

Neilson Architects

Demolition of all existing structures and staged construction of a freestanding supermarket (shop – 1266m²) with associated earthworks, retaining wall, car park, advertising displays, fencing, solar panels and landscaping

499 Portrush Road Glenunga

180/E012/19

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Image Nielsen Architects



OVERVIEW

Application No	180/E012/19
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Unique ID/KNET ID	2019/12294/01
Applicant	Nielsen Architects
Proposal	Demolition of all existing structures and staged construction of a freestanding supermarket (shop – 1266m ²) with associated earthworks, retaining wall, car park, advertising displays, fencing, solar panels and landscaping
Subject Land	499 Portrush Road Glenunga
Zone/Policy Area	Local Centre Zone
Relevant Authority	State Commission Assessment Panel
Lodgement Date	19/09/2019
Council	City of Burnside
Development Plan	Burnside (City) – Consolidated 19 December 2017
Type of Development	Merit
Public Notification	Category 2
Representations	7 Representations – with 5 wishing to be heard
Referral Agencies	Commissioner of Highways, City of Burnside
Report Author	Yasmine Alliu – Planning Officer
RECOMMENDATION	Development Plan Consent subject to conditions

EXECUTIVE SUMMARY

The proposal is a merit, Category 2 form of development and was subject to referrals to the Commissioner of Highways and the City of Burnside Council. A total of 7 valid representations were received during the notification process. Concerns raised by the representations related to the increase in traffic impacts, noise generated from operational and vehicular means, the visual appearance including building setbacks, built form, height and scale, intensification of use, interface amenity (including overshadowing), waste removal, construction noise and dust.

The Commissioner of Highways offers in-principle support for the proposal, and has recommended a number of conditions be imposed to any approval. Council is generally supportive but would like further consideration given to interface issues. The applicant revised the landscaping and provided further information as a result of the notification and referral comments to support their proposal.

This assessment considers the relevant provisions of the Burnside (City) Development Plan, in determining the suitability of the proposal. It is acknowledged that the proposal departs from gross leasable floor area for a shop. Whilst there is no height limit prescribed, it is implied that the level above an undercroft car parking area is in the order of two storeys. The proposal has a larger footprint than the existing Mitre 10. The car parking allocation does not comply with the requirements of the Development Plan. What's provided given the gross leasable floor area is acceptable. Whilst some of the provisions are not met, on balance the proposal does fulfil the intent of the zone. These shortfalls are not considered detrimental to the Local Centre Zone given its current profile and historical context on a primary arterial road. The management of the impact of the development on the interface between the residential component and associated operations, on balance is acceptable. The assessment concludes with a recommendation to grant Development Plan Consent subject to conditions.

ASSESSMENT REPORT

1. DESCRIPTION OF PROPOSAL

The proposal is for the demolition of all existing structures (the Mitre 10 building) and staged construction of a freestanding supermarket (shop – 1266m² retail floor area) with associated earthworks, retaining wall, car park, advertising displays, fencing, solar panels and landscaping.

The built form comprises the following:

ALDI Supermarket

- The 2006m² (total floor area) building (including forecourt) located towards the southern side of the site.
- Vehicle access is provided on the northern side of the site for safety and functionality and sufficiently distanced from the Hay Road intersection and pedestrian crossing.
- Retail floor area comprises 1266m² with 537m² for back of house functions.
- The supermarket building (including southern tower) has a setback to Portrush Road, its primary frontage, of approximately 4.6m. The northern tower, ramp and canopy are set back approximately 1.2 metres.

Residential Interface

- The building is set back 24.5 metres from the northern (side) boundary, the tower 17 metres
- From the western (rear) boundary a portion of the building is set back 2 metres with the balance of the building being setback 10 metres to accommodate the loading dock.
- The setback on the southern side boundary is 2.34 metres with the tower being set back 3.5 metres. The paved undercroft parking area is setback 900mm

Hours

- 6 -10 employees present on the site at any one time (typical employment 25 full time equivalent)
- Operating hours will be in accordance with any limitations set out by the *Shop Trading Hours Act, 1977*.

The supermarket building has an overall height of 9.4 metres to 10 metres to the top of its parapet. The variation in height being due to the slope of the land and change in levels for the undercroft car park set beneath the supermarket level. The north tower element comprising lift core/stair is the highest part of the built form at 11.6 metres.

Materiality

- Built form construction primarily pre-cast concrete panels (*finish Dulux 'Drivetime Grey' & Fluorescent Fire Red*)
- Gutters/rainhead/downpipes – Colorbond 'basalt grey'
- Fibre Cement Fascia
- The tower will be sleeved in a powder coated perforated screen (*finish Dulux 'Copper Kinetic'*) – steel framed (*finish Colorbond 'Monument'*)
- Aluminium anodised large shop front glazed windows
- Colorbond roof sheeting and capping
- Colorbond panel lift door 'basalt grey'
- A lightweight canopy with steel frames will overhang the pedestrian ramp
- Sunscreen shades on northern elevation(*finish Colorbond 'Monument'*)
- Powdercoated plant louvres (*finish Colorbond 'Monument'*)

Signage – 5 signs are proposed in total - 4 x aluminium light boxes with the ALDI branding to be located on the towers – 2.4m (h) x 2m (w).

1 digital sign – in the forecourt area

Landscaping – a combination of plant species and planting heights are proposed along the boundaries for screening and improved visual amenity

Earthworks/retaining/fencing

- Southern boundary *cut* – 0 metres(south west corner) – 1.75m(south east corner) –existing retaining wall to be removed – installation of a 2.4m (h) fence – All the retaining is proposed in the *cut*, only visible from the ALDI site
- Western boundary *cut* – 0 metres(south west corner) – 1.3m(north west quadrant) – existing retaining wall (1.3m (h)) to be retained – installation of a 1.8 fence on the retaining wall - an acoustic fence to be installed 3m (h) inside the western boundary extending the length of the loading dock
- Northern boundary *cut*- 500mm (north eastern corner) -2m (north west quadrant) – existing retaining wall is proposed to be made good.

Application details are contained in the ATTACHMENTS.

A summary of the proposal is as follows:

Land Use Description	Supermarket
Building Height	Single story with undercroft car parking to supermarket building. Supermarket building – 9.425m -10m to the top of parapet 11.6m to the top of the north tower element
Description of levels	Ground - undercroft car parking (1835m ²) to supermarket building 98 car parks – 30 at grade (open air) lift and stairs to upper level (north tower) Loading dock Compactor Enclosed bin storage Upper level - Supermarket (retail floor area 1266m ²) - back of house area (537m ²)– lift/stairs (north tower) Office and amenities (south tower) Roof –plant and solar panel array
Building Floor Area	2006m ² – for the upper level footprint
Site Access	Utilises the northern existing crossover on Portrush Road – it will be widened at the northern end. - switch back ramp for pedestrian access - lift and stairs from undercroft to the upper level
Staged Construction	Stage 1 – demolition and preliminary site works Stage 2 – superstructure

2. SITE AND LOCALITY

2.1 Site Description

The site comprises 3 rectangular shaped allotments, described as follows:

Lot No	Plan	Street	Suburb	Hundred	Title
29	FP 16174	Portrush Road	Glenunga Adelaide	Adelaide	5452/287
28	FP 16174	Portrush Road	Glenunga Adelaide	Adelaide	5452/286
27	FP 16174	Portrush Road	Glenunga Adelaide	Adelaide	5452/298

The subject site of 4366m² comprising 3 allotments is located at 499 Portrush Road (a primary arterial road) at Glenunga. The site is opposite a T junction formed by Portrush

Road and Hay Road. The site has a frontage of 61 metres and a depth of 71 metres. The site currently contains a Mitre 10 Hardware store which has a spatial footprint of approximately 1,435m² (roofed area) plus an outdoor fenced display area located in front of the store which occupies an area of around 225m² with a height of approximately 5 metres and approximately 67 car parks.

The site is bounded by residential units to the south separated from the site by a common driveway, 3 detached dwellings to the west whose backyards abut the site and the Best Western Granada Motor Inn to the north. To the north east diagonally across Portrush Road is a group of shops comprising a Bakery, Thai restaurant, Laundromat, Enzos at home and an Asian Cuisine and Noodles restaurant. Across Portrush Road to the south east it is residential, primarily units and detached dwellings. Linden Park Primary School is east from the site along Hay Road.

Further south of the site are a Dan Murphy Liquor Store, a Tony and Marks Produce Market and a Liberty Petrol Station. Further north is the Burnside Shopping Village.

The site itself has a general overall fall from the south east to the north-west and a cross fall from south to north. However, at the interface between the site and the residences on the western side is a steep embankment with a 45o slope. The slope ends at a retaining wall and then the private open space of the residences that back onto the site.

The current amenity experienced by the residents would be noise, car movements, 7 day week activity, a large built form close to the boundary as would be expected at the junction of two different zones and a lesser level of amenity than typically found within the heart of the residential zone.

2.2 Locality

The locality is characterised by commercial development to the north and south (the adjacent units being the exception), partially to the east (the rest being residential) and residential to the west. The residential area comprising a mixture of dwelling types, a number of units, predominantly single storey interspersed with some two storey dwellings.



Figure 1 – Location Map

3. COUNCIL COMMENTS

3.1 City of Burnside

Council is generally supportive of the proposal to establish a supermarket in this location based largely on the nature and scale of the existing land use. However consideration should be given to design modifications aimed at reducing the impact of the proposed building such to ensure it is more in keeping with the “small scale” character envisaged by the Zone provisions and avoids adverse impacts on the surrounding Residential Zone.

Accordingly, Council advised the following should be given further consideration:

- Appropriateness of the level of illumination of the proposed signage, and the potential impact on the amenity of surrounding sensitive land uses and motorists;
- Appropriately conditioning operating hours, delivery time and service arrangements so as to not detract from residential amenity of adjoining properties; and
- Appropriately conditioning the waste storage area to be maintained so to not be unsightly from public view and to not result in unpleasant smells that could be experienced from adjoining allotments.

4. STATUTORY REFERRAL BODY COMMENTS

Referral responses are contained in the ATTACHMENTS.

The Commissioner of Highways is a mandatory referral body in accordance with Schedule 8 of the *Development Regulations 2008*.

4.1 Commissioner of Highways

Portrush Road is an arterial road in the care and control of Department of Planning Transport and Infrastructure (DPTI) and has a speed limit of 60km/h, carrying 37,800 vehicles per day. DPTI have offered in principle support for the proposed access and median modifications. The signage has been assessed against the ‘Advertising signs – assessment guidelines for Road safety’ and DPTI recommends conditions in relation to the signage proposed. A number of conditions have been imposed in the interest of road safety.

5. PUBLIC NOTIFICATION

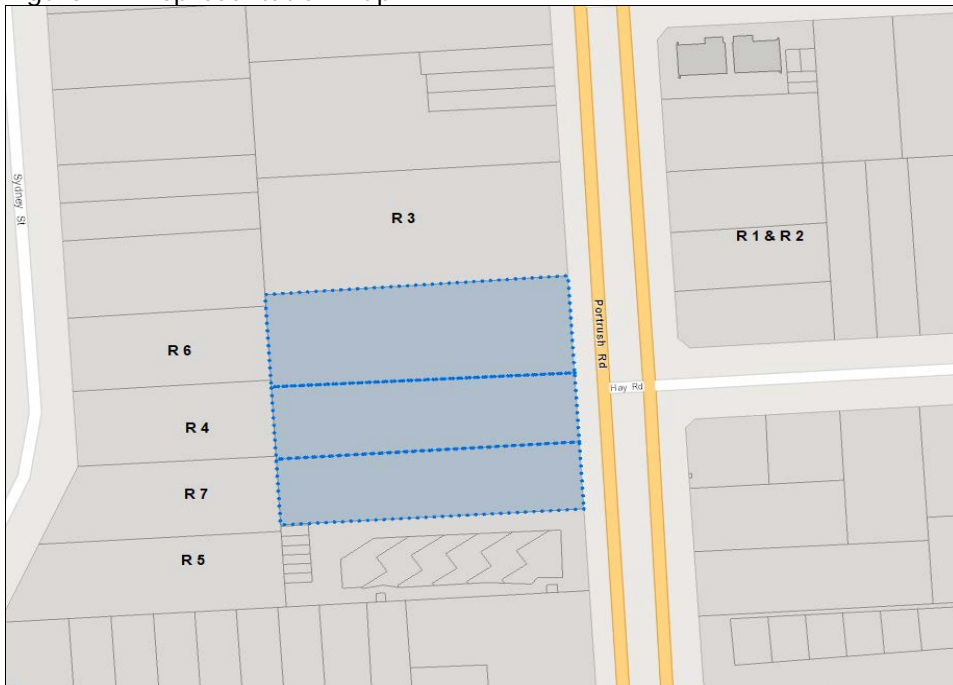
The application was notified as a Category 2 development pursuant to Schedule 9 6 (1) (h) any kind of development within a Local Centre Zone. Public notification was undertaken (by directly contacting adjoining owners and occupiers of the land) and a total of 7 representations were received (see Figure 2). Five (5) representors wish to be heard by the State Commission Assessment Panel.

A high level summary of matter raised through the public consultation includes:

- Height, bulk and scale, setbacks
- Traffic impacts, access, traffic flow, traffic volumes, car parking
- Noise – operations and service noise, construction noise
- Interface issues – visual amenity, screening, overshadowing
- Odour impacts
- Waste
- Local Centre Zone provisions and character

A copy of each representation and the applicant’s response is contained in the ATTACHMENTS

Figure 2 – Representation Map



6. POLICY OVERVIEW

The subject site is within the Local Centre Zone as described within the Burnside (City) Development Plan Consolidated – 19 December 2017.

Relevant planning policies are contained in Appendix One and summarised below.

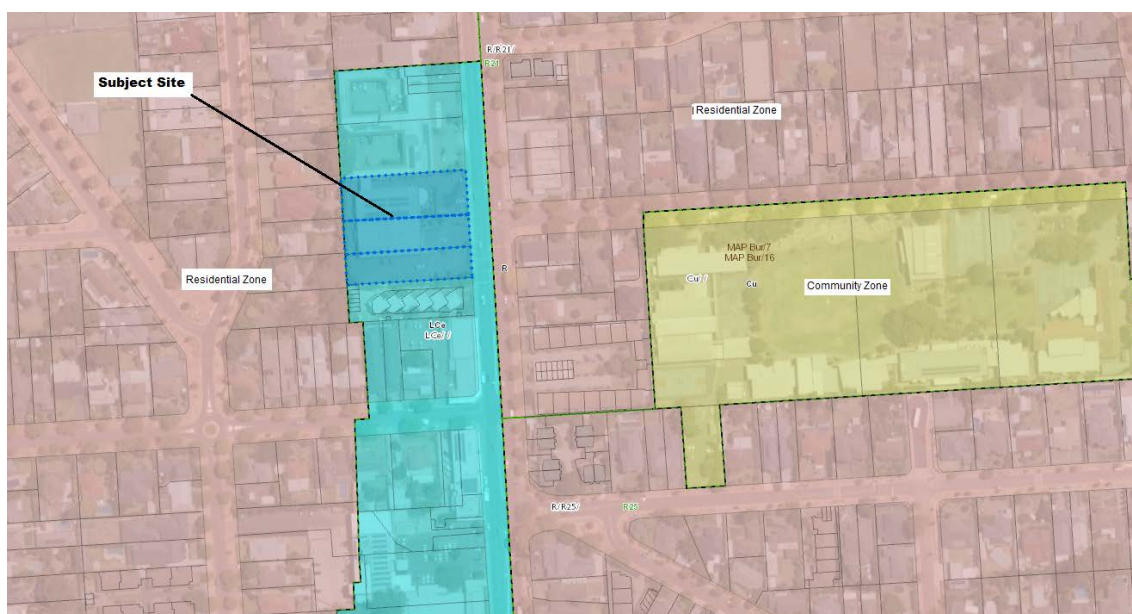


Figure 3 – Zoning Map

6.1 Local Centre Zone

A zone accommodating a range of shops, offices, business, health and community service providers, all being of a small-scale suited to servicing the needs of the local community. Development should be developed only in a manner that establishes a small-scale low traffic generating kinds of land uses such as offices or consulting rooms containing only one floor level are envisaged.

One floor level directly above another floor level is acceptable where the lower floor level is for the provision of car parking space as undercroft vehicle parking and should be reasonably screened from view from public roads by the use of landscaping or other means which enhance the external appearance of the land and buildings.

Shop development should generally comprise a maximum gross leasable floor area in the order of 450 square metres.

6.2 Council Wide

Centres established and developed in accordance with a hierarchy based on function, so that each type of centre provides a proportion of the total requirement of goods and services commensurate with its role. Local centres to include shopping and local facilities and services to serve the day-to-day needs of the local community. A shop or group of shops with a gross leasable area of greater than 250 square metres should be located in the District Centre Zone, a Neighbourhood Centre Zone, or the Urban Corridor Zone.

Development located within any centre zone should be designed and located to minimise its impact on existing or potential dwellings in an adjacent zone and where appropriate, be designed, sited, constructed, landscaped and operated in a manner which will minimise the impact of such activities on adjacent residential development and occupants.

The appearance of buildings and objects shall not impair the amenity of the locality in which they are situated. Advertisements should complement and improve the character and amenity of the area within which they are located.

7. PLANNING ASSESSMENT

The application has been assessed against the relevant provisions of the Burnside (City) Development Plan, which are contained in Appendix One.

7.1 Quantitative Provisions

	Development Plan Guideline	Proposed	Guideline Achieved	Comment
Gross Leasable Floor Area	In the order of 450m ²	Supermarket 1266m ² retail space	YES <input type="checkbox"/> NO <input checked="" type="checkbox"/> PARTIAL <input type="checkbox"/>	<i>The site comprises 3 allotments with potential for a shop of 450m² on each allotment this equates to 1350m² of retail which is greater than the 1266m² proposed</i>
Building Height	Buildings should not contain one floor level directly above another floor level except where the lower floor level is for the provision of car parking space and the level of that floor is located not less than one metre below the predominant natural ground level for the site.	The height of the built form towers range from 10.4m – 11.6m depending on the elevation The supermarket height being 9.4m -10m to the parapet.	YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> PARTIAL <input type="checkbox"/>	<i>No building height is prescribed – noting the intention of the policy is for a building no greater than 2 levels.</i>
Land Use	Shop	Supermarket	YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> PARTIAL <input type="checkbox"/>	<i>The proposed land use meets the intent of servicing the community.</i>
Car Parking	7 spaces per 100m ² of total floor area	98 car spaces – (6 staff)	YES <input type="checkbox"/> NO <input checked="" type="checkbox"/> PARTIAL <input type="checkbox"/>	<i>140 required – however the rate of 7 per 100m² is widely acknowledged as out of date and a reduced rate is generally accepted</i>
Bicycle Parking	No bicycle rates provided	5 bicycle parks 1 rack by the front entrance		<i>Adequate bicycle parking</i>

7.2 Land Use and Character

The Local Centre Zone speaks of small scale development suited to servicing the needs of the local community such as a range of shops, offices, business, health and community service providers. Shop development should generally comprise a maximum gross leasable floor area (GLFA) in the order of 450m². The proposal at 1266m² of retail floor area is smaller than a full size supermarket and a lesser area than the 1350m² of retail area that could exist on the 3 allotments (potentially at 450m² retail area per allotment). From this perspective it meets the guideline.

Historically the subject site has a history of larger retail use. The applicant's report states that since the late 1970s/early 1980s, the site was occupied by a 'Half Case Warehouse' supermarket, which was then followed by a 'Bi-Lo' supermarket. Similarly, the site is presently occupied by a 'Mitre 10', which is also a form of shop. The current proposal is not dissimilar to the previous uses in its comparable gross leasable floor area and external activities related to the operation of the supermarket e.g. loading/unloading, servicing arrangements and customers moving to and from the site. The Mitre 10 has a 1435m² roofed area plus outdoor area of 225m² which is a similar retail area to the proposal.

There is merit in the applicant's argument that the site comprises three (3) separate allotments and that each of these allotments could potentially be developed separately, and each support a 'shop' with a gross leasable floor area in the order of 450m² (as expressly contemplated by the Local Centre Zone). This would result in a combined floor area of 1350m², which is as the applicants report states, comparable to the overall floor area encapsulated by the proposed development (1,266m² retail area/ 2,006m² total net area).

The proposal is larger than the small scale type development envisaged in the zone and challenges the notion of small scale. However, given the sites history and the development will amalgamate 3 original allotments, the proposal is not out of place. The applicant points out there are other larger floor plates within the Local Centre Zone, within the locality further south on Portrush Road, Dan Murphy's Liquor Store, Tony and Marks Produce Market and the neighbouring Motel – the Best Western Granada Motor Inn, all greater than the 450m² envisaged.

The Local Centre Zone is part of a hierarchy of Centre Zones, the hierarchy being based on function so that each type of centre has a proportion of goods and services commensurate to its role. The Council Wide provision speaks of a gross leasable area of greater than 250m² should be located in the District Centre Zone, a Neighbourhood Centre Zone, or the Urban Corridor Zone which is somewhat contradictory as the Local Centre Zone in the same Development Plan prescribes 450m². These leasable floor area caps are somewhat redundant as recent current development exceeds these levels.

The applicant engaged a Consultant; Deep End Services to prepare an analysis of supply and demand for supermarket floor space in the Glenunga catchment (a 2km radius of the site). Their findings being that the area had a very low rate of supermarket floor space provision. The Consultant states that the benefits an ALDI Store offers are lower prices and discount products, compared to larger supermarkets chains, which is beneficial to low income and other price conscious families/individuals. As the site is central to the Local Centre Zone there is potential for vehicle and pedestrian traffic to utilise the site that would otherwise go elsewhere. ALDI's presence may also potentially attract new businesses to establish within the immediate locality. Deep Ends analysis concluded that the proposed ALDL as a smaller type supermarket would therefore complement the range of existing shops and functionalities within the Local Centre Zone.

The proposal is a consolidated development site over 3 separate allotments with 1266m² of retail area where there is potential for 1350m² of retail. The proposal is a smaller type of supermarket and is of a size to service the needs of the local community. The impact of the proposed new development would be similar in nature to the current site use and would therefore not be expected to significantly increase the intensity of the site. Also, the impact of retail activity although different in nature would not be expected to significantly disrupt the orderly function of other centre activities.

7.3 Design and Appearance

There are a number of provisions that emphasize the need for development to be of a high standard and contribute to the streetscape qualities. ALDI has a corporate design template that makes each store recognisable as a specific type of retail outlet with certain colours and materiality. The contemporary design of the proposed supermarket is such that there are sufficiently articulated façade treatments, roof design and diversity in materiality to provide visual interest within the streetscape. The supermarket frontage provides sufficient street activation with the full glazed frontages as well as pedestrian shelter in the form of canopies, landscaping, and a clearly delineated entrance. The design is such that the taller tower elements, a visual feature are located away from the residential property boundaries.

The proposal has a streetscape presence with sufficient visual interest within the vicinity. Landscaping is provided around the perimeter of the site adding to the visual amenity. There are a number of buildings that have a tired quality within the immediate locality. The development will improve visual amenity of the immediate area and may encourage surrounding developments consider their street frontage.

7.4 Height/Bulk and Scale /Setbacks

Local Centre Zone

PDC 5 Buildings should not contain one floor level directly above another floor level except where the lower floor level is for the provision of car parking space and the level of that floor is located not less than one metre below the predominant natural ground level for the site.

A concern with this proposal is its height, bulk and scale and visual impact with the residences to the west and south of the site. As the site is elevated somewhat compared to the surrounding properties, the proposal includes excavation across the majority of the land, with the ALDI Store to be 'cut' into the site. The overall strategy of 'cutting' the development into the site will ensure that the undercroft car park is provided and also be screened from view from Portrush Road. It is considered that the overall height of the building will also be reduced when viewed from the public realm and from adjoining properties owners because of this intervention.

Where setbacks are concerned, a portion of the building, measuring approximately 17 metres in length, is to be setback 2 metres from the rear boundary, in the south-west corner of the site. The remainder of the ALDI building will be setback from the rear (west) boundary by approximately 10 metres, which is greater than the current setback. The proposed building on the western elevation will exhibit a wall height of 9.9m, including the upper parapet. Also visible is a retaining wall of 1.3m which sits on the actual western boundary of the site. However, as stated by the applicant this section of the building, is located more than 25 metres from the residences and is screened in part by existing vegetation within a rear yard, and, as illustrated in Figure 1 and 2 below, will be significantly screened by the proposed fencing and (mature) landscape treatments along the western boundary.

Figure 1

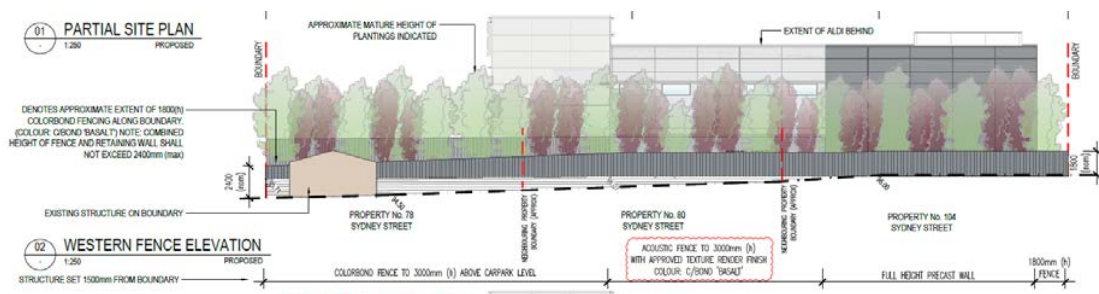
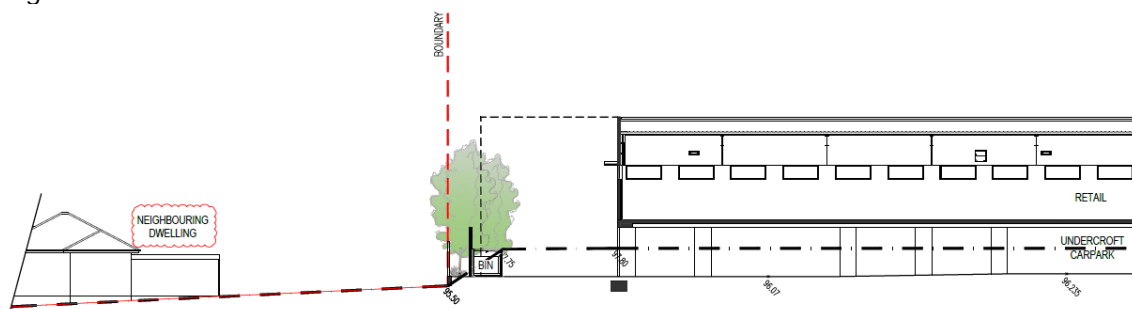
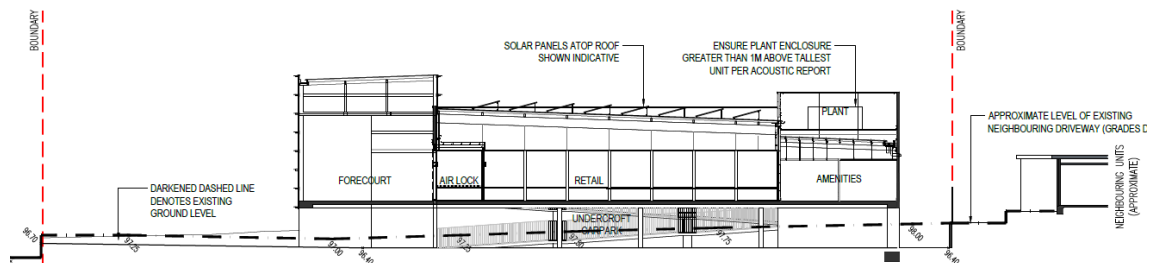


Figure 2



The proposed ALDI building is not centrally located, occupying the southern half of the site as compared to the location of the Mitre 10 building. This is expected to provide some visual relief for some adjacent residents to the west but not all the residents. Repositioning the built form on the site will effectively swap the orientation of the view corridor offered by the side car park currently for the residents to the south. Their view being a built form with a maximum height of around 9.9m (close to the western boundary), dropping to a height of around 8m (closer to Portrush Road).



Section showing southern boundary

The tallest elements of the proposal are the two (2) tower elements that front Portrush Road. Whilst more visible than the Mitre 10 building at the Portrush Road frontage it is considered its prominence is sufficiently consistent with the existing streetscape.



The setbacks vary from zero setback to 12.3m for adjacent properties with frontages to Portrush Road. The setbacks provided for the ALDI store area are considered acceptable given the frontage is to an arterial road and the ad hoc nature of setbacks within the immediate locality.

The proposal exceeds the traditional two storey building height and will be a prominent building within the streetscape with some visual impacts. It is considered it will not be to the detriment of the envisaged small scale character of the zone as the small scale character has already been lost in this area as the pattern of development has changed. The proposal is not introducing a bulk and scale that is dramatically different to the existing character, having already been influenced by the existing (Mitre 10) and other recent development. The zone is silent on a specific building height. The structure is of a commercial nature as anticipated by the zone and accordingly would reasonably constitute an acceptable built form addition within the setting. The height is generally compatible with other buildings in the locality and therefore will not make a significant impact. A condition would be assigned to any consent to ensure that regular maintenance of the landscaping to sustain plant health and its ability to screen the appearance of the building and partially address the concerns of representors. On balance the proposal is considered acceptable.

The building format for the supermarket is similar to other ALDIs where an undercroft car park is required and therefore indicates the height necessary to achieve the ALDI format.

Other examples of ALDI building heights are as follows:

Glenelg North – 11.5 metres (residential interface and undercroft car park)

Blackwood – 11.93 metres (residential interface and undercroft car park)

Port Adelaide – 10.4 metres (no residential interface)

Norwood - 10.48 metres (no residential interface)

7.5 Overshadowing

CW PDC 21 The design and location of buildings should enable direct winter sunlight into adjacent dwellings and private open space and minimise the overshadowing of:

- (a) windows of main internal living areas;*
- (b) upper-level private balconies that provide the primary open space area for a dwelling; and*
- (c) solar collectors (such as solar hot water systems and photovoltaic cells).*

Access to sunlight

PDC 183 Except in the Urban Corridor Zone, buildings should be designed and located to ensure that on the site of any dwelling:

- (a) at least three hours of sunlight is available to windows to north-facing habitable rooms and*
- (b) at least two hours of sunlight is available to at least 50 percent of the main outdoor living areas of the dwelling, between 9am and 5pm on the winter solstice (21 June).*

The shadow diagrams for the winter solstice show that there are no significant impacts to the residences located to the west. The overshadowing is limited between the period of 8am and 11am and is limited to portions of their rear yards.

As per the applicants report overshadowing to the units to the south is influenced by the existing landscape/tree buffer (located on the site of the units) and the proposed ALDI building and boundary fencing treatments. The applicant states that sunlight is available to the north-eastern and north-western aspects of the units between the hours of 11.00am and 2.00pm. The overshadow period being limited to the early morning and late afternoon. Further to that the proposed development does not affect the rear yards of the units, and they remain affected by their own shadow profile. The units to the south are located in the Local Centre Zone.

The proposal generally accords with the above provisions and adjacent residents will have access to the requisite amount of sunlight.

7.6 Landscaping

A variety of landscaping is proposed as part of the proposal. Further consideration was given to the landscaping with a revised landscaping plan being submitted that provides additional and more substantial tree planting along the western boundary, additional screening shrubs adjacent to the northern tower and elevations showing plant heights to give a better understanding to the extent of the screening.

The landscaping on the western boundary now provides more adequate screening from the servicing activities (loading dock, bin area) on the site. The screen will have denser planting with no gaps. There will also be a 3 metre high steel acoustic fence behind the landscaping to assist in noise attenuation. Species have been selected that have moderate to fast growth rates providing an improved visual amenity for those residences affected from the west. The landscaping is intended to provide a softening affect to the taller built form and with the placement of more mature trees (3m in height). The proposed landscaping is more proportionate to the scale of the building as sought by Council and more specifically on the western boundary where the trees will grow taller. Maintenance to the western boundary landscaping is from the undercroft car park.

Advice was sought regarding shade tolerant plants. The landscaping plans provided are acceptable with adequate screening and visual amenity provided.

7.7 Traffic Impact, Access and Parking Impacts

The applicant has provided a traffic and parking assessment by GTA Consultants. GTA considers the existing traffic and parking conditions, the parking demand likely to be generated by the proposed development, the supply of parking and suitability of the proposed parking layout, proposed access arrangements, and the impacts on the surrounding road network.

As per GTAs report the proposed ALDI development is predicted to generate up to 232 and 288 two-way vehicle movements during the Thursday PM Peak and Saturday Peak Hour respectively. The comparison being the Mitre 10 store currently generates 20 movements during the Thursday PM Peak Hour and 129 movements during the Saturday Peak Hour, this equates to an additional 212 and 159 trips onto the road network respectively from the development. The proposed additional traffic movements can be safely accommodated within the road network. The proposed right turn lane will accommodate the anticipated additional traffic, while the reduced right turn lane length into Mariner Street won't adversely impact on this approach. Right turns out of the site will experience higher queue lengths and delays, however based on gap analysis and Pedestrian Activated Crossing (PAC) activations, there would be more than sufficient opportunities to facilitate the anticipated demands. There is adequate capacity in the surrounding road network to cater for the traffic generated by the proposed development. The traffic/vehicle movement is in principle supported by DPTI.

7.7.1 Access

The existing vehicle access at the northern end of the site from Portrush Road is to be utilised and widened to an offset of 2 metres from the northern boundary with adequate clearance from the existing stobie pole to the south and to enable pedestrian lines of site to be maintained. The central existing access point opposite Hay Road is to be closed.

The following modifications on Portrush Road are proposed:

- The central median on the adjacent section of Portrush Road is to be closed to enable vehicles waiting to turn right into the ALDI site to queue in a sheltered lane so as not to interfere with the free flow of traffic on Portrush Road.
- The right turn lane proposed to accommodate this is 55 metres long and tapered for 20 metres allowing storage for 6 vehicles. This means the right turnout median for the existing motel will be closed and the Mainer Street right turn lane will be reduced.
- Delivery Vehicles – the largest vehicle to enter the site is a 15.5 metre long articulated semi-trailer. The semi-trailer will generally approach from the south and do a left turn in and left turn out. GTA show the semi-trailer can enter and exit the site with minimal disturbance to light vehicle traffic circulating the aisle.

DPTI in principle supports the proposed access and median modifications and the truck arrangements. Council was also generally supportive of the traffic management.

7.7.2 Parking

The Development Plan requirement for *Off- Street Vehicular Parking Requirements Table Bur/5* is 7 spaces per 100m² of total floor area (however, at 1266m² the parking provision would be 89 car parks which would be met by the proposed development). At 2006m² of total floor area means 140 car parks would be required, the shortfall in this case being 42 car parks. This is considered excessive when

compared to other ALDI stores. The proposal provides a total of 98 car parks utilising a rate of 5.2 spaces per 100m² which is considered reasonable. Thirty of those car parks are provided in the at-grade open air section with the balance (68, including 2 disabled car parks) in the undercroft car park. There are six (6) staff designated car parks within the car parking area. There is no on street car parking permitted adjacent the site.

GTA (the traffic consultants) have based their parking calculations on traffic generation from a number of the Victorian ALDI stores where the traffic utilising ALDI sites is likely to be slightly more than its South Australian counterparts. The parking rates proposed are reasonable considering the Development Plan in question has not been updated since 2017. Other Council Development Plans that have been updated have a lesser degree of parking requirements and utilise the gross retail floor area as opposed to total floor area which is much more reasonable given the type of Zone it is to be located in (Local Centre). The site is also accessible to public transport with a bus stop adjacent the site.

On the basis of Development Plan provisions which suggest a building's total floor area is the most accurate indicator of likely parking demand, empirical data provided by GTA based on more contemporary car parking conditions at existing ALDI stores and the proximity of public transport, it is considered the proposed on-site parking provided is reasonable. Worth noting is that the current Mitre 10 land use does not meet the car parking ratios as set out in the Development Plan.

7.7.3 Bicycle Parking

The Development Plan is silent when it comes to parking requirements for this type of development. Nonetheless, the proposal provides five bicycle parks in a convenient and legible location by the front entrance.

