submission  
(Please tick one)

By:

(please  
tick one)

- ☒ appearing personally  
☐ being represented by the following person  
(Please tick one)

Signature: E. KANSY

Date: 28/9/18

**COMMENTS/CONCERNS RE APPLICATION NO. 100/E074/18**  
**PROPOSED NEW ALDI SUPERMARKET AND CHEMIST WAREHOUSE**  
**CLOVELLY PARK SA 5042**  
**Opposite - 8 Norrie Avenue, Clovelly Park SA 5042**  
**Owners: J & EM KANSY**

1. The Application states that 24 hour delivery access to the site is required twice per day. Our house has a bedroom at the front facing Norrie Avenue and any noise (engine, braking, reversing etc) associated with having a delivery made during the night/early morning by a 19 metre semi-trailer would impact on sleep.
2. Potential ALDI and Chemist Warehouse customers driving in a southerly direction on South Road and looking for either of the two stores will already have driven past Newton Street (on their right) before they realise and/or see the signage indicating the location of the stores and car-park and the majority of them would then use Norrie Avenue as a means of access into the park instead of Newton Street, the result being more traffic flowing into Norrie Avenue. This would undoubtedly add to traffic congestion around that intersection and along Norrie Avenue.
3. In addition to the above, if daily local traffic, ALDI/Chemist Warehouse customer traffic, plus a 19 metre semi-trailer and/or other delivery or waste collection trucks during the day (and potentially during peak hour traffic) attempt to exit Norrie Avenue onto South Road it will result in long delays to turn left, and turning right will virtually become impossible. We note the Application states that this is not considered to be a major issue as it is already an existing problem. Having lived at our address for over 13 years we are obviously quite aware of it and strongly believe that over time trying to make both left and right hand turns onto (and also from) South Road will become a lot more difficult with the additional traffic and it will present a major problem in the future.
4. The noise from the ALDI store compactor behind the 2.4m high Colorbond fence is of major concern to us, once again in view of the fact that the front room of our house is a bedroom. If the fence, as the drawing in the application shows, has acoustic absorption then obviously it does emit significant noise. We would therefore like further evidence that the noise from the compactor will not impact on us significantly. We note also that there does not appear to be any mention or

indication on the drawings of acoustic absorption for that portion of the fence behind the bin storage area. The constant opening/closing of the bin/s by ALDI staff and particularly the bin and compactor content collections on average three times per week will no doubt create noise at various levels.

5. Graffiti has become an ongoing problem in many suburbs around Adelaide on business premises and fencing. With a Colorbond fence proposed on the outer southern boundary of the ALDI store we have concerns in that regard. We would like assurance that ALDI has a policy in place that provides for immediate attention to graffiti removal from their stores and surrounding fences should it appear.
6. We would also like some clarification on the exact size and illumination details of the ALDI sign on the southern wall of the building. Further, we would like details, locations and timing of all other external lighting including security, sensors and CCTV proposed for the development and car-park. We value our privacy and would like assurances that it will not be compromised in any way.
7. The following section may or may not be relevant as far as SCAP is concerned and will obviously have to be taken up with the City of Marion however it is an issue that the ALDI/Chemist Warehouse development will impact upon and it is therefore being included for SCAP to be made aware of and as a reference.

*8 Norrie Avenue only has a 4.5 metre designated street frontage (for parking of one average size vehicle) which is currently signposted by the Council as "One hour parking between 8am and 5pm", however this appears to be very rarely policed by Council. Customers of Jaycar on the corner of South Road and Norrie Avenue, Affordable Spas on the opposite corner and the 4 shops/tenancies along the Western side of South Road already park their cars in front of our house as a "quick stop" (and sometimes "long stop") to save them having to use the existing Chemist Warehouse/Rite Price or the Jaycar car-parks. We believe that if an ALDI supermarket is opened even more people will use whatever park they can find in Norrie Avenue or in other adjoining streets for their "quick park". With our house being the closest to the Norrie Avenue entry of the new ALDI/Chemist Warehouse carpark we believe that cars will be parked in front of our house most of day. To add to this problem, the Application before SCAP recommends "No parking" signage be installed along approximately 35 metres on Norrie Avenue from South Road to the Jaycar car-park, thereby only leaving our small frontage available for*



*short term parkers. As a possible solution we would request the Marion Council to erect "Resident Only Permit Parking" signage in place of the current "One hour parking" and provide us with the relevant Permit. Council hard rubbish collection is already proving difficult for trucks to access and empty bins from our street verge due to the constant parkers in front of our house. We regularly have to phone the collection company (Solo Resource Recovery) to let them know our bin/s have not been emptied.*

## **CONCLUSION**

The proposed development has raised significant and real issues for us and as the owners of a thirteen year old house we do have concerns that the value our property will significantly reduce with such a large proposed development so close to our property. More importantly, the possible impact it will have on our health, safety and quality of life.

In 2005 when we purchased our property there were three existing houses (Nos. 1, 3 & 5 Norrie Avenue) across the road which we thought may be demolished at some point in the future and the land redeveloped. We were made aware that the land was a Neighbourhood Centre Zone and understood that it could be developed for possible housing, consulting rooms, a child care centre or similar type of business. We were never made aware, or envisaged, that a large supermarket could be built on that parcel of land and that one day, as is now the case, our house could potentially be facing the back end of one. We acknowledge that the Chemist Warehouse location in the Application does not impact on us but the ALDI Supermarket does, in fact quite significantly.

We therefore would appreciate our concerns and queries being addressed and fully considered by the State Commission Assessment Panel.

South Australian  
DEVELOPMENT ACT, 1993  
REPRESENTATION ON APPLICATION – CATEGORY 2

RECEIVED 20 SEP 2018

Applicant: Nielsen Architects  
Development Number: 100/E074/18  
Nature of Development: Demolition of all existing buildings and construction of a new ALDI supermarket and Chemist Warehouse building with shared car parking for up to 117 cars.  
Zone / Policy Area: Neighbourhood Centre Zone  
Subject Land: 1150-1154 & 1156-1158 South Road, Clovelly Park and 19 Wingfield Street, Clovelly Park, and 1, 3, and 5 Norrie Avenue, Clovelly Park.  
Contact Officer: Malcolm Govett  
Phone Number: 7109 7094  
Close Date: 4 October 2018

My Name: Thomas Link My phone number: 0418821558

Primary method(s) of contact: Email: terraequitiespl@bigpond.com  
Postal Address: PO Box 426 Have Postcode: 5048

You may be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.

My interests are:  
(please tick one)

- ☒ owner of local property  
☐ occupier of local property  
☐ a representative of a company/other organisation affected by the proposal  
☐ a private citizen

The address of the property affected is:

1133-1137 South Rd 3 Marys Postcode \_\_\_\_\_

My interests are:  
(please tick one)

- ☒ I support the development  
☐ I support the development with some concerns  
☐ I oppose the development

The specific aspects of the application to which I make comment on are:

This should improve the parking problem  
in the area for people wanting to use the  
Chemist.

I: ☐ wish to be heard in support of my submission  
(please tick one) ☒ do not wish to be heard in support of my submission  
(Please tick one)

By: ☐ appearing personally  
(please tick one) ☐ being represented by the following person  
(Please tick one)

Signature: \_\_\_\_\_

Date: 19/9/18



South Australian  
DEVELOPMENT ACT, 1993  
REPRESENTATION ON APPLICATION – CATEGORY 2

Applicant: Nielsen Architects  
Development Number: 100/E074/18  
Nature of Development: Demolition of all existing buildings and construction of a new ALDI supermarket and Chemist Warehouse building with shared car parking for up to 117 cars.  
Zone / Policy Area: Neighbourhood Centre Zone  
Subject Land: 1150-1154 & 1156-1158 South Road, Clovelly Park and 19 Wingfield Street, Clovelly Park, and 1, 3, and 5 Norrie Avenue, Clovelly Park.  
Contact Officer: Malcolm Govett  
Phone Number: 7109 7094  
Close Date: 4 October 2018

My Name: JIM PETRE My phone number: 0411 852 604  
Primary method(s) of contact: Email: Jim.petre@bypad.net.au  
Postal Address: PO Box 18 Postcode: 5063  
HIGHTGATE SA

**You may be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.**

My interests are:  
(please tick one)

- ☒ owner of local property  
☒ occupier of local property  
☒ a representative of a company/other organisation affected by the proposal  
☐ a private citizen

The address of the property affected is:

1151 SOUTH ROAD ST MARYS (opposite) Postcode: \_\_\_\_\_

My interests are:  
(please tick one)

- ☒ I support the development  
☐ I support the development with some concerns  
☐ I oppose the development

The specific aspects of the application to which I make comment on are:

Design takes into consideration Residential on West side, creates open vista, increase visual amenity from our property!

I:

(please tick one)

- ☐ wish to be heard in support of my submission  
☒ do not wish to be heard in support of my submission  
(Please tick one)

By:

(please tick one)

- ☐ appearing personally  
☐ being represented by the following person  
(Please tick one)

Signature: \_\_\_\_\_

Date: \_\_\_\_\_

[Signature]  
24/9/18



DEVELOPMENT ACT, 1993  
REPRESENTATION ON APPLICATION - CATEGORY 2

Applicant: Nielsen Architects  
Development Number: 100/E074/18  
Nature of Development: Demolition of all existing buildings and staged construction of a new ALDI supermarket and Chemist Warehouse building with shared car parking for up to 120 cars, signage, fencing and landscaping.  
Zone / Policy Area: Neighbourhood Centre Zone  
Subject Land: 1150-1154 & 1156-1158 South Road, Clovelly Park and 17-19 Wingfield Street, Clovelly Park, and 1, 3, and 5 Norrie Avenue, Clovelly Park.  
Contact Officer: Malcolm Govett  
Phone Number: 7109 7094  
Close Date: 4 February 2019

My Name: Mr VENGATESH MUTRU & SUGANTHI THANGARAJU My phone number: 0449539843  
Primary method(s) of contact: Email: SUGAN 79-97 @YAHOO.COM  
Postal Address: 3 FAWKNER PLACE Postcode: 3023  
CAROLINE SPRINGS VIC

**You may be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.**

My interests are:  
(please tick one)

- ☒ owner of local property (OWNERS)  
☐ occupier of local property  
☐ a representative of a company/other organisation affected by the proposal  
☐ a private citizen

The address of the property affected is: 26 WINGFIELD ST,  
CLOVELLY PARK, SA

Postcode 5042

My interests are:  
(please tick one)

- ☐ I support the development  
☒ I support the development with some concerns  
☐ I oppose the development

The specific aspects of the application to which I make comment on are:

We are happy to have the newer development which will be useful to the community. We want to make sure the development process - during and after - should not interfere with the normal living conditions of the people in the surrounding area - such as interfering with

- I: ☐ wish to be heard in support of my submission  
(please tick one) ☒ do not wish to be heard in support of my submission  
(Please tick one)

- By: ☐ appearing personally  
(please tick one) ☐ being represented by the following person  
(Please tick one)

Signature:

Date:

T Suganthi

29/1/19

PRIVACY,  
TRAFFIC CONGESTION  
and any form  
POLLUTION.



South Australian  
DEVELOPMENT ACT, 1993  
REPRESENTATION ON APPLICATION – CATEGORY 2

Applicant: Nielsen Architects  
Development Number: 100/E074/18  
Nature of Development: Demolition of all existing buildings and construction of a new ALDI supermarket and Chemist Warehouse building with shared car parking for up to 117 cars.  
Zone / Policy Area: Neighbourhood Centre Zone  
Subject Land: 1150-1154 & 1156-1158 South Road, Clovelly Park and 19 Wingfield Street, Clovelly Park, and 1, 3, and 5 Norrie Avenue, Clovelly Park.  
Contact Officer: Malcolm Govett  
Phone Number: 7109 7094  
Close Date: 4 October 2018

RECEIVED 05 OCT 2018

My Name: Anne Marie Scime My phone number: 0407 715 751  
Vince Scime 0409 046 077  
Primary method(s) of contact: Email: amscime@hotmail.com  
Postal Address: 20 Wingfield St Postcode: 5042  
Clovelly PK SA

You may be contacted via your nominated PRIMARY METHOD(S) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.

My interests are:  
(please tick one)

- ☒ owner of local property  
☒ occupier of local property  
☐ a representative of a company/other organisation affected by the proposal  
☐ a private citizen

The address of the property affected is:

20 Wingfield St. Clovelly PK SA Postcode 5042

My interests are:  
(please tick one)

- ☐ I support the development  
☒ I support the development with some concerns  
☐ I oppose the development

The specific aspects of the application to which I make comment on are:

Car Traffic and access to property  
on Wingfield St.