7.7.4 Deliveries – Hours of Operation

ALDI oversees and undertakes its own deliveries and seeks as part of this proposal to have the delivery trucks access the ALDI site 24/7 at any time during the day or night with 2 x deliveries by semi-trailers per day and 1 bakery delivery daily by a small rigid vehicle. The delivery vehicles, more specifically the semi-trailer will turn left into the site from Portrush Road and before the loading will do an 180° turn and then reverse into the loading dock to be able to leave in a forward direction. Exit will be via a left turn into Portrush Road. The loading dock area is located away from the front of the building at the western end of the site near the boundary and adjacent the rear yards of residential properties. The ALDI loading dock will be fitted with a shroud around the loading area to further mitigate noise.

To ease the impact this might have on the residences to the west – all delivery trucks accessing the site will have down swept (low level discharge) exhausts, attenuated compressed air release and noise level equivalent to that measured at the ALDI Derrimut distribution centre (Victoria, 2014) refrigeration. Delivery vehicles are able to enter and exit the site in a forward direction with truck manoeuvres taking place at the rear of the site with minimal interruption to light vehicle traffic circulating the aisle.

The proposal provides safe and convenient access for pedestrians and vehicles, adequate off street parking with limited and safe points of access and egress with minimal.

7.8 Environmental Factors

7.8.1 Crime Prevention

Council-wide policy seeks that development be designed to maximise surveillance of public spaces (through incorporation of clear sight lines, appropriate lighting and permeable barriers), to also maximise passive surveillance, allow appropriate levels of surveillance of car parking areas, service areas and entrances to development to minimise opportunities for crime; and avoid pedestrian entrapment spots and routes and vandalism.

In addition to fostering surveillance of the surrounding public realm simply through increased activation, the proposal adopts the following crime prevention measures:

- an activated glazed frontage to Portrush Road which encourage casual surveillance of the public realm.
- The car parking area is open to view from Portrush Road and will include lighting to Australian Standards.
- Signage will be provided to assist with wayfinding and to highlight the entrances and pathways to and within the site.
- the proposed landscaping will maintain view-lines to entrances and exits as well as allowing clear views to areas where people may gather.
- The risk of vandalism and graffiti will be minimised through the use of a variety of building materials and colours.
- The under-croft car park will be closed and secured outside shop trading hours and will be installed with both appropriate public lighting and Closed-Circuit Television (CCTV) systems and will be secured by roller shutters when the store is closed.
- The overall layout of the site and design of buildings (including the locations of dense/tall landscaping) also minimise the opportunities for crime to occur outside of the view from other public places.

It is considered the above treatments are acceptable.

7.8.2 Noise – Operations

An acoustic report, undertaken by Sonus acoustic engineers, has been commissioned by the applicant. This assessment summarises the prediction of noise from the proposed ALDI operations and compares those predictions with the relevant Development Plan and Environment Protection (Noise) Policy 2007 noise criteria and provides recommendation for acoustic treatments to ensure that the noise levels do not detrimentally affect the amenity of the locality.

Based on the noise levels of Environment Protection (Noise) Policy 2007, the following acoustic treatments have been recommended by Sonus for the ALDI proposal:

- *Construct fences which are 1.8m high, 2.4m high, and 3.0m high, for the extents shown in Figure 1 as RED, YELLOW, and GREEN, respectively(refer to Sonus Attachment). All fences should be constructed from a minimum of 0.42 BMT sheet steel ("Colorbond" or similar) or a material with the same or greater surface density (kg/m²). An airtight seal should be achieved at all junctions, including at the ground, joins to other fences, and returns to the building. The heights specified for the 1.8m and 2.4m fences are to be achieved relative to the ground level of the adjacent residence on the boundary. The height of the 3.0m fence is to be relative to the level of the loading area.*
- *The fence along the northern boundary should be constructed to return to the adjacent masonry wall to provide an airtight seal at the junction.*

- *Locate all mechanical plant on the roof of the ALDI building within the designated area (refer to Sonus Attachment).*
- *Construct the mechanical plant screens on the building roof and ensure a minimum height of 1.0m above the tallest unit for the extent of the unit. All screens should be constructed from a minimum of 0.42 BMT sheet steel ("Colorbond" or similar) or a material with the same or greater surface density (kg/m²). The barrier should be sealed air tight at all junctions, noting that a small gap may be required at the roof join for drainage. The barrier may be open on the northern side for the extent shown for ventilation purposes.*
- *Install acoustic absorption material, such as 50mm thick polyester insulation with a minimum density of 32kg/m³ in accordance with Figure 2, or a proprietary weather proof product with an "NRC" rating of 0.8 or greater ("Stratocell Whisper" or similar), to the full extent of the mechanical plant screen and fence.*
- *To ensure there is not unreasonable interference from noise from rubbish collection, it is recommended that the hours of rubbish collection from the site be restricted to the hours of Division 3 of the Environment Protection (Noise) Policy 2007. That is, only between the hours of 9:00am and 7:00pm on a Sunday or public holiday, and 7:00am and 7:00pm on any other day.*

SCAP received representations (including an acoustic report from Resonate) during the public notification period that mentioned concerns with noise potentially generated by the development and the adequacy of noise attenuation measures, particularly as they relate to the adjoining properties to the west. The representors concerns have been reviewed by the applicant's planning consultant in conjunction with Sonus Acoustic Engineers and their response is provided in the attachments. In summary, the Sonus response is provided below for clarity:

- *ALDI has adopted a consistent approach to the management and implementation of their Delivery and Loading Procedures which applies across all ALDI Stores within South Australia. ALDI has invested in specific trucks and training of their drivers to ensure that all loading and operating procedures are consistently followed. In particular, ALDI trucks are fitted with;*
 - » *Low-level exhausts to maximise the effectiveness of noise barriers;*
 - » *Reversing cameras to enable reversing alarms to be switched off; and*
 - » *Control of refrigeration and reversing alarms from within the driver's cab.*

ALDI truck drivers unload their own trucks and therefore the truck is not left idling while unloading occurs;

- *Waste is collected on Mondays, Wednesdays and Fridays using a truck fitted with a low-level exhaust. These collections occur between the hours of 7:00am to 7:00pm to achieve the requirements of the Environment Protection (Noise) Policy 2007. In addition, the noise barriers proposed in the vicinity of the loading dock will significantly reduce the noise during the day;*
- *The noise from waste collection has been measured at ALDI West Lakes. Based on this noise measurement, it is predicted that the maximum instantaneous noise level in the rear yard of the closest residence would be lower than the expected noise at the front facade of the residence from Council rubbish collection. The average noise level (LAeq) in the rear yard is expected to be 52 dB(A). This level achieves the compliance level of the Environment Protection (Noise) Policy 2007 for activity which might occur continuously all day;*
- *ALDI does not operate a Public Address (PA) system;*

- *The Sonus assessment of noise generated within the Loading Dock included consideration of delivery activity, rubbish collection and operation of the compactor. The proposed noise attenuation measures were carefully considered and assessed and have been designed to reduce the noise to suitable levels in accordance with the Environment Protection (Noise) Policy 2007.*
- *The unique design of the ALDI Loading Dock, which includes a 'shroud' around the loading dock which assists to reduce noise associated with loading.*

Based on the above, Sonus consider that the facility has been designed to take place in a manner which will not cause nuisance, avoid unreasonable interference on amenity, and will not detrimentally affect the amenity of the locality by way of noise, thereby achieving the relevant provisions of the Development Plan related to environmental noise.

In the event of SCAP support, a condition of approval will require the recommendations in the Sonus report (S4217.30C5 dated Sep 2019) are undertaken to ensure that the development is designed in accordance with the relevant noise criteria. The measures to mitigate the acoustic impacts are consistent with measures applied in a number of other ALDI sites. The acoustic fence proposed is an improvement on what is currently on the Mitre 10 site which is a non-acoustically treated 2 metre high fence. There is also legal precedence that ALDI have to adhere to the acoustic recommendations.

7.8.3 Waste Management

The refuse and compactor vehicles will access the site in medium rigid vehicles and will undertake a more flexible turning manoeuvre similar to the delivery vehicles.

The representors concerns were with the proximity of the waste area to the western boundary and potential for odour as well as the management of rats and vermin. Also the bin is fully enclosed and is not anticipated to generate offensive odour due to ALDI's Waste Management strategy that takes into consideration waste to landfill, recycling of cardboard and plastics, and management of waste generated on site (food packaging and office waste). Given there is no food is prepared or cooked on site, and 99% of food reaching its best before date is redistributed to Foodbank and Secondbite. This type of approach allows little opportunity for the encouragement of rats/vermin. Also it is in ALDI's best interest to keep a tidy and presentable site so rat traps would be maintained on a regular basis. The waste bin is to be serviced by a waste collection vehicle 3 times per week during daytime hours.

The acoustic treatment to the bin area as stated previously in the Noise Section is considered acceptable. The Department has been made aware of a complaint regarding excessive noise and vibration made by an adjoining land owner adjacent an existing ALDI store at Seaton (44 Frederick Road West Lakes). This is understood to be caused by the removal and replacement of the supermarket's compactor which the complainant claims is causing structural damage to their property. ALDI the applicant has undertaken to investigate these claims and respond accordingly to Council and the Department. That response has yet to be provided.

In this instance the policy regarding amenity impacts caused by vibration are considered likely to be satisfied due to the 25 metre separation distance between the location of the compactor and the nearest residential structure.

7.8.4 Odour

As noted in the applicant's response if odour was to become an issue Council has powers to manage unreasonable levels of odour through the *Local Nuisance and Litter Control Act 2016*. It is considered the applicant has taken reasonable steps to manage odour by ensuring that the bin is fully enclosed and refuse is collected 3 times per week.

7.8.5 Light Spill

Outdoor lighting is yet to be determined by the applicant. However, all lighting will be designed in accordance with *Australian Standard 4282 – 1997 'Control of the obtrusive effects of outdoor lighting'*, to ensure lux levels and light glare does not unreasonably impact on the amenity of the locality. Additionally, all lighting within the car park will be designed in accordance with *Australian Standard AS 1158 – Lighting for roads and public spaces*. A condition has been recommended for final details of lighting for the site.

7.8.6 Energy Efficiency

Development should be designed to take advantage of renewable sources of energy, especially solar energy, and minimise use of non-renewable sources of energy, including fossil fuels, in construction, and for heating, cooling, orientation and cross ventilation, lighting and other purposes.

- Solar panels on have been integrated into the design of the building such that they will be located on a flat roof with a northerly aspect
- Open sides for natural ventilation to the undercroft car park
- Deciduous trees that allow sun access in the cooler months and shade in summer

It is considered that the proposal creates significant opportunity to harvest and reuse rainwater (given the extent of roofed area and the proposed extent and nature of landscaping). Unfortunately, infrastructure to achieve this does not form part of the proposal.

Whilst the proposal does not exemplify sustainable design, it is considered that it satisfies policy regarding this matter.

7.8.7 Stormwater

A preliminary stormwater management plan has been prepared by FMG Engineering. Stormwater will drain to the west of the site and be diverted to an underground tank and pumped to an existing side entry it on Portrush Road. Stormwater from the roof run off will be diverted to above ground tanks and discharged to the same pit.

Council is generally satisfied with the stormwater management proposed subject to approval being sought for any and all alterations to the existing verge as a separate process under Section 221 of the Local Government act 1993, noting that the existing redundant portion of driveway/gutter crossing must be removed and reinstated to kerb and gutter upon the completion of the proposed altered gutter crossing.

In the context of the site characteristics and available stormwater infrastructure, the proposed stormwater management satisfies the relevant provisions of the Development Plan.

7.9 Signage

The development plan seeks integrated signage with the built form and the avoidance of visual clutter. The proposal has five (5) illuminated façade wall signs (2m x 2.4m “light boxes”) which are integrated in to the built form and sit lower than the buildings parapet and one digital sign box within the forecourt. All the proposed signage is contained on the subject land.

As the signage abuts an arterial road, DPTI has reviewed the proposed signage against the *Advertising Signs – Assessment Guidelines for Road Safety* and has placed conditions that limit the illumination and the types of elements that will form the sign, specifically that there will be no flashes, scrolling, moving or changing or the imitation of a traffic control device within the signage. The reason for the limitation is to minimise the potential for driver distraction. Floodlighting is also to be shielded to avoid light spillage.

The scale of the signage is considered appropriate in its context, and the built form. It is typical of the ALDI brand. It accords with the Council Wide provisions of the Development Plan with respect to signage.

8 CONCLUSION

Demolition of all existing structures and staged construction of a freestanding supermarket (shop – retail floor area 1266m²) with associated earthworks, retaining wall, car park, advertising displays, fencing, solar panels and landscaping is considered appropriate in its location.

The proposed development does depart from the gross leasable floor area of 450m² for a shop which is envisaged in the Development Plan. However, as the subject site does contain 3 allotments and each of these allotments could entertain a development of 450m² of gross leasable floor space it is not considered to be an overdevelopment of the site.

The proposed development has similarities to the existing use of the site and comparable use to the previous uses of the site and for that reason is unlikely to have a detrimental impact on the locality. The interface issues between the residential component and the proposal can be managed through a number of appropriate mitigation measures in regard to noise and servicing activities. The strategies provided by ALDI have to be legally adhered to. Odour can be reasonably controlled given the operation does not prepare food on site. Generally the management of overshadowing is acceptable. There are some impacts to the southern boundary but these are limited to early morning and late afternoon.

The scale and built form, though somewhat larger than what currently exists is not dissimilar to other commercial development currently existing within the locality, south of the site and within the Local Centre Zone. The more extensive landscaping proposed allows adequate screening between the residences, the motel and the proposal and on the western boundary is more proportionate to the scale of the building. It will also improve the streetscape amenity between the public space and built form and will help to soften the visual impact of the towers.

The traffic management and modifications have been supported by DPTI. The additional traffic movements generated by the proposal whilst more intense than what currently exists with Mitre 10 can be accommodated within the existing road network. Whilst the parking numbers provided do not meet the prerequisite amount as prescribed by the Development Plan, the parking provided is adequate and reasonable as calculated using existing ALDI data for the proposed retail area.

The provision of solar panels is a plus for the development and meets the provisions in the Development Plan in regard to energy conservation.

In recommending consent, consideration is given to the appropriateness to the development, the understanding of the land tenure and zoning and the balancing of information provided by the applicant with respect to the residential interface, noise, visual amenity (bulk and scale) and traffic impacts including car parking. It is considered the site can be operated in a manner which will minimise the impact of such activities on adjacent residential development and occupants. A number of conditions are included that are generally from an operational perspective that further seek to minimise the potential impacts of the proposed development.

9 RECOMMENDATION

It is recommended that the State Commission Assessment Panel:

- 1) RESOLVE that the proposed development is NOT seriously at variance with the policies in the Development Plan.
- 2) RESOLVE that the State Commission Assessment Panel is satisfied that the proposal generally accords with the related Objectives and Principles of Development Control of the City of Burnside Development Plan.
- 3) RESOLVE to grant Development Plan Consent to the proposal by Nielsen Architects for Demolition of all existing structures and staged construction of a freestanding supermarket (shop – 1266m²) with associated earthworks, retaining wall, car park, advertising displays, fencing, solar panels and landscaping at 499 Portrush Road Glenunga subject to the following reserved matters and conditions of consent.

PLANNING CONDITIONS

1. The development herein granted Development Plan Consent shall be undertaken and completed in accordance with the stamped plans and documentation, except where varied by conditions below.

Reason for condition: To ensure the development is undertaken in accordance with the endorsed plans and application details.

2. The development shall be undertaken in accordance with the recommended acoustic treatments contained in the Environmental Noise Assessment report (S4217.30C5) prepared by Sonus Pty Ltd and dated September 2019

Reason for condition: To reduce the potential noise impacts and to protect the amenity of the locality.

3. Access to the site shall be in general accordance with Nielsen Architects Lower Site Plan, Project 2353SL1, Drawing No. DA02.3 dated September 2019.

Reason for condition: To ensure safe vehicle movement to and from the site.

4. The proposed modifications to the central median on the adjacent section of Portrush Road, as shown in 'Figure 5.1: Proposed Intersection Treatment – Portrush Road' in the Traffic Impact Assessment (TIA) prepared by GTA Consultants dated 13 October 2019, shall be designed in accordance with the relevant Austroads Guides, Australian Standards and to DPTI's requirements. All costs associated with the design and construction of the proposed median modifications (including, but not limited to, project management and any necessary public consultation, road drainage, lighting upgrades etc.) shall be borne by the applicant. The applicant shall enter into a Developer Agreement with DPTI regarding these works and shall contact DPTI's Traffic

Services Section, Network Planning Engineer, Ms Teresa Xavier on (08) 8226 8389 or via email at Teresa.Xavier@sa.gov.au to obtain approval and discuss any technical issues regarding the proposed median modifications. The proposed median modifications shall be completed prior to the proposed development becoming operational.

Reason for condition: To ensure the safe free flow of traffic on Portrush Road.

5. All off-street parking shall be designed in accordance with AS/NZS 2890.1:2004 and AS/NZS 2890.6:2009. Additionally, clear sightlines, as shown in Figure 3.3 'Minimum Sight Lines for Pedestrian Safety' in AS/NZS 2890.1:2004, shall be provided at the Portrush Road property line to ensure adequate visibility between vehicles leaving the site and pedestrians on the adjacent footpath.

Reason for condition: To ensure safe operation of the development.

6. All off-street commercial vehicle facilities shall be designed in accordance with AS 2890.2:2018.

Reason for condition: To ensure safe operation of the development.

7. The largest vehicle permitted on-site shall be restricted to a 15.5 metres long articulated vehicle.

Reason for condition: To ensure safe operation of the development.

8. A Traffic Management Plan for the construction period of the development shall be produced to the satisfaction of DPTI in consultation with Council prior to the commencement of construction. This plan shall detail the types, volumes and distributions of traffic and how they will be managed.

Reason for condition: To ensure safe operation of the development.

9. Illuminated signage that is visible from the adjacent roads shall not contain any element that flashes, scrolls, moves or changes, or imitates a traffic control device and shall be limited to a low level of illumination (i.e. < 150Cd/m²).

Reason for condition: To ensure driver safety

10. Any flood lighting shall be located and shielded in order to minimise the potential for driver distraction and discomfort.

Reason for condition: To ensure driver safety

11. All external lighting of the site, including in car parking areas and external to buildings, shall be designed, located, shielded and constructed to conform to the pertinent Australian Standards.

Reason for condition: To ensure safe operation of the development.

12. Stormwater run-off shall be collected on-site and discharged without jeopardising the integrity and safety of the adjacent road network. Any alterations to drainage infrastructure required to facilitate this shall be at the applicant's cost.

Reason for condition: To ensure safe operation of the development.

13. One delivery of baked goods per day is permitted outside the regular store trading hours.

Reason for condition: To reduce the impact of potential conflict issues and to protect the amenity of the locality.

14. Any waste collection associated with the site shall occur between 9:00 am – 7:00 pm on a Sunday or a public holiday and between 7:00 am – 7:00 pm on any other day.

Reason for condition: To reduce the potential noise impacts and to protect the amenity of the locality.

15. All other deliveries and third party service vehicles to the site (other than for a purpose identified above) shall only enter and exit the subject land between 9:00 am – 7:00 pm on a Sunday or a public holiday and between 7:00 am – 9:00 pm on any other day.

Reason for condition: To reduce the impact of potential conflict issues and to protect the amenity of the locality.

16. Delivery vehicles accessing the site shall be restricted to a maximum length of 15.5 metres, with these vehicles having a down-swept (low level discharge) exhaust system and attenuated compressed air release.

Reason for condition: To reduce the impact of potential conflict issues and to protect the amenity of the locality.

17. All service and delivery vehicles operated by ALDI (or on behalf of ALDI) may only enter, exit, and be used on the subject land in accordance with the 'ALDI Delivery & Loading Procedures', as submitted with this application.

Reason for condition: To reduce the impact of potential conflict issues and to protect the amenity of the locality.

19. Landscaping shown on the approved plans shall be well established prior to the operation of the development. Plantings shall be irrigated, maintained, and nurtured at all times with any diseased or dying plants being replaced.

Reason for condition: To ensure the development is constructed and maintained in accordance with the approved plans.

20. All stormwater design and construction shall be in accordance with Australian Standard AS/NZS3500.3:2015 (Part 3) to ensure that stormwater does not adversely affect any adjoining property or public road.

Reason for condition: To ensure safe operation of the development.

21. All driveways and vehicle entry and manoeuvring areas shall be designed and constructed in accordance with Australian Standards (AS/NZS 2890.1:2004 and AS/NZS 2890.6:2009) prior to the occupation or use of the development.

Reason for condition: To ensure safe operation of the development.

22. All bicycle parks shall be designed and constructed in accordance with Australian Standard AS2890.3-2015.

Reason for condition: To ensure safe operation of the development.

23. The existing redundant portion of driveway/gutter crossing shall be removed and reinstated to kerb and gutter to the Councils specification at the cost of the applicant upon the completion of the proposed altered gutter crossing.

Reason for condition: To ensure safe pedestrian access

ADVISORY NOTES

- a. This Development Plan Consent will expire after 12 months from the date of this Notification, unless final Development Approval from Council has been received within that period or this Consent has been extended by the State Commission Assessment Panel.
- b. The applicant is also advised that any act or work authorised or required by this Notification must be substantially commenced within 1 year of the final Development Approval issued by Council and substantially completed within 3 years of the date of final Development Approval issued by Council, unless that Development Approval is extended by the Council.
- c. The applicant has a right of appeal against the conditions which have been imposed on this Development Plan Consent. Such an appeal must be lodged at the Environment, Resources and Development Court within two months from the day of receiving this notice or such longer time as the Court may allow. The applicant is asked to contact the Court if wishing to appeal. The Court is located in the Sir Samuel Way Building, Victoria Square, Adelaide, and (telephone number 8204 0289).
- d. The cost of works relating to the alterations to crossovers and associated line marking are to be borne fully by the applicant, and are subject to separate approvals required under the *Local Government Act 1999*.
- e. The applicant is reminded of their obligations under the *Local Nuisance and Litter Control Act 2016* and the *Environment Protection Act 1993*, in regard to the appropriate management of environmental impacts and matters of local nuisance. For further information about appropriate management of construction sites, please contact the City of Burnside Council.

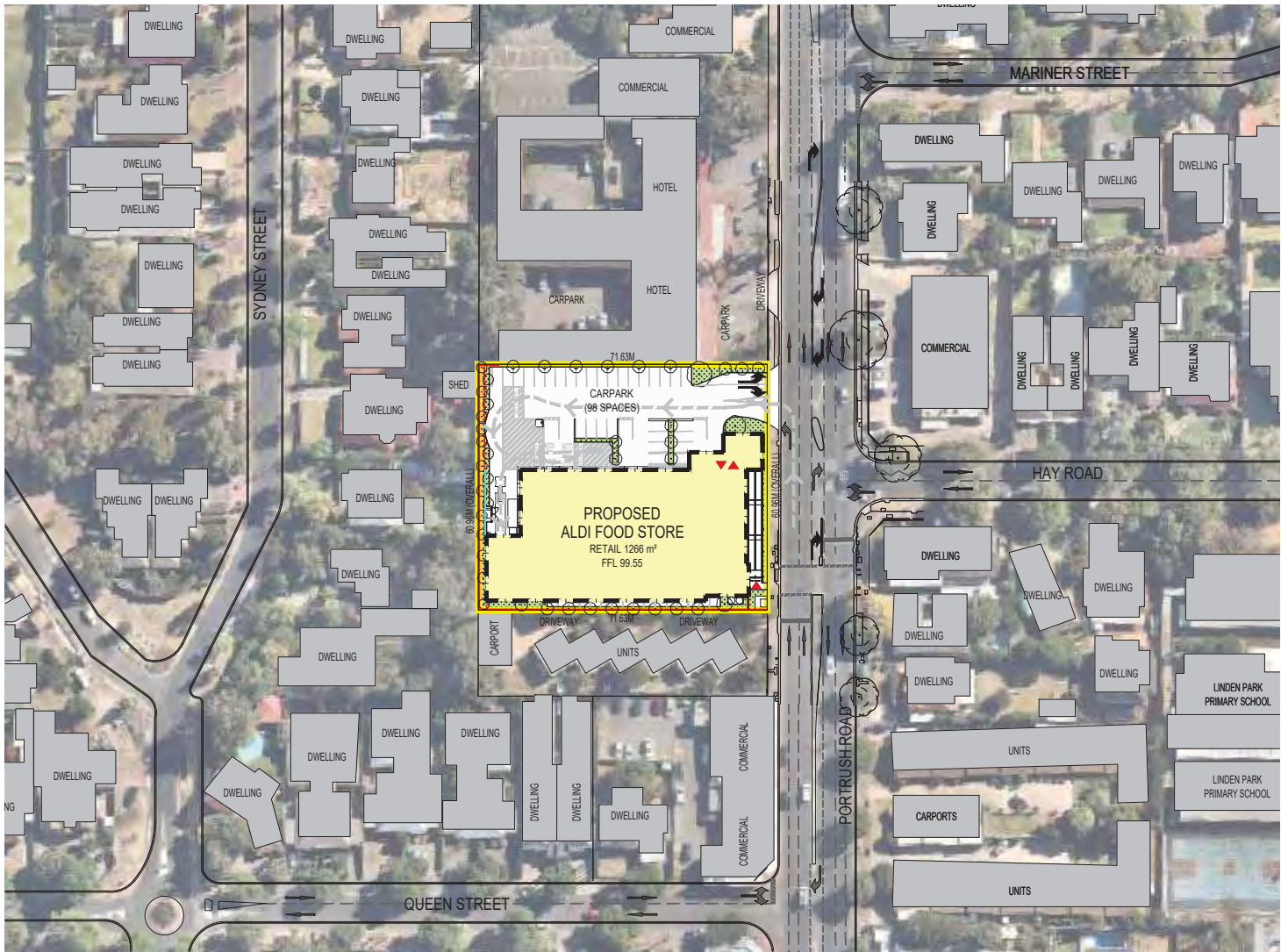


Yasmine Alliu

PLANNING OFFICER

DEVELOPMENT DIVISION

DEPARTMENT OF PLANNING, TRANSPORT and INFRASTRUCTURE



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REV	DATE	DESCRIPTION	DRN	CHKD
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01 LOCALITY PLAN
1:1000 PROPOSED

CLIENT
ALDI STORES

PROJECT
ALDI GLENUNGA
No. 499 PORTRUSH ROAD
GLENUNGA S.A. 5064

DRAWING
LOCALITY PLAN

 108 Mt Barker Road Stirling
South Australia 5152
p: 08 8339 8008
f: 08 8339 2004
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www.nielsenarchitects.com.au

SCALE	DATE	DRAWN	CHECKED
1:1000	SEP 2019	LT	TB
PROJECT No	DRAWING No	STATUS	REV
2353SL1	DA01.1	DA	.

ORIGINAL A3

REV	DATE	DESCRIPTION	DRN	CHKD
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REV	DATE	DESCRIPTION	DRN	CHKD
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01 CONTEXT PLAN
1:500 PROPOSED

CLIENT
ALDI STORES



PROJECT
ALDI GLENUNGA
No. 499 PORTRUSH ROAD
GLENUNGA S.A. 5064

DRAWING
CONTEXT PLAN

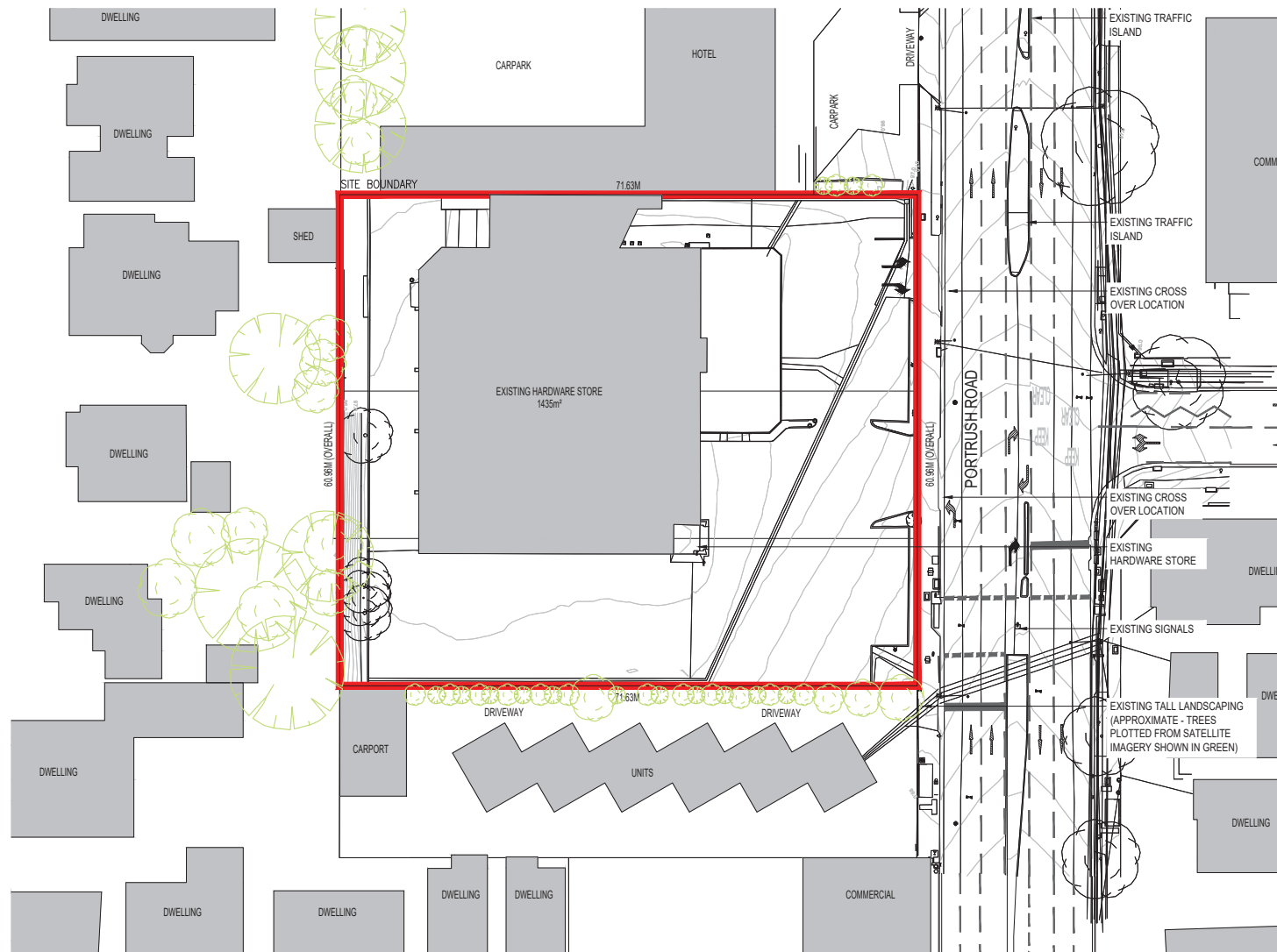
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SCALE 1:500	DATE SEP 2019	DRAWN LT	CHECKED TB
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01 EXISTING SITE PLAN
1:500 EXISTING

CLIENT
ALDI STORES



PROJECT
ALDI GLENUNGA
No. 499 PORTRUSH ROAD
GLENUNGA S.A. 5064

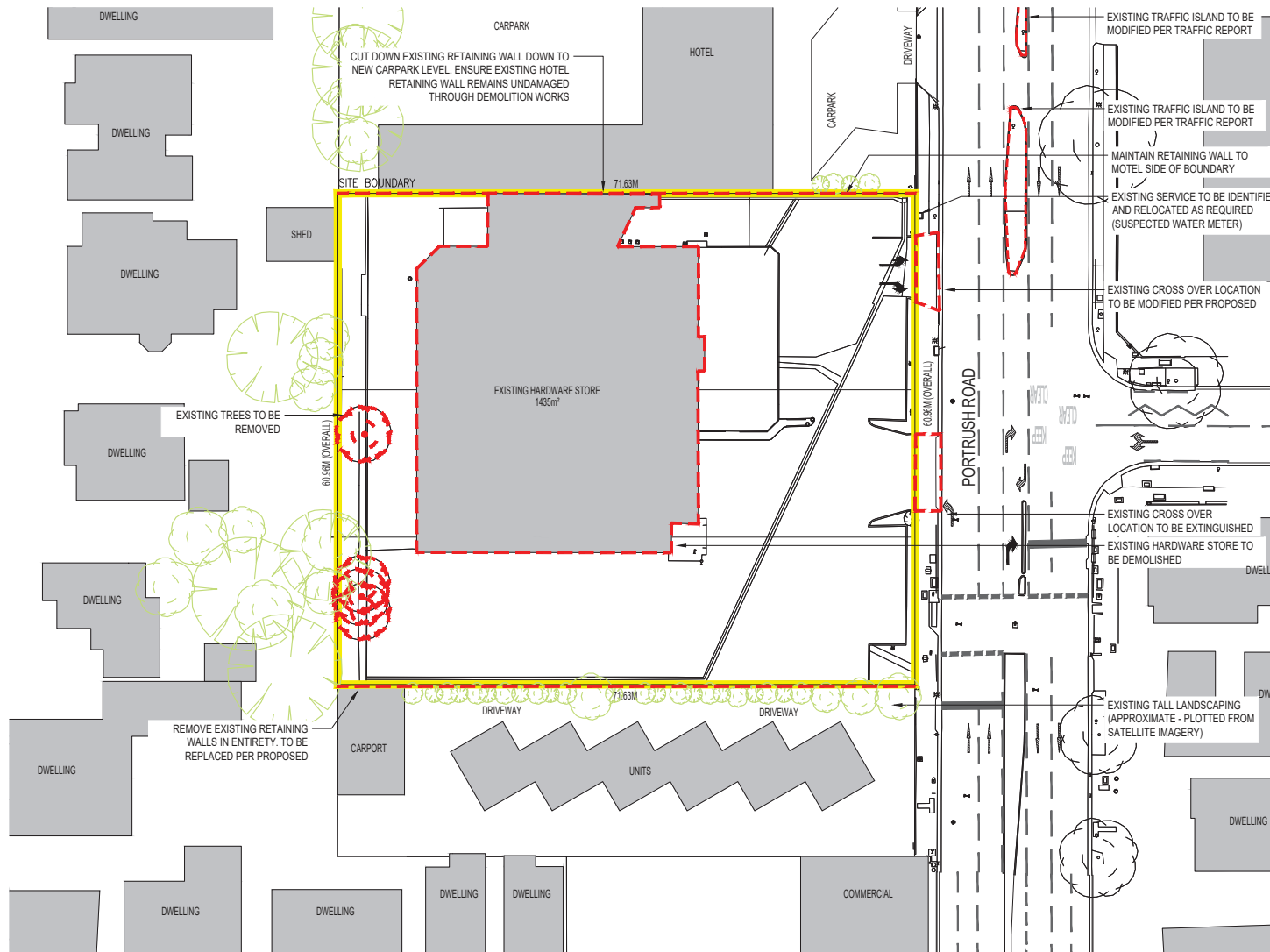
DRAWING
EXISTING SITE PLAN



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SCALE 1:500	DATE SEP 2019	DRAWN LT	CHECKED TB
PROJECT No 2353SL1	DRAWING No DA02.1	STATUS DA	REV -



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01 DEMOLITION PLAN
1:500 PROPOSED

CLIENT
ALDI STORES



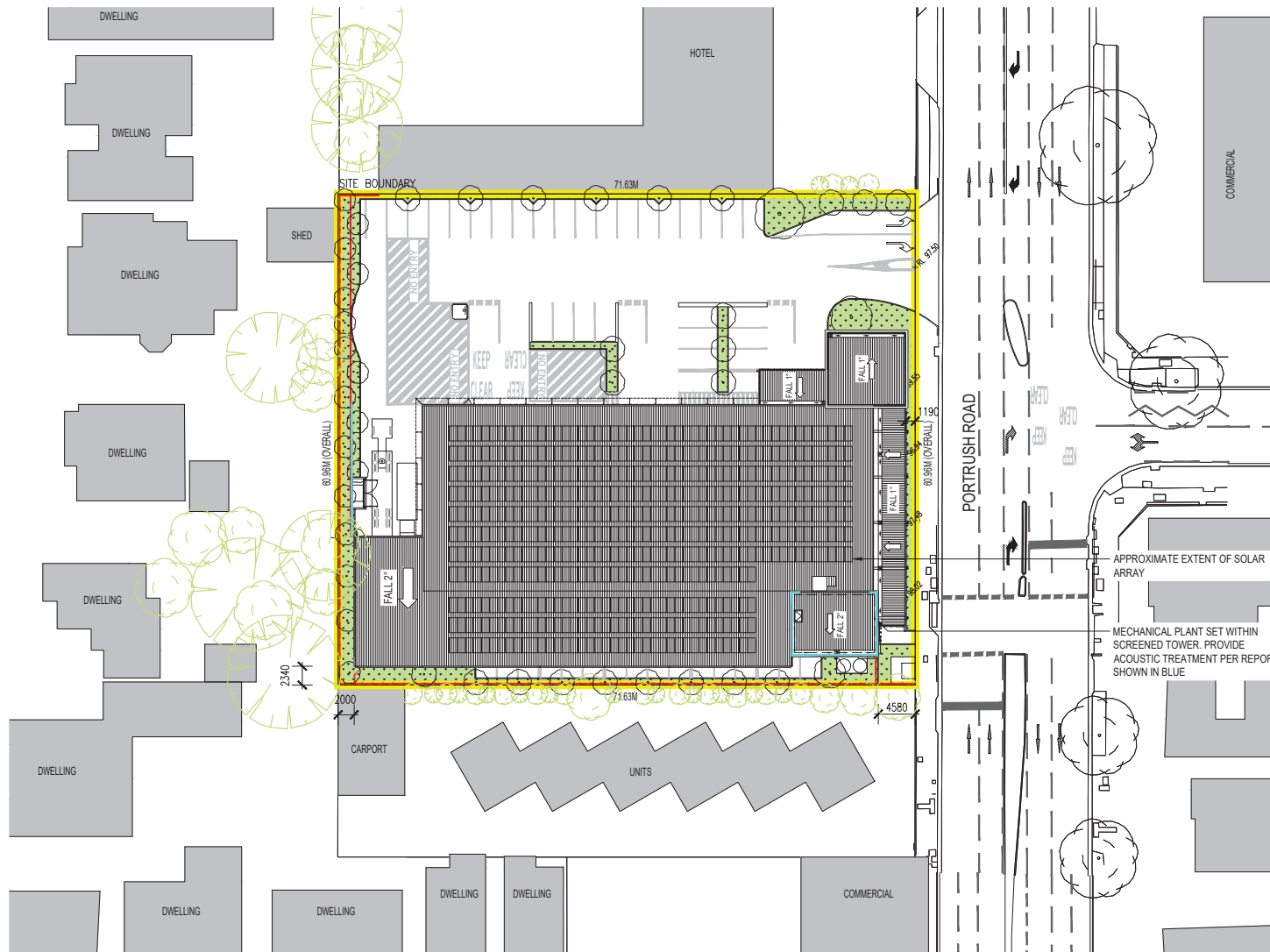
PROJECT
ALDI GLENUNGA
No. 499 PORTRUSH ROAD
GLENUNGA S.A. 5064

DRAWING
DEMOLITION PLAN

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SCALE	DATE	DRAWN	CHECKED
1:500	SEP 2019	LT	TB

PROJECT No	DRAWING No	STATUS	REV
2353SL1	DA02.2	DA	.



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REV	DATE	DESCRIPTION	DRN	CHKD
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01 ROOF DRAINAGE
1:500 PROPOSED

CLIENT
ALDI STORES



PROJECT
ALDI GLENUNGA
No. 499 PORTRUSH ROAD
GLENUNGA S.A. 5064

DRAWING
ROOF DRAINAGE PLAN



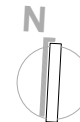
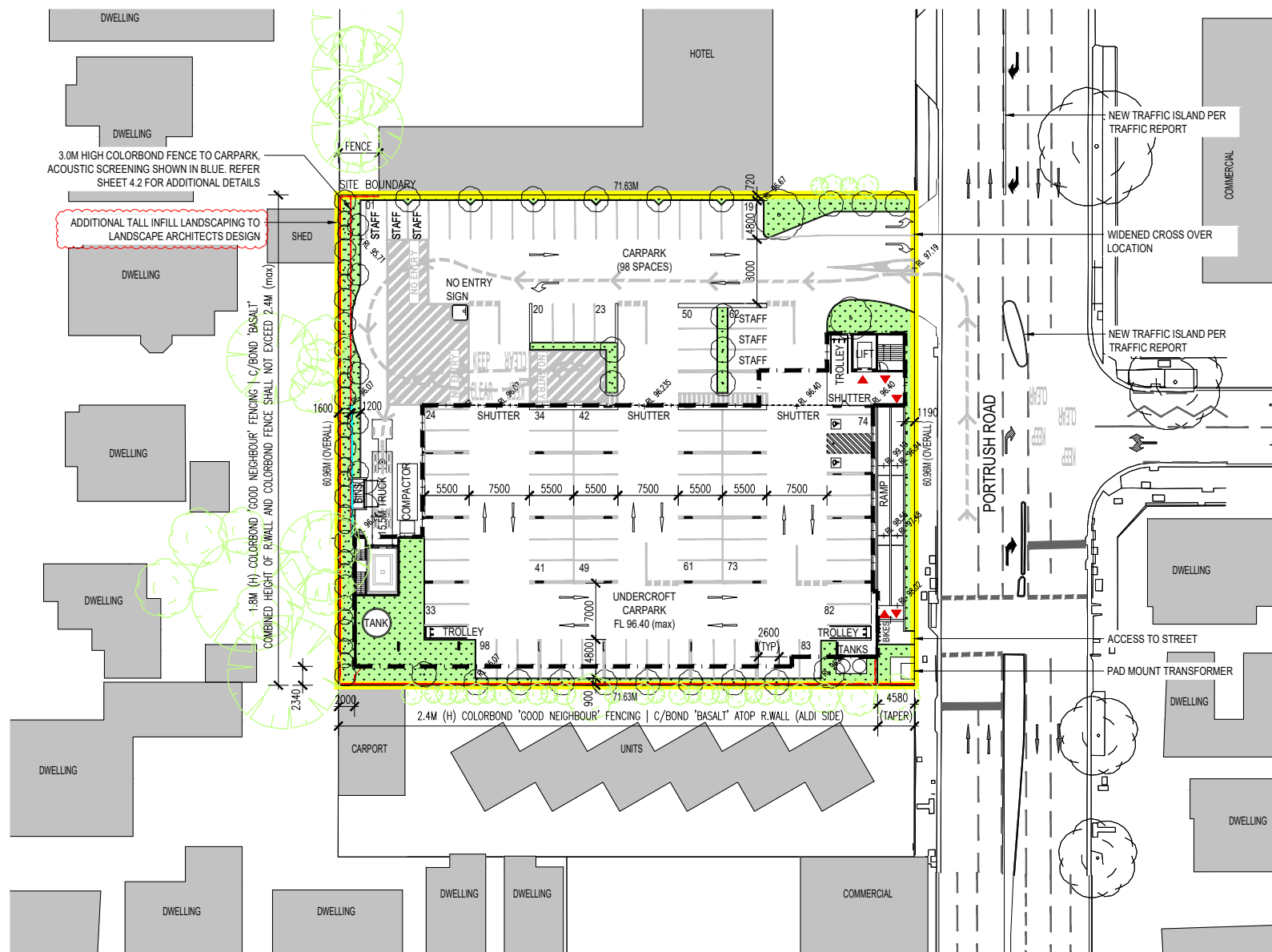
1108 Mt Barker Road Stirling
South Australia 5152
p: 08 8339 8008
f: 08 8339 2004
P.O. Box 931 Stirling SA 5152
info@nielsenarchitects.com.au
www.nielsenarchitects.com.au

SCALE	DATE	DRAWN	CHECKED
1:500	SEP 2019	LT	TB
PROJECT No	DRAWING No	STATUS	REV
2353SL1	DA02.5	DA	.

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A	14.11.19	PLANNING	LT	TB



01 LOWER SITE PLAN
1:500 PROPOSED

CLIENT
ALDI STORES



PROJECT
ALDI GLENUNGA
No. 499 PORTRUSH ROAD
GLENUNGA S.A. 5064

DRAWING
LOWER SITE PLAN



108 Mt Barker Road Stirling
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PROJECT No 2353SL1	DRAWING No DA02.3	STATUS DA	REV A

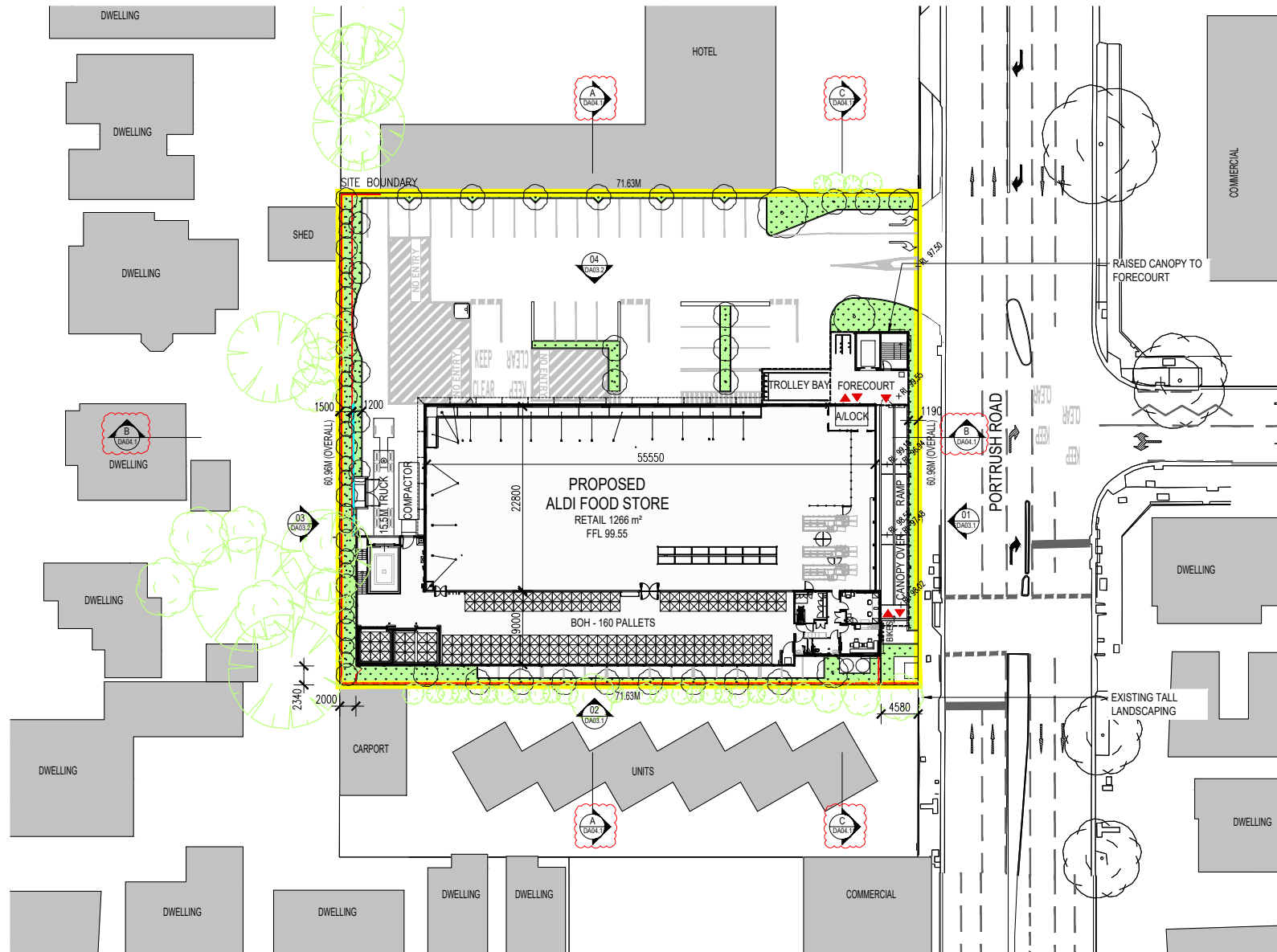
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A	14.11.19	PLANNING	LT	TB

SITE PLAN PROPERTY DESCRIPTION	
TOTAL SITE AREA	4366m ²
ALDI GROSS AREA	2070m ²
ALDI NETT AREA (INC F.C.)	2006m ²
RETAIL NETT AREA	1266m ²
BOH NETT AREA	537m ²
AMENITIES NETT AREA	80m ²
FORECOURT NETT AREA	123m ²
REMAINING NETT AREA (INC F.C.)	740m ²
BOH PALLETS	160
CHILLER LENGTH	43.75
TRUCK LENGTH	15.50M
TOTAL CARPARKS	98



01 UPPER SITE PLAN
1:500 PROPOSED

CLIENT
ALDI STORES



PROJECT
ALDI GLENUNGA
No. 499 PORTRUSH ROAD
GLENUNGA S.A. 5064

DRAWING
UPPER SITE PLAN

NIELSEN ARCHITECTS
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PROJECT No 2353SL1	DRAWING No DA02.4	STATUS DA	REV A

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REV	DATE	DESCRIPTION	DRN	CHKD
A	14.11.19	PLANNING	LT	TB

MATERIAL SCHEDULE

01	PRECAST RC PANEL, PAINT FINISH DULUX 'FLUORESCENT FIRE'
02	PRECAST RC PANEL, PAINT FINISH DULUX 'DRIVETIME'
03	PERFORATED POWDERCOATED SCREEN DULUX 'COPPER KINETIC'
04	FIBRE CEMENT FASCIA DULUX 'DRIVETIME'
05	EXPOSED METALWORK TO CANOPY COLORBOND 'MONUMENT'
06	POWDERCOATED PLANT LOUVERS & FRAME DULUX 'BASALT'
07	ANODISED ALUMINIUM WINDOW FRAMES NATURAL FINISH
08	SHOPFRONT - ANODISED ALUMINIUM FRAMES, NATURAL FINISH
09	STEEL SUNSHADE FRAMES - PAINTED FINISH COLORBOND 'MONUMENT'
10	COLORBOND CAPPING COLOUR PER ADJACENT PRECAST PANEL
11	COLORBOND RAIN HEAD AND DOWNPIPES COLORBOND 'BASALT'
12	DOOR & FRAME DULUX 'DRIVETIME'
13	ALDI ILLUMINATED SIGNS
14	TROLLEY BAY
15	SOLAR PANELS BEHIND ROOF FORM
16	ALUMINIUM FRAMED GLASS BALUSTRADE CLEAR GLAZING INFILL
17	PANEL LIFT DOOR COLORBOND 'BASALT'
18	PRECAST PANELS TO BIN STORE, PAINTED FINISH DULUX 'DRIVETIME'
19	COLORBOND PROFILED ROOF SHEETING COLORBOND 'SURFMASTER'
20	COMPACTOR DOOR & FRAME DULUX 'OLDE PEWTER'

CLIENT
ALDI STORES



PROJECT
ALDI GLENUNGA
No. 499 PORTRUSH ROAD
GLENUNGA S.A. 5064

DRAWING
EXTERNAL ELEVATIONS

NIELSEN ARCHITECTS
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SCALE
1:250

DATE
NOV 2019

DRAWN
LT

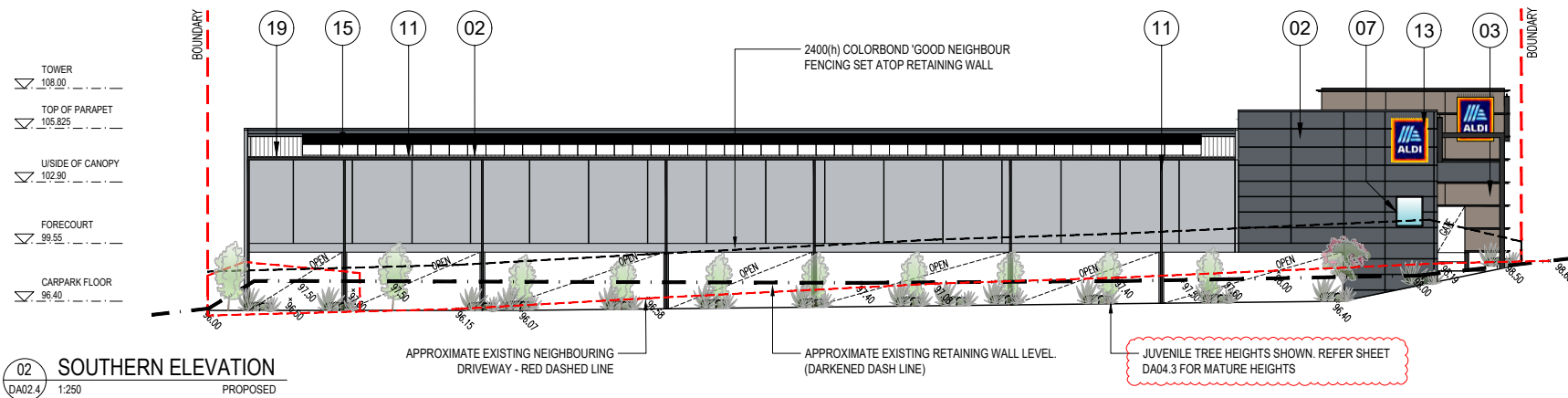
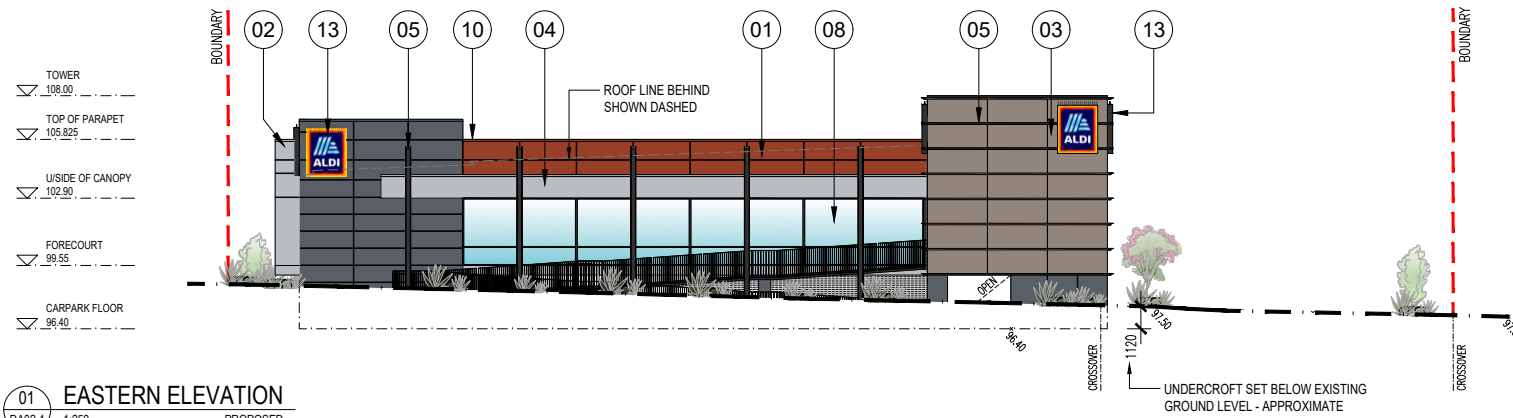
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PROJECT No
2353SL1

DRAWING No
DA03.1

STATUS
DA

REV
A



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REV	DATE	DESCRIPTION	DRN	CHKD
A	14.11.19	PLANNING	LT	TB

MATERIAL SCHEDULE

01	PRECAST RC PANEL, PAINT FINISH DULUX 'FLUORESCENT FIRE'
02	PRECAST RC PANEL, PAINT FINISH DULUX 'DRIVETIME'
03	PERFORATED POWDERCOATED SCREEN DULUX 'COPPER KINETIC'
04	FIBRE CEMENT FASCIA DULUX 'DRIVETIME'
05	EXPOSED METALWORK TO CANOPY COLORBOND 'MONUMENT'
06	POWDERCOATED PLANT LOUVERS & FRAME DULUX 'BASALT'
07	ANODISED ALUMINIUM WINDOW FRAMES NATURAL FINISH
08	SHOPFRONT - ANODISED ALUMINIUM FRAMES, NATURAL FINISH
09	STEEL SUNSHADE FRAMES - PAINTED FINISH COLORBOND 'MONUMENT'
10	COLORBOND CAPPING COLOUR PER ADJACENT PRECAST PANEL
11	COLORBOND RAIN HEAD AND DOWNPIPES COLORBOND 'BASALT'
12	DOOR & FRAME DULUX 'DRIVETIME'
13	ALDI ILLUMINATED SIGNS
14	TROLLEY BAY
15	SOLAR PANELS BEHIND ROOF FORM
16	ALUMINIUM FRAMED GLASS BALUSTRADE CLEAR GLAZING INFILL
17	PANEL LIFT DOOR COLORBOND 'BASALT'
18	PRECAST PANELS TO BIN STORE, PAINTED FINISH DULUX 'DRIVETIME'
19	COLORBOND PROFILED ROOF SHEETING COLORBOND 'SURFIMIST'
20	COMPACTOR DOOR & FRAME DULUX 'OLDE PEWTER'

CLIENT
ALDI STORES



PROJECT
ALDI GLENUNGA
No. 499 PORTRUSH ROAD
GLENUNGA S.A. 5064

DRAWING
EXTERNAL ELEVATIONS



SCALE
1:250

DATE
NOV 2019

DRAWN
LT

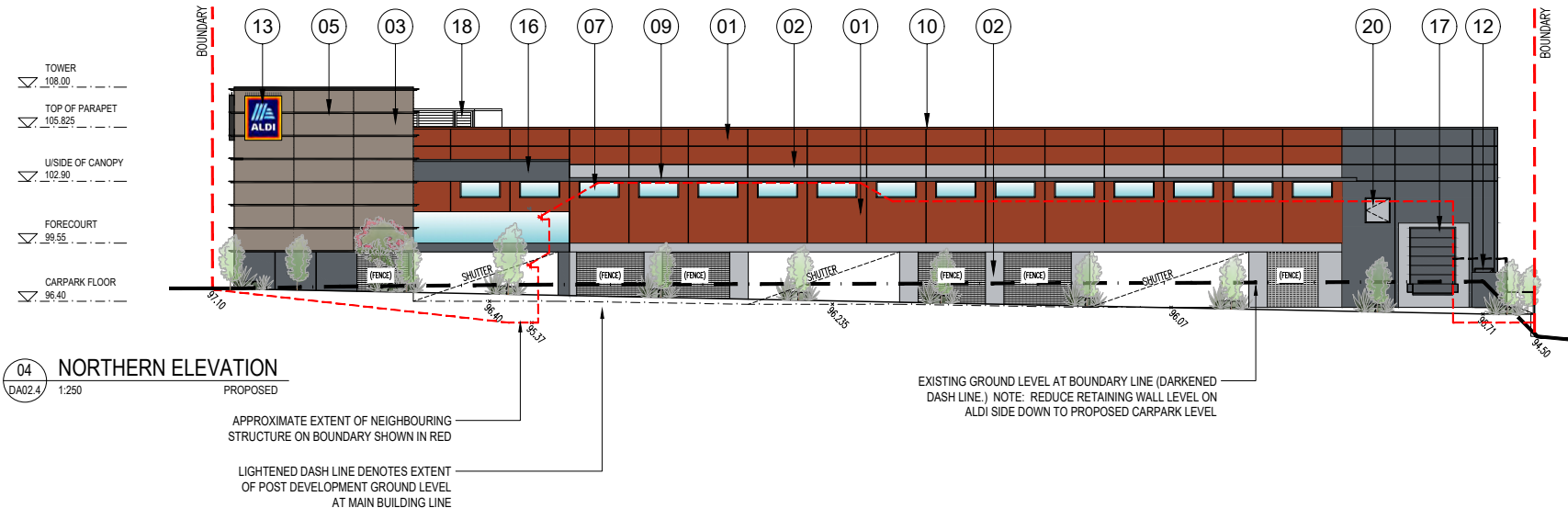
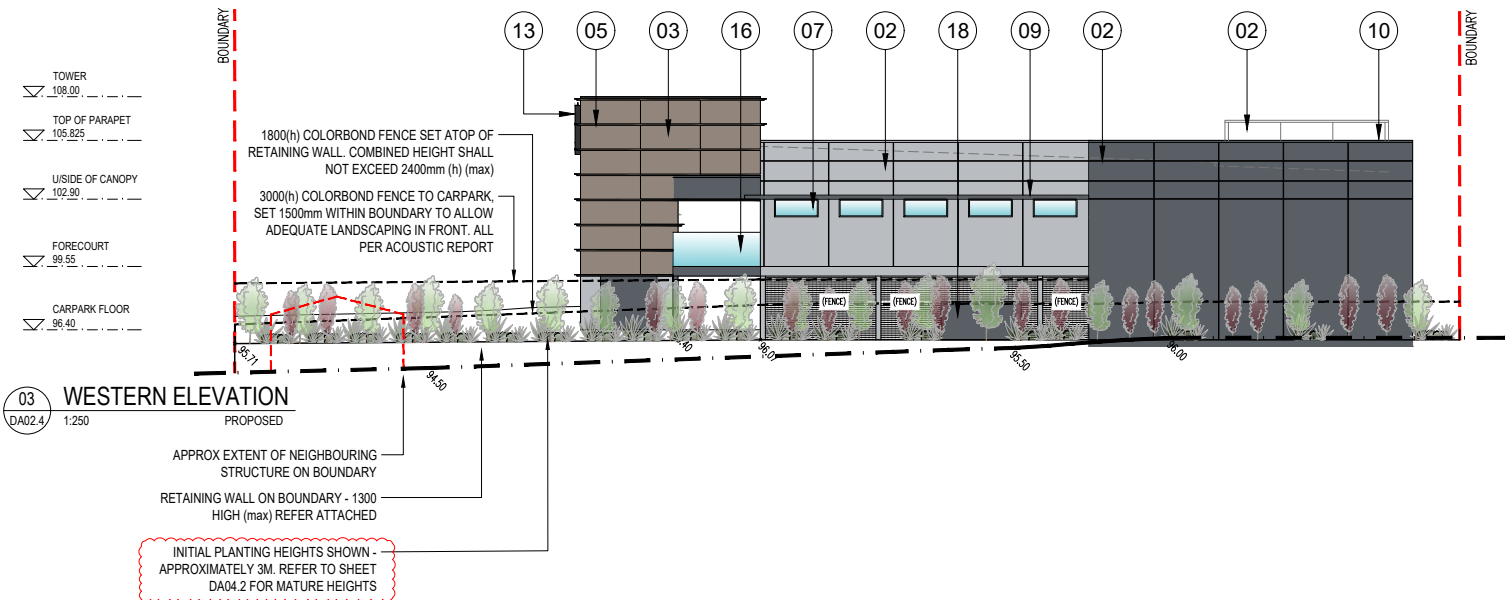
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PROJECT No
2353SL1

DRAWING No
DA03.2

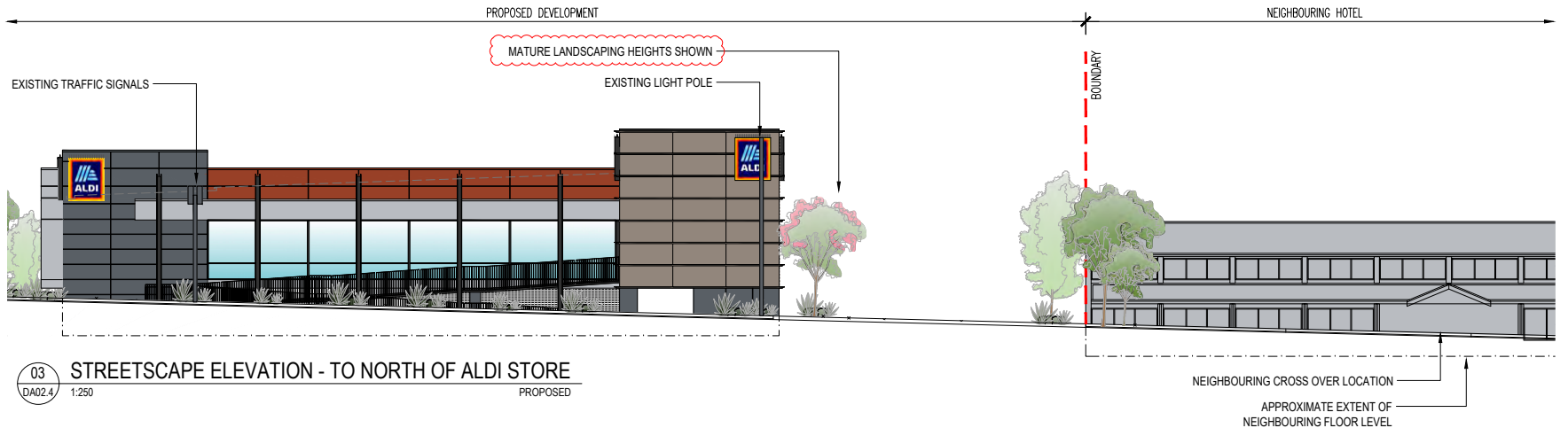
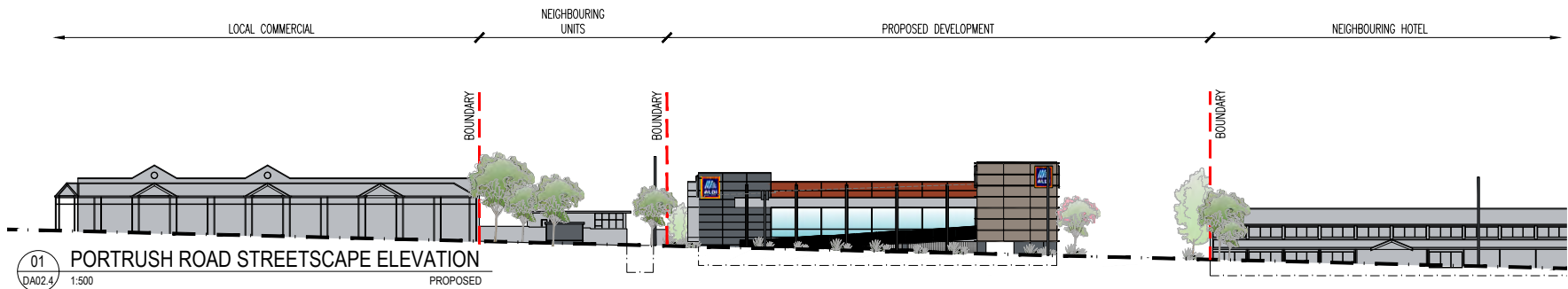
STATUS
DA

REV
A



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ALDI STORES



PROJECT
ALDI GLENUNGA
No. 499 PORTRUSH ROAD
GLENUNGA S.A. 5064

DRAWING
STREETSCAPE ELEVATION

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admin@nielsenarchitects.com.au
www.nielsenarchitects.com.au

SCALE AS SHOWN DATE NOV 2019 DRAWN LT CHECKED TB

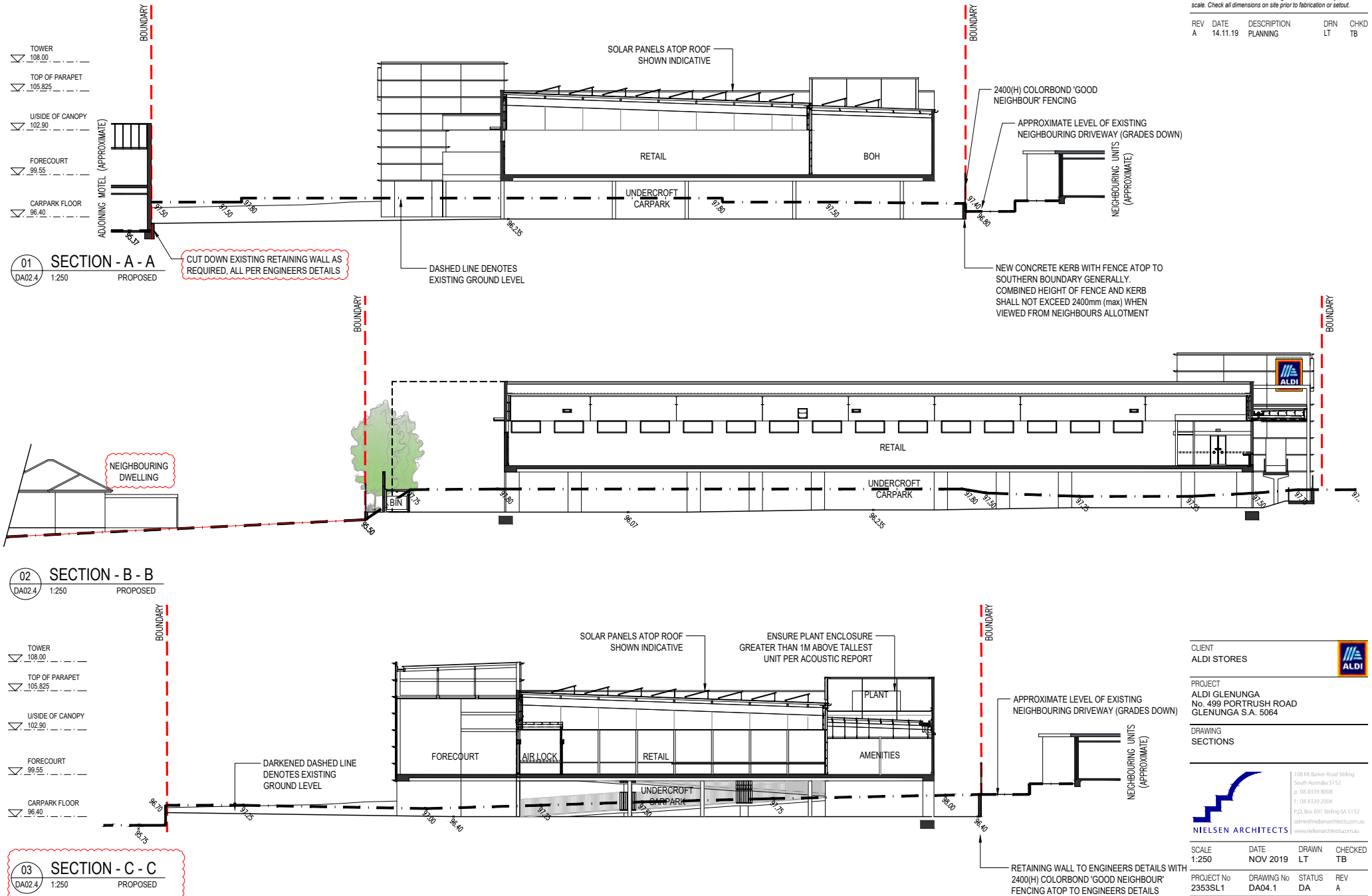
PROJECT No 2353SL1 DRAWING No DA03.3 STATUS DA REV A

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ALDI STORES



PROJECT
ALDI GLENUNGA
No. 499 PORTRUSH ROAD
GLENUNGA S.A. 5064

DRAWING
SECTIONS



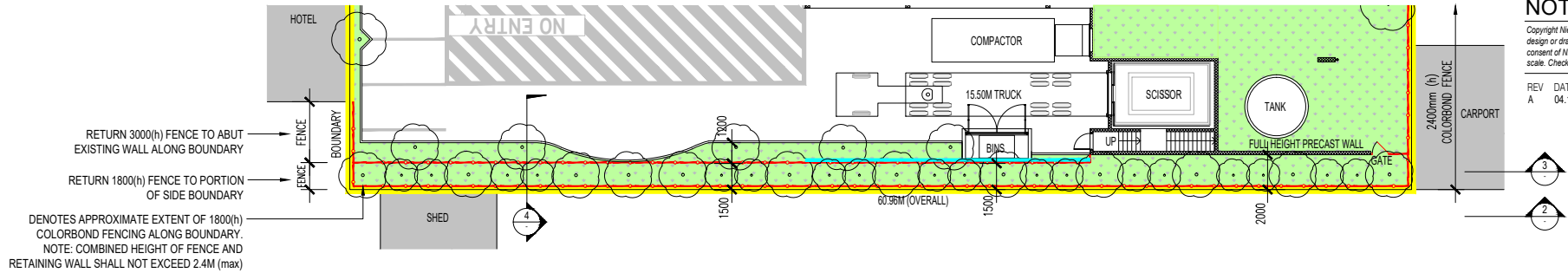
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PROJECT No	DRAWING No	STATUS	REV
2353SL1	DA04.1	DA	A