I:

(please  
tick one)

- ☒ wish to be heard in support of my submission  
☐ do not wish to be heard in support of my submission  
(Please tick one)

By:

(please  
tick one)

- ☐ appearing personally  
☐ being represented by the following person  
(Please tick one)

Vince being represented  
by spouse - Anne Marie Scime

Signature:

Date:

Malcolm Govett & Vincent Scime  
2/10/18

South Australian  
DEVELOPMENT ACT, 1993  
REPRESENTATION ON APPLICATION – CATEGORY 2

Applicant: Nielsen Architects  
Development Number: 100/E074/18  
Nature of Development: Demolition of all existing buildings and construction of a new ALDI supermarket and Chemist Warehouse building with shared car parking for up to 117 cars.  
Zone / Policy Area: Neighbourhood Centre Zone  
Subject Land: 1150-1154 & 1156-1158 South Road, Clovelly Park and 19 Wingfield Street, Clovelly Park, and 1, 3, and 5 Norrie Avenue, Clovelly Park.  
Contact Officer: Malcolm Govett  
Phone Number: 7109 7094  
Close Date: 4 October 2018

My Name: Cally Tierney <sup>(Reid)</sup> My phone number: 0413596823

Primary method(s) of contact: Email: ctierney82.ct@gmail.com  
Postal Address: \_\_\_\_\_ Postcode: \_\_\_\_\_

**You may be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.**

- My interests are:  
(please tick one)
- ☒ owner of local property
  - ☐ occupier of local property
  - ☐ a representative of a company/other organisation affected by the proposal
  - ☐ a private citizen

The address of the property affected is:

7 Norrie Ave Clovelly Park Postcode 5042

- My interests are:  
(please tick one)
- ☐ I support the development
  - ☒ I support the development with some concerns
  - ☐ I oppose the development

The specific aspects of the application to which I make comment on are:

Noise of Trucks, boxing machines and other machinery outside of the 7am-7pm once development is finished.  
This is also a concern during development and one that has been broken with all development immediately affecting our residence. I also fear that there is not enough adequate car parks to stop over flow onto the street which will directly impact our ability to park and have other visitors park in front / next to our residence. Hawthorn Aldi only has 30 less parks, no chemist and that causes traffic problems

I: ☐ wish to be heard in support of my submission  
(please tick one) ☒ do not wish to be heard in support of my submission  
(Please tick one)

By: ☐ appearing personally  
(please tick one) ☐ being represented by the following person  
(Please tick one)

Signature: CT  
Date: \_\_\_\_\_



on the weekends. \* The Chemist Warehouse + Rite Price car park currently gets full on the weekends. Therefore I foresee this being a major problem for residents unless the number of car parks is increased, for this current site.

South Australian  
DEVELOPMENT ACT, 1993  
REPRESENTATION ON APPLICATION – CATEGORY 2

Applicant: Nielsen Architects  
Development Number: 100/E074/18  
Nature of Development: Demolition of all existing buildings and construction of a new ALDI supermarket and Chemist Warehouse building with shared car parking for up to 117 cars.  
Zone / Policy Area: Neighbourhood Centre Zone  
Subject Land: 1150-1154 & 1156-1158 South Road, Clovelly Park and 19 Wingfield Street, Clovelly Park, and 1, 3, and 5 Norrie Avenue, Clovelly Park.  
Contact Officer: Malcolm Govett  
Phone Number: 7109 7094  
Close Date: 4 October 2018

RECEIVED 27 SEP 2018

My Name: R. WATTS My phone number: 82764845

Primary method(s) of contact: \_\_\_\_\_ Email: \_\_\_\_\_  
Postal Address: 16 NORRIE AVE Postcode: 5042  
CLOVELLY PARK

**You may be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.**

- My interests are:  
(please tick one)
- ☐ owner of local property
  - ☒ occupier of local property
  - ☐ a representative of a company/other organisation affected by the proposal
  - ☐ a private citizen

The address of the property affected is:

16 NORRIE AVE CLOVELLY PARK Postcode 5042

- My interests are:  
(please tick one)
- ☐ I support the development
  - ☐ I support the development with some concerns
  - ☒ I oppose the development

The specific aspects of the application to which I make comment on are:

NARROW STREETS  
EXCESS TRAFFIC

- I: ☐ wish to be heard in support of my submission  
(please tick one) ☒ do not wish to be heard in support of my submission  
(Please tick one)

- By: ☐ appearing personally  
(please tick one) ☐ being represented by the following person  
(Please tick one)

Signature: R. Watts  
Date: 21-9-18

26 February 2019

REF No.: 00263-004

State Commission Assessment Panel  
GPO Box 1815  
ADELAIDE SA 5001

**Attention:** Malcolm Govett, Planning Officer

By Email: [Malcolm.Govett@sa.gov.au](mailto:Malcolm.Govett@sa.gov.au)

Dear Malcolm,

**RE: DA 100/E074/18 - SOUTH ROAD, CLOVELLY PARK - RESPONSE TO REPRESENTATIONS, CITY OF MARION AND DPTI TRANSPORT DIVISION**

We refer to the proposed development of an ALDI Store and Chemist Warehouse (shops) at 1150-1154, 1156-1158 South Road, 1, 3 and 5 Norrie Avenue and 17-19 Wingfield Street, Clovelly Park. Ekistics has been engaged by ALDI to respond to the Category 2 representations as well as the comments from Council and DPTI.

We note that following the first round of public notification undertaken in October 2018, the proposal was amended to address the comments received from representors and Council. In addition, the applicant amended the subject site area which was expanded to include one (1) adjoining allotment to the north (17 Wingfield Street).

The corresponding revised plans together with the amended planning statement and updated sub-consultant reports were submitted to the State Planning Commission in December 2018 and the amended development proposal was renotified to agencies and underwent a second round of Category 2 public notification.

The amendments to the original proposal include the following:

- The subject site has expanded to include 17 Wingfield Street, acquired by Chemist Warehouse;
- The proposed Chemist Warehouse building floor area has expanded to occupy 17 Wingfield Street and consequently the Gross Leasable Floor Area (GLA) has increased by 401m<sup>2</sup> (from 700m<sup>2</sup> to 1101m<sup>2</sup>);
- The proposed Chemist Warehouse building setback from the northern and western boundary has increased from 0m to 1.5 metres;
- The South Road crossover location has been relocated further south, to utilise the existing South Road crossover;
- Seven (7) additional Capital Callery Pear trees are proposed adjacent Wingfield Street, to the rear of Chemist Warehouse;
- Eight (8) additional Capital Callery Pear trees are proposed in the landscaping bed between Chemist Warehouse and the northern boundary;



- Two (2) additional Crepe Myrtle trees are proposed adjacent the southern boundary, near Norrie Avenue;
- Additional glazing is proposed to the eastern elevation of the Chemist Warehouse building;
- The car parking layout has been reconfigured to accommodate the revised crossover location and three (3) additional car parking spaces are proposed (total of 120 car parking spaces);
- Pylon sign 1 moved slightly south, closer to revised South Road crossover location;
- Western elevation of the ALDI building amended to incorporate steel feature screens with perforated metal panels; and
- The Chemist Warehouse bike rack has been relocated near the northern boundary.

In addition, further amendments have been made to enhance the western elevation of the Chemist Warehouse building and slightly modify the car park. The revised plans are included and summarised as follows:

- Improvement to the western elevation of the Chemist Warehouse building through the inclusion of steel feature screens with perforated metal panels, similar to the western elevation of the ALDI building
- Slightly modify the car park near the South Road access, as sought by DPTI; and
- Corrected drainage levels nominated on Siteworks Plan SK05 RevC to reflect the Stormwater Management Plan, as desired by Council.

The revised architectural plans are contained within **Appendix 1**. The landscaping plan was correspondingly revised to accommodate the perforated screens to the Chemist Warehouse building (**Appendix 2**).

## 1. DPTI Referral Response

The Department of Planning, Transport and Infrastructure (DPTI) provided a formal referral response to the original development proposal on 29 October 2018, on behalf of the Commissioner of Highways.

DPTI confirmed their overall support for the proposal yet noted their preference for the South Road access to be relocated to utilise the existing crossover that services the site. The South Road access was revised to align with DPTI's preferred South Road access location, as detailed on the amended plans that were subsequently referred to DPTI.

The DPTI second referral response received 14 February 2019 noted the revised South Road access location and similarly expressed their general support and satisfaction that the development will not adversely impact the adjacent road network, provided that minor car park modifications are undertaken. More specifically, DPTI requested the first north-south carpark aisle (i.e. adjacent South Road frontage) be amended to operate as a left in and left out only. DPTI's view is that such restricted movements will avoid instances where vehicles entering the site are not impeded by vehicles attempting to turn right.

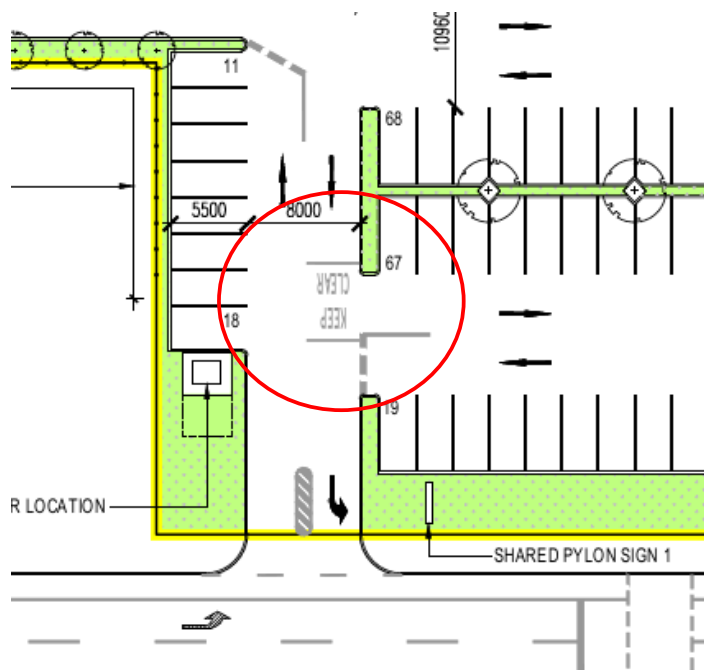
GTA Traffic Engineers have reviewed DPTI's suggestion but have concerns that the design change may accentuate the likelihood of vehicle queuing and would force vehicles out of the car park onto South Road when the driver may seek to turn right and remain within the car park.

GTA has advised as follows:

*The recommendation to modify the first aisle from South Road within the car park is impractical and will impact the parking layout causing a loss of parking. There is sufficient distance within the car park from the South Road kerb to ensure vehicles will not queue back to South Road based on the anticipated traffic volumes during the peak hour. Up to 3 vehicles can queue on this section of driveway which would be appropriate to avoid queuing unto South Road. Typical driver behaviour will see most drivers wanting to park as close to the buildings as possible, with most vehicles driving straight into the car park further to the west. If there is a queue of vehicles waiting to exit the site which extends over the aisle, then vehicles entering will not be able to turn right into the aisle and will continue further into the car park. Notwithstanding the above, the addition of a 'Keep Clear' marking at the aisle intersection will assist in managing this issue.*

As stated, GTA have suggested an alternative solution, being the addition of a 'Keep Clear' bitumen marking at the aisle intersection (refer to extract below). The revised plans illustrating this addition are provided in **Appendix 1**.

**Figure 1** Proposed 'Keep Clear' carpark marking



In addition, DPTI note that appropriate traffic management arrangements will need to be put in place to ameliorate the potential interference between delivery vehicles utilising the ALDI loading bay and traffic utilising the Norrie Avenue access.

In response to this comment, GTA offer the following:

*The frequency of truck deliveries to the proposed development will not impact the operation of the Norrie Avenue access point, with on average 2 truck deliveries per day by vehicles of 19.0 metre semi-trailer class, with one occurring generally during daytime and another in the evenings, and generally outside of peak hours. ALDI Store car parks typically share heavy vehicle movements with light vehicles including many with loading docks in close proximity to access points. No issues have arisen for existing ALDI Stores operating in Australia with similar layouts to the proposed development. Notwithstanding the above, ALDI will provide effective traffic management with line marking and signage as and when required.*

Following GTA's advice and the minor amendment to the carpark markings, we are comfortable that the proposal achieves DPTI's aim to ensure the development will not adversely impact on the adjacent road network.

## 2. City of Marion

The City of Marion ("the Council") has provided a number of comments in relation to various elements of the proposed development. We note that the initial Council referral response letter dated 22 October 2018 stated that *"whilst Council is generally supportive of the proposal as, when assessed against the applicable Development Plan provisions it achieves relative compliance, there are several elements of the proposal which Council believes require attention prior of a decision being made"*.

In response to this initial advice, several amendments were made to the proposed development in an attempt to improve the proposal and directly address Council's concerns, as well as the issues raised by DPTI and the representors.

Council's second referral response dated 20 February 2019 acknowledged the positive changes made in relation to the amended design. Notwithstanding, Council has reiterated some of their initial comments in relation to bulk, scale, interface and landscaping.

Council states that *"whilst the comprehensive redevelopment of the site will generally result in an improve(d) amenity outcome by providing modern building and car parking, existing occupants along Wingfield Street and Norrie Avenue will experience a changed outlook, particularly in relation to the bulk/scale of the built form and minimal setbacks to boundaries."*

We also recognise that the proposed development will result in a changed outlook for adjacent and adjoining occupants. However, the Development Plan seeks to guide future development rather than limit change or maintain the status quo. Further, the proposal represents a reasonable expansion of an existing Neighbourhood Centre Zone, that aligns with Development Plan *Concept Plan Map Mar/1 – Centre and Commercial*.

Additionally, the proposal has incorporated reasonable measures to minimise interface impacts with the adjoining Residential Zone including landscaping, screening features, building setbacks and acoustic measures.