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REV	DATE	DESCRIPTION	DRN	CHKD
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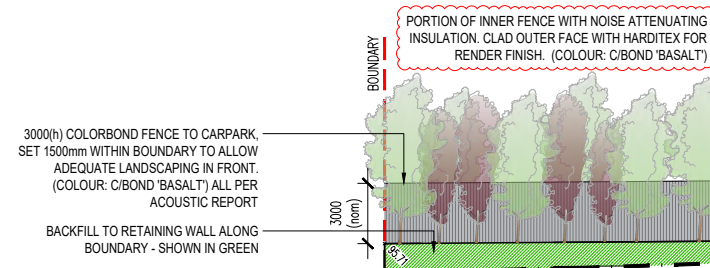
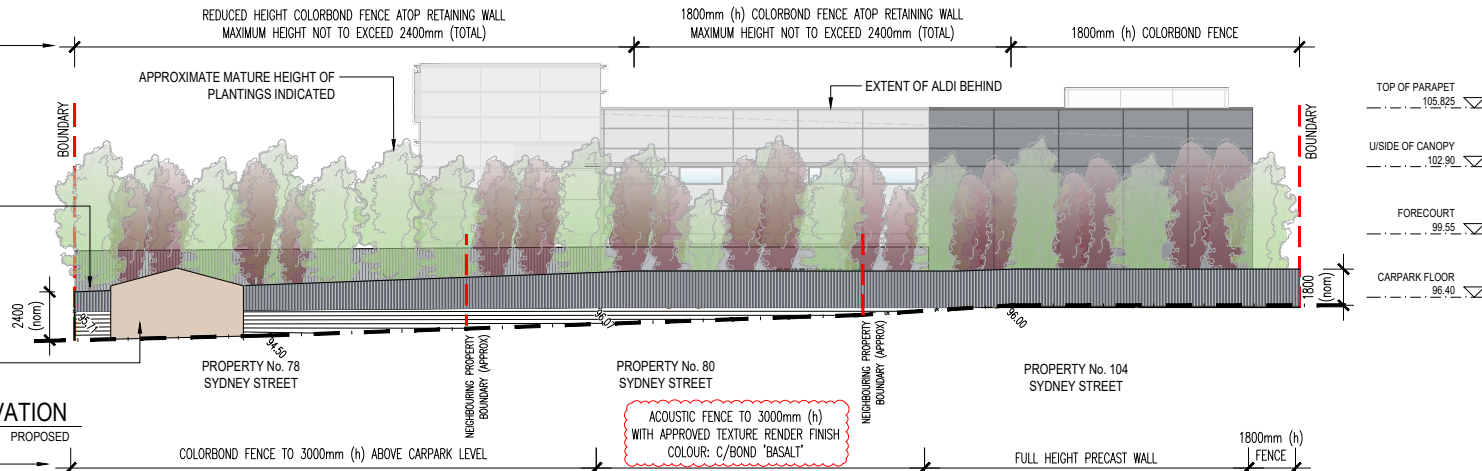


01 PARTIAL SITE PLAN
1:250 PROPOSED

DENOTES APPROXIMATE EXTENT OF 1800(h) COLORBOND FENCING ALONG BOUNDARY. (COLOUR: C/BOND 'BASALT') NOTE: COMBINED HEIGHT OF FENCE AND RETAINING WALL SHALL NOT EXCEED 2400mm (max)

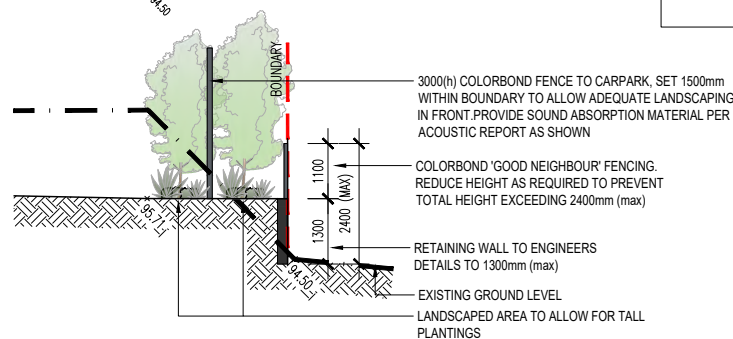
02 WESTERN FENCE ELEVATION
1:250 PROPOSED

STRUCTURE SET 1500mm FROM BOUNDARY



03 ACOUSTIC FENCE ELEVATION
1:250 PROPOSED

04 SECTION THRU REAR BOUNDARY
1:100 PROPOSED



CLIENT
ALDI STORES



PROJECT
ALDI GLENUNGA
No. 499 PORTRUSH ROAD
GLENUNGA S.A. 5064

DRAWING
WESTERN FENCE DETAIL
PLAN, ELEVATION & SECTION

NIELSEN ARCHITECTS
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SCALE
AS SHOWN

DATE
NOV 2019

PROJECT No
2353SL1

DRAWING No
DA04.2

STATUS
DA

REV
A

DRAWN
LT

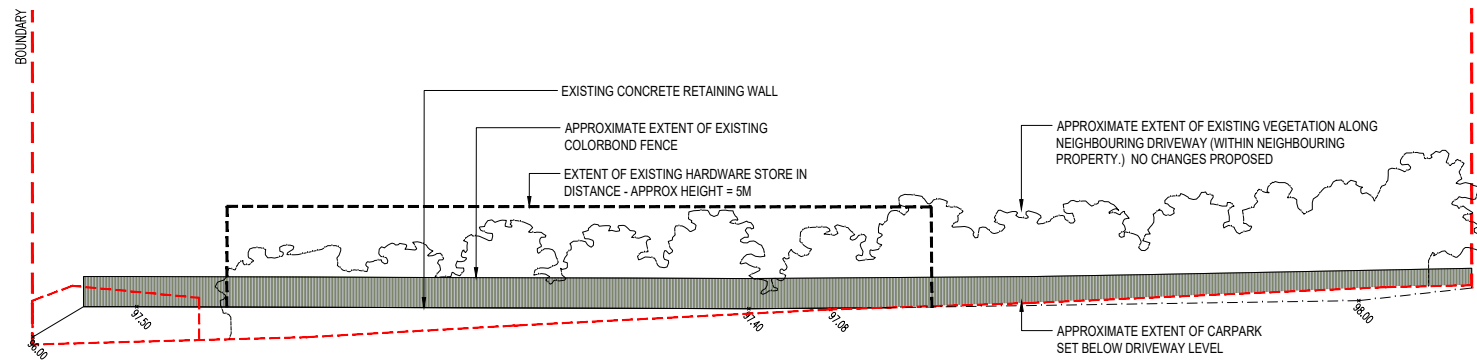
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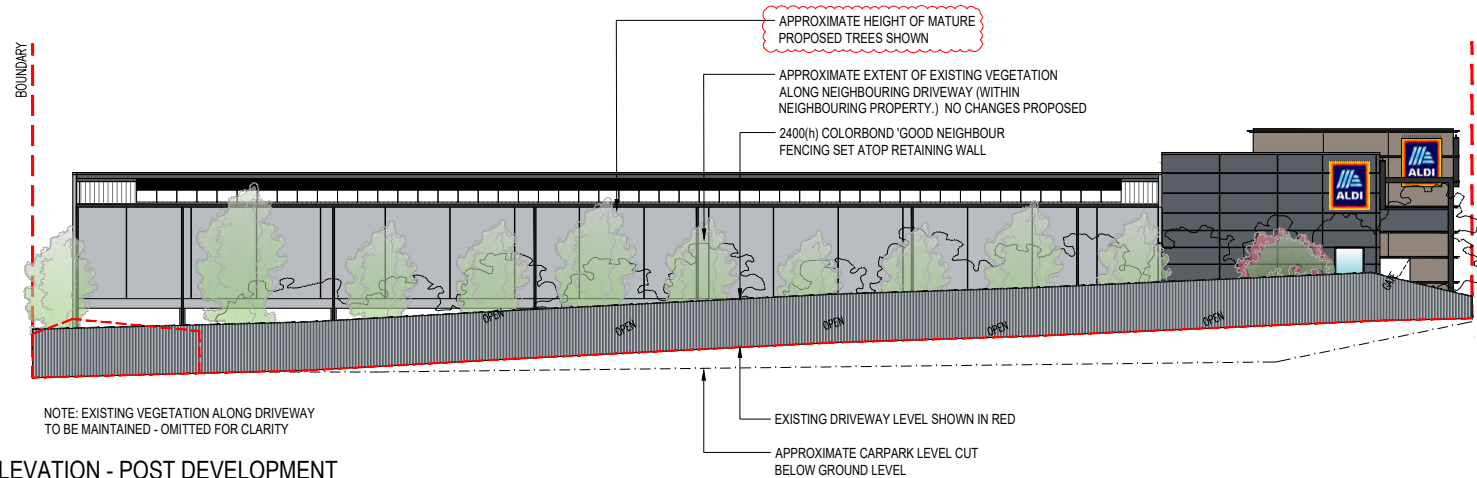
REV	DATE	DESCRIPTION	DRN	CHKD
A	27.09.19	SHEET ADDED	LT	TB
B	14.11.19	PLANNING	LT	TB



01 SOUTHERN ELEVATION - PRE DEVELOPMENT

1:250

EXISTING



02 SOUTHERN ELEVATION - POST DEVELOPMENT

1:250

PROPOSED

CLIENT
ALDI STORES



PROJECT
ALDI GLENUNGA
No. 499 PORTRUSH ROAD
GLENUNGA S.A. 5064

DRAWING
SOUTHERN FENCE
ELEVATIONS

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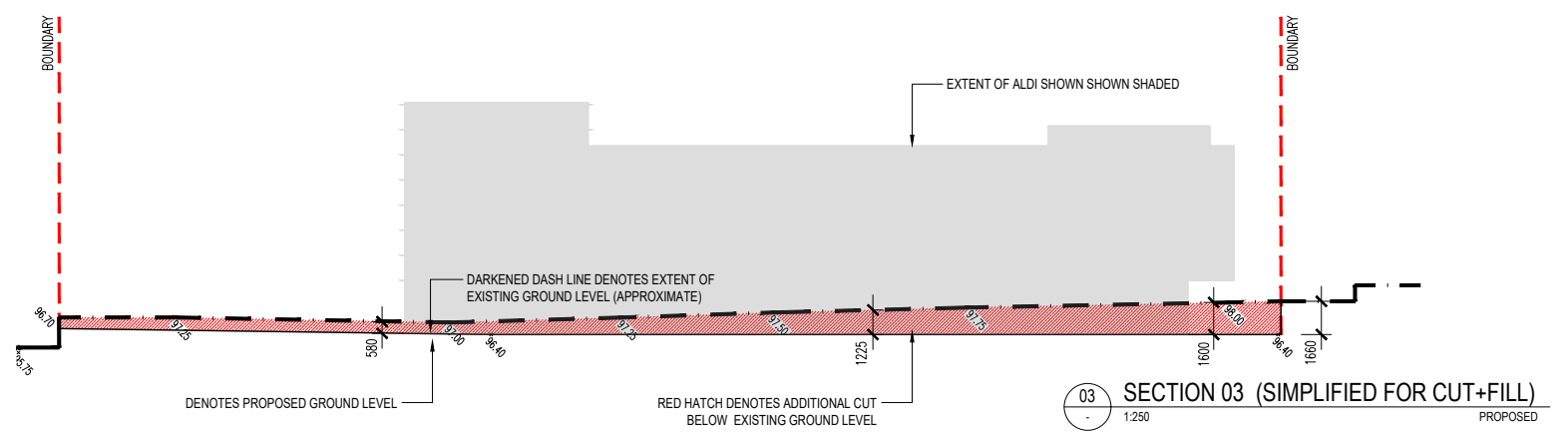
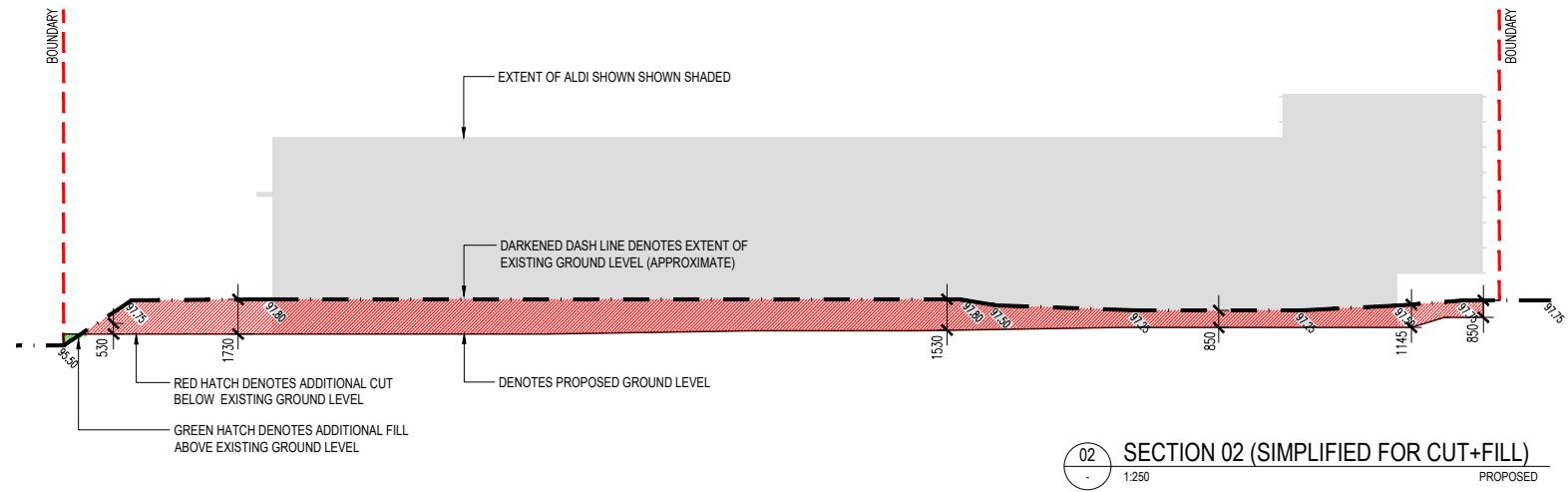
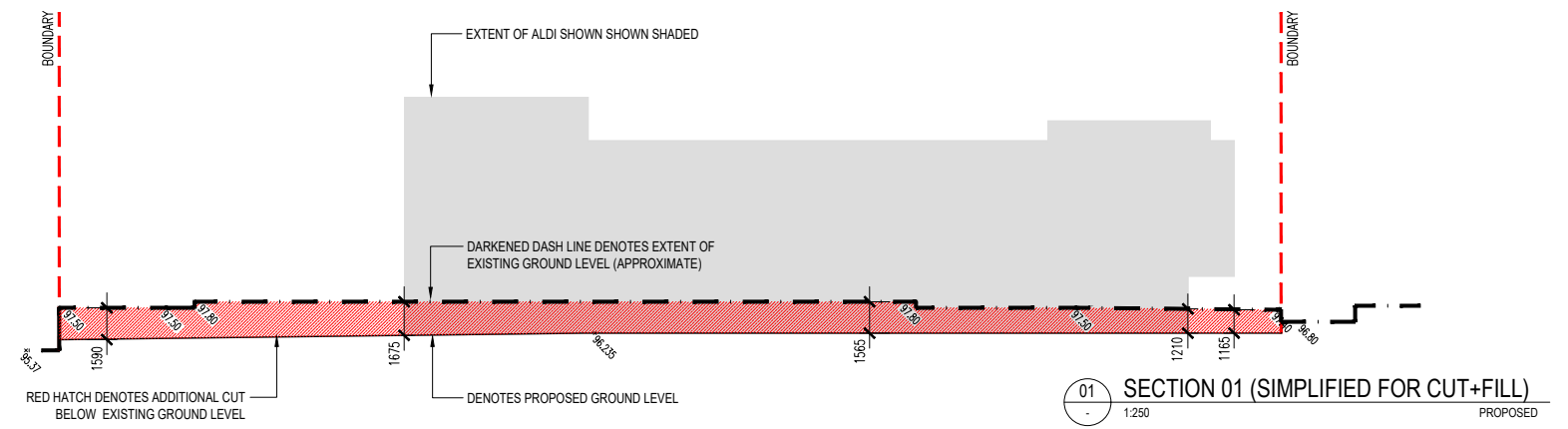
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PROJECT No	DRAWING No	STATUS	REV
2353SL1	DA04.3	DA	8

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ALDI STORES



PROJECT
ALDI GLENUNGA
No. 499 PORTURUSH ROAD
GLENUNGA S.A. 5064

DRAWING
SIMPLIFIED CUT & FILL
SECTIONS



SCALE
1:250, 1:1000
DATE
NOV 2019
DRAWN
LT
CHECKED
TB

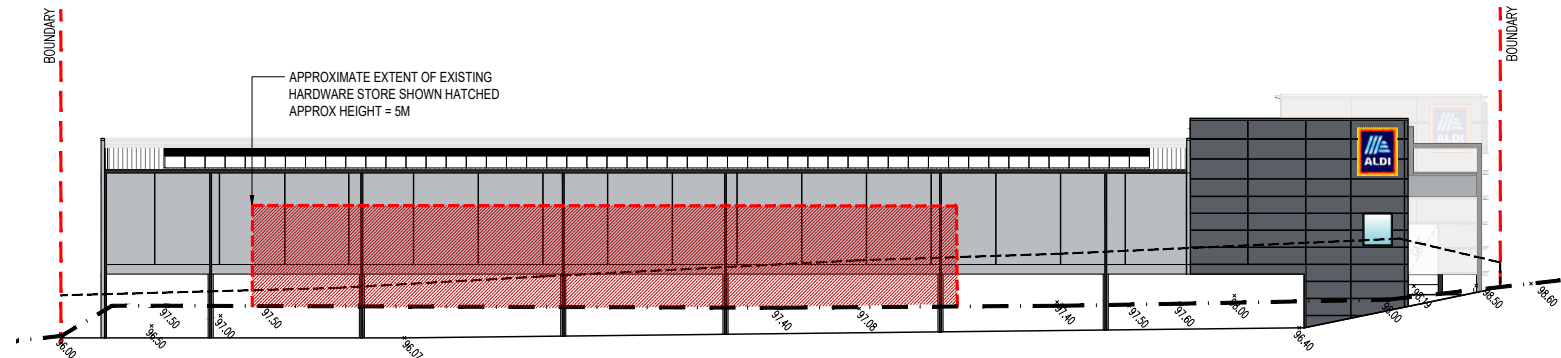
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2353SL1
DRAWING No
DA04.4
STATUS
DA
REV
A

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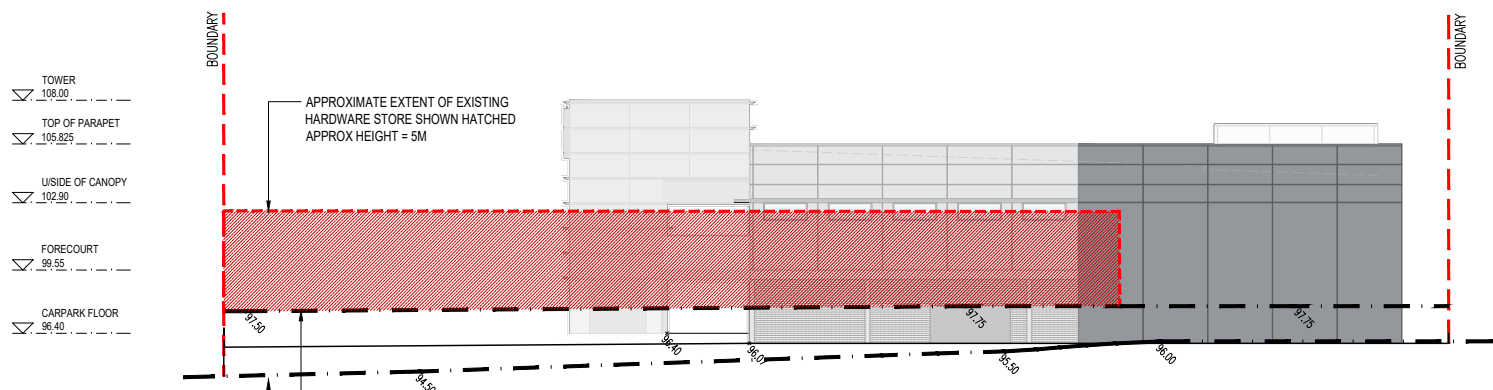
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A	14.11.19	SHEET ADDED	LT	TB



01 SOUTHERN ELEVATION WITH EXISTING OVERLAY
1:250 PROPOSED



02 WESTERN ELEVATION WITH EXISTING OVERLAY
1:250 PROPOSED



03 KEY PLAN
1:1000

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ALDI STORES



PROJECT
ALDI GLENUNGA
No. 499 PORTRUSH ROAD
GLENUNGA S.A. 5064

DRAWING
EXISTING OVERLAY
ELEVATIONS

1108 Mt Barker Road Stirling
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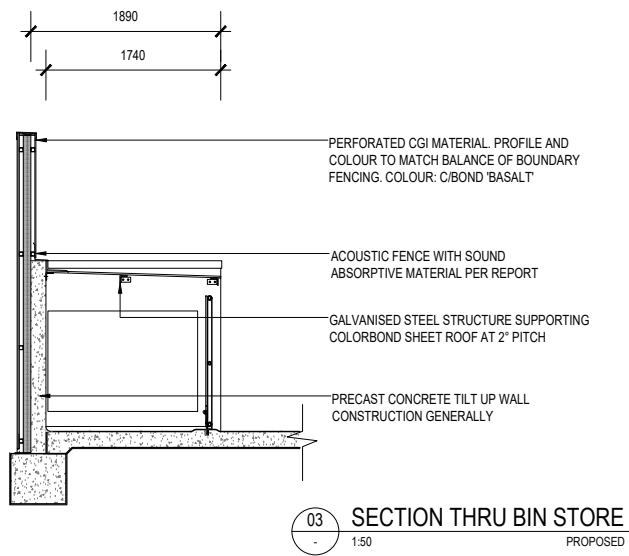
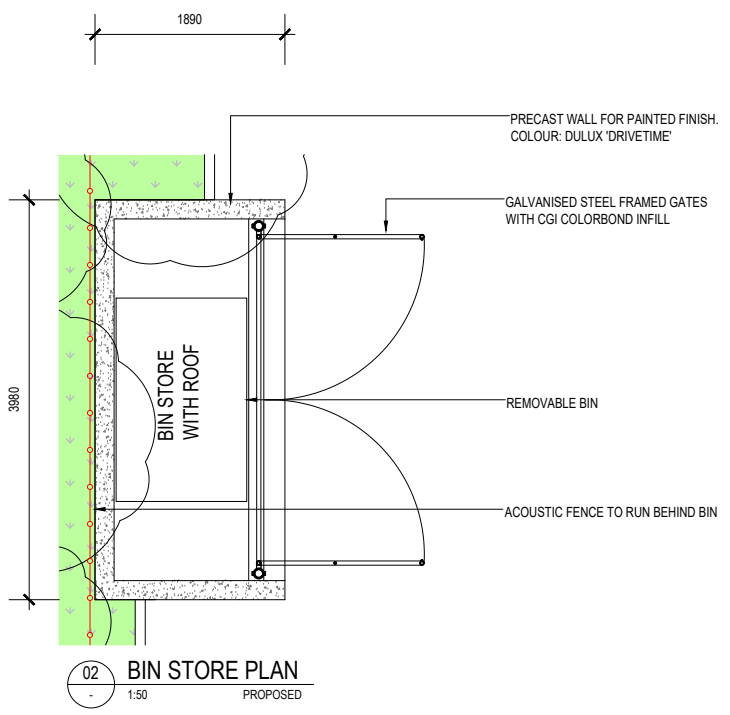
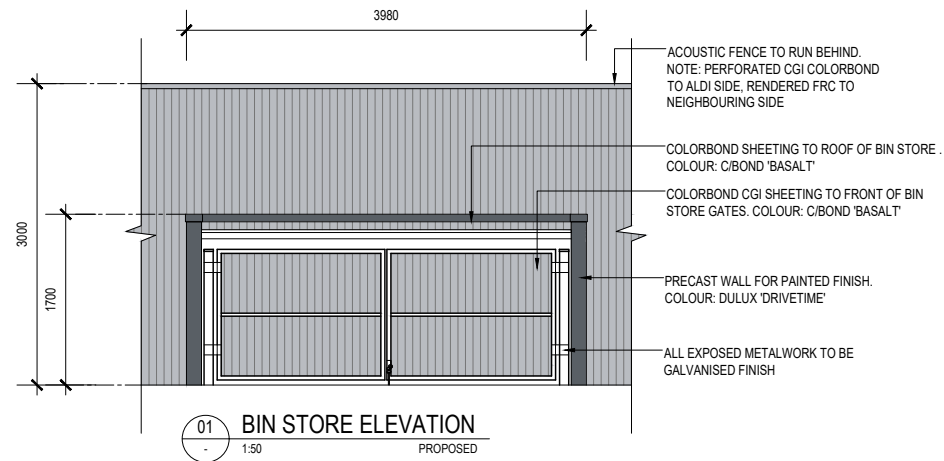
SCALE	DATE	DRAWN	CHECKED
1:250, 1:1000	NOV 2019	LT	TB
PROJECT No	DRAWING No	STATUS	REV
2353SL1	DA04.5	DA	A

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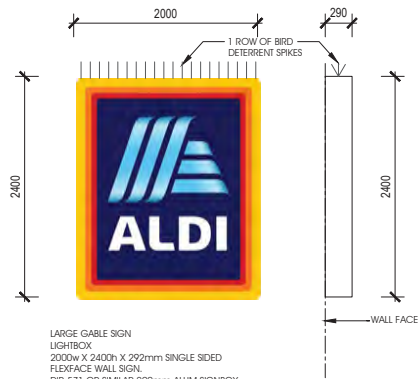


PROJECT
ALDI GLENUNGA
No. 499 PORTRUISH ROAD
GLENUNGA S.A. 5064

DRAWING
BIN STORE DETAILS

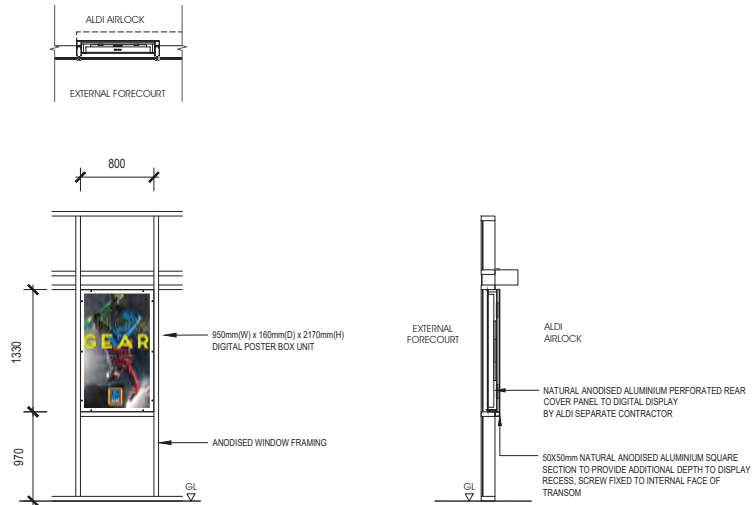


SCALE	DATE	DRAWN	CHECKED
1:50	NOV 2019	LT	TB
PROJECT No	DRAWING No	STATUS	REV
2353SL1	DA04.6	DA	A



LARGE GABLE SIGN
LIGHTBOX
2000w X 2400h X 292mm SINGLE SIDED
FLEXFACE WALL SIGN.
DIR-571 OR SIMILAR 200mm ALUM SIGNBOX
EXTRUSION WITH 1.6mm ALUMINIUM
CLADDING TO BACK.
PAINT FINISH EXTERNAL: DULUX OLD PEWTER
50243 70% GLOSS IN 2 PACK POLYURETHANE
OR POWDERCOAT FP964
PAINT FINISH INTERNAL: FLAT WHITE
INTERNAL ILLUMINATION: DAYLIGHT
FLUORESCENT TUBES SPACED EVENLY
LIGHT OUTPUT: CALVIN - 40,000
LUMENS 16,500

01 LARGE GABLE SIGN
DA05.1 1:50 SIGNS 01, 02, 03, 04 & 05



02 DIGITAL SIGN BOX
DA05.1 1:50 SIGN 06

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ALDI STORES



PROJECT
ALDI GLENUNGA
No. 499 PORTRUISH ROAD
GLENUNGA S.A. 5064

DRAWING
SIGNAGE DETAILS

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1:50	SEP 2019	LT	TB
PROJECT No	DRAWING No	STATUS	REV
2353SL1	DA05.2	DA	.



01 3D RENDERED IMAGE
NTS IMAGE 01

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CLIENT
ALDI STORES



PROJECT
ALDI GLENUNGA
No. 499 PORTRUISH ROAD
GLENUNGA S.A. 5064

DRAWING
3D RENDERED IMAGE
IMAGE 01

 108 Mt Barker Road Stirling
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NTS	SEP 2019	LT	TB
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2353SL1	DA06.1	DA	.



01 3D RENDERED IMAGE
NTS IMAGE 02

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.

CLIENT
ALDI STORES



PROJECT
ALDI GLENUNGA
No. 499 PORTRUISH ROAD
GLENUNGA S.A. 5064

DRAWING
3D RENDERED IMAGE
IMAGE 02

 108 Mt Barker Road Stirling
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f: 08 8339 2004
P.O. Box 691 Stirling SA 5152
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SCALE	DATE	DRAWN	CHECKED
NTS	SEP 2019	LT	TB
PROJECT No	DRAWING No	STATUS	REV
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01 3D RENDERED IMAGE
NTS IMAGE 03

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REV	DATE	DESCRIPTION	DRN	CHKD
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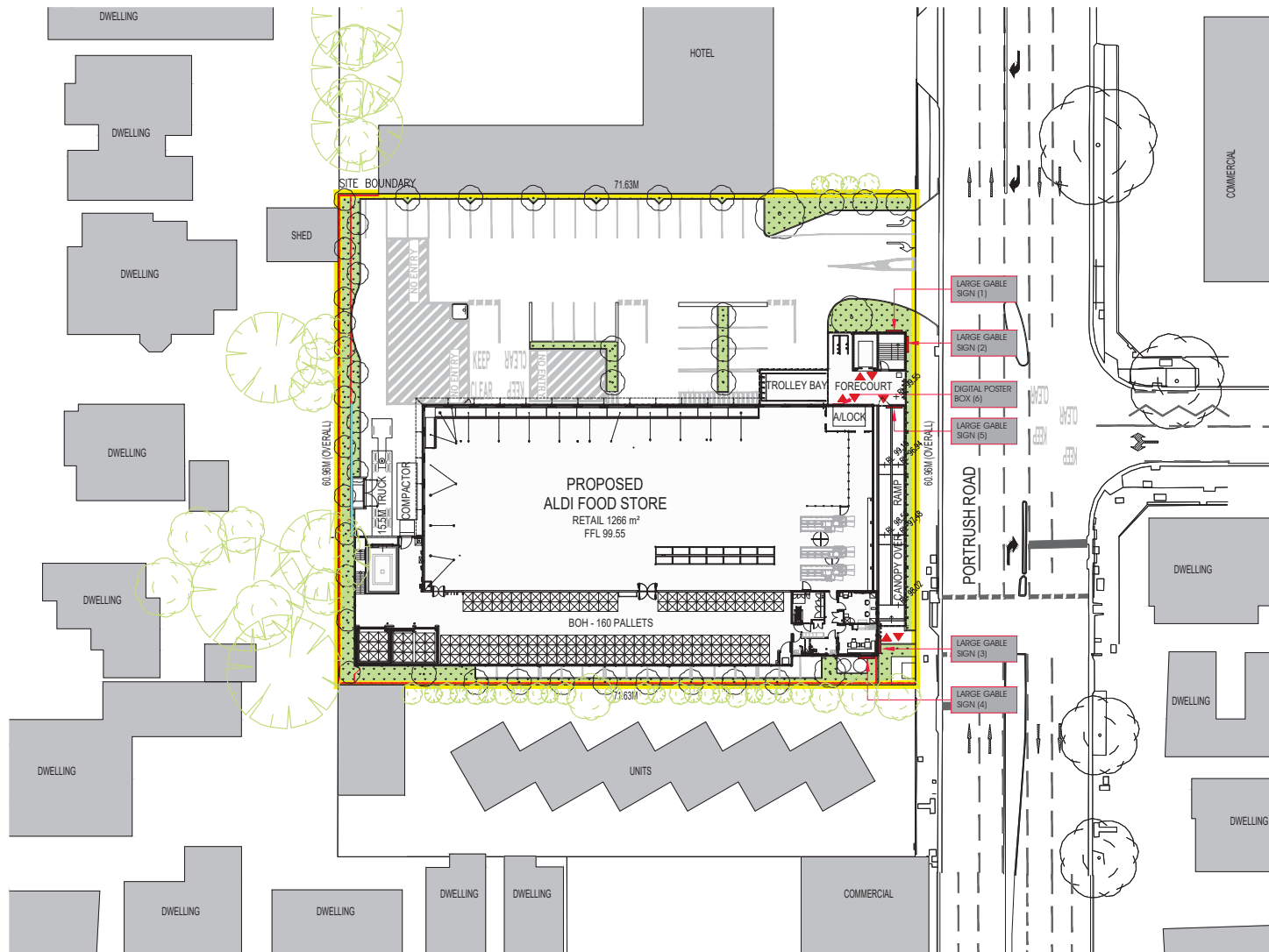
CLIENT
ALDI STORES

PROJECT
ALDI GLENUNGA
No. 499 PORTRUSH ROAD
GLENUNGA S.A. 5064

DRAWING
3D RENDERED IMAGE
IMAGE 03

 108 Mt Barker Road Stirling
South Australia 5152
p: 08 8339 8008
f: 08 8339 2004
P.O. Box 691 Stirling SA 5152
admin@nielsenarchitects.com.au
www.nielsenarchitects.com.au

SCALE	DATE	DRAWN	CHECKED
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PROJECT No	DRAWING No	STATUS	REV
2353SL1	DA06.3	DA	.



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REV	DATE	DESCRIPTION	DRN	CHKD
.



01 SIGNAGE PLAN

1:500

PROPOSED

CLIENT
ALDI STORES



PROJECT
ALDI GLENUNGA
No. 499 PORTRUSH ROAD
GLENUNGA S.A. 5064

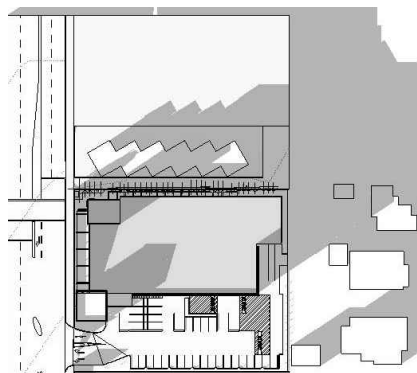
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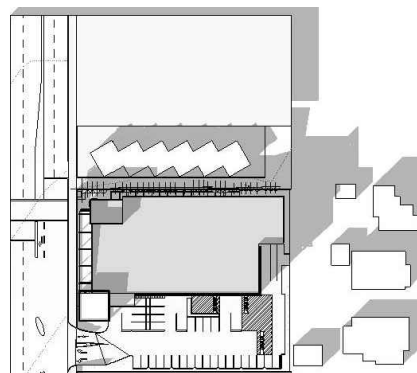
1108 Mt Barker Road Stirling
South Australia 5152
p: 08 8339 8008
f: 08 8339 2004
info@nielsenarchitects.com.au
www.nielsenarchitects.com.au

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1:500	SEP 2019	LT	TB

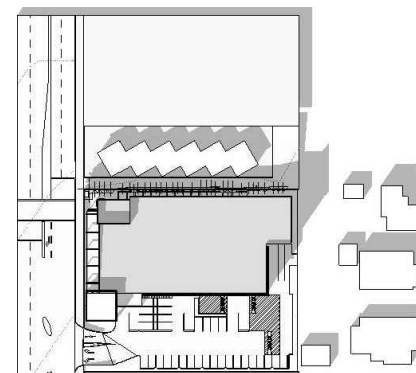
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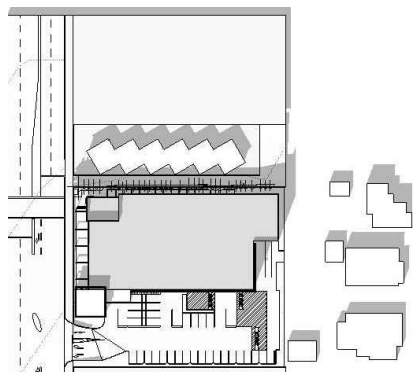
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NTS PROPOSED



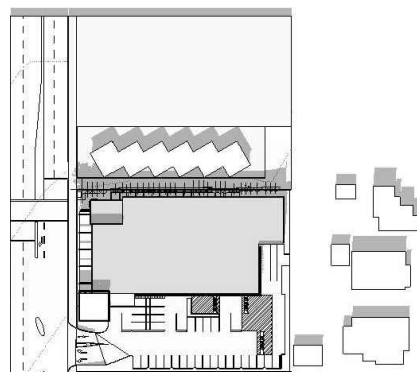
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NTS PROPOSED



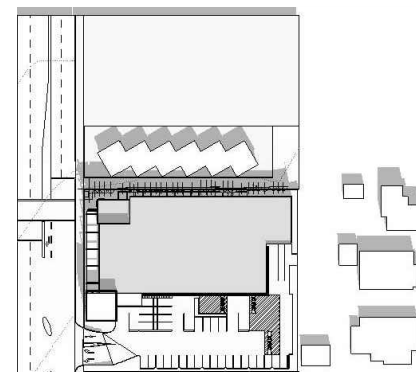
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NTS PROPOSED



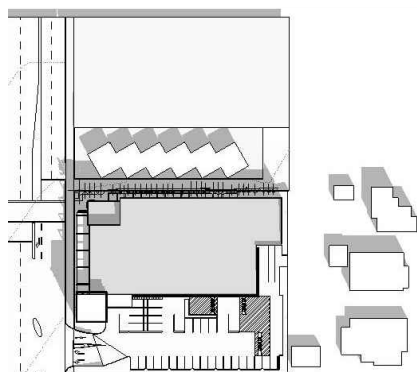
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NTS PROPOSED



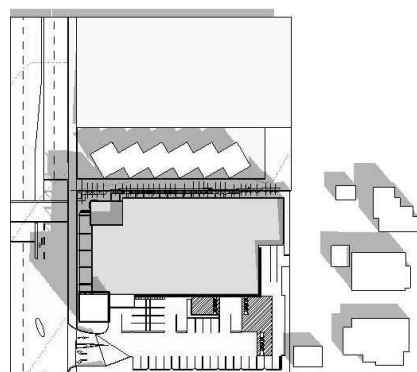
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NTS PROPOSED



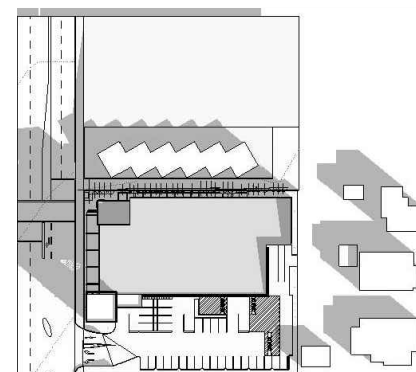
06 SHADOW ANALYSIS | 1PM JUNE 21
NTS PROPOSED



07 SHADOW ANALYSIS | 2PM JUNE 21
NTS PROPOSED



08 SHADOW ANALYSIS | 3PM JUNE 21
NTS PROPOSED



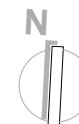
09 SHADOW ANALYSIS | 4PM JUNE 21
NTS PROPOSED

NOTES

ORIGINAL A3

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REV	DATE	DESCRIPTION	DRN	CHKD
A	14.11.19	SHEET ADDED	LT	TB



CLIENT
ALDI STORES



PROJECT
ALDI GLENUNGA
No. 499 PORTRUSH ROAD
GLENUNGA S.A. 5064

DRAWING
SHADOW DIAGRAMS
PROPOSED CONDITIONS

NIELSEN ARCHITECTS
108 Mt Barker Road Stirling
South Australia 5152
p: 08 8339 8008
f: 08 8339 2004
P.O. Box 691 Stirling SA 5152
admin@nielsenarchitects.com.au
www.nielsenarchitects.com.au

SCALE
1:500

DATE
NOV 2019

DRAWN
LT

CHECKED
TB

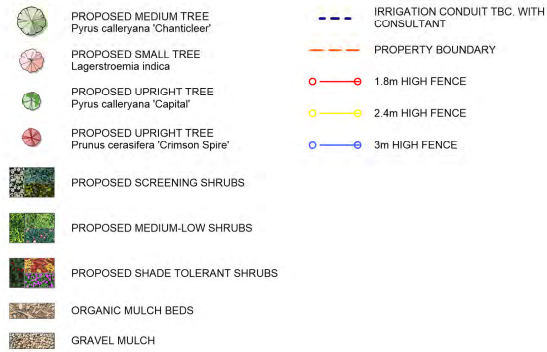
PROJECT No
2353SL1

DRAWING No
SA01.3

STATUS
DA

REV
A

LEGEND



PLANT PALETTE

TREES



Lagerstroemia indica
Crape Myrtle
Height: 7m
Width: 5m



Pyrus calleryana 'Capital'
Capital Callery Pear
Height: 8m
Width: 3m



Pyrus calleryana 'Chanticleer'
Chanticleer Callery Pear
Height: 11m
Width: 6m



Prunus cerasifera 'Crimson Spire'
Crimson Spire Plum
Height: 7m
Width: 2m

SHRUBS & GROUNDCOVERS



Correa backhouseana 'Dusky Belts'
Australian Fuschia
Height: 0.8m
Width: 1.5m



Dianella caerulea 'Little Jess'
Little Jess
Height: 0.4m
Width: 0.4m

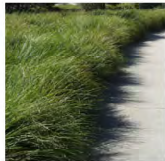


Dianella tasmanica 'Tas Red'
Tas Red Flax Lily
Height: 0.6m
Width: 0.5m



Dietes grandiflora
Butterfly Iris
Height: 0.7m
Width: 0.7m

SCREENING



Lomandra longifolia 'Tanika'
Tanika Mat Rush
Height: 0.6m
Width: 0.6m



Westringia fruticosa 'Grey Box'
Grey Box
Height: 0.4m
Width: 0.4m



Raphiolepis x delacourii
Cosmic White
Height: 1.6m
Width: 1.5m



Viburnum tinus
Laurustinus
Height: 2m
Width: 2m

SHADE TOLERANT



Clivia miniata
Natal Lily
Height: 0.6m
Width: 0.8m



Dianella tasmanica
Tasman Flax Lily
Height: 1.2m
Width: 1m



Liriope muscari
Blue Lily Turf
Height: 0.6m
Width: 0.3m



Rhagodia candolleana ssp. *candolleana*
Seaberry Saltbush
Height: 0.5m
Width: 1.5m

SCREENING SHRUBS & TREES
ON BOTH SIDES OF FENCE



OUTER SPACE

258a north street Adelaide, SA 5000
p 08 8233 3228
e info@outerspaceadelaide.com.au
w www.outerspaceadelaide.com.au

PROJECT PROPOSED ALDI FOODSTORE - GLENUNGA

CLIENT ALDI STORES

DRAWING LANDSCAPE CONCEPT PLAN - DEVELOPMENT APPROVAL

DRAWN BY AMIAI

APPROVED BY GH

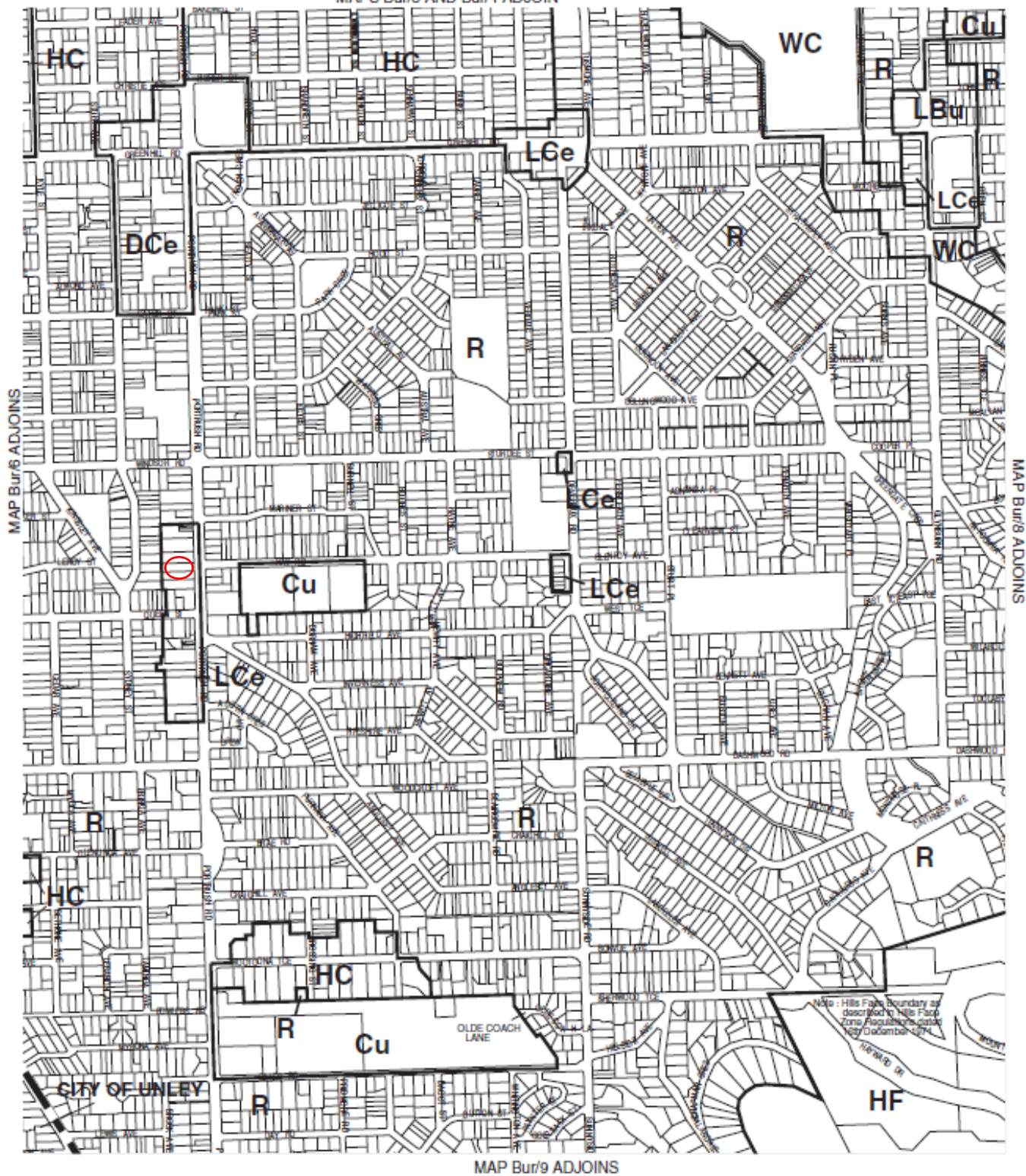
DRAWING NO. OS680_CP01D

ISSUE DATE 14/11/19



0 2 4 6 8 10 20
SCALE 1:200 @ A1

PRELIMINARY
NOT FOR CONSTRUCTION



NOTE : For Policy Areas: See MAP Bur/16

Cu	Community
DCE	District Centre
HC	Historic Conservation
HF	Hills Face
LBU	Local Business
LGe	Local Centre
R	Residential
WC	Watercourse

	Zone Boundary
	Development Plan Boundary



BURNSIDE (CITY) ZONES

MAP Bur/7

Consolidated - 19 December 2017



Current view from rear yard adjacent Mitre 10 – fence line



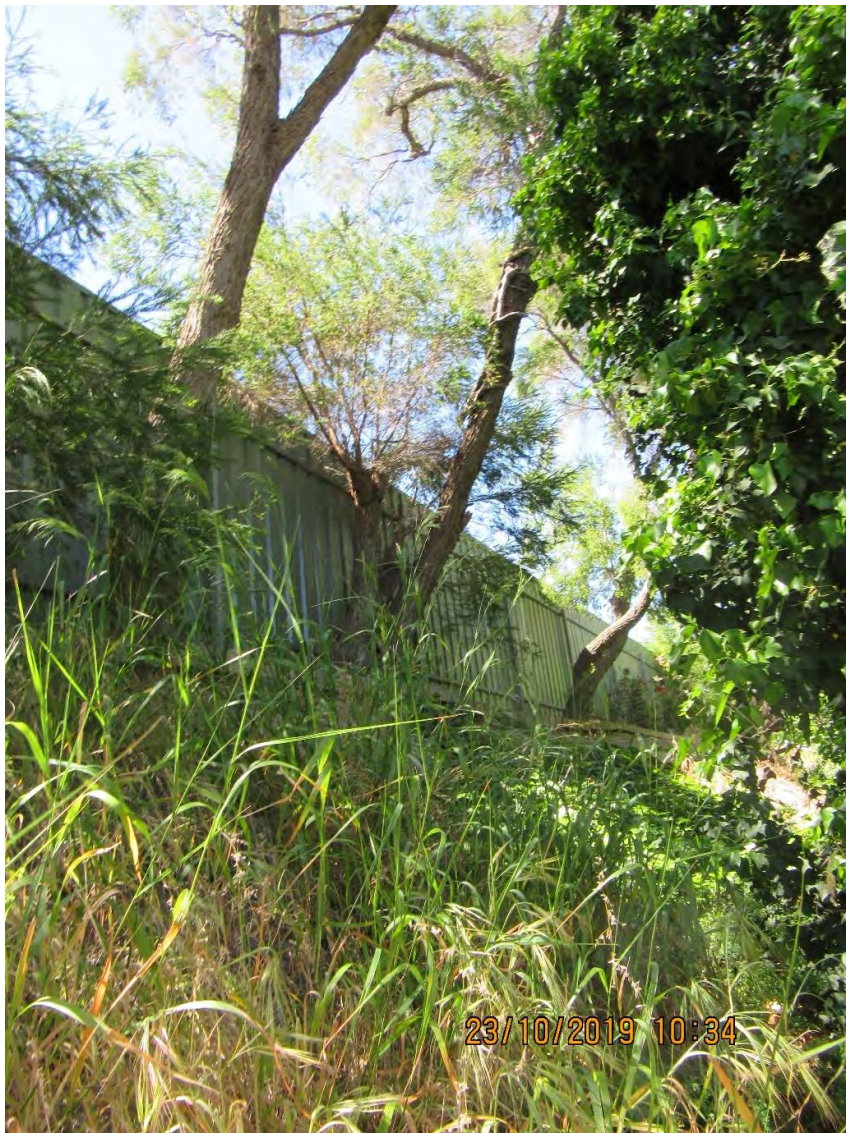
Current view looking east from rear yard towards Mitre 10



Trellis marks boundary with rear yard and Mitre 10 looking east from adjacent property



Close up of actual boundary wall between Mitre 10 and neighbour



Standing on the boundary wall looking south (boundary between Mitre 10 and neighbour)



Existing retaining wall showing drop in levels between Mitre 10 and Motel

DEVELOPMENT APPLICATION FORM

PLEASE USE BLOCK LETTERS

COUNCIL: CITY OF BURNSIDE

APPLICANT: NIELSEN ARCHITECTS

Postal Address: 108 MOUNT BARKER

ROAD, STIRLING.

Owner: TBA

Postal Address: _____

BUILDER: TBC.

Postal Address: _____

_____ Licence No: _____

CONTACT PERSON FOR FURTHER INFORMATION

Name: EXISTICS PLANNING C/-

Telephone: 0434 484 046 [work] 08 723 10286 [Ah]

Fax: _____ [work] _____ [Ah]

EXISTING USE: BULKY GOODS OUTLET

(MITRE 10).

DESCRIPTION OF PROPOSED DEVELOPMENT:

SUPERMARKET, EARTHWORKS, RETAINING, CARPARK,

LOCATION OF PROPOSED DEVELOPMENT:

House No: 499 Lot No: _____ Street: PORTER RD

Section No [full/part] _____ Hundred: _____

Section No [full/part] _____ Hundred: REFER

ATTACHED

LAND DIVISION: N/A.

Site Area [m²] _____ Reserve Area [m²] _____

Number of additional allotments [excluding road and reserve]: _____

BUILDING RULES CLASSIFICATION SOUGHT: N/A.

If Class 5,6,7 or 9 classification is sought, state the proposed number of employees: _____

If Class 9a classification is sought, state the number of persons for whom accommodation is provided: _____

If Class 9b classification is sought, state the proposed number of occupants of the various spaces at the premises: _____

DOES EITHER SCHEDULE 21 OR 22 OF THE DEVELOPMENT REGULATIONS 2008 APPLY? YES ☐ NO ☒

HAS THE CONSTRUCTION INDUSTRY TRAINING FUND ACT 2008 LEVY BEEN PAID? YES ☐ NO ☒

DEVELOPMENT COST [do not include any fit-out costs]: \$ 7.4M

I acknowledge that copies of this application and supporting documentation may be provided to interested persons in accordance with the Development Regulations 2008.

SIGNATURE: _____

FOR OFFICE USE

Development No: _____

Previous Development No: _____

Assessment No: _____

☐ Complying

☐ Non Complying

☐ Notification Cat 2

☐ Notification Cat 3

☐ Referrals/Concurrences

☐ DA Commission

Application forwarded to DA

Commission/Council on

Decision: _____

Type: _____

Date: / /

DEVELOPMENT REGULATIONS 1993

Form of Declaration
(Schedule 5 clause 2A)

To: STATE COMMISSION ASSESSMENT PANEL

From: MELOEN ARCHITECTS

Date of Application: 16/9/19

Location of Proposed Development: 499 PORTRUOH RD, GLENONGA

House No: 499 Lot No: Street: PORTRUOH RD Town/Suburb: GLENONGA

Section No (full/part): Hundred: REFER ATTACHED CT'S

Volume: Folio:

Nature of Proposed Development:

SUPERMARKET, EARTHWORKS/RETAINING, CAR PARK,
SIGNAGE, FENCING, SOLAR PANELS, LANDSCAPE

I, MELOEN ARCHITECTSbeing the applicant/
~~a person acting on behalf of the applicant~~ (delete the inapplicable statement) for
the development described above declare that the proposed development will
involve the construction of a building which would, if constructed in accordance
with the plans submitted, not be contrary to the regulations prescribed for the
purposes of section 86 of the *Electricity Act 1996*. I make this declaration under
clause 2A(1) of Schedule 5 of the *Development Regulations 1993*.

Date: 16/09/19

Signed: 

Note 1

This declaration is only relevant to those development applications seeking
authorisation for a form of development that involves the construction of a building
(there is a definition of 'building' contained in section 4(1) of the *Development Act*
1993), other than where the development is limited to –

- a) an internal alteration of a building; or
- b) an alteration to the walls of a building but not so as to alter the shape of the building.

Note 2

The requirements of section 86 of the *Electricity Act 1996* do not apply in relation to:

- a) a fence that is less than 2.0 m in height; or
- b) a service line installed specifically to supply electricity to the building or structure by the operator of the transmission or distribution network from which the electricity is being supplied.

Note 3

Section 86 of the *Electricity Act 1996* refers to the erection of buildings in proximity to powerlines. The regulations under this Act prescribe minimum safe clearance distances that must be complied with.

Note 4

The majority of applications will not have any powerline issues, as normal residential setbacks often cause the building to comply with the prescribed powerline clearance distances. Buildings/renovations located far away from powerlines, for example towards the back of properties, will usually also comply.

Particular care needs to be taken where high voltage powerlines exist; where the development:

- is on a major road;
- commercial/industrial in nature; or
- built to the property boundary.

Note 5

Information brochures 'Powerline Clearance Declaration Guide' and 'Building Safely Near Powerlines' have been prepared by the Technical Regulator to assist applicants and other interested persons. Copies of these brochures are available from council and the Office of the Technical Regulator. The brochures and other relevant information can also be found at www.technicalregulator.sa.gov.au

Note 6

In cases where applicants have obtained a written approval from the Technical Regulator to build the development specified above in its current form within the prescribed clearance distances, the applicant is able to sign the form.

PLN/06/0024



Appendix 1. Certificate of Title

Certificate of Title - Volume 5452 Folio 298

Parent Title(s)	CT 4146/404				
Creating Dealing(s)	CONVERTED TITLE				
Title Issued	24/09/1997	Edition	6	Edition Issued	06/07/2016

Estate Type

FEE SIMPLE

Registered Proprietor

IAN STEWART NOMINEES PTY. LTD. (ACN: 007 864 959)
OF CARE MCCORMACK REYNOLDS 55 JERNINGHAM STREET NORTH ADELAIDE SA 5006
90 / 100 SHARE

MARJORIE JANET STEWART
OF CARE MCCORMACK REYNOLDS 55 JERNINGHAM STREET NORTH ADELAIDE SA 5006
10 / 100 SHARE

Description of Land

ALLOTMENT 27 FILED PLAN 16174
IN THE AREA NAMED GLENUNGA
HUNDRED OF ADELAIDE

Easements

NIL

Schedule of Dealings

Dealing Number	Description
8390480	MORTGAGE TO AUSTRALIA & NEW ZEALAND BANKING GROUP LTD.
12793209	MORTGAGE TO AUSTRALIA & NEW ZEALAND BANKING GROUP LTD. (ACN: 005 357 522)

Notations

Dealings Affecting Title	NIL
--------------------------	-----

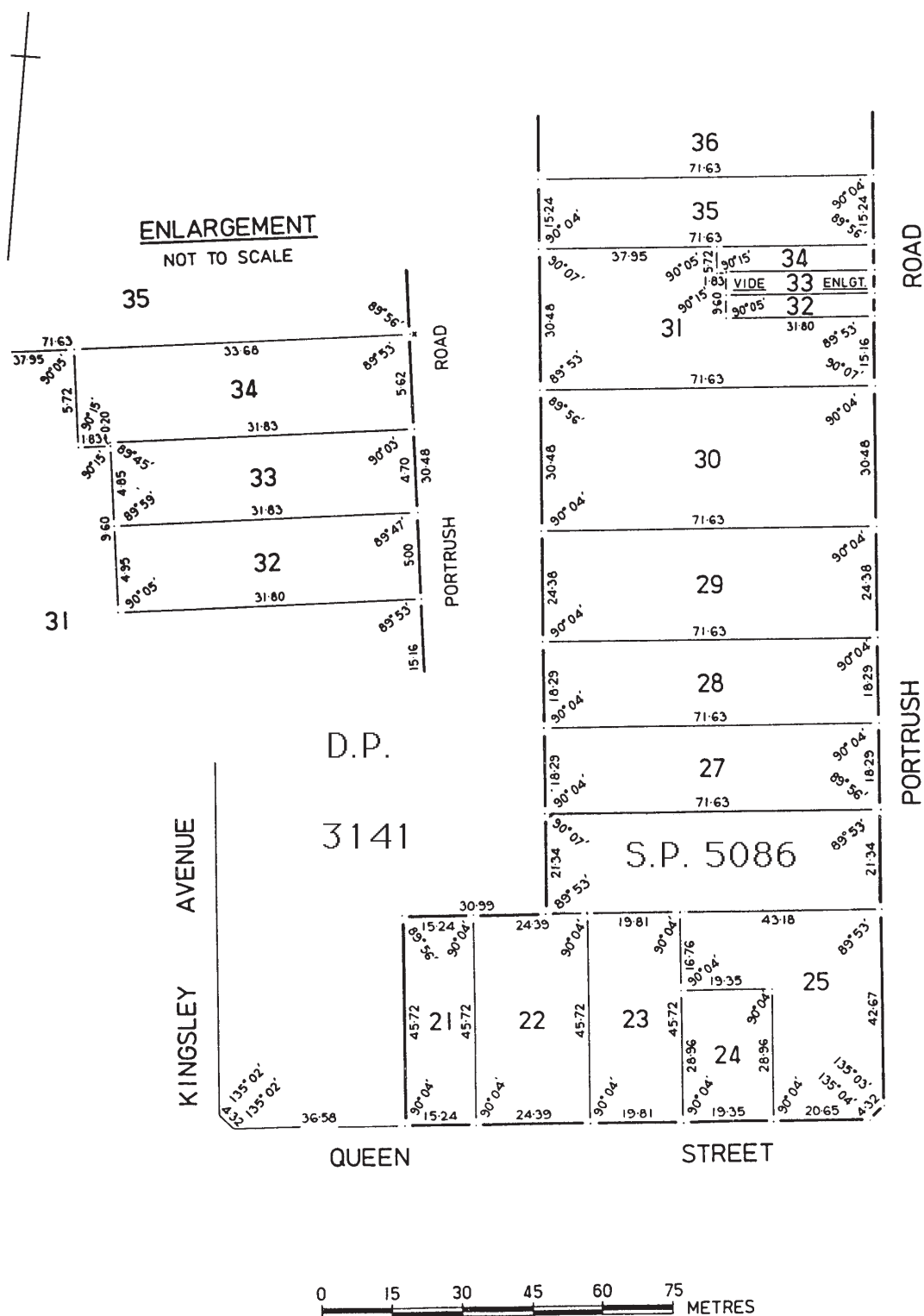
Priority Notices	NIL
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Notations on Plan	NIL
-------------------	-----

Registrar-General's Notes

NEW EDITION CREATED DUE TO EXPIRATION OF LEASE

Administrative Interests	NIL
--------------------------	-----



Certificate of Title - Volume 5452 Folio 286

Parent Title(s)	CT 2033/5				
Creating Dealing(s)	CONVERTED TITLE				
Title Issued	24/09/1997	Edition	6	Edition Issued	06/07/2016

Estate Type

FEE SIMPLE

Registered Proprietor

IAN STEWART NOMINEES PTY. LTD. (ACN: 007 864 959)
OF CARE MCCORMACK REYNOLDS 55 JERNINGHAM STREET NORTH ADELAIDE SA 5006
90 / 100 SHARE

MARJORIE JANET STEWART
OF CARE MCCORMACK REYNOLDS 55 JERNINGHAM STREET NORTH ADELAIDE SA 5006
10 / 100 SHARE

Description of Land

ALLOTMENT 28 FILED PLAN 16174
IN THE AREA NAMED GLENUNGA
HUNDRED OF ADELAIDE

Easements

NIL

Schedule of Dealings

Dealing Number	Description
8390480	MORTGAGE TO AUSTRALIA & NEW ZEALAND BANKING GROUP LTD.
12793209	MORTGAGE TO AUSTRALIA & NEW ZEALAND BANKING GROUP LTD. (ACN: 005 357 522)

Notations

Dealings Affecting Title	NIL
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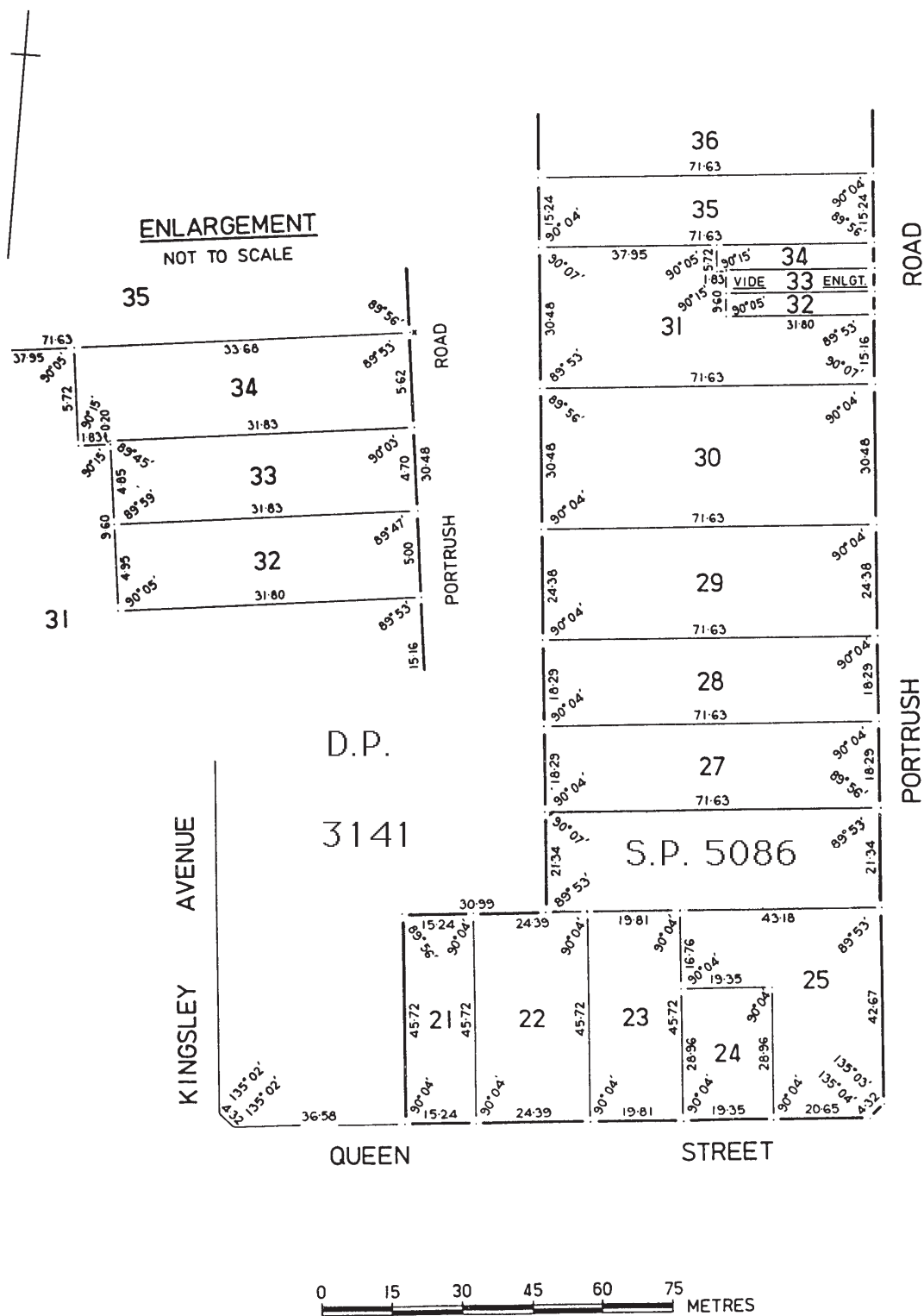
Priority Notices	NIL
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Notations on Plan	NIL
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Registrar-General's Notes

NEW EDITION CREATED DUE TO EXPIRATION OF LEASE

Administrative Interests	NIL
--------------------------	-----



Certificate of Title - Volume 5452 Folio 287

Parent Title(s)	CT 2045/5				
Creating Dealing(s)	CONVERTED TITLE				
Title Issued	24/09/1997	Edition	6	Edition Issued	06/07/2016

Estate Type

FEE SIMPLE

Registered Proprietor

IAN STEWART NOMINEES PTY. LTD. (ACN: 007 864 959)
OF CARE MCCORMACK REYNOLDS 55 JERNINGHAM STREET NORTH ADELAIDE SA 5006
90 / 100 SHARE

MARJORIE JANET STEWART
OF CARE MCCORMACK REYNOLDS 55 JERNINGHAM STREET NORTH ADELAIDE SA 5006
10 / 100 SHARE

Description of Land

ALLOTMENT 29 FILED PLAN 16174
IN THE AREA NAMED GLENUNGA
HUNDRED OF ADELAIDE

Easements

NIL

Schedule of Dealings

Dealing Number	Description
8390480	MORTGAGE TO AUSTRALIA & NEW ZEALAND BANKING GROUP LTD.
12793209	MORTGAGE TO AUSTRALIA & NEW ZEALAND BANKING GROUP LTD. (ACN: 005 357 522)

Notations

Dealings Affecting Title	NIL
--------------------------	-----

Priority Notices	NIL
------------------	-----

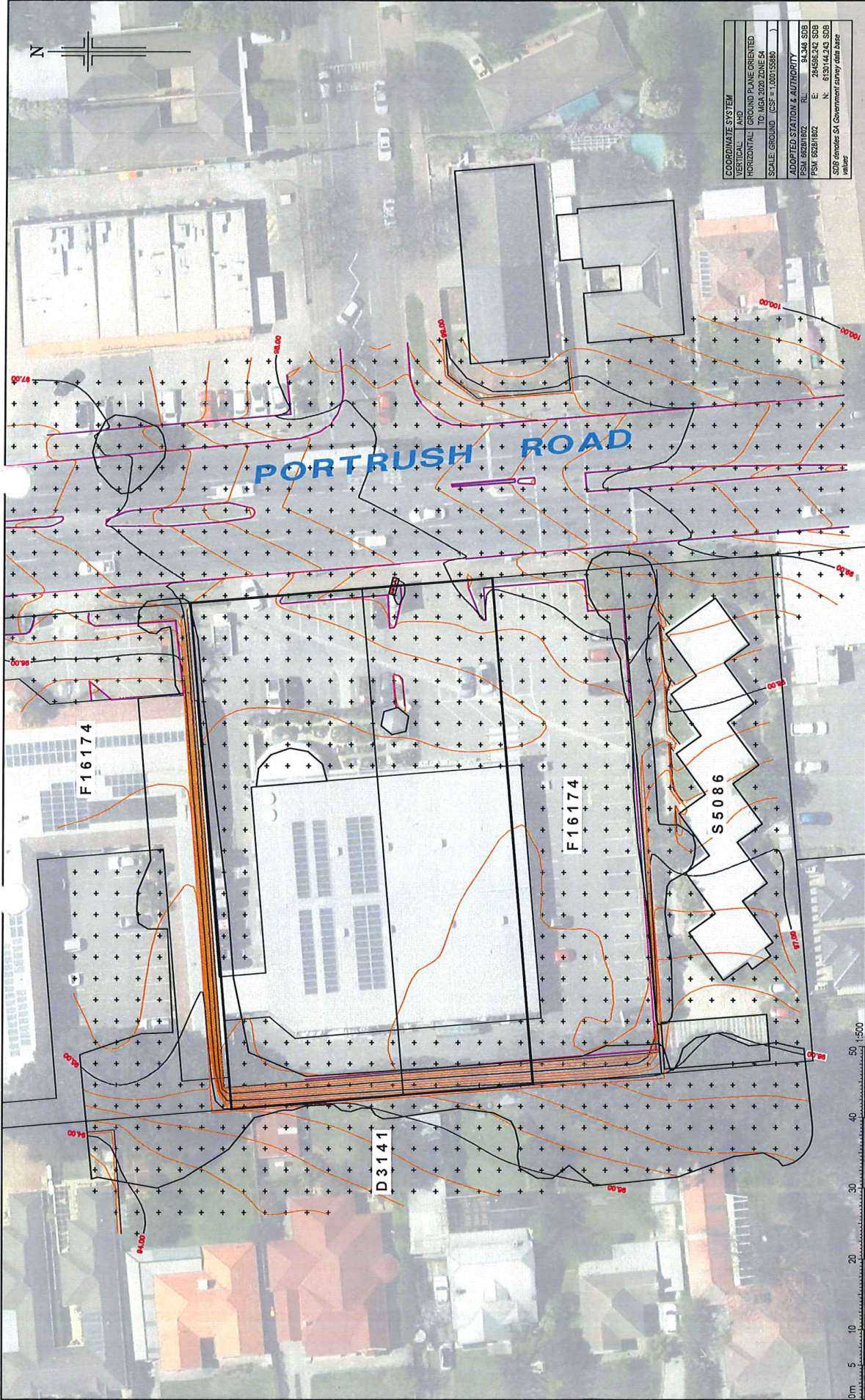
Notations on Plan	NIL
-------------------	-----

Registrar-General's Notes

NEW EDITION CREATED DUE TO EXPIRATION OF LEASE

Administrative Interests	NIL
--------------------------	-----





COORDINATE SYSTEM	
HORIZONTAL:	AND
VERTICAL: GROUND PLANE ORIENTED	
TO MAG 2020 ZONE 54	
SCALE: GROUND CSF = 1.000153860	
ADOPTED STATION & AUTHORITY	
PSM 85261002	REL. 54.348 S2B
PSM 85261002	REL. 24456242 S2B
PSM 85261002	N. 8131444.243 S2B
S2B derives SA Government survey data base values	

DTM SURVEY
499 PORTRUSH ROAD
GLENUNGA

SHEET	DRAWING No.	REVISION
1 OF 1	A047719 DET(0)	0

Alexander Symonds
Surveying
Consultants

Alexander & Symonds Pty Ltd
14/15 The Esplanade, Kent Town,
South Australia 5071
PO Box 1000 Kent Town, SA 5071
ABN 93007 753 988
T (08) 8130 1666
F (08) 8362 0599
E alex@alexanders.com.au
alex@alexanders.com.au

Notes:
Property boundaries shown herein have been
derived from aerial photography and have
not been verified by field survey.