## 2.1 Design, Appearance & Landscaping

The Council has raised a number of concerns regarding the design and appearance of the proposed development. In particular, concern was raised regarding the visual bulk and scale of the buildings as viewed from the surrounding streetscape and the adjoining residential allotment to the north, as well as the setback to the side and rear boundaries.

In response to these concerns, a number of amendments were made to the building designs and are discussed below.

### 2.1.1 Chemist Warehouse amendments

The Chemist Warehouse component of the proposal was amended following the first referral response from Council as follows:

- » The setback of the Chemist Warehouse building from the northern and western boundaries has been increased from zero to 1.5 metres;
- » Seven (7) additional Capital Callery Pear trees are proposed adjacent Wingfield Street, to the rear of Chemist Warehouse;
- » Eight (8) additional Capital Callery Pear trees are proposed in the landscaping bed between Chemist Warehouse and the northern boundary, to create a landscaping buffer between the current residential property and the proposal;
- » Additional glazing is proposed to the eastern elevation of the Chemist Warehouse building; and
- » Use of alternating grey colour panels on the western elevation of the Chemist Warehouse building.

The amendments to the Chemist Warehouse building setback and increased landscaping achieves Council's request for enhancements to the western façade and streetscape amenity. The provision of a 1.5m setback between the northern boundary and northern façade of the Chemist Warehouse building to accommodate a landscaping buffer will further soften the built form and improve the visual amenity.

Council's second referral response acknowledged these improvements and the resulting visual relief to the adjacent residential property to the north. However, Council did note the proposal retained an expanse of blank wall to the height of 6.25m along Wingfield Street.

In response, the Chemist Warehouse western elevation has been revised further to include the addition of perforated metal panels to add articulation and depth to the wall and complement the western elevation of the ALDI building (*Figure 2*).

Figure 2 Proposed Chemist Warehouse Western Elevation (c/-Nielsen Architects)



### 2.1.2 ALDI Store Amendments

The ALDI Store amendments include the following:

- » Further colour treatments to the southern and western façade;
- » Western elevation of the ALDI building amended to incorporate steel feature screens with perforated metal panels; and
- » Two (2) additional Crepe Myrtle trees are proposed adjacent the southern boundary, near Norrie Avenue.

In addition to the above amendments to the ALDI Store and supplementary landscaping to the southern boundary, we wish to reiterate that the proposed ALDI Store is setback approximately 3.1m from the Wingfield Street and Norrie Avenue boundaries. Wingfield Street and Norrie Avenue provides a clear delineation between the adjacent residential properties and the subject site and the boundary between the Residential Zone and the Neighbourhood Centre Zone is located along the middle of Wingfield Street and Norrie Avenue. The subject site is located approximately 5.3m from the Residential Zone boundary. As a result, the ALDI building is setback approximately 8.4m from the middle of Norrie Avenue and Wingfield Street.

Accordingly, the occupants of adjacent residential properties will view the proposed development in the context of the respective streetscapes. The proposed built form articulation and colour treatments will improve the design and appearance of the proposed development and the landscaping will further enhance the streetscape amenity and soften the built form.

**Figure 3** displays the high quality of the Wingfield Street elevation and integrated landscaping.

**Figure 3** *Proposed Wingfield Street and Norrie Ave corner elevation*



### 2.1.3 Landscaping

Council's second referral response suggested that additional ground level plantings should be incorporated adjacent the southern and western ALDI facades and further small-scale planting should be located adjacent the South Road frontage, to assist in providing an improved streetscape outcome and increase the overall attractiveness of the locality.

In response, we reiterate that a variety of landscaping is proposed in association with the development including 46 medium sized trees and a range of densely planted shrubs and groundcovers. In particular, landscaping strips will be established adjacent road frontages including the 4.5-metre-wide landscaping strip adjacent South Road and a 2.0-metre-wide landscaping buffer to Wingfield Street and Norrie Avenue to soften the appearance of the built form and the car park. Further, following Council's initial request for additional landscaping near the southern boundary, two (2) additional Crepe Myrtle trees were added adjacent the southern boundary, near Norrie Avenue to soften the built form of the ALDI building and enhance the attractiveness of the site as viewed from properties along Norrie Avenue.

We consider that, given the limited landscaping currently on the subject site and within the locality, the proposed landscaping is generous and will substantially enhance the amenity of the locality.

## 2.2 Traffic

Council note that whilst they are satisfied that the proposed number of car parking spaces exceeds the Development Plan requirement, they suggest SCAP should be satisfied that the overall increase in traffic movements can be accommodated by the existing road network and will not result in unreasonable amenity impacts on occupants of adjacent land. In response to this comment, we reiterate that the proposed development has been assessed by GTA Consultant and reviewed by DPTI and both parties express their support and satisfaction with the proposal from a traffic perspective.

### 2.2.1 Norrie Avenue Street Parking

Council has suggested that the SCAP should be satisfied that the request for the removal of on-street parking along the southern side of Norrie Avenue for the first 40m west from South Road has been resolved prior to the issuing of Development Plan Consent. We respectfully disagree with this request and note the practical realities



of the situation in which the need to remove on-street parking is hypothetical until the Development Application is granted Development Plan Consent.

Further, Council initially suggested that the removal of the limited number of car parks may impact existing adjacent businesses and local stakeholders (i.e. Jaycar). However, we note that adjacent business (including Jaycar) were given the opportunity to review and comment on the proposed development during the two, separate rounds of public notification and this matter was not raised as a concern of adjoining businesses. In addition, it is reasonable to expect that adjacent business will accommodate sufficient off-street car parking as to not rely upon on-street parking, in a similar way to the proposed development.

Lastly, we note the proposal development offers 120 on site car parking spaces, 36 more spaces than the minimum car parking rate suggested by the Marion Development Plan (84 spaces) and as such, the proposal more than supplements the few spaces that may need to be removed from Norrie Ave.

### 2.2.2 Delivery Hours

Council recommend that SCAP attach a condition of consent limiting/controlling proposed delivery hours as to ensure no unreasonable impacts on adjacent allotments occurs.

This is contrary to the proposed application which has been designed to allow 24-hour deliveries. Accordingly, we respectfully disagree with Council's request and note the independent noise impact assessment undertaken by Sonus acoustic engineers specifically considers the noise impact of deliveries at night. Sonus conclude that the predicted noise level from all noise sources (including deliveries) has been determined to achieve the relevant requirements of the Environment Protection (Noise) Policy 2007 during the day and night. Sonus also state that the maximum noise levels from ALDI deliveries during the night have been predicted to achieve the 60dB(A) criterion of Environment Protection (Noise) Policy 2007. As such, there are no ground to restrict deliveries to the site.

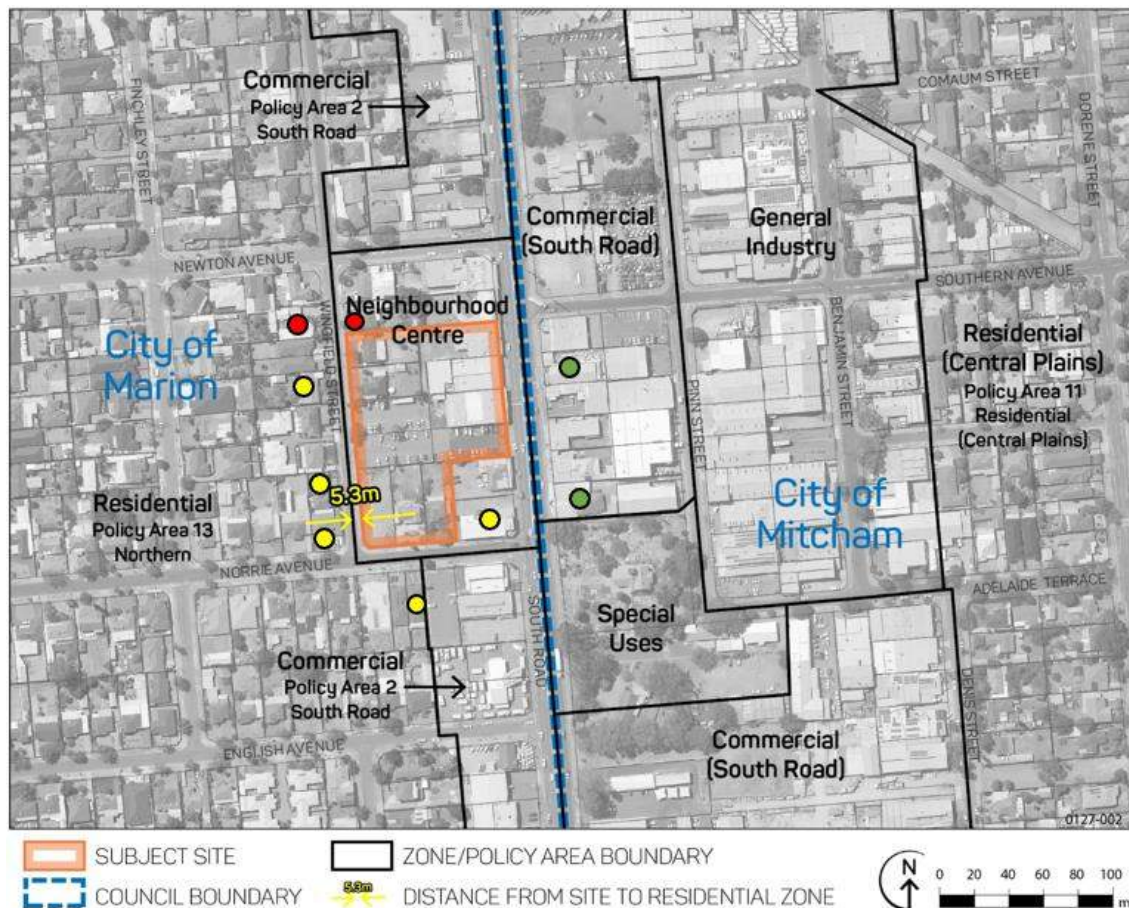
## 3. Representations

During the Category 2 notification process, approximately 70 owners and occupiers of adjacent land were notified of the proposed development. Following the first and second round of public notification, nine (9) valid representations were received and forwarded to the applicant to arrange a response in accordance with Section 38(8) of the *Development Act 1993*. We note that Ms. Furlani of 15 Wingfield Street submitted two (2) representations, one (1) during the first round of public notification and another during the second round of public notification and these have been combined and considered as one (1) representation as they are from the same representor.

Of the nine (9) representations received, two (2) representations support the development, five (5) representations support the development with some concerns and two (2) representations oppose the development.

**Figure 4** below illustrates the location of the nine (9) representors. The location of the two (2) representations in support are specified by a **green** dot, the five (5) representations in support with concerns are shown in **yellow** and the two (2) opposing representations detailed in **red**.

**Figure 4** Representations – location



A summary of the comments made by the representors is highlighted in **Table 3.1**.

**Table 3.1 Summary of Representations**

Submission	Seeking to be heard	Affected Property	Comments / Issues
R. Furlani	Yes	15 Wingfield Street, Clovelly Park	<ul style="list-style-type: none"> <li>Size and location of proposed supermarket</li> <li>Demolition of 19 Wingfield Street</li> <li>Quality of Life</li> </ul>
M. Gannon	No	116 South Road, Clovelly Park	<ul style="list-style-type: none"> <li>Cost of the shared boundary fence to be borne by the applicant</li> </ul>
E. Kansy	Yes	8 Norrie Avenue, Clovelly Park	<ul style="list-style-type: none"> <li>Property value</li> <li>Resident Only Permit Parking along Norrie Ave</li> <li>24 hour delivery</li> <li>Traffic congestion, Norrie Avenue &amp; South Road intersection traffic delays</li> <li>Bin and compactor noise, traffic noise</li> <li>Signage illumination, lighting, privacy (CCTV), Graffiti</li> </ul>
T. Link	No	1133-1137 South Road, St Marys	<ul style="list-style-type: none"> <li>Improved parking</li> </ul>
J. Petre	No	1151 South Road, St Marys	<ul style="list-style-type: none"> <li>Design creates open vista to the west</li> <li>Increased visual amenity</li> </ul>
V. Mutru	No	26 Wingfield Street, Clovelly Park	<ul style="list-style-type: none"> <li>Benefits of a new development</li> <li>Construction impacts</li> <li>Privacy, traffic congestion, pollution</li> </ul>
A.M Scime	Yes	20 Wingfield Street, Clovelly Park	<ul style="list-style-type: none"> <li>Traffic congestion</li> <li>Access to property</li> </ul>
C. Tierney	No	7 Norrie Ave, Clovelly Park	<ul style="list-style-type: none"> <li>Noise of trucks / machinery outside 7am-7pm</li> <li>Construction noise</li> <li>Inadequate off-street car parking</li> </ul>
R. Ways	No	16 Norrie Avenue, Clovelly Park	<ul style="list-style-type: none"> <li>Narrow streets and excess traffic</li> </ul>

### 3.2 Support for the proposal

The two (2) representations received in support of the proposed development are located along South Road, opposite the proposed development. Mr T. Link of 1133-1137 South Road, St Mary's stated that he believes the proposal *'should improve the parking problem in the area for people wanting to use the chemist'*.

Mr Jim Petre of South Road, St Mary's commented that in his opinion *'the design takes into consideration Residential (development) on the west(ern) side, creates (an) open vista (and will) increase visual amenity from our property'*.

An adjoining property owner at 1166 South Road, Clovelly Park, Mr Michael Gannon, noted that he supported the development with one concern regarding fencing. Mr Gannon's sole concern related to the cost of the proposed 2.4 metre high dividing fence between his land and the subject site. We can confirm that all costs

associated with the proposed fencing will be borne by the developer, as desired by Mr Gannon. Consequently, we assume that Mr Gannon's fencing concern has been resolved and his support for the proposed development remains.

### 3.3 Overview of Representor's Concerns

The remaining six (6) representations raised concerns primarily relating to on-street car parking, traffic congestion, property value, noise associated with business operations (bin store, compactor, delivers, etc.), delivery hours, construction noise, demolition of the detached dwelling at 19 Wingfield Street and the size and location of the proposed supermarket.

The concerns raised have been addressed below:

#### 3.3.1 *Amenity expectations*

Several of the representors raised concerns broadly relating to the potential loss of amenity the proposed development may have on their property, in respect to issues such as noise, traffic, delivery hours and quality of life.

We note that the respective properties of the representors who raise amenity concerns are located within the Residential Zone and sited adjacent a Neighbourhood Centre Zone, with the exception of Ms R Furlani's property (15 Wingfield Street) which is located within the Neighbourhood Centre Zone. In particular, Ms Kansy's property (8 Norrie Avenue) also abuts a Commercial Zone to the east for the full length of the side boundary which adjoins a commercial car park (Jay Car business).

It is reasonable to recognise that residential properties adjacent non-residential zones, sited close to main roads, will typically be exposed to greater amenity-related impacts created by commercial built form, greater volumes of traffic, noise and the like when compared with residential properties situated within the heart of a Residential Zone.

Whilst the preservation of practical levels of amenity is important, it is also necessary to consider existing and anticipated land use activities within the locality when determining what is an acceptable amenity level for a locality and recognise that proximity to non-residential zones will likely impact the amenity of dwellings within the locality.

This view is consistent with the approach adopted by the Environmental Resources and Development Court, as considered in the matter of *Wilkins v City of Unley* [ERDC No. 524 of 2000]:

*If people choose to live at or near the boundary between a residential zone and a business zone, they must expect some noise, traffic, overshadowing and the like which would not be appropriate further into the residential zone. Likewise, the businesses must expect some residentially based activities which may annoy.*

*[our emphasis]*

Specific amenity considerations such as noise and traffic impacts are discussed later within this letter.

### 3.3.2 Property Value

Ms E. Kansy commented that the value of their property may decrease as a consequence of the development. In response, we note that the effect that a development may have on the value of adjacent property (whether the value increases or decreases) is not a legitimate planning consideration. We note that the Environment Resources and Development Court has (on numerous occasions) confirmed that property values should not be considered when assessing the planning merits of an application. As such, this concern has not been considered further.

### 3.3.3 Demolition of the dwelling at 19 Wingfield Street, Clovelly Park

The representation received from Ms Rita Furlani (15 Wingfield Street) raised concern regarding '*the demolition of (the dwelling at) 19 Wingfield Street*'. The single storey detached dwelling and ancillary structures at 19 Wingfield Street and 17 Wingfield Street, Clovelly Park are located within a Neighbourhood Centre Zone.

In response to this concern, we note that within the Neighbourhood Centre Zone, a dwelling that is not in conjunction with a non-residential use (i.e. 17-19 Wingfield Street) is a '*non-complying*' form of development meaning that a dwelling in isolation is simply not envisaged or desired within the Zone.

The proposed replacement the detached dwelling with a commercial land use (shop) that is specifically envisaged by the Neighbourhood Centre Zone, is a desirable land use outcome that strongly aligns with the intent of the Development Plan.

Further, Ms Furlani's property will be the last remaining detached dwelling and residential land use within the Neighbourhood Centre Zone (should the proposal be approved and constructed) and it is reasonable to assume that at some point in time, 15 Wingfield Street may be developed to accommodate a commercial land use given the supportive centre zoning and surrounding land uses.

### 3.3.4 Size of the proposed supermarket

As mentioned within our planning statement, the proposed ALDI Supermarket and Chemist Warehouse have a combined gross leasable floor area of 2,833m<sup>2</sup>. The existing retail buildings, which will be demolished to make way for the new development currently account for approximately 2,100m<sup>2</sup> gross leasable floor area. The proposal presents an increase of approximately 733m<sup>2</sup> of additional retail floor area which represents approximately 5% of the 14,500m<sup>2</sup> Neighbourhood Centre Zone bound by Norrie Avenue, Wingfield Street, Newton Avenue and South Road.

Whilst the proposed retail floor area exceeds the recommended threshold of 2000m<sup>2</sup> detailed within PDC 5 of the Neighbourhood Centre Zone, the existing retail floor area similarly exceeds 2000m<sup>2</sup> and in the context of a new, integrated site redevelopment, this additional floor area is considered appropriate and will not compromise the intent of the Neighbourhood Centre Zone or the viability of any other Centres within the region.

Further, the location and size of the proposed supermarket represents a reasonable, practical and contemporary development that aligns with Development Plan *Concept Plan Map Mar/1 – Centre and Commercial*.

### 3.3.5 Car parking

A number of representors raised concerns regarding sufficient off-street car parking and potential overflow to nearby local roads.

As mentioned within our planning statement, the proposed development will provide a total of 120 car parks across the site and equates to a parking rate of 4.3 spaces per 100m<sup>2</sup> which comfortably exceeds the Development Plan requirements. As such, it is anticipated that there will be sufficient off-street car parking to satisfy the demand for of the proposed development, without overflow to local road network. We also reiterate that GTA consultants and DPTI have reviewed the proposed car parking arrangement and are satisfied suitable off-street car parking has been provided.

### 3.3.6 Norrie Avenue / South Road Intersection

The representations received from Ms E. Kansy included concerns relating to existing and anticipated traffic issues associated with turning from Norrie Avenue into South Road. Specifically, the representors raised concerns that the proposed development would result in additional traffic congestion causing long delays to turn onto (and also from) South Road from Norrie Avenue and the ability of South Road to accommodate additional traffic.

In response to these concerns, it is noted that GTA has reviewed the South Road/Norrie Avenue intersection utilising a SIDRA analysis and concluded that:

*‘the intersection will operate similar to existing conditions with some increases to the average delay and 95th percentile queue length on a couple of approaches. During the Weekday Peak, the right turn into Norrie Avenue would experience a 5.7 metre and 6.8 second increase in the 95th percentile queue length (less than one vehicle) and average delay respectively. The right turn out will experience an increase in queue length by 2 metres which is marginal. During the Saturday Peak, the right turn in will experience a 29 second increase in the average delay and an 11.7 metre increase in the 95th percentile queue. While this is a more notable increase, and the approach will experience a drop of Level of Service (LOS) to F, this delay is considered acceptable, with the 95th percentile queue being within the storage capacity of the right turn lane. The degree of Saturation for the Saturday Peak was 0.779 for the right turn indicating it will operate within the maximum capacity. Therefore, there will be no adverse impacts from the right turn lane into the site. The right turn onto South Road will experience a 3 metre increase in the 95th percentile queue length which is marginal.’*

In terms of the concerns that additional cars will add to traffic congestion within the surrounding road network, GTA note that *‘the additional traffic generated by the ALDI Development won’t adversely impact on the surrounding road network. While there will be increases in the average delays and queue lengths, these will generally be marginal.’*



Based on the traffic assessment undertaken by qualified traffic engineers (GTA) and the fact that DPTI has reviewed the proposal and are generally supportive of GTA's assessment, we remain of the view that the application appropriately addresses the relevant Transportation and Access provisions of the Development Plan.

### 3.3.7 Resident Only Permit Parking along Norrie Ave

Ms E. Kansy noted their preference for the existing one-hour street parking adjacent their property to become 'Resident Only Parking' to prevent members of the public parking a vehicle on the public road, forward of their property. We note that this matter is not a valid planning consideration in the assessment of the proposed development and should be raised with Marion Council.

### 3.3.8 Noise

The representations received from Ms E. Kansy (8 Norrie Avenue) and Ms C. Tirney (7 Norrie Avenue) raised concerns regarding the potential noise implications of the compactor and bin storage area. Ms Kansy is particularly concerned about the noise associated with the delivery trucks, the compactor and bin storage area and the potential impacts on the amenity of their property opposite the proposed ALDI supermarket.

As mentioned previously, it is important to note that residential properties abutting commercial zones and close to main roads, will typically be exposed to greater amenity-related impacts created by greater volumes of noise and the like when compared with residential properties situated within the heart of a Residential Zone.

Notwithstanding, acoustic engineers from Sonus have reviewed the proposal and completed an Environmental Noise Assessment. In particular, the Sonus assessment considers nearby residences from car park activity, vehicle movements, delivery activity, mechanical plant, and rubbish collection within the context of the surrounding acoustic environment to ensure the proposal does not adversely impact on the amenity of the locality.

Sonus has advised that subject to certain acoustic treatments, which have been wholly integrated into the proposal, the development will satisfy the requirements of the Environment Protection (Noise) Policy 2007.

Sonus conclude that:

*'the facility has been designed to minimise adverse impact and conflict between land uses, avoid unreasonable interference on amenity, and will not detrimentally affect the locality by way of noise, thereby achieving the relevant provisions of the Development Plan related to environmental noise'*

### 3.3.9 Signage, Lighting, Graffiti and Privacy

The representation from Ms E. Kansy (8 Norrie Avenue) queried the dimensions and illumination of the ALDI sign on the southern wall of the building, any additional lighting and the potential impact of CCTV on her residential privacy.

The proposed small gable sign on the southern elevation of the ALDI building is 1.2m wide by 1.4m high and incorporates internal 'daylight' (cool white) fluorescent lighting, light output: Calvin – 40,000, Lumens – 16,500.

The proposed illuminated signs across the site will comprise the same light output detailed above, which is considered to be a low level of illumination.

In addition, we note that all site lighting will be installed in accordance with relevant Australian Standards which will ensure that lighting is directed internal to the site any impacts are kept to a minimum. We also note that DPTI does not object to the car parking area being illuminated provided that any lighting is appropriately located and/or shielded in order to minimise the potential for driver distraction or discomfort. Accordingly, the suggested DPTI condition to provide a low level of signage illumination (i.e.  $<150\text{Cd/m}^2$ ) will be adhered to.

Further, should CCTV cameras be installed by ALDI, they will be directed inwards towards the subject site, will not impact adjacent residential properties and will comply with the relevant laws including the *Privacy Act 1988* and the Australian Privacy Principles (APPs).

In the event that graffiti appears on the site, this matter will be resolved by store management as soon as practical. It is in the interest of the store managers to ensure the amenity of the site is maintained. Notwithstanding, this is not a valid planning consideration.

### *3.3.10 Construction impacts*

Mr. V. Mutru raised concern with possible disruption during the construction of the proposed development associated with construction noise and dust associated with vehicles entering and leaving the construction site, etc. Whilst this is not a valid planning consideration, if the proposal is approved, construction will be undertaken strictly in accordance with the *Environment Protection Act 1993* and the *Environment Protection (Noise) Policy, 2007*.

## 4. Conclusion

Based on our review of the comments from Council and DPTI as well as the Category 2 representations, we maintain our view that the proposed ALDI Store and Chemist Warehouse represents appropriate and orderly development that deserves favourable consideration for approval.

In particular, we note that DPTI has carefully reviewed the access and parking arrangements and has expressed their satisfaction with the proposal, subject to minor car park modification which have been addressed by GTA. We also wish to reiterate that Council state they are generally supportive of the proposal. Notwithstanding, the plans have been amended to further address concerns raised by Council.

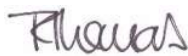
The proposed development is aligned with the most relevant provisions of the Development Plan and warrants Development Plan Consent, subject to reasonable and relevant conditions.

We thank you for the opportunity to respond to these matters and we trust this submission offers a constructive response to the issues raised.

We confirm our desire to be heard and/or answer any questions raised at the State Commission Assessment Panel meeting on March 14<sup>th</sup> 2019.

Please do not hesitate to contact the undersigned should you require any further clarification in relation to this submission.