CONTOUR INTERVAL: MIN 0.25m; MAX 1.00m
SURVEY: -
DRAWN: AMP 22.05.2018
CHECKED: BT 22.05.2018

NOTES:
IMAGERY SHOWN HAS BEEN PROVIDED BY AEROMATRIX, FLOWN 24th SEPT 2018
DTM DERIVED FROM AERIAL PHOTOGRAPHY - VERTICAL ACCURACY 150mm AT 1σ

REV	DATE	DESCRIPTION	ADDITIONS, AMENDMENTS AND APPROVALS	CALC	FIELD

ekistics

ALDI GLENUNGA PLANNING STATEMENT

Proposed Retail Development at
499 Portrush Road, Glenunga

Prepared for:
Nielsen Architects

Date:
16 September 2019

ekistics

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Document Control

Revision	Description	Author	Date
V1	Draft Planning Statement	CC	12 September 2019
V2	Final Statement	CC	16 September 2019

Approved by: Rebecca Thomas

Date: 16 September 2019

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1. Executive Summary

Category	Details
PROJECT	ALDI Glenunga
ADDRESS OF SITE	499 Portrush Road, Glenunga
CERTIFICATE OF TITLE	Allotment 27 in Certificate of Title Volume 5452 Folio 298 Allotment 28 in Certificate of Title Volume 5452 Folio 286 Allotment 29 in Certificate of Title Volume 5452 Folio 287
SITE AREA	4,366m ²
PRIMARY FRONTAGE	Approximately 61m
DEPTH	Approximately 71m
LOCAL GOVERNMENT	City of Burnside
RELEVANT AUTHORITY	State Commission Assessment Panel (SCAP) – Schedule 10 Cl. 20
DEVELOPMENT PLAN	Burnside (City) - consolidated 19 December 2017
ZONING	Local Centre Zone
POLICY AREA/PRECINCT	Nil
EXISTING USE	Bulky Goods Outlet ('Shop')
PROPOSAL DESCRIPTION	Demolition of all existing structures and staged construction of a freestanding supermarket (shop) with associated earthworks and retaining, advertising displays, fencing, car parking, solar panels and landscaping
SEPARATE CONSENTS	N/A
AGENCY REFERRALS	Commissioner of Highways (DPTI)
PUBLIC NOTIFICATION	Category 2
APPLICANT	Nielsen Architects
CONTACT PERSON	Richard Dwyer or Chris Carrey – Ekistics Planning and Design – (08) 7231 0286
OUR REFERENCE	00835-002

2. Introduction/Background

Nielsen Architects, on behalf of ALDI Stores is seeking to construct a supermarket with associated earthworks and retaining, signage, car parking, fencing, solar panels and landscaping on a site presently occupied by 'Mitre 10' (a 'bulky goods outlet') at 499 Portrush Road, Glenunga.

This planning statement provides information about the subject site and proposed development and addresses the merits of the development application against the relevant provisions of the 'Local Centre Zone' of the Burnside (City) Development Plan, as well as the most relevant 'Council Wide' provisions.

For the purposes of this statement, the Burnside (City) Development Plan (consolidated 19 December 2017) will be referred to as the 'Development Plan', the *Development Act 1993* will be referred to as the 'Act' and the *Development Regulations 2008* will be referred to as the 'Regulations'.

This Planning Statement has been prepared based on the plans, elevations and perspectives prepared by Nielsen Architects, as well as other supporting documentation, which form part of the application and are appended as follows:

- *Appendix 1:* Relevant Certificates of Title
- *Appendix 2:* Survey Plan prepared by Alexander Symonds Surveyors
- *Appendix 3:* Architectural Plans and Perspectives by Nielsen Architects
- *Appendix 4:* Stormwater Management Report prepared by FMG Engineers
- *Appendix 5:* Environmental Noise Assessment by Sonus
- *Appendix 6:* Traffic Impact Assessment prepared by GTA Consultants
- *Appendix 7:* ALDI Loading Procedures
- *Appendix 8:* Landscape Plan prepared by Outerspace Landscape Architects
- *Appendix 9:* Legal Advice – Botten Levinson Lawyers
- *Appendix 10:* Retail Demand and Impact Report prepared by Deep End Services
- *Appendix 11:* Overshadow Diagrams prepared by Nielsen Architects

3. The Site and Locality

3.1 The Site

The subject land is located at 499 Portrush Road, Glenunga and comprises the following Certificates of Title which are all provided in **Appendix 1**:

- Allotment 27 in Certificate of Title Volume 5452 Folio 298
- Allotment 28 in Certificate of Title Volume 5452 Folio 286
- Allotment 29 in Certificate of Title Volume 5452 Folio 287

There are no easements, encumbrances or caveats registered to the Certificates of Title.

The subject site is located on the western side of Portrush Road, opposite the Hay Road intersection. The site is of regular shape measuring approximately 4,366m² with a primary frontage to Portrush Road of approximately 61 metres.

We understand the site was previously occupied by a 'Bi-Lo' Discount Supermarket (a 'shop') and is presently occupied by a 'Mitre 10' bulky goods outlet (also a 'shop').

The existing Mitre 10 building is located within the north-west quadrant of the site and has a spatial footprint of approximately 1,435m² (roofed area) plus an outdoor fenced display area located in front of the store which occupies an area of around 225m². The existing building is of a 'big-box' format with a wall height of approximately 7 metres, setback 27 metres from the front property boundary, 6 metres from the north (side) boundary (with a canopy connected to the boundary), 15 metres from the south (side) boundary, and 7.5 metres from the rear boundary (approximate measurements using aerial photography).

The site has two (2) existing points of access/egress, one (1) located centrally and the other located towards the northern end of the property frontage. There are around 67 onsite car parking spaces which are provided forward (east) of the building, to the side (south) of the building and within the south-east corner of the site. Existing loading/unloading and servicing arrangements occur to the rear and north side of the building, with servicing movements facilitated via one-way clockwise movement around the building.

There are no regulated or significant trees located on the subject site.

The majority of the site is sealed with hardstand (concrete or similar) with a small landscape strip provided along the front property boundary. The Mitre 10 utilises numerous advertising strategies and the site includes a freestanding pylon sign of approximately 7-8 metres height adjacent the central driveway, on-building signage across the majority of the upper fascia of the building, window signage, and signage attached to the fence of the outdoor display area.

The front property boundary is unfenced, while the side (south) boundary is fenced with low metal fencing (around 1.2 metres height) of varying standard of condition. The north (side) boundary largely comprises the

side wall of the adjoining building to the north as well as a low-level masonry wall which occupies a length of approximately 12 metres adjacent the Mitre 10 car park.

The surface of the existing site has been 'built up' and while the resultant developed area is relatively flat, there is a general overall fall from the front (south-east) to rear (north-west), as well as a general cross-fall from south to north. However, existing drainage associated with the Mitre 10 grades to the north-east to allow discharge to the Council stormwater network.

The rear of the site is fenced with metal fencing around 2 metres in height. However, as illustrated within the Survey Plan prepared by Alexander Symonds (refer **Appendix 2**), the existing fence is located 2-3 metres inside the 'true' property boundary. Behind the fence, there is a steep embankment which slopes down to the rear yards of the adjoining residential sites to the west. As illustrated within Figure 3.1, the embankment generally consists of small trees, shrubs and grasses and appears to be unused.

Figure 3.1 View to the North, 'behind' the Rear/Western Fence



An aerial photograph of the site is provided in Figure 3.2 and images of the subject site are provided in Figure 3.3 below.

Figure 3.2 Aerial Photograph of the Site



Figure 3.3 Images of the Subject Site



Looking west at the site from Portrush Road



Within the car park – looking north-west



Within the car park – looking west along the south side of the building

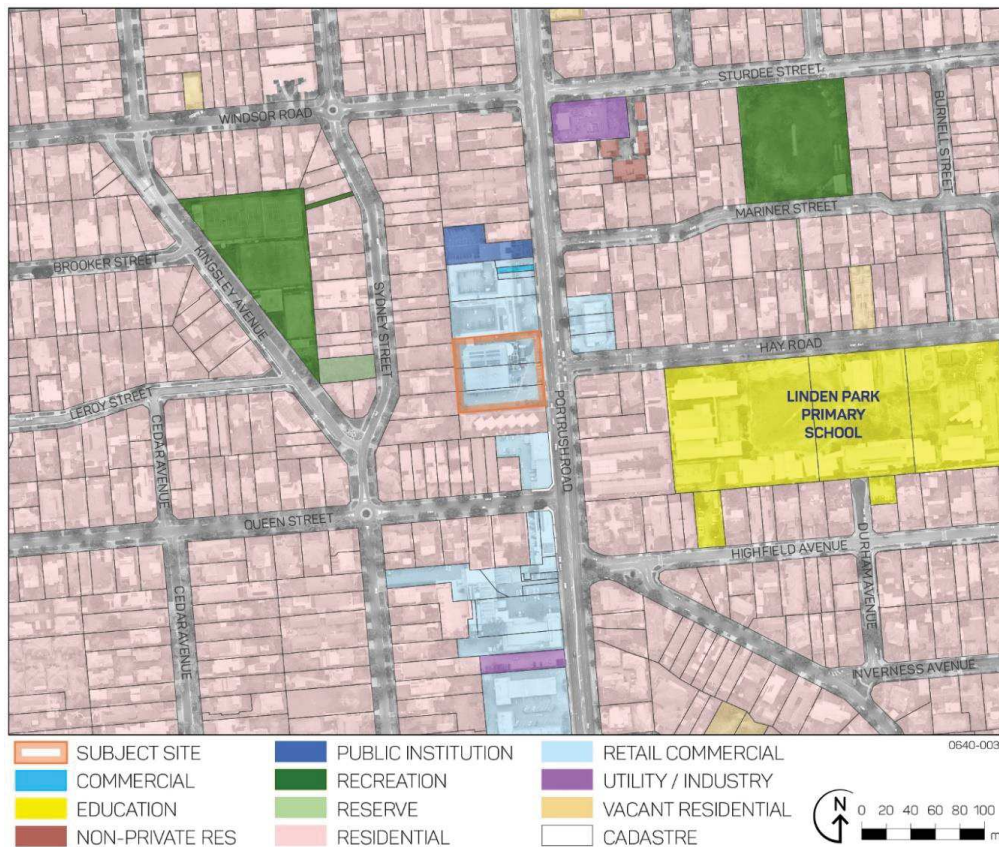


Looking south-west from Portrush Road

3.2 The Locality and Surrounding Development

A land use and locality plan are provided in Figure 3.4 below.

Figure 3.4 Land Use Mix within Locality



Land within the immediate locality accommodates the following uses:

- Adjoining the site to the north is the *Best Western Adelaide Granada Motor Inn* (Motel). The Motel comprises a two-storey building with an overall roofed area of approximately 1,450m². As mentioned above, and as illustrated in Figure 3.5, the wall of the Motel is built to the boundary of the subject site.

Figure 3.5 View of the Northern Boundary



- Adjoining the site to the south there are six (6) residential units, with a common access driveway and high-level established landscaping to the north (i.e. adjacent the subject site); common carport to the rear (west); and individual private yards to the southern side of each unit. The common driveway slopes significantly to the rear (west), and the floor levels of the residential units have been 'built up' above the level of driveway. An existing retaining wall, which increases in height from east to west, and associated metal fence, follows the alignment of the northern boundary. The overall slope of this site is such that the roof of the rear carport is approximately level with the existing surface level of the Mitre 10 car park. Images of this adjoining site are provided in Figure 3.6.

Figure 3.6 Residential Units to South of Subject Site



Looking west along the common driveway (subject site on right)



Above the carport, looking east, with the subject site on the left

- Adjoining the site to the west, there are four (4) single storey detached residential dwellings. These dwellings have a primary frontage to Sydney Street with large rear yards adjoining the subject site. The dwellings are well setback from the boundary of the subject site; and

- To the east, across Portrush Road: To the northern side of Hay Road there is a small commercial development comprising five (5) tenancies which include a bakery, laundry and three (3) cafes/restaurants with associated car parking and signage. To the south of Hay Road there is a 'residential flat building' (two (2) units facing Hay Road) and a separate detached dwelling (facing Portrush Road).

Beyond these immediate sites, the locality comprises a mix of commercial and residential land uses. In particular:

- The western side of the Portrush Road corridor generally comprises commercial activities including a restaurant, shops and church further to the north of the site; as well as shops, cafes and petrol filling station further to the south (including 'Dan Murphy Liquor Merchant', 'Tony and Mark's Produce Market' and a 'Liberty' Petrol Filling Station');
- The eastern side of the Portrush Road corridor generally comprises residential uses at varying densities and types, including detached dwellings and unit-style developments with common access driveways; and
- Beyond the Portrush corridor to the west and east generally comprises residential development at low to medium density.

We also note that to the east of the site along Hay Road is the Linden Park Primary School and around 550 metres to the north is the Burnside Shopping Village.

Portrush Road is a 'primary arterial road' under the care and control of the Department of Planning, Transport and Infrastructure (DPTI), who have advised there are no road-widening requirements. It comprises dual carriageways aligned in a north-south direction, with two (2) lanes in each direction. The road reserve adjacent the site is approximately 25 metres wide, including an approximate 3.0 metre wide central median. Kerbside parking is not permitted immediately adjacent the site, with no stopping parking controls in place. Further from the site, parking is generally permitted on both sides of the road outside of clearway times (7.00am - 10.00am and 3.00pm - 7.00pm Monday to Friday).

Portrush Road has a speed limit of 60km/h and carries approximately 39,800 vehicles per day¹.

There are pedestrian footpaths on both sides of Portrush Road and a Pedestrian Actuated Crossing is located adjacent the site, south of Hay Road.

Adjacent the site is Hay Road which is a local road managed by the City of Burnside with an urban default speed limit of 50km/h. It is aligned in an east-west direction and forms a T-Junction with Portrush Road. Kerbside parking is generally permitted on both sides of the street, subject to time constraints and loading/permit restrictions. Traffic controls have been installed in the form of 'road humps'.

¹ Sourced from Location SA, which is based on DPTI Counts undertaken for Portrush Road in 2017.

There are no nearby heritage listed items in proximity to the subject site.

Advertising signage associated with existing commercial and retail uses forms highly evident features of the immediate locality. This is particularly notable with those existing commercial developments located along the western side of Portrush Road.

Figure 3.7 provides images of the locality.

Figure 3.7 *Images of the Locality*



Looking north along Portrush Road (adjacent the subject site)



Looking south along Portrush Road (adjacent the subject site)



Looking south towards the group of shops directly opposite the subject site



Looking north along Portrush Road, with the existing Dan Murphy's on the left



Looking east along Hay Road with Linden Park Primary School on the right



Looking south along Portrush Road (adjacent the Queen Street intersection).

4. Proposed Development

4.1 Land Use

The proposed development involves the demolition of all existing buildings and structures and staged construction of an ALDI supermarket on a 4,366m² site. The building is to be elevated partially above street level to allow for provision of at grade and under-croft parking and associated earthworks and retaining, advertising signage, fencing, solar panels and landscaping.

To ensure the provision of safe and functional vehicular access to the northern side of the site (relative to the layout of the adjacent road network), the store will be located towards the southern side of the site and comprises a net floor area of 2,006m² (including the forecourt), of which 1,266m² will be retail floor space and 537m² will be 'back of house' functions.

A supermarket is a form of a 'shop', as defined in Schedule 1 of the Regulations:

shop means—

(a) premises used primarily for the sale by retail, rental or display of goods, foodstuffs, merchandise or materials; or

(b) a restaurant; or

(c) a bulky goods outlet or a retail showroom; or

(d) a personal service establishment,

but does not include—

(e) a hotel; or

(f) a motor repair station; or

(g) a petrol filling station; or

(h) a plant nursery where there is no sale by retail; or

(i) a timber yard; or

(j) service trade premises; or

(k) service industry;

4.1.1 Operational Aspects

ALDI stores are in many ways similar to 'typical' supermarkets in that they offer traditional grocery products in a familiar aisle-by-aisle format. However, there are a number of key differences that distinguish ALDI from other supermarket operations. These differences include:

- Predominately exclusive ALDI label branded products;
- 'Hard discount' food and grocery model;
- Simplified, consistent supply chain, building development, internal layout, merchandising, store operations and marketing;
- All delivery and logistics undertaken by ALDI employees, with only two (2) 15.5 metre truck deliveries per 24 hours and one daily bakery delivery via a small rigid vehicle;
- Low swept exhaust delivery vehicles (ALDI trucks are to European standards with the exhaust discharge at wheel level on the Prime Mover, i.e. there is no exhaust discharge above the vehicle cabin);
- Limited 'night fill' or store replenishment occurring outside of store operating hours, with staff leaving shortly after store closure;
- Regulated product range of approximately 1,350 items (compared with typical full-line supermarkets which offer between 20,000-30,000 items); and
- Considerably smaller retail floor plates (in this case 1,266m²) compared with full-size supermarkets which are typically between 3,000m² to 4,000m².

Of particular note is that ALDI Stores, of which there are in excess of 520 across Australia in Victoria, NSW, ACT and QLD, successfully operate in close proximity to other major supermarket chains (i.e. Coles and Woolworths), with direct competition in approximately 80% of the existing locations.

In South Australia, ALDI commenced its State-wide store roll-out in February 2016 and now has 31 operating stores (24 in Greater Adelaide and 7 in Regional areas). Notwithstanding, ALDI has less than a 5% market share of supermarket and grocery store turnover in the State.

Unlike most supermarket operations, ALDI own, operate and control all its supply and logistics via its purpose-built distribution centre located in Regency Park. This streamlined system of operation enables ALDI to position itself as a discount supermarket quite different to its competitors. Accordingly, a loading bay will be situated on the western side of the proposed building which will provide for ALDI's dedicated delivery by 15.5-metre-long semi-trailers.

A typical ALDI store employs 25 full-time equivalent (FTE) employees with 6-10 employees present on site at any one time.

The hours of operation for the supermarket will be established and potentially varied in response to customer demands, however, will be in accordance with any limitations set out by the *Shop Trading Hours Act, 1977*. The store is not proposed to operate extended hours, with likely opening times no earlier than 7am and likely closing times no later than 9pm, other than in limited seasonal peaks such as Christmas and Easter.

4.2 Built Form

A site responsive, spatially efficient and architecturally designed ALDI Store is proposed for the Glenunga site. The building has been elevated to allow for provision of at grade and under-croft parking and has been sited and designed to provide visual address to Portrush Road while being respectful of adjoining properties.

Nielsen Architects have prepared perspective images of the ALDI Store as viewed from the road frontage which are provided below.

Figure 4.1 Proposed Building (view South)



Figure 4.2 Proposed Building (view North)



Figure 4.3 Proposed Building (looking west from Hay Road)



Nielsen Architects have developed a comprehensive plan package, including Perspective and Streetscape drawings which clearly illustrate the built form proposed which is attached as **Appendix 3**.

The proposed building will be setback as follows:

- From the Portrush Road frontage:
 - » The main building line and southern 'tower' will be setback approximately 4.6 metres (behind the pedestrian ramp);

- » The northern 'tower' will be setback approximately 1.2 metres; and
- » The switch back ramp and associated canopy projection will be setback approximately 1.2 metres in line with the northern tower.
- From the northern (side) boundary:
 - » The main building line will be setback approximately 24.5 metres, while the 'tower' will be setback approximately 17 metres.
- From the western (rear) boundary:
 - » A portion of the building will be setback 2 metres, with the balance setback around 10 metres to accommodate access to the loading dock.
- From the southern (side) boundary:
 - » The main building line will be setback approximately 2.34 metres;
 - » The 'tower' will be setback approximately 3.5 metres; and
 - » The paved extent of the 'open' under-croft car park will be setback approximately 900mm.

As mentioned above, the building has been sited towards the southern side of the site, to allow for provision of safe, functional and convenient vehicle access/egress to the northern side of the site, located as far as possible from the Hay Road intersection and Pedestrian Actuated Crossing.

The proposed building incorporates a varied roof line with a maximum height of approximately 11.6 metres, measured from the basement car park level, while the majority of the building will have a height of approximately 9.4 metres above the basement car park level. The highest portion of the building will be the two (2) 'tower' elements, one (1) of which will be located to the northern end of the Portrush Road frontage, and the other located to the southern end of the street frontage.

The tower elements are a key design feature of ALDI Stores and are intended to create visual interest and clearly identify the ALDI brand. As noted on the elevations, a distinctive corner tower treatment is proposed for the northern tower which includes a wrap-around powder coated perforated screen (finished in Dulux® 'Copper Kinetic'). This treatment utilises different materials and colours to the balance of the store and assists to create visual interest.

The store is to be elevated to accommodate the under-croft car parking area. Pedestrian access will be provided via a switch back ramp located in front of the building along the Portrush Road frontage as well as via lifts and stairs from within the under-croft parking area. The pedestrian ramps and landscaping will screen the under-croft car park from view which is to remain 'open' facing Portrush Road.

The main entrance to the ALDI Store will be located within the northern tower and the main shopfront will face Portrush Road. Large shopfront windows are included on the Portrush Road frontage which overlook the street. A lightweight canopy with feature steel frames will project out from the building and overhang the pedestrian ramp which add articulation to the building frontage and provide weather protection. The southern tower will

accommodate office and amenities and an office window is provided along the southern elevation which overlooks Portrush Road and offers additional visual interest and a surveillance opportunity to the street.

The northern (side) elevation of the building, facing the car park will feature the northern tower element mentioned above and includes high level windows with associated sunscreen shades to soften the visual mass of the building. The northern elevation also includes access points to the under-croft car park, as well as the loading dock area. Facing the outdoor car park, the under-croft parking area will be screened by a combination of screening treatments which will be integrated with the design of the building and landscaping. Points of entry/exit to the basement car park will be secured by roller shutters when the store is closed.

The southern (side) elevation of the building, will feature the southern tower element mentioned above which will be visible from the public realm. The balance of the building will comprise precast concrete panels, finished in Dulux® 'Drivetime' grey, with Colorbond® gutters and downpipes finished in Colorbond® 'basalt' grey.

The loading dock will be located on the western side (rear) of the building, well setback from Portrush Road and set behind the main building line to enable efficient transfer of goods from trucks to the store. An enclosed waste bin storage area is also provided in this area, set 1 metre off the rear boundary with landscaping behind.

The proposed supermarket will be primarily constructed of the following materials and colours:

- Precast RC panel – painted 'Fluorescent Fire' Red and 'Drivetime' Grey;
- Fibre Cement Fascia - 'Drive Time' Grey;
- Precast tower element (southern tower) – 'Drive Time' Grey;
- Perforated Powder-coated Screen (northern tower) – 'Copper Kinetic' Bronze affixed over a steel frame, with exposed metalwork finished in Colorbond® 'Monument';
- Anodised aluminium window frames – natural finish;
- Colorbond® roof sheeting – concealed by parapet on three (3) sides;
- Colorbond® capping – to match the adjacent precast RC panel;
- Colorbond® panel lift door, rain head and downpipes – Basalt; and
- Sunscreen frames – finished in Colorbond® 'Monument'.

The external rooftop plant area will be screened from view by the southern tower fascia and powder-coated plant louvres and frames finished in Colorbond® 'Basalt'. Full details of the proposed external materials and finishes are contained on the elevation plans (**Appendix 3**) prepared by Nielsen Architects.

Solar panels will be installed on the roof of the ALDI Store and will, generally, be located behind the parapet. Consequently, other than some minor visibility from the south, the majority of the solar panels won't be visible from surrounding land.

The variation in height together with the high-level feature windows, design elements and colour and material variations provide articulation to the building façade.

4.3 Earthworks, Retaining and Fencing

As illustrated within the architectural drawings, and further illustrated within the 'cut/fill concept depths' drawing which accompanies the proposed Stormwater Management Plan prepared by FMG Engineers (refer **Appendix 4**), the development includes excavation across the majority of the land, with the ALDI Store to be 'cut' into the site. While the level of cut/excavation varies in response to existing site and boundary levels, the extent of excavation is broadly summarised as follows:

- Along the southern boundary – the proposed level of 'cut' will range from approximately zero (0) metres in the south-west corner to a maximum height of approximately 1.75 metres, with a taper to around 900mm in the south-east corner. The existing retaining wall along the southern boundary will be removed and replaced with a new retaining wall which responds to the associated levels of excavation, noting that the new retaining wall will only be visible from the subject site;
- Along the western side of the site – the existing battered embankment between the rear fence of the Mitre 10 and the 'true' western boundary will be removed. The proposed level of 'cut' will range from approximately zero (0) metres in the south-west corner, up to around 1.3 metres in the north-west quadrant. To support this approach, a retaining wall of varying height up to a maximum of 1.3 metres will be required along the 'true' western boundary which will be visible from those adjoining residential properties to the west;
- Along the northern boundary, adjacent the boundary wall of the Motel, the level of 'cut' will range from approximately 500mm in the north-east corner of the site, to over two (2) metres in the north-west quadrant. There is an existing retaining wall in this location, on the subject site, which will be 'cut' down, with appropriate protection afforded to the wall of the Motel.

In general terms, the proposed level of excavation is lowest in the north-east and south-west corners of the site, where it will range from approximately 0-1 metres, while the majority of the site will be excavated in the order of 1-2 metres.

The overall strategy of 'cutting' the development into the site will ensure that the basement level car park is screened from view from Portrush Road and reduces the overall height of the building when viewed from the public realm and from adjoining properties, in particular those adjoining residential sites to the south and west.

In addition, the following boundary fencing treatments are proposed. These treatments either accord with, or exceed the recommendations contained within the Environmental Noise Assessment Report prepared by Sonus Acoustic Engineers (refer **Appendix 5**):

- Set 1.5 metres inside the western (rear) boundary, a 3.0-metre-high, acoustically treated fence extending for the length of the loading dock to the northern (side) boundary, with a small section of return to the northern (side) boundary where it will meet the existing boundary wall of the Motel;

- 1.8-metre-high fence along the 'true' western (rear) property boundary, set on top the proposed retaining wall, noting that the fence height will be adjusted to ensure that the maximum combined height of fencing and retaining does not exceed 2.4 metres at any point; and
- 2.4-metre-high fence along the southern (side) property boundary, set on top the proposed retaining wall, noting that the retaining wall will not be visible from the adjoining site to the south.

4.4 Transport, Parking and Access

GTA Consultants have undertaken a detailed traffic and parking assessment to confirm that the proposed access/egress, vehicle manoeuvring, and parking arrangements are feasible, safe and achieve the relevant Australian Standards (refer to **Appendix 6**). GTA's report sets out an assessment of the anticipated transport implications of the proposed development, including:

- Existing traffic and parking conditions surrounding the site;
- Parking demand likely to be generated by the proposed development;
- Suitability of the proposed parking in terms of supply (quantum) and layout;
- Traffic generation characteristics of the proposed development;
- Proposed access arrangements for the site; and
- Transport impact of the development proposal on the surrounding road network.

4.4.1 Access/Egress

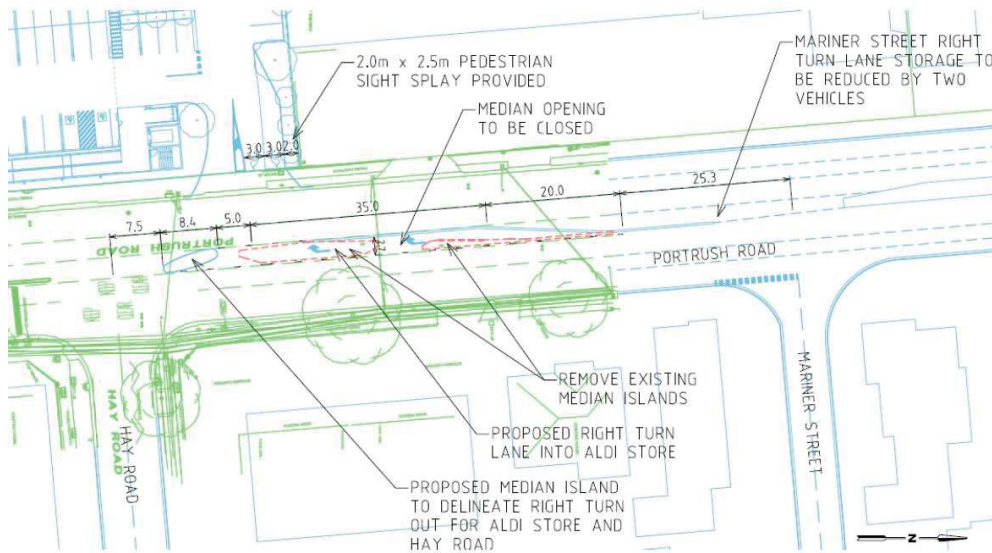
A new access/egress point is proposed along Portrush Road, which will replace the existing Mitre 10 northern driveway. The existing access point to the south of the site will be removed.

The proposed access will be offset two (2) metres from the northern (side) boundary, enabling pedestrian sight lines to be maintained, and will also be located 1.0 metres clear of an existing stobie pole located to the south.

The unrestricted access will accommodate all vehicle movements.

As illustrated within Figure 4.4, a right turn lane is proposed into the site to accommodate entry for south-bound traffic.

Figure 4.4 Proposed Access/Intersection Treatment (Source: GTA Report)



The proposed access arrangements will result in the following:

- The Motel to the north has a right turn out gap within the existing Portrush Road median – this will be closed; and
- The existing right turn lane into Mariner Street (to the north of the subject site) will be reduced by 10 metres to allow additional queuing storage for the right turn lane into ALDI.

15.5 metre semi-trailers will access the site via a left turn in movement from Portrush Road via the proposed crossover. Once onsite, the truck will then perform a 180-degree turn within the designated manoeuvring area and then reverse into the loading dock area at a 90-degree angle. The trucks can then exit the site in a forward direction, turning left onto Portrush Road.

GTA have confirmed that while the trucks share access/egress with light vehicles, as truck manoeuvres occur to the rear of the site, there will be minimal interruption to light vehicle traffic circulating within the site.

4.4.2 Parking

A total of 98 car parking spaces are proposed for the site which includes:

- 30 at-grade ('open-air') car parks to the north of the building (including three (3) designated 'staff' car parks to the rear of the site and three (3) designated staff car parks close to the entry); and
- 68 under-croft car parks located beneath the building including two (2) disabled parking spaces.

It is important to note that both car park areas are set at approximately the same level, with the store elevated above the under-croft section.

The parking layout has been reviewed by GTA who have confirmed that it has been designed in accordance with the relevant Australian Standards.

4.4.3 Deliveries

A loading area is proposed to be located at the western end of the building. The loading dock will be provided in accordance with ALDI's standard detail and will accommodate 24-hour delivery access with two (2) main deliveries made per day by 15.5 metre long semi-trailers, which will enter the site from Portrush Road, reverse into the loading area and exit in a forward direction. A third delivery, via a small rigid vehicle, will deliver bakery products once a day just prior to store opening, entering and exiting the site in a forward direction.

A copy of the ALDI Delivery and Loading procedures is located at **Appendix 7**.

As mentioned, ALDI oversees and undertakes all its own deliveries and therefore has full control of the operations and logistics, which enables the business to minimise the impact of deliveries on customers and surrounding properties.

GTA has assessed the movements associated with delivery vehicles entering and exiting the loading area and has concluded that they meet relevant standards and will allow vehicles to enter and exit the site in a forward direction (refer to **Appendix 6**).

4.4.4 Refuse Collection

Recycling and rubbish will be stored within a compactor and bin enclosure located within the loading dock which will be screened from view. The compactor and bins will be emptied regularly in accordance with ALDI's standard operating procedures.

4.4.5 Pedestrian and Cyclist Accessibility

The subject land is well served by pedestrian infrastructure with footpaths located on both sides of Portrush Road as well as a Pedestrian Actuated Crossing located in front of the site directly aligned with the switch back ramp which will provide access to the ALDI Store.

The subject land is also well serviced by public transport with bus stops located on both sides of Portrush Road, immediately to the south of the site, providing various local and city-wide connections as outlined within the GTA Report (refer **Appendix 6**).

It is noted there are no bicycle lanes on Portrush Road, however Portrush Road forms part of the city-wide Bike Direct Network and the proposed ALDI Store includes one (1) bicycle parking rack (capable of accommodating five (5) bicycles) located close to the store entrance.

4.5 Landscaping and Site Works

A variety of landscaping is proposed in association with the development. A landscape plan, prepared by Outerspace Landscape Architects, is included in **Appendix 8**.

Specifically, a combination of small, medium and upright trees will be planted at the driveway entry, within the car park and along the northern boundary of the property.

Upright trees are also proposed along the southern (side) boundary and to rear of the store within the south-west corner of the site which will assist to screen and soften the appearance of the ALDI Store from adjoining sites to the south and west.

To preserve residential amenity to the west, a landscape area which includes upright trees and shrubs, is proposed to the rear of the 3.0-metre-high acoustic fence, which will screen and soften views from the west.

The location of all proposed trees is illustrated on the Outerspace Landscape plan and includes *crepe myrtle* and two (2) *callery pear* varieties.

The trees will be complemented by landscape beds located around the site which will be densely planted with a range of shrubs, grasses and groundcovers. In particular, landscaping strips will be established along the Portrush Road frontage to soften the appearance of the built form and the car parking areas.

4.6 Signage

The application proposes five (5) internally illuminated gable signs measuring 2.0 metres by 2.4 metres to be attached to the building. Two (2) of these signs will face Portrush Road (one (1) on each tower); two (2) will face south; and one (1) will face north. These signs are all located below the roof line of the building.

One (1) digital poster box sign will be installed within the Store adjacent the main entrance.

Specific details of each proposed sign are outlined in the Nielsen Architects drawings located in **Appendix 3**.

No freestanding signage is proposed.

4.7 Stormwater Management

FMG Engineers have prepared a Stormwater Management Report for the proposed development (refer **Appendix 4**). The purpose of the report is to conceptually outline the stormwater management design for the proposed development and detail the stormwater management methodology. Final detailed design and construction documentation will be prepared once Development Plan Consent has been issued.

FMG note there is an existing 750mm diameter stormwater pipe, which runs north, located in front of the site within Portrush Road. They also note there is an existing stormwater side entry pit (SEP) adjacent to the site which is assumed to connect to the 750mm pipe.

FMG have prepared the concept based on the following assumed requirements:

- Council will not require detention storage on the basis that post development impervious conditions will not increase from pre-development conditions;
- No stormwater is permitted to be discharged from the site as surface flows for all events up to and including the 100-Year ARI (1% Annual Exceedance Probability (AEP)) storm event;

- Stormwater runoff from all areas will be diverted from the site, directly into the underground drainage infrastructure, at rates not exceeding the minor 5-Year ARI (20% AEP) storm event;
- Stormwater runoff should minimise the discharge of sediment, suspended solids, organic matter, nutrients, bacteria, litter and other contaminants;
- Stormwater runoff for all events up to and including the 100-Year ARI storm event should not be diverted towards private property; and
- The minor drainage network shall have a 5-Year ARI (20% AEP) level of service.

Accordingly, FMG has proposed the following methodology for the management of stormwater:

- Stormwater from all roof areas will be diverted to above ground tanks to limit flows from the development to the pre-development minor storm event to Portrush Road;
- Above ground (roof water) tanks will be capable of detaining all events up to and including the 20-Year ARI storm event (equivalent to the capacity of the eaves gutters). The water tanks are to be located to the southern side of the site, adjacent the southern tower. These water tanks are less than 2 metres in height and will be screened by the proposed southern boundary fence;
- As it is not feasible to drain the site via gravity, stormwater runoff from hardstand areas and overflow from the gutters will be diverted to an underground detention tank with underground pump;
- The underground detention tank will be capable of storing runoff for all events up to and including the 100-Year-ARI storm event such that no surface flows are diverted to adjacent private property;
- Stormwater runoff from the underground detention tank will be pumped directly to the underground drainage network in Portrush Road at rates not exceeding the 5-Year ARI (20% AEP) storm event;
- In accordance with Australian/New Zealand Standards, the pump storage shall be not less than the volume able to be pumped in 30 minutes and the volume generated by the 10-Year ARI 120-minute event;
- An approved duty pump with standby will be provided; and
- A gross pollutant trap (GPT) or similar approved proprietary device (such as the Rocla First Defense FD-HCM) will treat stormwater runoff prior to discharging to the underground drainage network.

4.8 Waste Management

ALDI Stores have a consistent and environmentally sound approach to waste management which includes:

- Waste disposed of in the general/waste bin is limited to waste generated by staff on their lunch breaks (food packaging) and office-based waste;
- Organic waste comprising fruit, vegetables and meat that are nearing the product's best before date are collected and re-distributed every day by ALDI's partner companies – Foodbank and SecondBite;

- Cardboard waste generated from used cardboard packaging is collected and stored in an onsite compactor within the back of house store area and collected and returned by ALDI transport to the Distribution Centre for recycling;
- Plastic wrapping is collected by ALDI transport and recycled at the ALDI distribution centre;
- The ALDI Store will have a single 1.5 cubic metre bin which will be equipped with a hinged lid and contained in the Bin Enclosure in the loading dock; and
- The collection of the bin is undertaken by a third-party contractor who will attend the Store on average three times per week to collect the contents of the bin utilising a 10.5 metre front load truck.