Yours Sincerely



**Rebecca Thomas**  
Senior Associate

Cc Nigel Uren, Property Director – ALDI Stores

## Advertisements

### OBJECTIVES

- 1 Urban and rural landscapes that are not disfigured by advertisements and/or advertising hoardings.
- 2 Advertisements and/or advertising hoardings that do not create a hazard.
- 3 Advertisements and/or advertising hoardings designed to enhance the appearance of the building and locality.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 The location, siting, design, materials, size, and shape of advertisements and/or advertising hoardings should be:
  - (a) consistent with the predominant character of the urban or rural landscape
  - (b) in harmony with any buildings or sites of historic significance or heritage value in the area
  - (c) co-ordinated with and complement the architectural form and design of the building they are to be located on.
- 2 The number of advertisements and/or advertising hoardings associated with a development should be minimised to avoid:
  - (a) clutter
  - (b) disorder
  - (c) untidiness of buildings and their surrounds
  - (d) driver distraction.
- 3 Buildings occupied by a number of tenants should exhibit co-ordinated and complementary advertisements and/or advertising hoardings to identify the tenants and their type of business.
- 4 The content of advertisements should be limited to information relating to the legitimate use of the associated land.
- 5 Advertisements and/or advertising hoardings should:
  - (a) be completely contained within the boundaries of the subject allotment
  - (b) be sited to avoid damage to, or pruning or lopping of, on-site landscaping or street trees
  - (c) not obscure views to vistas or objects of high amenity value.
- 6 Advertisements and/or advertising hoardings should not be erected on:
  - (a) a public footpath or veranda post
  - (b) a road, median strip or traffic island
  - (c) a vehicle adapted and exhibited primarily as an advertisement

- (d) residential land.
- 7 Advertisements and/or advertising hoardings attached to buildings should not be sited on the roof or higher than the walls of a building, unless the advertisement or advertising hoarding is appropriately designed to form an integrated and complementary extension of the existing building.
- 8 Advertisements and/or advertising hoardings erected on a veranda or that project from a building wall should:
  - (a) have a minimum clearance above a footway of 2.5 metres to allow for safe and convenient pedestrian access
  - (b) where erected on the side of a veranda, not exceed the width of the veranda or project from the veranda
  - (c) where erected on the front of a veranda, be setback a minimum of 400 millimetres from the kerbline and not exceed the length of the veranda or project from the veranda
  - (d) where projecting from a wall, have the edge of the advertisement or advertising hoarding abutting the surface of the wall.
- 9 Signs should not be silhouetted against the sky or project beyond the architectural outline of the building.
- 10 Advertisements should be designed to conceal their supporting advertising hoarding from view.
- 11 Advertisements should convey the owner/occupier and/or generic type of business, merchandise or services using simple, clear and concise language, symbols, print style and layout and a small number of colours.
- 12 Signs that primarily use an architectural or sculptural form (rather than text) to identify with the on-site enterprise should have maximum dimensions that approximate with those recommended for free-standing signs.
- 13 Advertisements which perform a secondary role in identifying the business, goods or services should only be readable in the immediate vicinity of the site.
- 14 Outside of townships and country settlements advertisements other than traffic signs, tourist signs or advertisements on an existing tourist information bay display board, should not be erected in road reserves.

## Safety

- 15 Advertisements and/or advertising hoardings should not create a hazard by:
  - (a) being so highly illuminated as to cause discomfort to an approaching driver, or to create difficulty in the driver's perception of the road or persons or objects on the road
  - (b) being liable to interpretation by drivers as an official traffic sign, or convey to drivers information that might be confused with instructions given by traffic signals or other control devices, or impair the conspicuous nature of traffic signs or signals
  - (c) distracting drivers from the primary driving task at a location especially where the demands on driver concentration are high
  - (d) obscuring a driver's view of other road or rail vehicles at/or approaching level crossings, or of pedestrians or of features of the road that are potentially hazardous (eg junctions, bends, changes in width, traffic control devices).



- 16 Advertisements should not be erected in positions close to existing electricity mains so that potentially hazardous situations are created.
- 17 Any internally illuminated advertising signs and/or advertising hoardings which utilise LED, LCD or other similar technologies should be located a minimum of 80 metres from traffic signals, level crossings and other important traffic control devices.

### Freestanding Advertisements

- 18 Freestanding advertisements and/or advertising hoardings should be:
  - (a) limited to only one primary advertisement per site or complex
  - (b) of a scale and size in keeping with the desired character of the locality and compatible with the development on the site.
- 19 Freestanding advertisements and/or advertising hoardings for multiple-business tenancy complexes should:
  - (a) incorporate the name or nature of each business or activity within the site or complex in a single advertisement
  - (b) be integrally designed and mounted below the more predominant main complex or site identity advertisement.
- 20 The maximum height and display area or panel size of freestanding advertisements and/or advertising hoardings should not exceed the following dimensions within the respective zones:

Zone	Maximum Height (metres)	Maximum Display Area or Panel Size (square metres)
Non-residential sites within the Residential Zone	4	4 (2 per side if double-sided)
District Centre Zone	10 – associated with the retail core 8 – elsewhere	16 – associated with the retail core (8 per side if double-sided) 12 – elsewhere (6 per side if double-sided)
Industry Zone excluding Industry/Commerce Policy Area 5	10	16 (8 per side if double-sided)
Local Centre Zone	5	8 (4 per side if double-sided)
Neighbourhood Centre Zone	8	12 (6 per side if double-sided)
Regional Centre Zone	12 – associated with the retail core 8 – elsewhere	20 – associated with the retail core (10 per side if double-sided) 12 – elsewhere (6 per side if double-sided)
Primary Production Zone	3.5	2 (1 per side if double-sided)

- 21 Portable, easel or A-frame advertisements should be displayed only where:
  - (a) no other appropriate opportunity exists for an adequate co-ordinated and permanently erected advertisement and/or advertising hoarding
  - (b) they do not obstruct or compromise the safety of pedestrians or vehicle movement



- (c) there is no unnecessary duplication or proliferation of advertising information
  - (d) there is no damage to, or removal of, any landscaping on the site.
- 22 Portable, easel or A-frame advertisements associated with a development should be displayed only during the hours the development is open for trading.

### **Flags, Bunting and Streamers**

- 23 Advertisements and/or advertising hoardings should not comprise bunting, streamers or attached floating objects.

### **Advertising along Arterial Roads**

- 24 Advertising and/or advertising hoardings should not be placed along arterial roads that have a speed limit of 80 km/h or more.

### **Advertising in Mixed Use and Corridor Zones**

- 25 Advertisements and/or advertising hoardings should be:
- (a) no higher than the height of the finished floor level of the second storey of the building to which it relates
  - (b) where located below canopy level, flush with the wall or projecting horizontally
  - (c) where located at canopy level, in the form of a fascia sign
  - (d) where located above the canopy, flush with the wall and within the height of the parapet.
- 26 Advertisements or advertising hoardings should not exceed 25 per cent of the ground floor wall area on the façade the sign is placed.

## Centres and Retail Development

### OBJECTIVES

- 1 Shopping, administrative, cultural, community, entertainment, educational, religious and recreational facilities located in integrated centres and mixed use zones.
- 2 Centres that ensure rational, economic and convenient provision of goods and services and provide:
  - (a) a focus for community life
  - (b) safe, permeable, pleasant and accessible walking and cycling environments.
- 3 The provision of safe pedestrian and cycling environments within centres which gives high priority to pedestrians, public and community transport.
- 4 Increased vitality and activity in centres through the introduction and integration of housing.
- 5 **Development of the Marion Regional Centre as a focus for a large part of the southern Adelaide metropolitan area.**
- 6 The central business district of the City of Adelaide providing the principal focus for the economic, social and political life of Greater Adelaide and the State.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development within centres should:
  - (a) integrate facilities within the zone
  - (b) allow for the multiple use of facilities and the sharing of utility spaces
  - (c) allow for the staging of development within the centre
  - (d) be integrated with public and community transport
  - (e) **should not include service trade premises except where located on the periphery of the centre.**
- 2 Development within centres should be designed to be compatible with adjoining areas. This should be promoted through landscaping, screen walls, centre orientation, location of access ways, buffer strips and transitional use areas.
- 3 Development within centres should provide:
  - (a) public spaces such as malls, plazas and courtyards
  - (b) street furniture, including lighting, signs, litter bins, seats and bollards, that is sited and designed to complement the desired character
  - (c) unobtrusive facilities for the storage and removal of waste materials
  - (d) public facilities including toilets, infant changing facilities for parents, telephones and community information boards
  - (e) access for public and community transport and sheltered waiting areas for passengers

- (f) lighting for pedestrian paths, buildings and associated areas
  - (g) a single landscaping theme
  - (h) safe and secure bicycle parking.
- 4 A single architectural theme should be established within centres through:
- (a) constructing additions or other buildings in a style complementary to the existing shopping complex
  - (b) renovating the existing shopping complex to complement new additions and other buildings within the centre
  - (c) employing a signage theme.

### **Arterial Roads**

- 5 Centres should develop on one side of an arterial road or in one quadrant of an arterial road intersection.
- 6 Centre development straddling an arterial road should:
- (a) concentrate on one side of the arterial road or one quadrant of the arterial road intersection
  - (b) minimise the need for pedestrian and vehicular movement from one part of the centre to another across the arterial road.

### **Retail Development**

- 7 Other than in relation to the **Regional Activity Zone and Suburban Activity Node Zone**, a shop or group of shops located outside of zones that allow for retail development should:
- (a) be of a size and type that will not hinder the development, function or viability of any centre zone
  - (b) not demonstrably lead to the physical deterioration of any designated centre
  - (c) be developed taking into consideration its effect on adjacent development
  - (d) incorporate a road or thoroughfare at the rear for the use of vehicles which is not less than 6 metres wide and which communicates with a public road at each end
  - (e) incorporate a site having a depth of not less than 24 metres.
- 8 Bulky goods outlets located within centres zones should:
- (a) complement the overall provision of facilities
  - (b) be sited towards the periphery of those centres where the bulky goods outlet has a gross leasable area of 500 square metres or more.



## Design and Appearance

### OBJECTIVES

- 1 Development of a high design standard and appearance that responds to and reinforces positive aspects of the local environment and built form.
- 2 Roads, open spaces, paths, buildings and land uses laid out and linked so that they are easy to understand and navigate.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Buildings should reflect the desired character of the locality while incorporating contemporary designs that have regard to the following:
  - (a) building height, mass and proportion
  - (b) external materials, patterns, colours and decorative elements
  - (c) roof form and pitch
  - (d) façade articulation and detailing
  - (e) verandas, eaves, parapets and window screens.
- 2 Buildings should be sited with respect to side and rear property boundaries to:
  - (a) maintain or enhance the amenity of adjoining properties in terms of noise, privacy and sunlight
  - (b) minimise the impact of bulk and scale of development on adjoining properties
  - (c) maintain the character of the locality in regards to the patterns of space between buildings (to the side and rear) and the opportunity for landscaping.
- 3 The external walls and roofs of buildings should not incorporate highly reflective materials which will result in glare to neighbouring properties, drivers or cyclists.
- 4 Structures located on the roofs of buildings to house plant and equipment should be screened from view and should form an integral part of the building design in relation to external finishes, shaping and colours.
- 5 Balconies should:
  - (a) be integrated with the overall form and detail of the building
  - (b) include balustrade detailing that enables line of sight to the street
  - (c) be recessed where wind would otherwise make the space unusable.
- 6 Transportable buildings and buildings which are elevated on stumps, posts, piers, columns or the like, should have their suspended footings enclosed around the perimeter of the building, and the use of verandas, pergolas and other suitable architectural detailing to give the appearance of a permanent structure.

### Development Adjacent Heritage Places

- 7 The design of multi-storey buildings should not detract from the form and materials of adjacent State and local heritage places listed in [Table Mar/4 - State Heritage Places](#) or in [Table Mar/3 - Local Heritage Places](#).
- 8 Development on land adjacent to a State or local heritage place, as listed in [Table Mar/4 - State Heritage Places](#) or in [Table Mar/3 - Local Heritage Places](#), should be sited and designed to reinforce the historic character of the place and maintain its visual prominence.

### Overshadowing

- 9 The design and location of buildings should enable direct winter sunlight into adjacent dwellings and private open space and minimise the overshadowing of:
  - (a) windows of habitable rooms
  - (b) upper-level private balconies that provide the primary open space area for a dwelling
  - (c) solar collectors (such as solar hot water systems and photovoltaic cells).
- 10 Except where otherwise specified in a zone, policy area or precinct, development should ensure that:
  - (a) north-facing windows to living rooms of existing dwelling(s) on the same allotment, and on adjacent allotments, receive at least 3 hours of direct sunlight over a portion of their surface between 9 am and 3 pm on the 21 June
  - (b) ground level private open space of existing buildings receive direct sunlight for a minimum of 2 hours between 9 am and 3 pm on 21 June to at least the smaller of the following:
    - (i) half of the existing ground level private open space
    - (ii) 35 square metres of the existing ground level private open space
  - (c) where overshadowing already exceeds the requirements contained in part (b), development should not increase the area overshadowed.

### Visual Privacy

- 11 Buildings with upper level windows, balconies, terraces and decks should minimise direct overlooking of habitable rooms and private open spaces of dwellings through one or more of the following measures:
  - (a) off-setting the location of balconies and windows of habitable rooms with those of other buildings so that views are oblique rather than direct
  - (b) building setbacks from boundaries (including boundary to boundary where appropriate) that interrupt views or that provide a spatial separation between balconies or windows of habitable rooms
  - (c) screening devices (including fencing, obscure glazing, screens, external ventilation blinds, window hoods and shutters) that are integrated into the building design and have minimal negative effect on residents' or neighbours' amenity.
- 12 Permanently fixed external screening devices should be designed and coloured to complement the associated building's external materials and finishes.

### **Relationship to the Street and Public Realm**

- 13 Buildings (other than ancillary buildings, group dwellings or buildings on allotments with a battle axe configuration) should be designed so that the main façade faces the primary street frontage of the land on which they are situated.
- 14 Buildings, landscaping, paving and signage should have a coordinated appearance that maintains and enhances the visual attractiveness of the locality.
- 15 Buildings should be designed and sited to avoid extensive areas of uninterrupted walling facing areas exposed to public view.
- 16 Building design should emphasise pedestrian entry points to provide perceptible and direct access from public street frontages and vehicle parking areas.
- 17 The ground floor of mixed use buildings should comprise non-residential land uses.
- 18 In mixed use areas, development facing the street should be designed to activate the street frontage(s) by:
  - (a) including features that attract people to the locality such as frequent doors and display windows, retail shopfronts and/or outdoor eating or dining areas
  - (b) minimising the frontage for fire escapes, service doors, plant and equipment hatches
  - (c) avoiding undercroft or ground floor vehicle parking that is visible from the primary street frontage
  - (d) using colour, vertical and horizontal elements, roof overhangs and other design techniques to provide visual interest and reduced massing.
- 19 Where zero or minor setbacks are desirable, development should incorporate shelter over footpaths to enhance the quality of the pedestrian environment.

### **Outdoor Storage and Service Areas**

- 20 Outdoor storage, loading and service areas should be:
  - (a) screened from public view by a combination of built form, solid fencing and/or landscaping
  - (b) conveniently located and designed to enable the manoeuvring of service and delivery vehicles
  - (c) sited away from sensitive land uses.

### **Building Setbacks from Road Boundaries**

- 21 Except in areas where a new character is desired, the setback of buildings from public roads should:
  - (a) be similar to, or compatible with, setbacks of buildings on adjoining land and other buildings in the locality
  - (b) contribute positively to the function, appearance and/or desired character of the locality.

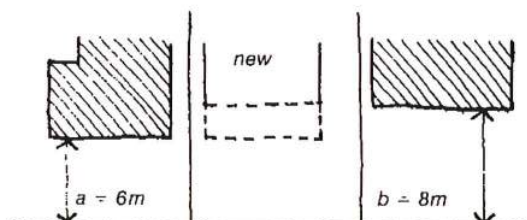


- 22 Except where specified in a particular zone, policy area or precinct the main face of a building should be set back from the primary road frontage in accordance with the following table:

Setback difference between buildings on adjoining allotments with the same primary street frontage	Setback of new building
--	-------------------------

Up to 2 metres

The same setback as one of the adjoining buildings, as illustrated below:



When  $b - a \leq 2$ , setback of new dwelling = a or b

Greater than 2 metres

At least the average setback of the adjoining buildings

- 23 Except where otherwise specified by another provision in this Development Plan or where specified in a particular zone, policy area or precinct buildings and structures should be set back at least **8 metres** from road boundaries.
- 24 All setbacks from the road frontage should be additional to the road widening setback established under the *Metropolitan Adelaide Road Widening Plan Act 1972*.

## **Interface between Land Uses**

### **OBJECTIVES**

- 1 Development located and designed to minimise adverse impact and conflict between land uses.
- 2 Protect community health and amenity from adverse impacts of development.
- 3 Protect desired land uses from the encroachment of incompatible development.

### **PRINCIPLES OF DEVELOPMENT CONTROL**

- 1 Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:
  - (a) the emission of effluent, odour, smoke, fumes, dust or other airborne pollutants
  - (b) noise
  - (c) vibration
  - (d) electrical interference
  - (e) light spill
  - (f) glare
  - (g) hours of operation
  - (h) traffic impacts.
- 2 Development should be sited and designed to minimise negative impacts on existing and potential future land uses desired in the locality.
- 3 Development adjacent to a Residential Zone should be designed to minimise overlooking and overshadowing of adjacent dwellings and private open space.
- 4 Residential development adjacent to non-residential zones and land uses should be located, designed and/or sited to protect residents from potential adverse impacts from non-residential activities.
- 5 Sensitive uses likely to conflict with the continuation of lawfully existing developments and land uses desired for the zone should be designed to minimise negative impacts.
- 6 Non-residential development on land abutting a residential zone should be designed to minimise noise impacts to achieve adequate levels of compatibility between existing and proposed uses.

### **Noise Generating Activities**

- 7 Development that emits noise (other than music noise) should include noise attenuation measures that achieve the relevant Environment Protection (Noise) Policy criteria when assessed at the nearest existing noise sensitive premises.
- 8 Development with the potential to emit significant noise (e.g. industry) should incorporate noise attenuation measures that prevent noise from causing unreasonable interference with the amenity of noise sensitive premises.

- 9 Outdoor areas (such as beer gardens or dining areas) associated with licensed premises should be designed or sited to minimise adverse noise impacts on adjacent existing or future noise sensitive development.
- 10 Development proposing music should include noise attenuation measures that achieve the following desired noise levels:

Noise level assessment location	Desired noise level
Adjacent existing <i>noise sensitive development</i> property boundary	Less than 8 dB above the level of background noise ( $L_{90,15min}$ ) in any octave band of the sound spectrum and Less than 5 dB(A) above the level of background noise ( $LA_{90,15min}$ ) for the overall (sum of all octave bands) A-weighted level.
Adjacent <i>land</i> property boundary	Less than 65dB(Lin) at 63Hz and 70dB(Lin) in all other octave bands of the sound spectrum or less than 8 dB above the level of background noise ( $L_{90,15min}$ ) in any octave band of the sound spectrum and 5 dB(A) overall (sum of all octave bands) A-weighted level.

### **Air Quality**

- 11 Development with the potential to emit harmful or nuisance-generating air pollution should incorporate air pollution control measures to prevent harm to human health or unreasonable interference with the amenity of sensitive uses within the locality.
- 12 Chimneys or exhaust flues associated with commercial development (including cafes, restaurants and fast food outlets) should be designed to ensure they do not cause a nuisance or health concerns to nearby sensitive receivers by:
  - (a) incorporating appropriate treatment technology before exhaust emissions are released to the atmosphere
  - (b) ensuring that the location and design of chimneys or exhaust flues maximises dispersion and takes into account the location of nearby sensitive uses.

## Landscaping, Fences and Walls

### OBJECTIVES

- 1 The amenity of land and development enhanced with appropriate planting and other landscaping works, using locally indigenous plant species where possible.
- 2 Functional fences and walls that enhance the attractiveness of development.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should incorporate open space and landscaping in order to:
  - (a) complement built form and reduce the visual impact of larger buildings (eg taller and broader plantings against taller and bulkier building components)
  - (b) enhance the appearance of road frontages
  - (c) screen service yards, loading areas and outdoor storage areas
  - (d) minimise maintenance and watering requirements
  - (e) enhance and define outdoor spaces, including car parking areas
  - (f) provide shade and shelter
  - (g) assist in climate control within buildings
  - (h) maintain privacy
  - (i) maximise stormwater re-use
  - (j) complement existing native vegetation
  - (k) contribute to the viability of ecosystems and species
  - (l) promote water and biodiversity conservation.
- 2 Landscaping should:
  - (a) include the planting of locally indigenous species where appropriate
  - (b) be oriented towards the street frontage
  - (c) result in the appropriate clearance from powerlines and other infrastructure being maintained.
- 3 Landscaped areas along road frontages should have a width of not less than 2 metres and be protected from damage by vehicles and pedestrians.
- 4 Landscaping should not:
  - (a) unreasonably restrict solar access to adjoining development
  - (b) cause damage to buildings, paths and other landscaping from root invasion, soil disturbance or plant overcrowding



- (c) introduce pest plants
- (d) increase the risk of bushfire
- (e) remove opportunities for passive surveillance
- (f) increase autumnal leave fall in waterways
- (g) increase the risk of weed invasion.

5 Fences and walls, including retaining walls, should:

- (a) not result in damage to neighbouring trees
- (b) be compatible with the associated development and with existing predominant, attractive fences and walls in the locality
- (c) enable some visibility of buildings from and to the street to enhance safety and allow casual surveillance
- (d) incorporate articulation or other detailing where there is a large expanse of wall facing the street
- (e) assist in highlighting building entrances
- (f) be sited and limited in height, to ensure adequate sight lines for motorists and pedestrians especially on corner sites
- (g) in the case of side and rear boundaries, be of sufficient height to maintain privacy and/or security without adversely affecting the visual amenity or access to sunlight of adjoining land
- (h) be constructed of non-flammable materials.

## Transportation and Access

### OBJECTIVES

- 1 A comprehensive, integrated, affordable and efficient air, rail, sea, road, cycle and pedestrian transport system that will:
  - (a) provide equitable access to a range of public, community and private transport services for all people
  - (b) ensure a high level of safety
  - (c) effectively support the economic development of the State
  - (d) have minimal negative environmental and social impacts
  - (e) maintain options for the introduction of suitable new transport technologies.
- 2 Development that:
  - (a) provides safe and efficient movement for all transport modes
  - (b) ensures access for vehicles including emergency services, public infrastructure maintenance and commercial vehicles
  - (c) provides off-street parking
  - (d) is appropriately located so that it supports and makes best use of existing transport facilities and networks
  - (e) provides convenient and safe access to public transport stops.
- 3 A road hierarchy that promotes safe and efficient transportation in an integrated manner throughout the State.
- 4 Provision of safe, pleasant, accessible, integrated and permeable pedestrian and cycling networks that are connected to the public transport network.
- 5 Safe and convenient freight and people movement throughout the State.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 Land uses arranged to support the efficient provision of sustainable transport networks and encourage their use.

#### Movement Systems

- 2 Development should be integrated with existing transport networks, particularly major rail, road and public transport corridors as shown on *Location Maps* and *Overlay Maps - Transport*, and designed to minimise its potential impact on the functional performance of the transport network.
- 3 Transport corridors should be sited and designed so as to not unreasonably interfere with the health and amenity of adjacent sensitive land uses.
- 4 Roads should be sited and designed to blend with the landscape and be in sympathy with the terrain.



- 5 Land uses that generate large numbers of visitors such as shopping centres, places of employment, schools, hospitals and medium to high density residential uses should be located so that they can be serviced by the public transport network and encourage walking and cycling.
- 6 Development generating high levels of traffic, such as schools, shopping centres and other retail areas, and entertainment and sporting facilities should incorporate passenger pick-up and set-down areas. The design of such areas should minimise interference to existing traffic and give priority to pedestrians, cyclists and public and community transport users.
- 7 The location and design of public and community transport set-down and pick-up points should maximise safety and minimise the isolation and vulnerability of users.
- 8 Development should provide safe and convenient access for all anticipated modes of transport.
- 9 Development at intersections, pedestrian and cycle crossings, and crossovers to allotments should maintain or enhance sightlines for motorists, cyclists and pedestrians to ensure safety for all road users and pedestrians.
- 10 Driveway crossovers affecting pedestrian footpaths should maintain the level and surface colour of the footpath.
- 11 Driveway crossovers should be separated and the number minimised to optimise the provision of on-street visitor parking (where on-street parking is appropriate).
- 12 Development should be designed to discourage commercial and industrial vehicle movements through residential streets and adjacent other sensitive land uses.
- 13 Industrial/commercial vehicle movements should be separated from passenger vehicle car parking areas.
- 14 Development should provide for the on-site loading, unloading and turning of all traffic likely to be generated.

### **Cycling and Walking**

- 15 Development should ensure that a permeable street and path network is established that encourages walking and cycling through the provision of safe, convenient and attractive routes with connections to adjoining streets, paths, open spaces, schools, pedestrian crossing points on arterial roads, public and community transport stops and activity centres.
- 16 Development should provide access, and accommodate multiple route options, for pedestrians and cyclists by enhancing and integrating with:
  - (a) open space networks, recreational trails, parks, reserves, and sport and recreation areas
  - (b) Adelaide's principal cycling network (Bikedirect), which includes arterial roads, local roads and off-road paths as depicted in [Overlay Maps - Transport](#).
- 17 New developments should give priority to and not compromise existing designated bicycle routes.
- 18 Where development coincides with, intersects or divides a proposed bicycle route or corridor, development should incorporate through-access for cyclists.
- 19 Development should encourage and facilitate cycling as a mode of transport by incorporating end-of-journey facilities including:
  - (a) showers, changing facilities and secure lockers
  - (b) signage indicating the location of bicycle facilities.

- 20 On-site secure bicycle parking facilities should be:
- (a) located in a prominent place
  - (b) located at ground floor level
  - (c) located undercover
  - (d) located where surveillance is possible
  - (e) well lit and well signed
  - (f) close to well used entrances
  - (g) accessible by cycling along a safe, well lit route.
- 21 Pedestrian and cycling facilities and networks should be designed and provided in accordance with relevant provisions of the *Australian Standards and Austroads Guides*.

### **Access**

- 22 Development should have direct access from an all-weather public road.
- 23 Development should be provided with safe and convenient access which:
- (a) avoids unreasonable interference with the flow of traffic on adjoining roads
  - (b) provides appropriate separation distances from existing roads or level crossings
  - (c) accommodates the type and volume of traffic likely to be generated by the development or land use and minimises induced traffic through over-provision
  - (d) is sited and designed to minimise any adverse impacts on the occupants of and visitors to neighbouring properties.
- 24 Development should not restrict access to publicly owned land such as recreation areas.
- 25 The number of vehicle access points onto arterial roads shown on *Overlay Maps - Transport* should be minimised and, where possible, access points should be:
- (a) limited to local roads (including rear lane access)
  - (b) shared between developments.
- 26 Development with access from roads with existing or projected traffic volumes exceeding 6000 vehicles per day should be sited to avoid the need for vehicles to reverse onto or from the road.
- 27 Development with access from arterial roads or roads as shown on *Overlay Maps – Transport* should be sited to avoid the need for vehicles to reverse onto or from the road.
- 28 The number of vehicle access points onto a public road should be minimised and each access point should be a minimum of 6 metres apart to maximise opportunities for on street parking.
- 29 Structures such as canopies and balconies that encroach onto the footpath of a road should not cause visual or physical obstruction to:
- (a) signalised intersections
  - (b) heavy vehicles

- (c) street lighting
  - (d) overhead electricity lines
  - (e) street trees
  - (f) bus stops.
- 30 Driveways, access tracks and parking areas should be designed and constructed to:
- (a) follow the natural contours of the land
  - (b) minimise excavation and/or fill
  - (c) minimise the potential for erosion from surface runoff
  - (d) avoid the removal of existing vegetation
  - (e) be consistent with *Australian Standard AS: 2890 - Parking facilities*.
- 31 The length of driveways should be minimised and together with manoeuvring areas be only sufficient to allow the proper functioning of the parking areas and their access.