ALDI Stores produce minimal putrescible waste as no food is cooked or prepared within the Stores (there being no onsite butcher, bakery or deli preparation).

5. Procedural Requirements

5.1 Relevant Authority

The relevant authority to assess and determine the Development Application is the State Commission Assessment Panel (SCAP) under the delegation of the State Planning Commission (SPC), with referral being made to the City of Burnside. A request made under Schedule 10, Clause 20 of the Regulations was accepted by the State Coordinator-General for the proposed development in correspondence dated 11 September 2019.

5.2 Nature of Development

The proposal is most accurately described as demolition of all existing buildings and staged development of a new supermarket (shop) with associated earthworks and retaining, advertising signage, car parking, fencing, solar panels and landscaping.

We note that a separate development application will be lodged in the future for land amalgamation (land division) if required.

Pursuant to the 'procedural matters' section of the Development Plan, the application is neither complying nor non-complying and must, therefore, be assessed on its merits against the relevant provisions of the Development Plan.

In forming this opinion, we have had regard to the following non-complying 'triggers' relating to the 'building level' and 'advertising signage' which are listed within the Local Centre Zone as follows:

Building Level

PDC 9 *The following kinds of development are **non-complying** in the Local Centre Zone:*

Building containing one floor level directly above another floor level except where the lower floor level is for the provision of car parking and is set at a level of not less than one metre below the predominant ground level of the site.

Provided in **Appendix 9** is a legal opinion from Botten Levinson Lawyers which confirms the proposed development meets the exceptions in PDC 9 (above). This is because:

- Based on the detailed site survey undertaken by Alexander Symonds Surveyors (refer *Appendix 2*) the ‘predominant ground level’ ranges from 97.41 - 97.7 metres AHD. A conservative interpretation is the lower end of this range which means the ‘predominant ground level’ of the site is 97.41 metres AHD;
- The development comprises a ‘building’;
- One floor level (the main store area) is located directly above another floor level (i.e. the under-croft car park);
- The lower level is for the provision of car parking; and
- The lower level has been set at a maximum height of 96.40 metres AHD, which is 1.01 metres below the conservatively calculated predominant ground level of 97.41 metres AHD (i.e. not less than one (1) metre below the predominant ground level of the site).

Advertising Signage

PDC 9 *The following kinds of development are **non-complying** in the Local Centre Zone:*

Advertisement or advertising display that:

- (a) if freestanding, is more than six metres above natural ground level at any point; or*
(b) is attached to a building and protrudes above the highest level of that building.

No freestanding signage is proposed – accordingly, exception a) is not relevant.

As discussed within Section 4.6, all on-building signage is located below the highest level of the building (i.e. below the roof line) and, therefore b) does not apply.

In summary, the non-complying ‘triggers’ listed within PDC 9 do not apply, and therefore the development is for a **Consent Use** which must be assessed **on merit** against the relevant provisions of the Development Plan.

5.3 Public Notification

The proposed development is located within the Local Centre Zone which identifies that categories of public notification are prescribed in Schedule 9 of the Regulations.

Schedule 9, Clause 6(1)(h) of the Regulations identifies that *any kind of development* within a Local Centre Zone is Category 1 for public notification purposes.

However, as the subject site abuts a different zone (i.e. the Residential Zone) to the west, the proposal is a **Category 2** form of development as identified within Schedule 9, Clause 19 of the Regulations.

5.4 Agency Referrals

Given that the State Planning Assessment Panel (SCAP) is the relevant Planning Authority and the subject site is located within the bounds of the **City of Burnside**, we understand the proposal will be referred to Council.

A referral to the **Commissioner of Highways** via the Department of Planning, Transport and Infrastructure (DPTI) will be required as the proposed development seeks to alter an existing access to Portrush Road, a 'primary arterial road'.

6. Development Plan Assessment

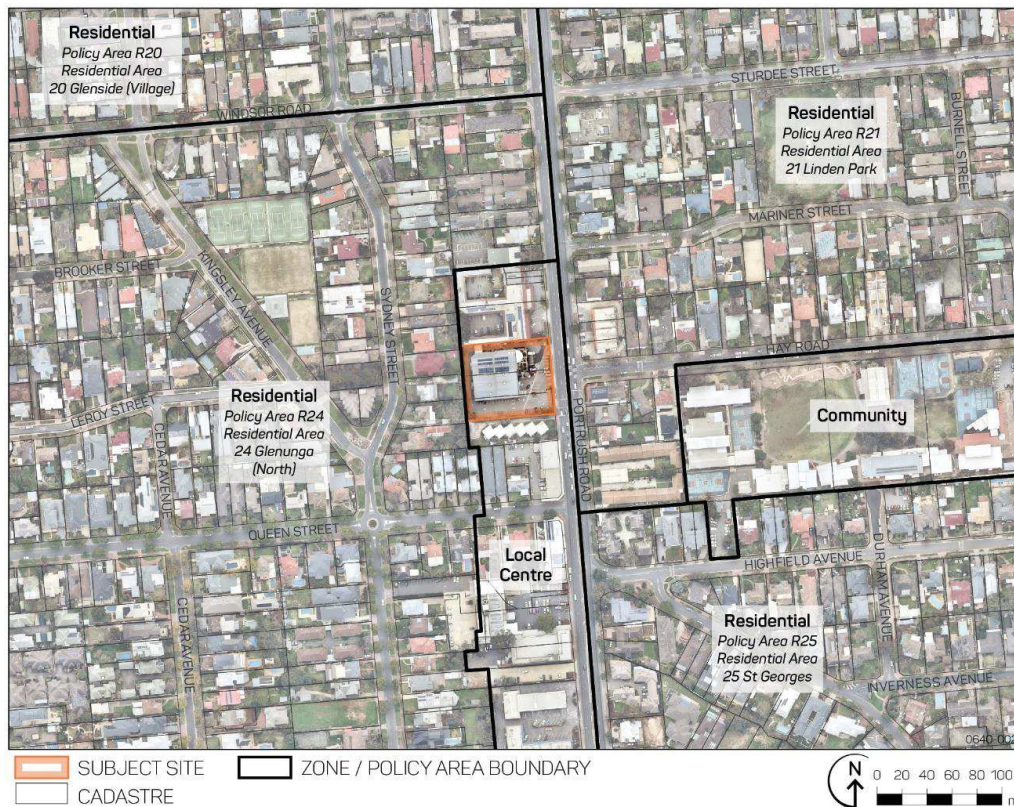
6.1 Overview

The subject land is located within the '**Local Centre Zone**' as identified within the Development Plan.

No policy area or precinct applies to the site.

Figure 6.1 depicts the zoning overlay of the site and locality.

Figure 6.1 Zoning and Policy Areas



The following section provides an assessment of the proposal against the Development Plan. For convenience, this assessment has been grouped under a series of headings which reflect the key relevant planning 'themes' from the Development Plan.

Emphasis (underlining) has been added to highlight specific provisions which are of particular relevance to the assessment of the proposal.

6.2 Land Use Suitability

As outlined in Objective 1 and PDC 1, the Local Centre Zone encourages a diverse range of activities, including ‘shops’ intended to suit the needs of the local community.

Local Centre Zone

OBJ 1 *A zone accommodating a range of shops, offices, business, health and community service providers all being of a small-scale suited to servicing the needs of the local community.*

PDC 1 *Development undertaken in the Local Centre Zone should generally consist of small-scale shops, offices, consulting rooms and commercial premises suited to meeting the needs of the local community.*

PDC 2 of the Zone identifies that shops should *generally* comprise a maximum gross leasable floor area in the order of 450m².

The proposal seeks to develop a retail net area of 1,266m² (excluding back-of-house etc.) which exceeds the envisaged floor area of PDC 2.

However, we are of the opinion that the departure from PDC 2 is not unreasonable for the following reasons:

- The site is presently occupied by a Mitre 10 Bulky Goods Outlet, a form of ‘shop’, which comprises an existing roofed area of approximately 1,435m² and outdoor display area of approximately 225m². Therefore, the site presently functions as a ‘shop’ with a floor area which considerably exceeds the 450m² limit generally anticipated within the Local Centre Zone;
- The site has an established history as a ‘shop’. Prior to occupation by Mitre 10, we understand the site was occupied by a ‘Bi-Lo’ supermarket, a form of ‘shop’, with a floor area which also exceeded the 450m² limit generally anticipated within the Local Centre Zone;
- The subject site comprises three (3) separate allotments. It is conceivable that each of these allotments could be developed separately, and each support a ‘shop’ with a floor area in the order of 450m². This would result in a combined floor area of 1350m², which is comparable to the overall floor area encapsulated by the proposed development;
- When considered as a collective, the three (3) allotments comprise an area of 4,366m² and having regard to the site’s location within a ‘centre’ zone, with frontage to an arterial road, the site lends itself to the development of a ‘shop’ of a larger size than sought by PDC 2; and
- We also note there are other examples of large floor plate commercial operations within the Local Centre Zone, including ‘shops’ which exceed 450m². For example; Dan Murphy’s has a roofed area of 1250m²; Tony and Mark’s Produce Market and Zeeva’s Eatery have a combined roof area of 1240m²; and Best Western Adelaide Granada Motor Inn has a roof area of 1450m² (all approximate measurements using satellite imagery).

Further, having regard to existing site arrangements, including access/egress, building design and loading/unloading arrangements, we are of the opinion the proposed development will significantly improve the visual amenity of the site, improve the interface with adjoining residential sites to the south and west, improve loading and servicing arrangements; and improve access/egress to Portrush Road.

In terms of the Zone's desire for shops to serve the needs of the local community, it is noted that the proposed ALDI Store will provide a wide range of groceries including bread, milk, vegetables, fruit and meat which will meet the daily needs of the local community. We note that within the Local Centre Zone, with the exception of Tony and Mark's Produce Market and the convenience shop associated with the Liberty Petrol Filling Station, there are no other shops within the Local Centre Zone which offer a similar service and product offering to local residents.

The conservative scale of shopping envisaged within the Local Centre Zone appears to be driven by the overall policy position relating to the 'hierarchy of centres' which is outlined within the following provisions of the Development Plan:

OBI 68 *Shopping, administrative, cultural, community, entertainment, educational, religious, and recreational facilities located in integrated centres and corridor zones..*

OBI 69 *Centres established and developed in accordance with a hierarchy based on function, so that each type of centre provides a proportion of the total requirement of goods and services commensurate with its role.*

OBI 70 *A hierarchy of centres located in centre zones or areas..*

PDC 238 *The development of shops should be as follows:*

(a) a shop or group of shops with a gross leasable area of greater than 250 square metres should be located in the District Centre Zone, a Neighbourhood Centre Zone, or the Urban Corridor Zone;

These provisions generally seek to ensure that:

- Centres provide an appropriate proportion of the total community requirement for goods and services relative to the population they serve;
- Centres provide an appropriate degree of consumer choice and convenience;
- Centre based development has appropriate regard to retail employment levels; and
- Centre development should not have a detrimental impact on the function of any other centre zone.

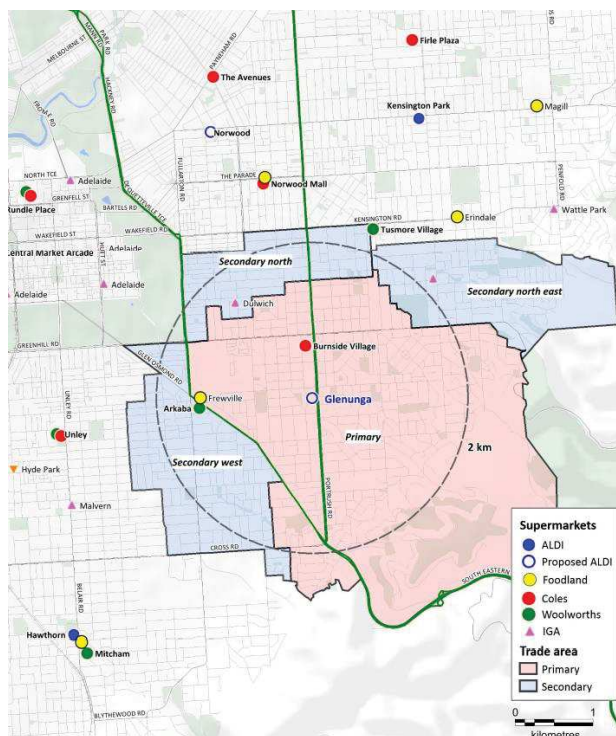
To understand the potential impacts of the proposed ALDI Store and the actual demand for retail development in the locality and catchment, the Applicant has engaged Deep End Services to prepare a supply and demand analysis of supermarket floorspace, as well as consideration of other needs, impacts and benefits relative to the proposed ALDI Store (refer **Appendix 10**).

A summary of the Deep End analysis is provided below under separate headings.

6.2.1 Supermarket Floorspace Provision

- ALDI has developed a network plan for Adelaide based on one (1) store for approximately 30,000 to 40,000 people;
- ALDI currently services the eastern suburbs with two (2) stores at Kensington Park and Newton, with a third to open in 2020 at Norwood;
- The proposed Glenunga Store would result in a four (4) store network across the eastern suburbs, resulting in an average of one (1) ALDI Store per 33,490 people;
- There are currently 23 existing supermarkets across the three (3) local government areas covering Adelaide's eastern suburbs;
- The two (2) existing ALDI stores have 7% of supermarket floorspace within the identified area. With the opening of the ALDI at Norwood, this will increase to 10%. Opening of the Glenunga ALDI Store would increase this to 14% which is still well below the other banner groups in the area (Coles 31%, Woolworths 19%, and Foodland/Drakes etc. 38%);
- As illustrated within Figure 6.2 below, a 'primary catchment area' is anticipated within 2km of the subject site with 'secondary catchment areas' anticipated to the west, north and north east;

Figure 6.2 Catchment Analysis and Location of Surrounding Supermarkets (Source: Deep End, page 8)



- The primary and secondary catchment areas have a combined population of 42,640 (based on the 2016 ABS Census) which is expected to increase to 46,000 by 2025;
- Average spending on food and grocery items within the total catchment area, is well above the Adelaide average due to a higher average income profile (compared to Adelaide);
- In terms of supermarket floorspace provision:
 - » Supermarkets in Adelaide yield an average metropolitan rate of 0.42m² of supermarket floor space per capita;
 - » The City of Burnside has an estimated 13,278m² of supermarket floorspace for its 45,706 residents yielding an average rate of 0.29m² per capita. This is the equal lowest rate of Adelaide's 20 municipalities. The City of Unley also has a similar rate of 0.29m², which exacerbates the generally low provision of supermarkets in the area;
 - » The opening of the proposed ALDI Store would increase the rate of provision to 0.34m² per capita within the City of Burnside, a rate still 20% below the Adelaide average;
 - » The identified catchment area (primary and secondary) has an estimated existing rate of 0.28m² per capita;
 - » The opening of the proposed ALDI would increase the rate of provision to 0.33m² per capita, a rate still 22% below the Adelaide average.

On this basis, Deep End conclude:

On either the municipal or trade area based spatial area, the Glenunga catchment area has very low rates of supermarket floorspace provision. This deficiency is more acute given the high-income levels and higher average spending on food and groceries which can support higher-than-average levels of supermarket floorspace supply.

Deep End note that the potential effects of low rates of supermarket floorspace provision include:

- A lack of choice and competition, which can result in higher prices for consumers;
- Residents required to travel further and longer to access shopping necessities;
- Congestion and parking difficulties during peak periods at centres or supermarkets;
- Lost employment opportunities; and
- Lost opportunities to underpin or support local and neighbourhood centre retailers with a strong supermarket tenant.

6.2.2 Impacts

Deep End assess that the expected external impacts of the proposed ALDI Store are low. This is because:

- The proposed ALDI Store is considered to be a small supermarket which will complement the range of existing shops and services within the locality and Local Centre Zone;

- A high proportion of ALDI's retail floorspace is dedicated to 'special buys' (such as clothing and household items unique to ALDI);
- Analysis of ALDI stores in Adelaide and other metropolitan areas shows that ALDI generate a low market share of food and grocery spending over a wide area - as opposed to very high, dominant market shares that larger full range supermarket chains tend to generate;
- ALDI has low market penetration, generally resulting from customers cross-shopping at a range of supermarkets depending on their needs (given there is no onsite butcher, bakery or deli preparation). This supports the position that ALDI's impact on surrounding supermarkets (and centres) in a competitive market are generally quite low;
- Supermarket provision rates are very low when combined with high average incomes and high average food and grocery spend levels. On this basis, nearby supermarkets at Burnside Village (Coles) and Frewville (Foodland and Woolworths) can easily withstand small, or even moderate trading impacts;
- Some of ALDI's sales at Glenunga will be transferred from other ALDI Stores, including those at Kensington Park and Hawthorn; and
- The proposed ALDI is a standalone store with no speciality shops or other retailing. Therefore, the small impacts will be confined to the other large well-resourced supermarkets in the area, not smaller retailers.

In terms of impact, Deep End conclude:

Since ALDI has been operating in South Australia for over three years, there are no instances that I am aware of where a small or large supermarket has closed due to ALDI's presence.

In overall terms, impacts on existing supermarkets in the area are likely to be relatively low and almost negligible on centre trading levels. The expected small sales re-allocations from strong-performing supermarkets across the catchment are likely to be within the tolerance levels of a normal competitive environment where retail turnover naturally fluctuates with changes in economic and market conditions.

6.2.3 Benefits

Deep End assess the expected benefits of the proposed ALDI Store as follows:

- The ALDI Store offers lower prices and discount products, compared to larger supermarket chains, which will be of benefit to low income and other price conscious families/individuals, notwithstanding the high-income levels within the catchment area;
- The eastern suburbs have very low levels of supermarket floorspace provision. The proposed ALDI Store will improve the range and choice of supermarkets (and products) and assist to address the current floorspace deficiency;

- The subject site allows for the establishment of a spatially efficient regional network of ALDI Stores extending from Newton to Kensington Park, and then south to Glenunga and across Hawthorn. This will mean that most residents will be no further than 2km from an ALDI Store;
- The site is central to the Local Centre Zone and is similar to the establishment of the ALDI Kensington Gardens on Magill Road (also within a Local Centre Zone of the City of Burnside). There is nothing to suggest that the Kensington Park store is incompatible with the nature of its adjoining retail and commercial uses in the zone. On the contrary, it has brought vehicle and foot traffic to the centre that would otherwise go elsewhere;
- ALDI customers may frequent other businesses within the Local Centre Zone, and ALDI's presence may attract new businesses to establish within the Centre, or existing businesses to be re-developed to leverage the improved profile of the centre; and
- ALDI will generate in the order of 15-20 employment positions, which are unlikely to result in the loss of jobs elsewhere.

In terms of overall benefit, Deep End conclude:

In all respects, the proposed ALDI store will result in a range of benefits to the local community, the broader catchment and the Glenunga Centre.

6.2.4 Land Use Summary

Having regard to all of the above, while the proposal is larger in scale than that envisaged within the Local Centre Zone, the proposed supermarket will complement the function of the Local Centre Zone, and will not detract from the function of other centre zones, or disrupt the centres hierarchy more generally.

The subject site is of an appropriate scale and conveniently located on an arterial road to support the proposed land use. Further, the site presently operates as a 'shop' with a floor area which significantly exceeds the 450m² envisaged within the Local Centre Zone.

The ALDI Store will provide a range of groceries and other goods which will serve the needs of the local community and brings a range of benefits to the locality more generally.

Finally, we are of the opinion the proposed development will significantly enhance the visual amenity of the site, provide more efficient loading and servicing arrangements compared to the existing situation; and improve access/egress to and from the site to Portrush Road.

Accordingly, we are of the opinion that the proposed land use is appropriate within the Local Centre Zone.

6.3 Design and Appearance

In general terms, the Development Plan seeks to ensure that development is of a high architectural standard, complements the character of the locality and minimises impacts on nearby existing development. Under separate headings, the following section will consider the external appearance of the development and address

potential overshadowing implications associated with siting of the building towards the southern portion of the site.

6.3.1 External Appearance

The following Zone-specific and Council Wide provisions are of most relevance to an assessment of the external appearance and design of the proposed ALDI Store.

Local Centre Zone

- PDC 5** Buildings should not contain one floor level directly above another floor level except where the lower floor level is for the provision of car parking space and the level of that floor is located not less than one metre below the predominant natural ground level for the site.
- PDC 6** Undercroft vehicle parking areas should be reasonably screened from view from public roads by the use of landscaping or other means which enhance the external appearance of the land and buildings.

Amenity

- PDC 55** The design of buildings should be of a high standard and related to adjacent buildings and other features which contribute to streetscape quality.

Design and Appearance

- OBJ 11** Development of a high design standard and appearance that responds to and reinforces positive aspects of the local environment and built form..
- PDC 14** Buildings should reflect the desired character of the locality while incorporating contemporary designs that have regard to the following:
- (a) building height, mass and proportion;
 - (b) external materials, patterns, colours and decorative elements;
 - (c) roof form and pitch;
 - (d) façade articulation and detailing; and
 - (e) verandahs, eaves, parapets and window screens.
- PDC 16** The external walls and roofs of buildings should not incorporate highly reflective materials which will result in glare to neighbouring properties, drivers or cyclists.
- PDC 17** Structures located on the roofs of buildings to house plant and equipment should be screened from view and should form an integral part of the building design in relation to external finishes, shaping and colours.
- PDC 24** Buildings should be designed and sited to avoid extensive areas of uninterrupted walling facing areas exposed to public view.

PDC 25 *Building design should emphasise pedestrian entry points to provide perceptible and direct access from public street frontages and vehicle parking areas.*

PDC 28 *Outdoor storage, loading and service areas should be:*

(a) screened from public view by a combination of built form, solid fencing and/or landscaping;

(b) conveniently located and designed to enable the manoeuvring of service and delivery vehicles;

(c) sited away from sensitive land uses.

The proposed development is of a high architectural standard which responds to the character of the locality and delivers a spatially efficient outcome.

By necessity, it also represents a practical and functional design outcome which reflects the overarching retail nature of the development. This has resulted in a design which features:

- A building of a size and shape that enables the efficient display of products for sale with attractive external facades aligned to the public realm;
- A loading dock and manoeuvring area for semi-trailers; and
- Substantial car parking areas conveniently located on the site.

In addition, ALDI stores have specific design requirements to enable the efficient operation and function of their supermarkets which are replicated wherever possible.

As discussed within Section 4.2, the majority of the building has a height of approximately 9.4 metres, while the ‘taller’ elements relate to the feature ‘tower elements’ which are appropriately located away from property boundaries. These ‘taller’ elements act as important features to add visual interest to the building and enhance its overall appearance.

The height of the ALDI Store, relative to the public realm and adjoining properties, is reduced by the proposed excavation works and the ‘cutting’ of the building into the site.

In addition, the adjoining sites to the south and west presently have established landscape/vegetated areas immediately adjacent the subject site. These existing vegetated areas presently offer a reasonable level of screening and separation from the subject site, and, other than removal of the existing embankment to the west (rear), the proposed development will not alter these existing arrangements.

The proposed overall height of the building is driven by the need to ensure that sufficient onsite car parking is provided to support the use. Under-croft parking is contemplated within the Local Centre Zone (PDC 5 and 6), provided it is appropriately screened from view. The under-croft parking area has been designed to integrate with the design of the building and is screened from Portrush Road by the switch back ramp, landscape treatments forward of the Store, and by the setting down of the car park level below the front property

boundary which all enhance the appearance of the development. Accordingly, the under-croft car park satisfies the intent of the Zone and will not have a detrimental impact on the amenity of the locality.

In addition, and as will be discussed within Section 6.4, the car park layout has been appropriately designed to accommodate safe and efficient vehicle movements to, from and within the site.

The proposed development responds to the key provisions of the Development Plan in the following ways:

- Siting of the building close to the Portrush Road frontage to activate the street and create a sense of urban enclosure. This is consistent with the setbacks of existing commercial development in the locality, particularly on the western side of Portrush Road. For example, the nearby Dan Murphy's, as well as the row of shops between 511-543 Portrush Road are all sited close to their Portrush Road frontage;
- Through a range of design elements intended to, where possible, avoid blank walls exposed to public view and reduce visual bulk including the use of:
 - » Large windows, canopy and frame projection and pedestrian switch back ramp access to clearly identify the entrance and primary frontage of the store, create visual interest and activate the Portrush Road frontage;
 - » Wrap around perforated screen to define the northern corner elevation and add visual interest when viewed from Portrush Road and the public realm generally;
 - » High level windows and sunshade screens along the northern elevation facing the 'open-air' car park; and
 - » Utilisation of a range of colours, materials and textures along the Portrush Road and northern elevations.
- Incorporating design elements such as a variety of colours, and the vertical element of the ALDI towers and wrap around perforated screen which contrast with the horizontal elements of the building to create visual interest and reduce massing;
- Avoiding highly reflective materials that could cause glare to neighbouring properties, drivers and cyclists;
- Incorporating landscaping around the perimeter of the site and within the car-parking areas to soften the appearance of the building and provide an attractive environment for customers;
- Integration of plant equipment and solar panels within the overall design of the building; and
- Locating the loading and service areas away from the main frontage of the buildings.

Overall the development is of a high design standard and will contribute positively to the character of the streetscape and satisfies the key design provisions of the Development Plan.

6.3.2 Overshadowing

As the subject site adjoins residential units to the south (noting they are located within the Local Centre Zone), as well as the Residential Zone to the west, potential overshadowing impacts have been carefully assessed.

The Development Plan contains several Council-Wide provisions which provide general guidance regarding the assessment of overshadowing. For example:

Design and Appearance

PDC 15 Where a building is sited on or close to a side or rear boundary, the boundary wall should minimise:

- (a) the visual impact of the building as viewed from adjoining properties; and*
- (b) overshadowing of adjoining properties and allow adequate sunlight access to neighbouring buildings.*

PDC 21 The design and location of buildings should enable direct winter sunlight into adjacent dwellings and private open space and minimise the overshadowing of:

- (a) windows of main internal living areas;*
- (b) upper-level private balconies that provide the primary open space area for a dwelling; and*
- (c) solar collectors (such as solar hot water systems and photovoltaic cells).*

However, and while not directly relevant to the assessment of this proposal, PDC 183 of the Council-Wide Residential Development section, provides an arguably more rigorous, and quantified approach to the assessment of overshadowing as follows:

Access to sunlight

PDC 183 Except in the Urban Corridor Zone, buildings should be designed and located to ensure that on the site of any dwelling:

- (a) at least three hours of sunlight is available to windows to north-facing habitable rooms;*
- and*
- (b) at least two hours of sunlight is available to at least 50 percent of the main outdoor living areas of the dwelling, between 9am and 5pm on the winter solstice (21 June).*

While not technically applicable, this provision is still useful when considering the shadowing impacts to the adjoining units to the south. As illustrated in Figure 6.3, these units are configured with their main living area (including living room windows and point of entry) facing north-east, with a bedroom window facing north-west. The main outdoor living areas are located to the south, which are accessed via the rear laundry area of each respective unit.

Figure 6.3 Orientation of Adjoining Units to South



Provided in **Appendix 11**, Nielsen Architects have prepared overshadowing diagrams for 21 June as applies under the existing and proposed arrangements.

Analysis of each situation is provided below under separate headings.

Existing Situation

Under current arrangements, overshadowing of the adjoining units to the south is largely influenced by the boundary fencing treatment along the northern boundary of the units coupled with the existing landscape and tree buffer which is located inside the northern boundary.

While there is some variation, between 9.00am and 3.00pm, the north-east facing windows of the units currently have access to sunlight for approximately 6 hours of the day. For remaining hours of the day, the north-east orientation falls within the shadow of the trees/fence and/or the shadow cast by the units themselves.

To a varying extent, the rear outdoor yard areas are constantly within the shadow cast by the units themselves.

Overshadowing to the west is limited to the early hours of the morning only.

Proposed Situation

Under the proposed scenario, overshadowing is influenced by the existing landscape/tree buffer (located on the site of the units) and the proposed ALDI building and boundary fencing treatments.

As illustrated within **Appendix 11**, from 10.00am, sunlight is available to the north-east and north-west aspects of the units with clear and undisturbed provision between 11.00am and 2.00pm. The overshadow period is limited to the early morning and late afternoon.

The proposed development does not affect the rear yards of the units, and they remain affected by their own shadow profile.

While there is some increase, overshadowing to the west is limited to the early hours of the morning only and is not unreasonable.

Accordingly, and while not within a Residential Zone, the overshadowing impact associated with the proposal achieves the more onerous quantitative requirements for residential development (PDC 183), which in turn

satisfies the overarching intent of the overshadow provisions encapsulated within PDCs 15(b) and 21(a) respectively.

6.4 Transport, Access and Parking

The Development Plan contains numerous provisions which seek to ensure that traffic can move efficiently and safely while also ensuring that an appropriate amount of car parking is provided to meet the demands generated by the development. Given the number of 'traffic and parking' related provisions within the Development Plan, only the provisions of particular relevance to the proposed development have been referenced:

Movement and Parking of Vehicles

OBJ 33 *A form of development adjoining arterial roads which:*

- (a) ensures traffic can move efficiently and safely;*
- (b) discourages commercial ribbon development;*
- (c) prevents high traffic-generating uses outside designated centre zones, or the Office or Urban Corridor Zone;*
- (d) provides for adequate off-street parking; and*
- (e) provides limited and safe points of access and egress.*

PDC 93 *Development should provide for safe and convenient:*

- (a) access for pedestrians, cyclists and vehicles, including emergency and essential service vehicles;*
- (b) off-street parking to an extent and in a manner which will minimise interference with the flow of traffic on roads adjoining the site of development; and*
- (c) off-street loading, unloading and turning of vehicles, including service vehicles, likely to be required in association with the use of the land.*

PDC 95 *Development on sites with frontage to a Primary Arterial Road or Secondary Arterial Road depicted on Map Bur/1 (Overlay 1) should minimise, and where possible, avoid points of access to and egress from such roads, provided the amenity of residential localities is not impaired as a result of use of other roads for site access and egress.*

PDC 97 *No point of vehicular access or egress from a road to a non-residential development site should be nearer to another access or egress point than six metres, nor be located within 10 metres of the carriageway of an intersection.*

PDC 101 *Development should cater for the safety and convenience of the users of land by providing, where appropriate, pedestrian pathways, ramps and car parking spaces located and designed to foster ease of movement by aged or disabled persons and persons accompanied by children.*

PDC 106 *Spaces for off-street car parking should be provided with development at the rates prescribed in Table Bur/5 and Table Bur/6, subject to the extent to which other principles of development control concerning provisions for car parking are met, and except where the prescribed rates are in conflict with those prescribed in any part of a zone in which case the provisions for the part of a zone take precedence.*

PDC 107 *Development should include the provision of space for off-street car parking sufficient to accommodate the likely needs for such space generated by the development, taking into account:*

- (a) the scale and nature of the development and the activities of those who would use the land concerned;*
- (b) the availability of space for car parking on land other than the site of the development; and*
- (c) the likely use of public and/or non-motorised transport in travel to and from the site of the development.*

PDC 108 *Development should provide appropriately for the parking of vehicles by disabled persons.*

PDC 110 *An area for car parking and the manoeuvring of vehicles should:*

- (a) incorporate landscaping, including substantial trees and shrubs both on the perimeter and within the parking area, for shade and visual enhancement;*
- (b) be set-back not less than two metres from the boundary of a road except in the Urban Corridor Zone;*
- (c) be designed, surfaced and graded to facilitate stormwater infiltration and treatment (for example, by surfacing open car parking spaces with materials which allow stormwater infiltration and/or diverting runoff from paved areas to soft landscaped areas, or stormwater infrastructure specifically designed to accommodate on-site infiltration or treatment);*
- (d) be located and designed to ensure safe and convenient pedestrian access to and from the parked vehicles and between parked vehicles and the facilities served by the car parking area;*
and;
- (e) be located and designed to ensure safe and convenient traffic circulation, with minimal conflict with service access and functions; and*
- (f) include adequate provision for manoeuvring into and out of parking bays, along aisles and with reversing areas.*

PDC 112 *Loading and unloading areas and parking areas for service or commercial vehicles should have separate ingress and egress points from those for car parking areas, and should be screened from general view.*

6.4.1 Access/Egress

GTA has reviewed the proposed access/egress arrangements for the site.

The proposed crossover to Portrush Road will cater for unrestricted turning movements and maintains a clearance of 2.0 metres from the northern boundary and 1.0 metres from the existing stobie pole to the south.

The unrestricted access will accommodate all light vehicle movements.

As illustrated within Figure 4.2 earlier in this report, a right turn lane is proposed into the site to accommodate entry for south-bound traffic. The proposed right turn lane is 55 metres long, including a 20-metre-long taper and 35 metres of deceleration, which will accommodate up to five (5) queuing vehicles.

A central median is proposed south of the ALDI crossover to provide delineation between right turn movements out of both ALDI and Hay Road.

The proposed access arrangements will result in the following:

- The Motel to the north has a right turn out gap within the existing Portrush Road median – this will be closed;
- The existing right turn lane into Mariner Street (to the north of the subject site) will be reduced by 10 metres to allow additional queuing storage for the right turn lane into ALDI. The Mariner Street right turn lane will facilitate up to four (4) queuing vehicles.

15.5 metre semi-trailers will access the site via a left turn in movement from Portrush Road via the proposed crossover. Once onsite, the truck will then perform a 180-degree turn within the designated manoeuvring area and then reverse into the loading dock area at a 90-degree angle. The trucks can then exit the site in a forward direction, exiting left onto Portrush Road.

GTA have confirmed that while the trucks share access/egress with light vehicles, as truck manoeuvres occur to the rear of the site, there will be minimal interruption to light vehicle traffic circulating within the site.

6.4.2 Parking

GTA Consultants have assessed the proposed development against the relevant provisions of the Development Plan, as well as the applicable Australian Standards. The GTA assessment confirms that the parking layout has been designed in accordance with the relevant Australian Standards. This includes:

- The dimensions of the parking spaces (2.6m wide by 5.4m long) with a minimum 7.0 metre aisle width;
- Columns within the under-croft car park will be situated outside the vehicle design envelope;
- In locations where aisles are adjacent walls or columns, an additional 300mm clearance will be provided;
- A 2.0 metre by 2.5 metre pedestrian sight splay shall be provided for the access onto Portrush Road;
- Two (2) disabled car parking spaces will be located adjacent the lift entry to the ALDI Store with an associated shared space; and
- Appropriate grades within the parking area.

GTA has also undertaken an assessment of the car parking requirements for the proposed development. This assessment has included a review of the standards of off-street vehicle parking contained in *Table Bur/5 – Off Street Vehicle Parking Requirements* of the Development Plan.

Table Bur/5 includes a very outdated vehicle parking rate of 7 spaces per 100m² of total floor area for a 'shop'. Based on a gross leasable floor area (GLFA) of 1,883m² (excluding the forecourt area), this equates to a Development Plan parking requirement of 132 spaces.

Therefore, when measured against the Development Plan parking rate, the proposed provision of 98 spaces, represents a shortfall of 34 spaces.

We note that the City of Burnside has not adopted a 'Better Development Plan' (BDP) format as was to be required by all Councils, and as such, contains archaic parking rates. If the Development Plan had been converted to the BDP format, contemporary car parking rates, based on recent parking demand surveys for non-residential development would have applied, with most current 'centre' zones applying rates in the order of 5 spaces per 100m² of GLFA for a 'shop' (or even 3 spaces per 100m² in designated areas).

However, based on analysis of parking provision at other established ALDI Stores, GTA consider the Development Plan parking rate to be high, which would result in an over provision of onsite parking.

GTA also has regard to parking demand surveys which have been undertaken at other standalone ALDI Stores. These surveys have demonstrated that ALDI generates an average peak demand of 4.1 car parking spaces per 100m² of GLFA. Based on this average demand, and the proposed retail area of 1,883m², the proposed ALDI Store would generate a peak parking demand of 77 car parking spaces. We also note that a rate of approximately 4 spaces per 100m² has been adopted for all other approved ALDI Stores in Adelaide.

Through provision of 98 spaces (which equates to 5.2 spaces per 100m² of GLFA), the development exceeds this calculated average demand and GTA conclude that sufficient onsite car parking is provided to satisfy the demand generated by the ALDI Store.

6.4.3 Pedestrian and Cycling Accessibility

Bicycle parking rates are not specified within the Development Plan. However, PDC 109 and 116 within the 'Council wide – Movement and Parking of Vehicles' section provides:

PDC 109 Where appropriate, development should provide safe and secure facilities for the parking of bicycles.

PDC 116 On-site secure bicycle parking facilities should be:

- (a) located in a prominent place;
- (b) located at ground floor level
- (c) located undercover;
- (d) located where surveillance is possible;

(e) well lit and well signed;

(f) close to well used entrances; and

(g) accessible by cycling along a safe, well-lit route.