#### **Access for People with Disabilities**

- 32 Development should be sited and designed to provide convenient access for people with a disability.
- 33 Where appropriate and practical, development should provide for safe and convenient access to the coast and beaches for disabled persons.

#### **Vehicle Parking**

- 34 Development should provide off-street vehicle parking and specifically marked accessible car parking places to meet anticipated demand in accordance with [Table Mar/2 - Off-street Vehicle Parking Requirements](#).
- 35 Development should be consistent with *Australian Standard AS: 2890 - Parking facilities*.
- 36 Vehicle parking areas should be sited and designed to:
- (a) facilitate safe and convenient pedestrian linkages to the development and areas of significant activity or interest in the vicinity of the development
  - (b) include safe pedestrian and bicycle linkages that complement the overall pedestrian and cycling network
  - (c) not inhibit safe and convenient traffic circulation
  - (d) result in minimal conflict between customer and service vehicles
  - (e) avoid the necessity to use public roads when moving from one part of a parking area to another
  - (f) minimise the number of vehicle access points onto public roads
  - (g) avoid the need for vehicles to reverse onto public roads
  - (h) where practical, provide the opportunity for shared use of car parking and integration of car parking areas with adjoining development to reduce the total extent of vehicle parking areas and the requirement for access points
  - (i) not dominate the character and appearance of a site when viewed from public roads and spaces



- (j) provide landscaping that will shade and enhance the appearance of the vehicle parking areas
  - (k) include infrastructure such as underground cabling and connections to power infrastructure that will enable the recharging of electric vehicles.
- 37 Where vehicle parking areas are not obviously visible or navigated, signs indicating the location and availability of vehicle parking spaces associated with businesses should be displayed at locations readily visible to users.
- 38 Vehicle parking areas that are likely to be used during non-daylight hours should provide floodlit entry and exit points and site lighting directed and shaded in a manner that will not cause nuisance to adjacent properties or users of the parking area.
- 39 Vehicle parking areas should be sealed or paved to minimise dust and mud nuisance.
- 40 To assist with stormwater detention and reduce heat loads in summer, outdoor vehicle parking areas should include landscaping.
- 41 Vehicle parking areas should be line-marked to delineate parking bays, movement aisles and direction of traffic flow.
- 42 On-site visitor parking spaces should be sited and designed to:
- (a) not dominate internal site layout
  - (b) be clearly defined as visitor spaces not specifically associated with any particular dwelling
  - (c) be accessible to visitors at all times.

### **Vehicle Parking for Residential Development**

- 43 On-site vehicle parking should be provided having regard to:
- (a) the number, nature and size of proposed dwellings
  - (b) proximity to centre facilities, public and community transport within walking distance of the dwellings
  - (c) the anticipated mobility and transport requirements of the likely occupants, particularly groups such as aged persons
  - (d) availability of on-street car parking
  - (e) any loss of on-street parking arising from the development (e.g. an increase in number of driveway crossovers).
- 44 Vehicle parking areas servicing more than one dwelling should be of a size and location to:
- (a) serve users, including pedestrians, cyclists and motorists, efficiently, conveniently and safely
  - (b) provide adequate space for vehicles, including emergency service vehicles, to manoeuvre between the street and the parking area
  - (c) reinforce or contribute to attractive streetscapes.
- 45 The provision of ground level vehicle parking areas, including garages and carports (other than where located along a rear lane access way), should:
- (a) not face the primary street frontage

- (b) be located to the rear of buildings with access from a shared internal laneway
- (c) ensure vehicle park entries are recessed at least 0.5 metres behind the main face of the building.

### **Vehicle Parking for Mixed Use and Corridor Zones**

- 46 Development should provide off-street vehicle parking and specifically marked accessible car parking places to meet anticipated demand in accordance with [Table Mar/2 – Off-street Vehicle Parking Requirements](#).
- 47 Loading areas and designated parking spaces for service vehicles should:
  - (a) be provided within the boundary of the site
  - (b) not be located in areas where there is parking provided for any other purpose.
- 48 Vehicle parking spaces and multi-level vehicle parking structures within buildings should:
  - (a) enhance active street frontages by providing land uses such as commercial, retail or other non-car park uses along ground floor street frontages
  - (b) complement the surrounding built form in terms of height, massing and scale
  - (c) incorporate facade treatments along major street frontages that are sufficiently enclosed and detailed to complement neighbouring buildings consistent with the desired character of the locality.
- 49 In mixed use buildings, the provision of vehicle parking may be reduced in number and shared where the operating hours of commercial activities complement the residential use of the site.

### **Undercroft and Below Ground Garaging and Parking of Vehicles**

- 50 Undercroft and below ground garaging of vehicles should only occur where envisaged in the relevant zone or policy area or precinct and ensure:
  - (a) the overall height and bulk of the undercroft structure does not adversely impact on streetscape character of the locality or the amenity of adjacent properties
  - (b) vehicles can safely enter and exit from the site without compromising pedestrian or cyclist safety or causing conflict with other vehicles
  - (c) driveway gradients provide for safe and functional entry and exit
  - (d) driveways and adjacent walls, fencing and landscaping are designed to provide adequate sightlines from vehicles to pedestrians using the adjacent footpath
  - (e) openings to undercroft areas are integrated with the main building so as to minimise visual impact
  - (f) landscaping, mounding and/or fencing is incorporated to improve its presentation to the street and to adjacent properties
  - (g) the overall streetscape character of the locality is not adversely impaired (eg visual impact, building bulk, front setbacks relative to adjacent development)
  - (h) the height of the car park ceiling does not exceed 1 metre above the finished ground level.
- 51 In the case of undercroft and below ground car parks where cars are visible from public areas, adequate screening and landscaping should be provided so as to avoid any loss of amenity.

## Neighbourhood Centre Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

### OBJECTIVES

- 1 A centre providing a range of facilities to meet the shopping, community, business, and recreational needs of the surrounding neighbourhood.
- 2 A centre that provides the main focus of business and community life outside a district centre, and provides for the more frequent and regularly recurring needs of a community.
- 3 A centre accommodating residential development in conjunction with non-residential development.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged in the zone:
  - bank
  - child care facility
  - consulting room
  - dwelling in conjunction with non-residential land uses
  - library
  - health centre
  - office
  - petrol filling station
  - place of worship
  - playing field
  - pre-school
  - primary school
  - recreation area
  - restaurant
  - shop
  - supermarket.
- 2 Development listed as non-complying is generally inappropriate.
- 3 Residential development and development comprising a variety of residential and non-residential uses may be undertaken provided such development does not prejudice the operation of existing or future retail activity within the zone.

#### Form and Character

- 4 Dwellings should be located only behind or above non-residential uses on the same allotment.



- 5 The maximum gross leasable retail floor areas in the following centres should generally be in the order of the amounts shown in the table below:

Location of Neighbourhood Centre	Area (square metres)
Marion Road - Ascot Park	2000
Marion Road - Marion/Mitchell Park	2500
Marion Road - Park Holme	4500
Marion Road - South Plympton	1200
South Road - Clovelly Park	2000

- 6 Development of the following respective centres should be carried out in accordance with the concepts shown on:
- (a) [Concept Plan Map Mar/1 - Clovelly Park Centre and Commercial](#)
  - (b) [Concept Plan Map Mar/3 - Marion/Mitchell Park Neighbourhood Centre](#)
  - (c) [Concept Plan Map Mar/4 - Park Holme Neighbourhood Centre](#)

### Vehicle Parking

- 7 Vehicle parking should be provided in accordance with the rates set out in [Table Mar/2 - Off Street Vehicle Parking Requirements](#) or [Table Mar/2A - Off Street Vehicle Parking Requirements for Designated Areas](#) (whichever applies).<sup>1</sup>

### Land Division

- 8 Land division in the zone is appropriate provided new allotments are of a size and configuration to ensure the objectives of the zone can be achieved.

## PROCEDURAL MATTERS

### Complying Development

Complying developments are prescribed in schedule 4 of the *Development Regulations 2008*.

In addition, the following forms of development (except where the development is non-complying) are complying:

A change of use to a shop, office, consulting room or any combination of these uses where all of the following are achieved:

- (a) the area to be occupied by the proposed development is located in an existing building and is currently used as a shop, office, consulting room or any combination of these uses
- (b) the building is not a State heritage place
- (c) it will not involve any alterations or additions to the external appearance of a local heritage place as viewed from a public road or public space
- (d) if the proposed change of use is for a shop that primarily involves the handling and sale of foodstuffs, it achieves either (i) or (ii):

- (i) all of the following:
    - (A) areas used for the storage and collection of refuse are sited at least 10 metres from any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop)
    - (B) if the shop involves the heating and cooking of foodstuffs in a commercial kitchen and is within 30 metres of any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop), an exhaust duct and stack (chimney) exists or is capable of being installed for discharging exhaust emissions
  - (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the development is to be undertaken and operated in accordance with the conditions attached to the previously approved development
- (e) if the change in use is for a shop with a gross leasable floor area greater than 250 square metres and has direct frontage to an arterial road, it achieves either (i) or (ii):
- (i) the primary vehicle access (being the access where the majority of vehicles access / egress the site of the proposed development) is from a road that is not an arterial road
  - (ii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared
- (f) off-street vehicular parking is provided in accordance with the rate(s) specified in [Table Mar/2 - Off Street Vehicle Parking Requirements](#) or the desired minimum rate in [Table Mar/2A - Off Street Vehicle Parking Requirements for Designated Areas](#) (whichever table applies) to the nearest whole number, except in any one or more of the following circumstances:
- (i) the building is a local heritage place
  - (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the number and location of parking spaces is the same or substantially the same as that which was previously approved
  - (iii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared.

### Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

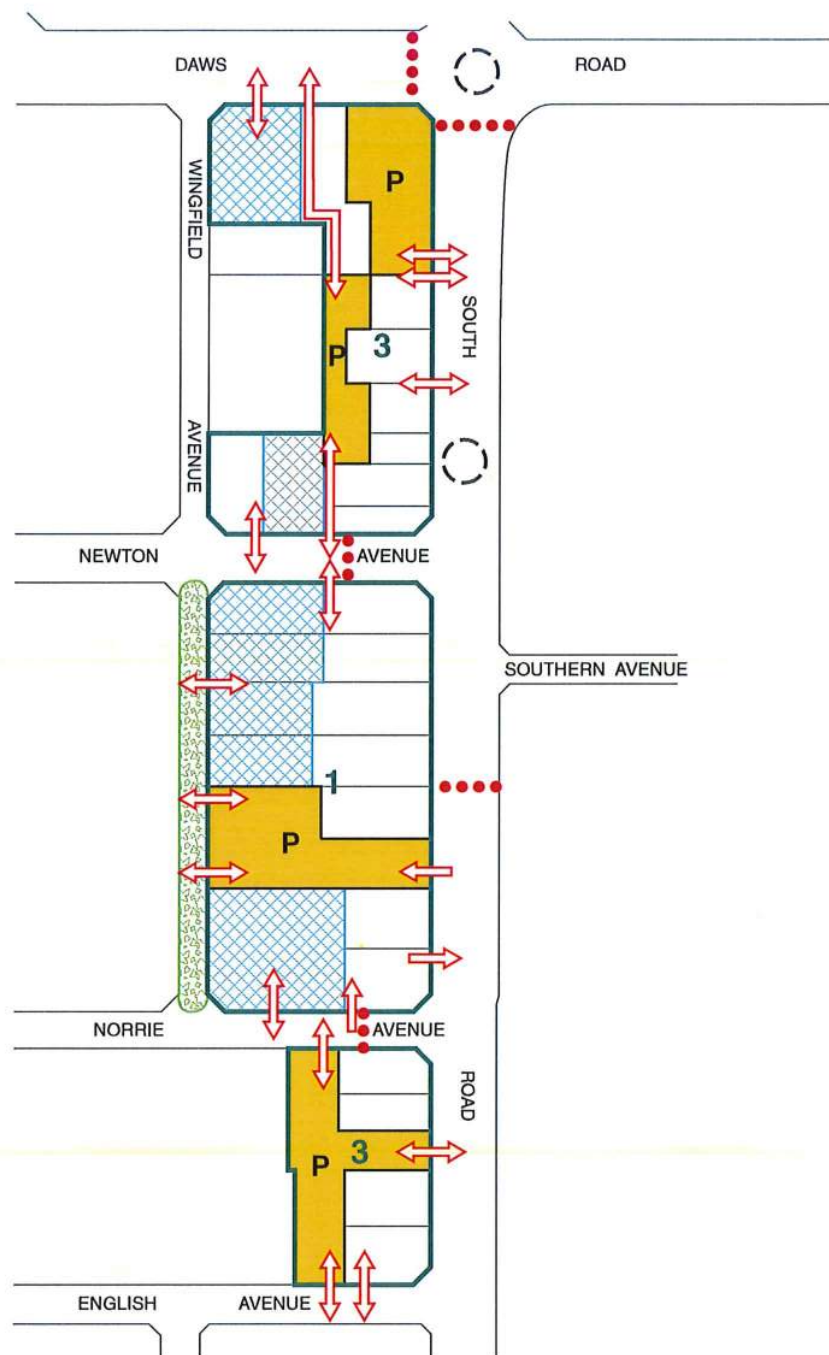
Form of Development	Exceptions
Advertisement and/or advertising hoarding	<p>Except where the advertisement and/or advertising hoarding achieves any one of the following:</p> <ul style="list-style-type: none"> <li>(a) it is attached to a building or structure where the height of the advertisement does not exceed the height of the roof of the walls or parapet of the building or structure by more than 2 metres</li> <li>(b) it is freestanding and has a height not exceeding 8 metres</li> <li>(c) it is located on a side or rear wall facing and not within 50 metres of an abutting residential zone.</li> </ul>

Form of Development	Exceptions
Dwelling	Except where in conjunction with a non-residential development.
Fuel depot	
Horticulture	
Industry	
Major public service depot	
Motor repair station	
Prescribed mining operation	
Road transport terminal	
Special industry	
Stadium	
Store	
Transmitting station above 30 metres in height	
Warehouse	
Waste reception, storage, treatment or disposal	
Wrecking yard	

### Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.





- 1 Retail
- 3 Commercial
- Potential Development Area
- Predominant Use Area
- Pedestrian Links
- Access Point (One-way)
- Access Point (Two-way)
- Car Parking
- Traffic Control Measure
- Possible Future Streetscape Measures

0 metres 50 100 150 200 250



# CLOVELLY PARK **Concept Plan Map Mar/1** **CENTRE AND COMMERCIAL**

Consolidated - 2014 MARION COUNCIL

## Table Mar/2 - Off Street Vehicle Parking Requirements

The following off street vehicle parking requirements apply except where otherwise stated in [Table Mar/2A - Off Street Vehicle Parking Requirements for Designated Areas](#).

Form of Development	Number of Required Car Parking Spaces
<b>Accommodation</b>	
Aged care retirement home	1 per unit
Display home	Assess on needs basis
<b>Dwelling</b>	
Detached	2 per dwelling containing up to 3 bedrooms one of which is to be covered. 3 per dwelling containing 4 or more bedrooms one of which is to be covered.
Semi-detached	
Row	
Group Residential flat building	1.5 per dwelling one of which is to be covered plus 1 visitor space per 3 dwellings.
Guesthouse or hostel	1 per 3 beds
Multiple dwelling	0.7 per bedroom
Nursing home	1 per 4 beds
Serviced apartment	1 per room plus 1 per employee
Tourist accommodation in the form of a bed and breakfast establishment	1 per guest room plus 1 per employee
<b>Commercial</b>	
Auction depot	1 per 100 square metres plus 2 spaces
Bank	5 per 100 square metres
Bulky goods outlet	4 per 100 square metres
Call centre	Assess on needs basis
Motor repair station	2 per 100 square metres or 4 per service bay (whichever provides the greater number of spaces).
Motor showroom	2 per 100 square metres
Office	4 per 100 square metres
Petrol filling station	6 per service bay plus 5 per 100 square metres retail floor area
Post office	7 per 100 square metres
Service trade premise	4 per 100 square metres
Shop	5 per 100 square metres (where located within a centre). 7 per 100 square metres (where not located within a centre).
Used car lot or vehicle sales yard	2 per 100 square metres of total site area
Video store	6 per 100 square metres

Marion Council  
Table Section  
Table Mar/2 - Off Street Vehicle Parking Requirements

Form of Development	Number of Required Car Parking Spaces
<b>Community and Civic</b>	
Child care centre	1 per 4 children
Civic administration office	4 per 100 square metres
Community centre	10 per 100 square metres
<b>Educational institution</b>	
Pre-school, primary school and secondary school	1 per full time employee plus 1 space for wheelchair users plus an additional 10 per cent of the total for visitors.
Tertiary institution	0.6 per full time student plus 0.2 per part time student.
Library	4 per 100 square metres
Meeting hall	1 per 5 seats
Place of worship	1 per 3 seats
<b>Industry</b>	
<b>Industry (exclusive of office component)</b>	
up to 200 square metres total floor area	2 per 100 square metres
plus between 200 and 2000 square metres total floor area	additional 1.33 per 100 square metres
plus greater than 2000 square metres total floor area	additional 0.67 per 100 square metres
Labour intensive industry (inclusive of office component)	0.75 per employee
Office component	3.3 per 100 square metres
<b>Medical</b>	
Consulting room	10 per 100 square metres
Hospital	2.5 per bed
Medical centre or day surgery	10 per 100 square metres
<b>Recreation</b>	
Amusement machine centre	7 per 100 square metres
Bowling club	10 per bowling green
Cinema complex	1 per 3 seats
Concert hall or theatre	1 per 3 seats
Conference facility	Assess on needs basis
Entertainment multiplex	Assess on needs basis
Exhibition hall	Assess on needs basis



Form of Development	Number of Required Car Parking Spaces
<b>Hotel or tavern</b>	
Public bar	1 per 2 square metres
lounge or beer garden	1 per 6 square metres
Dining room	1 per 3 seats
Gaming room	1 per 2 machines
Accommodation	Assess on needs basis
Indoor recreation centre or gymnasium	Assess on needs basis
Night club or late night venue	Assess on needs basis
Restaurant	1 per 3 seats or 1 per 15 square metres (whichever provides the greater number of spaces).
<b>Restaurant in the form of a fast food/ family restaurant</b>	
Without dine-in and drive through facilities only	12 per 100 square metres
With dine-in facilities only but no drive through	1 per 5 square metres (internal and external seating) or 1 per 2 seats (internal seating)—(whichever provides the greater number of spaces).
With dine-in and drive through facilities	1 per 3 square metres (internal and external seating) or 1 per 2 seats (internal seating)—(whichever provides the greater number of spaces).  (A car queuing area for a maximum of 12 cars is also required, to be assessed on a needs basis; there should also be at least 4 car spaces back from the ordering point.)
Squash or tennis court	4 per court (Additional spaces may be required if spectator seating is proposed.)
TAB facility	8 per 100 square metres
<b>Other</b>	
Funeral parlour	4 per 100 square metres plus 1 per 4 seats where a chapel is also involved
Interchange/transport station	Assess on needs basis
Radio and TV studio	5 per 100 square metres

## Table Mar/2A - Off Street Vehicle Parking Requirements for Designated Areas

### Interpretation

- 1 The vehicle parking rates table applies to Designated Areas listed below except where:
  - (a) any applicable condition(s) is/are not met
  - (b) the zone provisions require a lesser amount of on-site vehicular parking spaces than the amount determined using the vehicle parking rates tables below.

### Designated Areas

- 2 The following are Designated Areas:

Designated Area	Conditions
Regional Activity Zone	None
Suburban Activity Node Zone	
Mixed Use Zone	
District Centre Zone	Any part of the development site is located in accordance with at least one of the following: <ol style="list-style-type: none"> <li>(a) within 200 metres of any section of road reserve along which a bus service operates as a high frequency public transit service<sup>(2)</sup></li> <li>(b) within 400 metres of a bus interchange<sup>(1)</sup> that is part of a high frequency public transit service<sup>(2)</sup></li> <li>(c) within 400 metres of an O-Bahn interchange<sup>(1)</sup></li> <li>(d) within 400 metres of a passenger rail station<sup>(1)</sup> that is part of a high frequency public transit service<sup>(2)</sup></li> <li>(e) within 400 metres of a passenger tram station<sup>(1)</sup></li> <li>(f) within 400 metres of the Adelaide Parklands.</li> </ol>
Local Centre Zone	
Neighbourhood Centre Zone	
Regional Centre Zone	

<sup>(1)</sup> Measured from an area that contains any platform(s), shelter(s) or stop(s) where people congregate for the purpose waiting to board a bus, tram or train, but does not include areas used for the parking of vehicles

<sup>(2)</sup> A high frequency public transit service is a route serviced every 15 minutes between 7.30 am and 6.30 pm Monday to Friday and every 30 minutes at night, Saturday, Sunday and public holidays until 10.00 pm.

### Applicable off-street vehicular parking requirements

- 1 Development should provide off-street vehicle parking in accordance with the table(s) below. A lesser number of parking spaces may be provided based on the nature of the development and parking conditions in the wider locality including (but not limited to) the following:
  - (a) the development is a mixed use development with integrated (shared) parking where the respective peak parking demands across the range of uses occurs at different times
  - (b) the development is sited in a locality where the respective peak demands for parking for the range of uses (existing and proposed) occurs at different times and suitable arrangements are in place for the sharing of adjoining or nearby parking areas

**Table Mar/2A - Off Street Vehicle Parking Requirements for Designated Areas**

- (c) the development involves the retention and reuse of a place of heritage value, where the provision of on-site parking is constrained
- (d) suitable arrangements are made for any parking shortfall to be met elsewhere or by other means (including a contribution to a car parking fund)
- (e) generous on-street parking and/or public parking areas are available and in convenient proximity, other than where such parking may become limited or removed by future loss of access, restrictions, road modifications or widening
- (f) the site of the development is located within distances specified in the conditions applicable to Designated Areas for at least two different public transit modes
- (g) development that involves the reuse of the Main Assembly Building (MAB) at Tonsley which includes significant infrastructure to support cycling: where a reduction of up to 20 percent may be acceptable.

## TABLES: VEHICLE PARKING RATES

**Table 1: Non-residential development (excluding light industry and tourist accommodation)**

Location of development	Desired minimum number of vehicle parking spaces	Maximum number of vehicle parking spaces
<b>All Designated Areas (unless otherwise stated)</b>	3 spaces per 100 square metres of gross leasable floor area	6 spaces per 100 square metres of gross leasable floor area
<b>Core Area as shown on <a href="#">Concept Plan Map Mar/7 - Laffer's Triangle</a> and <a href="#">Concept Plan Map Mar/8 - Tonsley</a> of the <b>Regional Activity Zone</b> and <b>Suburban Activity Node Zone</b></b>	3 spaces per 100 square metres of gross leasable floor area	5 spaces per 100 square metres of gross leasable floor area

**Table 2: Tourist accommodation**

Location of development	Desired minimum number of required vehicle parking spaces	Maximum number of vehicle parking spaces
<b>Regional Activity Zone</b> <b>Suburban Activity Node Zone</b>	1 space for every 4 bedrooms up to 100 bedrooms and 1 space for every 5 bedrooms over 100 bedrooms	1 space for every 2 bedrooms up to 100 bedrooms and 1 space for every 4 bedrooms over 100 bedrooms



Table 3: Residential development in the form of residential flat buildings and residential development in multi-storey buildings

Location of development	Rate for each dwelling based on number of bedrooms per dwelling	Plus number of required visitor parking spaces
Core Area as shown on <a href="#">Concept Plan Map Mar/7 - Laffer's Triangle</a> and <a href="#">Concept Plan Map Mar/8 - Tonsley</a> of the Regional Activity Zone and Suburban Activity Node Zone	0.25 per studio (no separate bedroom) 0.75 per 1 bedroom dwelling 1 per 2 bedroom dwelling 1.25 per 3 + bedroom dwelling	0.25 per dwelling
Transition Area / Any other area not designated as shown on <a href="#">Concept Plan Map Mar/7 - Laffer's Triangle</a> and <a href="#">Concept Plan Map Mar/8 - Tonsley</a> of the Regional Activity Zone and Suburban Activity Node Zone	0.5 per studio (no separate bedroom) 1 per 1 bedroom dwelling 1.5 per 2 bedroom dwelling 2 per 3+ bedroom dwelling	0.25 per dwelling
Mixed Use Zone	1 per studio (no separate bedroom) 1 per 1 bedroom dwelling 1.25 per 2 bedroom dwelling 1.5 per 3+ bedroom dwelling	0.25 per dwelling

Table 4: Row, semi-detached and detached dwellings

Location of development	Number of bedrooms, or rooms capable of being used as a bedroom	Number of required vehicle parking spaces
Regional Activity Zone	1 or 2 bedrooms	1
Suburban Activity Node Zone	3 + bedrooms	2

Table 5: Student accommodation

Location of development	Number of required vehicle parking spaces	Number of required visitor parking spaces
Regional Activity Zone	0.25 per bedroom per dwelling	0.03 per bedroom per dwelling
Suburban Activity Node Zone		



## Zones

- |     |                      |
|-----|----------------------|
| C   | Commercial           |
| Cu  | Community            |
| In  | Industry             |
| LCe | Local Centre         |
| NCE | Neighbourhood Centre |
| OS  | Open Space           |
| R   | Residential          |
| MU  | Mixed Use            |

Development Plan Boundary



## Zone Map Mar/6