Based on observations of other stores, GTA conclude that the provision of two (2) bicycle parks should adequately cater for anticipated bicycle parking demand. Accordingly, given the development provides for the parking of five (5) bicycles, close to the Store entrance, the development provides appropriate onsite provision and sufficiently accords with PDCs 109 and 116.

With regards to pedestrian connectivity, the ALDI Store is aligned to provide pedestrian connection from the public footpath and Pedestrian Actuated Crossing on Portrush Road, directly to the Store via the proposed switch back ramp.

Consequently, the development provides appropriate connection to the existing public pedestrian network.

6.4.4 Traffic Impact

GTA has also undertaken an analysis of traffic generation, distribution and impact on Portrush Road and the local road network. Their assessment concluded that:

- The proposed development is predicted to generate up to 232 peak two-way movements during the Thursday PM Peak Hour period and 288 two-way movements during the Saturday Peak Hour;
- Given the existing Mitre 10 store generates 20 movements during the Thursday PM Peak Hour and 129 movements during the Saturday Peak Hour, this equates to an additional 212 and 159 trips onto the road network;
- Analysis of additional traffic generated by the proposed development during peak periods, indicates the proposed right turn lane will accommodate additional traffic, which will not be adversely impacted by the reduction in length of the Mariner Street right turn lane;
- Right turns out of the site will experience higher queue lengths and delays, however based on gap analysis and frequency of PAC activations (refer to discussion within the GTA Report in **Appendix 6**) there will be sufficient opportunities to accommodate the anticipated demands;
- The remaining approaches to the site are anticipated to operate at Level of Service (LOS) A with no notable queuing or delays; and
- The loading facility for the ALDI Store will accommodate truck access up to a 15.5 metre Semi Trailer. Access for recycling and waste collection vehicles will also be suitable.

Accordingly, the proposal will meet the relevant provisions of the Development Plan as they relate to traffic, parking and access and GTA conclude:

There is adequate capacity in the surrounding road network to cater for the traffic generated by the proposed development.

6.5 Landscaping

The Council Wide Development Plan provisions that follow are considered directly relevant to the provision of landscaping:

PDC 66 Development should incorporate appropriate landscaping which is designed to:

- (a) assist in visual enhancement of the development and its locality;
- (b) promote optimal micro-climate conditions in and around buildings (for example, by use of deciduous plants to allow penetration of heat from the sun in winter, and provide shade in summer);
- (c) shade, define and create windbreaks for pedestrian paths and open spaces;
- (d) screen, shade and enhance the appearance of car parking areas;
- (e) screen service yards, loading areas and outdoor storage areas;
- (f) promote conservation of water and biodiversity;
- (g) minimise risk, and not hinder protection from bushfires;
- (h) be durable and minimise the need for maintenance;
- (i) allow appropriate levels of surveillance of car parking areas, service areas and entrances to development to minimise opportunities for crime;
- (j) ensure safe traffic movement by not obstructing or obscuring the views of motorists and/or pedestrians; and
- (k) maintain the integrity of buildings, structures or infrastructure.

PDC 68 The selection of plant species should, where appropriate, include:

- (a) species indigenous to the local area, or others which will reinforce a sense of place and contribute to the objectives for the relevant zone and policy area;
- (b) trees in scale with buildings on the land;
- (c) taller tree species in localities where such trees are absent or scarce; and
- (d) species selected and located to minimise debris and leaf fall entering the nearby drainage system.

In order to address these provisions, Outerspace Landscape Architects have prepared a Landscape Plan (refer **Appendix 8**). This Landscape Plan proposes a range of trees, shrubs, grasses and groundcovers which have been selected to give a strong identity to the ALDI Store, with each species serving a specific function.

Landscaping is proposed along the road frontage; the northern and southern boundary fences, and within the car park which will soften the appearance of the building and car park.

Landscaping is also proposed to the rear of the site, facing the car park, which will enhance the internal amenity for users of the site, as well as screening the rear (western) acoustic fence and loading dock area.

Substantial landscaping is also proposed to the rear of the site, behind the proposed acoustic fence. This will ensure the acoustic fence is screened from view from those adjoining residential sites to the west and enhance their overall residential amenity. Access to this rear landscape area will be via the under-croft car park to the south-west corner of the site. This will allow the rear landscape area to be nurtured and maintained on an ongoing basis.

In addition, the adjoining sites to the south and west presently have established landscape/vegetated areas immediately adjacent the subject site. These existing vegetated areas presently offer a reasonable level of screening and separation from the subject site, and the proposed development will not alter these existing arrangements.

For these reasons, the proposed landscaping strategy satisfies the relevant provisions of the Development Plan.

6.6 Signage

The Development Plan contains a number of provisions which seek to ensure that advertising signage is sensitively designed and integrated with the associated building design while avoiding visual clutter. In addition, the Development Plan seeks to ensure that advertisements do not distract drivers from the task of driving or obstruct a driver's view of other vehicles.

The Development Plan provisions that follow are considered directly relevant to the issue of signage:

Outdoor Advertisements

OBJ 83 Advertisement and advertising displays confined to appropriate areas, and designed and located to:

(a) complement and improve the character and amenity of the area within which it is located, including the appropriate rectification of existing unsatisfactory signage;

(b) maintain equity of exposure for all business premises;

(c) avoid creating or contributing to any hazard; and

(d) be concise and efficient in communicating with the public to:

(i) avoid proliferation of confusing and cluttered information; and

(ii) minimise the number of advertisements displayed.

PDC 246 Advertisements and advertising displays should:

(a) convey in simple, clear and concise language, symbols, print style, layout and a small number of colours, the owner/occupier and/or generic type of business, merchandise or services being advertised;

- (b) be few in number to avoid proliferation of advertisements or advertising displays;*
- (c) be of a form, style, scale, height and size and in locations which:

 - (i) are appropriate to the character of the locality;*
 - (ii) preserve reasonable exposure to the public for all adjacent sites; and*
 - (iii) will not obscure a driver's view or distract drivers where demands on concentration are high such as intersections or bends in a road.**
- (d) not have an adverse impact on the amenity of adjacent premises and areas from which they are visible;*
- (e) rectify or improve existing unsatisfactory signage;*
- (f) ensure that pole and other structures with the sole purpose of supporting an advertisement are concealed from view, or of simple design and visually unobtrusive;*
- (g) not encroach beyond the boundaries of the subject property and if road widening is applicable, the proposed property boundary realignment;*
- (h) be unified in colour, style, placement and proportion to present a co-ordinated theme and design for a site, centre or building;*
- (i) be designed, made and presented in a high-quality manner, and be maintained in good repair and condition at all times; and*
- (j) be designed and located to avoid damage, pruning or lopping of on-site landscaping or street trees.*

PDC 248 An advertisement or advertising display attached to a building should:

- (a) be of appropriate colour, scale and proportion, coordinated with and complementing the architectural form and design of the building that it is attached to; and*
- (b) not be located so as to extend in a horizontal direction beyond the walls or other vertical surface of a building, unless the advertising display is appropriately designed to form an integrated and complementary extension of the existing building.*

The locality, particularly existing commercial development located along the western side of Portrush Road, incorporates a wide range of prominent signage which all form notable character elements of the urban landscape. The diversity of land uses and desire for visual exposure have resulted in a streetscape comprising freestanding signage, fascia signage, billboard signage and other forms of signage such as A-Frame signs. This includes the existing Mitre 10 Store which utilises a range of notable advertising strategies oriented towards Portrush Road.

The development proposes five (5) façade 'wall signs' which are all lower in height than the proposed ALDI Store and designed to deliver the brand message in a clear and concise format. The proposed signage placement

provides maximised commercial exposure for ALDI along the Portrush Road (arterial) frontage without resulting in undue signage clutter.

In terms of the proposal's consistency with the signage provisions within the Development Plan, it is noted that:

- The location, siting, design, materials and shape of the proposed signs are coordinated with, and complimentary to, the architectural form and design of the proposed building;
- The content of the advertising displays will be limited to information relating to the legitimate use of the subject land;
- Advertising displays are contained within the boundaries of the subject land;
- The advertising displays are coordinated in appearance, proportionate to the scale of the associated building and form integral architectural elements and features of the building;
- The advertising displays are designated to clearly identify the retail activity to passing traffic, without any flashing or animations;
- The advertising displays will not hinder the exposure of surrounding commercial sites or existing advertising signage within the locality; and
- The illuminated advertising displays will not cause discomfort to an approaching driver or create difficulty in the driver's perception of the road or persons or objects on the road due to their location and height above ground level.

For these reasons, the signage associated with the proposed development satisfies the relevant provisions of the Development Plan.

6.7 Stormwater

The Development Plan also seeks to ensure that stormwater is managed appropriately to improve the quality of stormwater, minimise pollutant transfer to receiving waters, protect downstream receiving waters from high levels of flow or flooding and minimise the concentrated discharge of stormwater from the site.

The Development Plan provisions that follow are considered directly relevant to the issue of stormwater:

Council Wide - Environmental Protection

OBJ 16 *Storage and use of stormwater which avoids adverse impact on public health and safety.*

PDC 32 *Development should be designed to maximise conservation, minimise consumption and encourage re-use of water resources.*

PDC 33 *Development should be sited and designed to:*

- (a) capture and re-use stormwater, where practical;*
- (b) minimise surface water runoff;*
- (c) prevent soil erosion and water pollution;*

- (d) protect and enhance natural water flows;*
- (e) protect water quality by providing adequate separation distances from watercourses and other water bodies;*
- (f) not contribute to an increase in salinity levels;*
- (g) avoid the water logging of soil or the release of toxic elements; and*
- (h) maintain natural hydrological systems and not adversely affect:*
 - (i) the quantity and quality of groundwater;*
 - (ii) the depth and directional flow of groundwater;*
 - (iii) the quality and function of natural springs.*

PDC 35 Water discharged from a development site should:

- (a) be of a physical, chemical and biological condition equivalent to or better than its pre-developed state; and
- (b) not exceed the rate of discharge from the site as it existed in pre-development conditions.

PDC 36 Development should include stormwater management systems to protect it from damage during a minimum of a 1-in-100 year average return interval flood.

PDC 37 Development should have adequate provision to control any stormwater over-flow runoff from the site and should be sited and designed to improve the quality of stormwater and minimise pollutant transfer to receiving waters.

PDC 38 Development should include stormwater management systems to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure the carrying capacities of downstream systems are not overloaded.

PDC 42 Where it is not practicable to detain or dispose of stormwater on site, only clean stormwater runoff should enter the public stormwater drainage system.

As outlined in Section 4.7, FMG Engineers have been engaged to prepare a Stormwater Management Report for the proposed development (refer to **Appendix 4**). As part of this report, FMG have provided advice on the management of stormwater and have prepared a management plan with supporting documentation which responds to Council and DPTI requirements, while also ensuring that stormwater is managed effectively and efficiently.

In essence, FMG have recommended:

- Stormwater from all roof areas will be diverted to above ground tanks to limit flows from the development to the pre-development minor storm event to Portrush Road;

- Above ground (roof water) tanks will be capable of detaining all events up to and including the 20-Year ARI storm event (equivalent to the capacity of the eaves gutters);
- As it is not feasible to drain the site via gravity, stormwater runoff from hardstand areas and overflow from the gutters will be diverted to the north-west corner of the site, diverted to an underground detention tank with underground pump;
- The underground detention tank will be capable of storing runoff for all events up to and including the 100-Year-ARI storm event such that no surface flows are diverted to adjacent private property;
- Stormwater runoff from the underground detention tank will be pumped directly to the underground drainage network in Portrush Road at rates not exceeding the 5-Year ARI (20% AEP) storm event;
- In accordance with Australian/New Zealand Standards, the pump storage shall be not less than the volume able to be pumped in 30 minutes and the volume generated by the 10-Year ARI 120-minute event;
- An approved duty pump with standby is recommended for redundancy purposes; and
- A gross pollutant trap (GPT) or similar approved proprietary device (such as the Rocla First Defense FD-HCM) will treat stormwater runoff prior to discharging to the underground drainage network.

On this basis, the proposed development satisfies the relevant provisions of the Development Plan in relation to the provision of infrastructure to manage stormwater.

6.8 Crime Prevention

The proposed development will front Portrush Road which will activate this frontage and encourage casual surveillance of the public realm.

The car parking area is open to view from Portrush Road and will include lighting to Australian Standards. Signage will be provided to assist with wayfinding and to highlight the entrances and pathways to and within the site.

In terms of landscaping, it is noted that the proposed landscaping will maintain view-lines to entrances and exits as well as allowing clear views to areas where people may gather. In this way, potential entrapment spots will be avoided, and a number of choices will remain available to pedestrians to avoid movement predictors.

The risk of vandalism and graffiti will be minimised through the use of a variety of building materials and colours and through the opportunities for casual surveillance which have been built into the design of the development.

The articulation of the building combined with clearly defined entrances will assist visitors to orient themselves and gain an understanding of their surroundings.

The under-croft car park will be closed and secured outside shop trading hours and will be installed with both appropriate public lighting and Closed-Circuit Television (CCTV) systems.

Accordingly, the proposed development satisfies the following relevant 'Crime Prevention' provisions of the Development Plan:

OBI 6 Buildings and environs which are safe, secure and accessible for all users.

PDC 5 *Development should:*

(a) facilitate shared use of facilities by adjoining communities;

(b) enhance personal safety; and

(c) minimise potential for crime.

6.9 Interface between Land Uses

There are a number of relevant provisions in the Development Plan which seek to address potential negative impacts and minimise conflict between land uses. In this regard, PDC 236 of the Council Wide – Retailing and Centres section provides guidance and states:

PDC 236 Development located within any centre zone should be designed and located to minimise its impact on existing or potential dwellings in an adjacent zone due to:

(a) vehicular access, egress and circulation within the site of the development;

(b) the location and arrangement of service, refuse and waste storage and collection facilities;

(c) the scale, height and bulk of buildings where located close to the boundaries of the zone;

(d) unreasonably adverse visual impact when viewed from the site of any dwelling;

(e) loss of privacy or overshadowing; and

(f) the generation of noise, odour, light or particulate matter.

Having regard to the nature of the land use proposed, the potential key interface issues relevant to the assessment of the proposal include noise and hours of operation, light spill and odour. These will be addressed below under separate headings.

6.9.1 Noise and Hours of Operation

While the subject site is located within a Local Centre Zone where a range of non-residential and commercial land uses are envisaged, as the subject site abuts the Residential Zone to the west, residential units to the south and a Motel to the north (noting that both are located within the Local Centre Zone) the potential noise impacts associated with the proposed land use and associated hours of operation have been carefully designed and assessed.

In order to confirm that the proposed development satisfies the requirements of the Development Plan, Sonus have prepared an Environmental Noise Assessment (**Appendix 5**). Sonus have reviewed the proposal against the Environment Protection (Noise) Policy 2007 and have considered potential noise generating activities on the site including noise from car park activity and vehicle movements, the mechanical plant, deliveries, rubbish

collection and compactor activity. To support the assessment, Sonus have undertaken an assessment of the existing acoustic environment, which included noise logging over a 5-day period.

Sonus have advised that, subject to the following acoustic treatments and operational procedures, the proposed development will satisfy and/or exceed the requirements of the Environment Protection (Noise) Policy 2007:

- Implementation of all measures described in the ALDI SA *Delivery and Loading Procedures* provided at **Appendix 7** which includes measures such as the turning off of refrigeration and reversing beepers prior to entering the site, minimising truck movements on site and turning off the truck during unloading;
- Restrict the hours of rubbish collection from the site and any use of the refuse area to the hours between 9.00am and 7.00pm on a Sunday or Public Holiday, and 7.00am and 7.00pm on any other day;
- Construct fences as follows:
 - » 3.0-metre-high fence to the rear (west) of the site, extending for the length of the loading dock to the northern (side) boundary, with a small section on the northern (side) boundary where it will meet the existing wall of the Motel, for the extent shown in green in Figure 6.4. The rear section will be set 1.5 metres inside the property boundary;
 - » A combination of 1.8-metre-high and 2.4-metre-high fence along the southern boundary (and for a small portion between the southern boundary and building), for the extent shown in red and yellow in Figure 6.4; and
 - » All fences should be constructed from a minimum of 0.42 BMT sheet steel (Colorbond® or similar) or a material with the same or greater surface density (kg/m^2) and an airtight seal should be achieved at all junctions.

In accordance with the plans provided in **Appendix 3** the proposed fencing scheme either achieves or exceeds these recommendations.

- In accordance with the architectural plans provided in **Appendix 3**, construct a barrier around the roof mounted mechanical plant to the extent shown in blue in Figure 6.4. The barrier should be a minimum height of 1.0 metres above the tallest unit and the barrier should be constructed from a minimum of 0.42 BMT sheet steel (Colorbond® or similar) and sealed airtight at all junctions, noting that a small gap may be required at the roof join for drainage purposes. The barrier may be open on the northern side for ventilation purposes.
- Install acoustic absorption material to the extent of the mechanical plant screen and rear fence as shown in purple in Figure 6.4, in accordance with the construction detail shown in Figure 6.5.

Figure 6.4 Location of Recommended Acoustic Treatments (Source: Sonus)

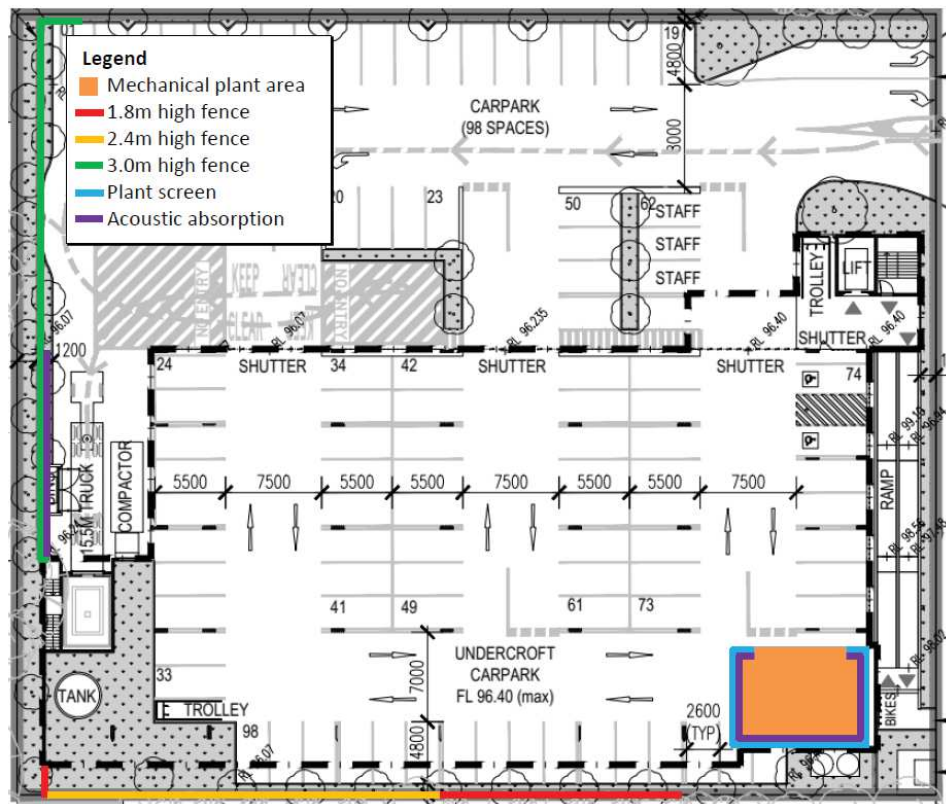
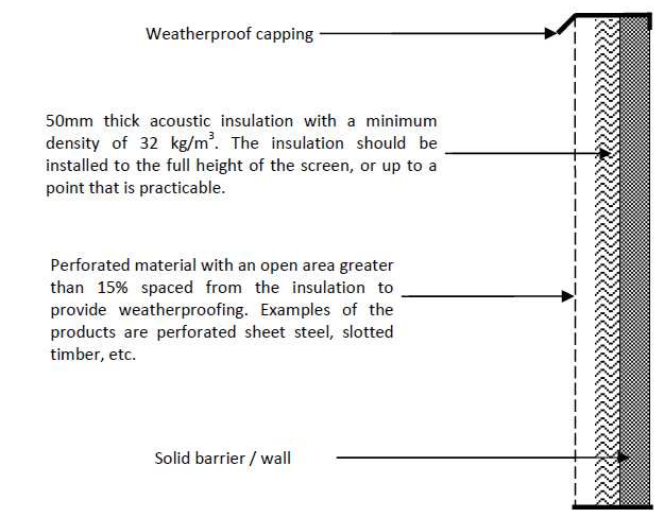


Figure 6.5 Acoustic Absorption Detail – Mechanical Plant Screen



Sonus concludes that with the above-mentioned acoustic measures in place, the development will be designed such that it will not detrimentally affect the amenity of the locality or cause unreasonable interference by the emission of noise, thereby achieving compliance with the Noise Policy, as well as PDC 60 of the Development Plan:

PDC 60 *Development that emits noise (other than music noise) should include noise attenuation measures that achieve the relevant Environment Protection (Noise) Policy criteria when assessed at the nearest existing noise sensitive premises.*

In addition, we also note that the proposed design and layout of the site, supported by the proposed acoustic treatments will significantly improve interface impacts as they relate to noise, vis-à-vis the existing arrangement for the Mitre 10 Store. Under current arrangements, loading and unloading occurs to the north and west of the existing building (adjacent the 'non-acoustically-treated' boundary fences) and includes a variety of activities including outdoor storage, use of forklifts and other equipment, loading/unloading of shop stock and loading/unloading of customer vehicles.

6.9.2 Light Spill

Although the specific location and nature of outdoor lighting is yet to be determined, all lighting will be designed in accordance with *Australian Standard 4282 – 1997 'Control of the obtrusive effects of outdoor lighting'*, to ensure lux levels and light glare does not unreasonably impact on the amenity of the locality. Additionally, all lighting within the car park will be designed in accordance with *Australian Standard AS 1158 – Lighting for roads and public spaces*.

We also note this has been a consistent approach with other ALDI sites that have been approved in ALDI, and there have been no subsequent issues raised regarding light spill or glare.

Accordingly, it is anticipated that lighting operating from within the site will not adversely impact on the amenity of the locality and can be managed through appropriately worded conditions attached to the Development Plan Consent.

6.9.3 Odour

Having regard to the nature of the proposed land use, potential odour impacts are restricted to waste management practices.

Section 4.8 of this Planning Statement outlined the overall waste management strategy associated with the development, and accordingly, there are no anticipated odour impacts having regard to the streamlined waste management practices that are proposed, or the types of wastes that are expected to be generated.

A dedicated waste storage area is provided to the rear of the site. This is located within the loading dock area and is to be screened from public view.

The waste storage area is also to be setback 1.5 metres from the rear property boundary, separated from the adjoining residential sites to the west by fencing (including the 3-metre-high acoustic fence) and landscaping.

The waste storage area will be located approximately 20 metres from the nearest residential dwelling located to the west.

Accordingly, there are no anticipated odour impacts associated with the proposed development and the proposed storage and management arrangements associated with waste accords with PDC 57(b):

PDC 57 *The location and design of areas and facilities for:*

(b) the storage and removal of waste materials should be unobtrusive, and not cause nuisance.

6.10 Energy Efficiency

The design of the development has been assessed against the following Energy Efficiency provisions of the Development Plan:

Energy Conservation

PDC 30 *Development should be designed to take advantage of renewable sources of energy, especially solar energy, and minimise use of non-renewable sources of energy, including fossil fuels, in construction, and for heating, cooling, lighting and other purposes.*

PDC 31 *Development should conserve energy, while providing adequate thermal comfort for the occupants of buildings, by:*

(a) optimal orientation and shape of allotments and the sites of buildings, to maximise access within those buildings to incident solar radiation (especially in relation to habitable rooms in dwellings);

(b) the orientation of dwellings and the arrangement of internal spaces and landscaping, to take advantage of climatic factors and maximise absorption of heat from the sun in winter on northern and eastern building surfaces;

(c) the pitching and orientation of roofs to facilitate the efficient use of solar energy collection;

(d) the sizing, orientation and shading of windows to reduce summer heat load and take advantage of winter sun;

(e) the use of deciduous trees, pergolas, eaves, verandas and awnings, to allow penetration of heat from the sun in winter and to provide shade in summer;

(f) the use of energy-efficient building materials and the incorporation of energy-efficient methods of lighting, air and water heating and cooling;

(g) the provision of adequate ceiling and wall insulation; and

(h) openings being designed to maximise the potential for cross-ventilation and use of cooling breezes in the summer months.

The following design features will reduce the buildings reliance on non-renewable resources:

- A flat roof with northerly aspect to accommodate photovoltaic cells which will not result in unreasonable overshadow to adjoining properties or have a visual impact;
- Small, high level windows with associated sunshade screens on the northern elevation;
- Open sides to the under-croft car park to prevent the need for mechanical ventilation; and
- Deciduous trees incorporated within the landscape palette which will assist to provide additional shade in summer, and allow sun access during colder periods.

Further to the above discussion on building design and building form, the proposal offers a contemporary building form which will, in our opinion significantly improve the visual amenity of the site and its immediate surrounds.

7. Conclusion

This development application seeks to establish an ALDI Store within a Local Centre Zone of the City of Burnside. Given the existing and previous use of the site as a 'shop', the location of the site and its ability to provide convenient access for customers and delivery trucks, the site is well suited for its intended use for retail development.

Following an inspection of the subject site and locality, a review of the proposed plans and associated documentation accompanying the application and a detailed assessment of the proposed development against the relevant provisions of the Burnside (City) Development Plan, we have formed the opinion that the proposed development represents appropriate and orderly development that deserves favourable consideration for approval. More specifically:

- The proposed land use is an appropriate form of development within the Local Centre Zone without compromise to the function or hierarchy of other centre zones;
- The ALDI Store will provide a range of groceries and other shopping products which will serve the needs of the local and broader community while also providing competition to other supermarkets which will help to drive down prices;
- The building features a contemporary design which, combined with the proposed materials and finishes, appropriately responds to the existing character of the locality and will enhance the overall appearance and function of the subject site;
- The under-croft parking area has been designed to integrate with the design of the building, is well screened from the public realm and will not have a negative impact on amenity;
- The proposed excavation works and 'cutting' of the ALDI Store into the subject site reduces the visual impact and height of the building, and reduces overshadowing to the adjoining site to the south to achieve compliance with quantitative parameters of the Development Plan relating to overshadowing;
- The proposal will not result in unreasonable interface impacts to residential properties or other sensitive land uses;
- The proposed landscaping will significantly improve the aesthetics of the proposed development and provide an attractive environment around the site, as well as providing appropriate address to adjoining residential sites to the south and west;
- Projected traffic generation and distribution will not adversely impact on the intended function and/or capacity of the adjacent road networks;
- The site has been designed to accommodate safe and convenient vehicle access, egress and circulation, including service vehicles that will enter the site in forward direction, safely reverse into the loading dock and exit the site in a forward direction;

- The supply of car parking spaces will satisfy the anticipated demand generated by the proposed ALDI Store;
- The development is located close to existing public transport routes, has been integrated with the existing pedestrian network, and provides safe and convenient access for pedestrians from the public realm and car park; and
- The proposed advertising signage will clearly and concisely identify the intended use, while also achieving consistency with the architectural style of the building and complementing the established character of the locality.

The proposed development is therefore highly aligned with the most relevant provisions of the Burnside (City) Development Plan and warrants Development Plan Consent, subject to reasonable and relevant conditions.



Appendix 4. Stormwater Management Plan (FMG)



Stormwater management report

JOB NUMBER:	S45999 - 266918
CLIENT:	Aldi Stores
SITE:	499 Portrush Road, GLENUNGA, SA 5064
DATE:	13/09/2019
REVISION:	1

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SYDNEY

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Document Status





REV NO.	STATUS	AUTHOR	REVIEWER			APPROVED FOR ISSUE		
			NAME	SIGNATURE	DATE	NAME	SIGNATURE	DATE
0	Preliminary	A. Vingelis	F.G. Barrie		20/8/2019	J.Clapp		20/8/2019
1	Preliminary	A. Vingelis	F.G. Barrie		13/9/2019	J.Clapp		13/9/2019

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Introduction

FMG Engineering (FMG) is engaged by Aldi Stores to prepare a stormwater management plan for a proposed Aldi Store at 499 Portrush Road, Glenunga. This report is preliminary in nature and was undertaken prior to preliminary design and was undertaken for planning purposes.

This stormwater management plan has been prepared in accordance with the City of Burnside's (Council's) Development Plan. It is understood that Council will review the stormwater strategy in this report and that Council may require further information and design prior to obtaining necessary planning approvals.

Project understanding

Existing site conditions

The site is located at 499 Portrush Road, Glenunga as shown in Figure 1.



Figure 1 Site location

The site is currently tenanted by a commercial development and is considered 100% impervious under pre-development conditions. The site is located on Portrush Road which is under the care and management of DPTI.

A review of Council's GIS stormwater data shows there is a 750 mm diameter pipe, which runs north, in Portrush Road. Additionally, a review of Google street view shows that there is an existing side entry adjacent to the site (assumed to connect into the 750 mm diameter trunk main).



Figure 2 Side entry pit in Portrush Road (Googlemaps)

Project understanding

The existing site is in fill and accordingly grades to the north east corner via a series of spoon drains towards Portrush Road. Under the proposed development the site will be in cut and grade to the east (rear) of the allotment and the roof area will drain to the south of the site.

The proposed development will comprise the following:

- Multi-level ALDI store (1 266 m²)
- Undercroft car parking and outdoor carparking (approximately 98 spaces)
- Truck loading bay
- Landscaping.

Stormwater management requirements

FMG has prepared this stormwater management plan in accordance with the Council's Development Plan and general DPTI requirements. This is general in nature and it is understood that Council / DPTI may have alternate and or additional requirements.

It is assumed the following is required:

- That Council will not require detention storage on the basis that post development fraction impervious will not increase from pre-development conditions
- That DPTI requires that no stormwater runoff is permitted to be discharged as surface flow for all events up to and including the 100-year Average Recurrence Interval (1% Annual Exceedance Probability) storm event
- Stormwater runoff from all areas will be diverted from the site directly to the underground drainage infrastructure at rates not exceeding the minor 5-year ARI (20% AEP) storm event
- Stormwater runoff should minimise the discharge of sediment, suspended solids, organic matter, nutrients, bacteria, litter and other contaminants
- Stormwater runoff for all events up to and including the 100-year ARI storm event should not be diverted towards private property
- The minor drainage network shall have a 5-year ARI (20%AEP) level of service.

Stormwater management plan

The stormwater management plan is shown in Appendix B and is described as follows:

- It is proposed that stormwater from the roof areas will be diverted to above ground tanks which will detain and limit flows from the development to the pre-development minor storm event to Portrush Road
- The above ground (roofwater) tanks will be capable of detaining all events up to and including the 20-year ARI storm event (equivalent to the capacity of the eaves gutters)
- Stormwater runoff from the hardstand areas and overflow from the gutters will be diverted to the north western corner of the site and it will not be feasible to drain the site via gravity and will be diverted to an underground pump
- Stormwater from the underground detention tank will be capable of storing runoff for all events up to and including the 100-year ARI storm event such that no surface flows are diverted towards the north west to adjacent private property
- Stormwater runoff from the underground stormwater tank will be pumped directly to the underground drainage network in Portrush Road at rates not exceeding the 5-year ARI (20% AEP storm event)
- AS/NZS requires that pump storage shall be not less than the volume able to be pumped in 30 minutes and the volume generated in the 10-year ARI 120-minute event. In this case this storage volume will be exceeded as the storage was sized such that no runoff for all events up to and including the 100-year ARI (1% AEP) will be diverted towards private property
- An approved duty pump with standby is recommended for redundancy purposes
- A gross pollutant trap or similar approved proprietary device is proposed to treat stormwater runoff prior to discharging to the underground drainage network.

Hydrological and hydraulic assessment

Catchment plan

Under pre-development conditions the site is considered 100% impervious. A summary showing the post development impervious fractions are shown in Table 1. The catchment plan is shown in Appendix B of this report.

Table 1 Catchment summary

CATCHMENT	AREA (M ²)	% IMPERVIOUS
Roof	1 585	100
Hardstand area	2 754	95

Hydrological parameters

Rainfall data was sourced from the AR&R Data Hub using latitude and longitude co-ordinates.

The DRAINS modelling hydrological input parameters are outlined below:

- Impervious area depression storage: 1 mm
- Supplementary area depression storage: 1 mm
- Pervious area depression storage: 5 mm
- Antecedent Moisture Condition: 3.0 which is considered typical of the heavy clay soils expected in the area

DRAINS modelling results

The results of the DRAINS modelling for the 5-year ARI (20% AEP) storm event are summarised in Figure 3. During the 5-year ARI (minor) storm event a peak flow of 28 L/s is from the roof catchment and 40 L/s from the hardstand area. This is considered the maximum allowable discharge to the underground drainage network.

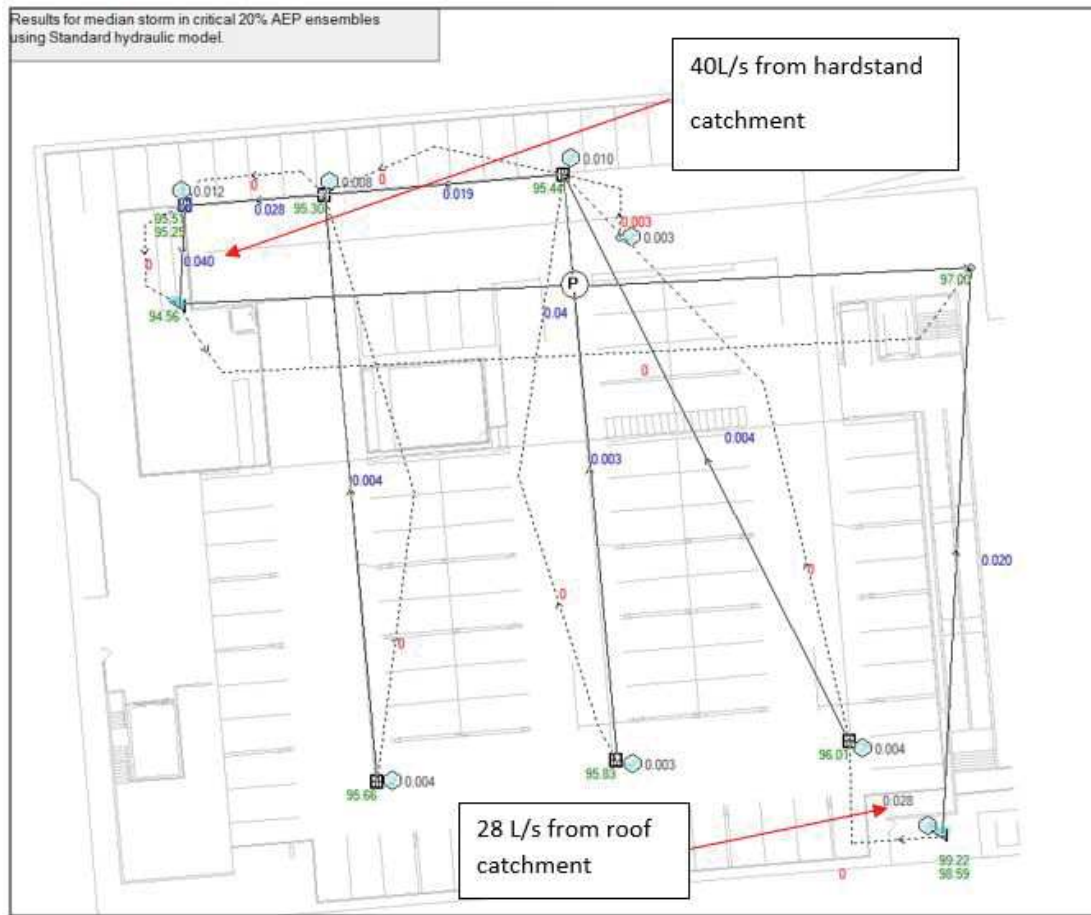


Figure 3 5-year ARI (20% AEP) storm event

DRAINS modelling during the 20-year ARI (5% AEP) storm event was undertaken to size the tanks for the roof water. During this event 7.5 kL will be stored in an above ground tank and a 110 mm orifice will be required to limit peak flows to the 5-year ARI minor storm event. The DRAINS modelling results during the 20-year ARI (5% AEP) storm event and the storage relationship in the tank is shown in Figure 4 and Figure 5.

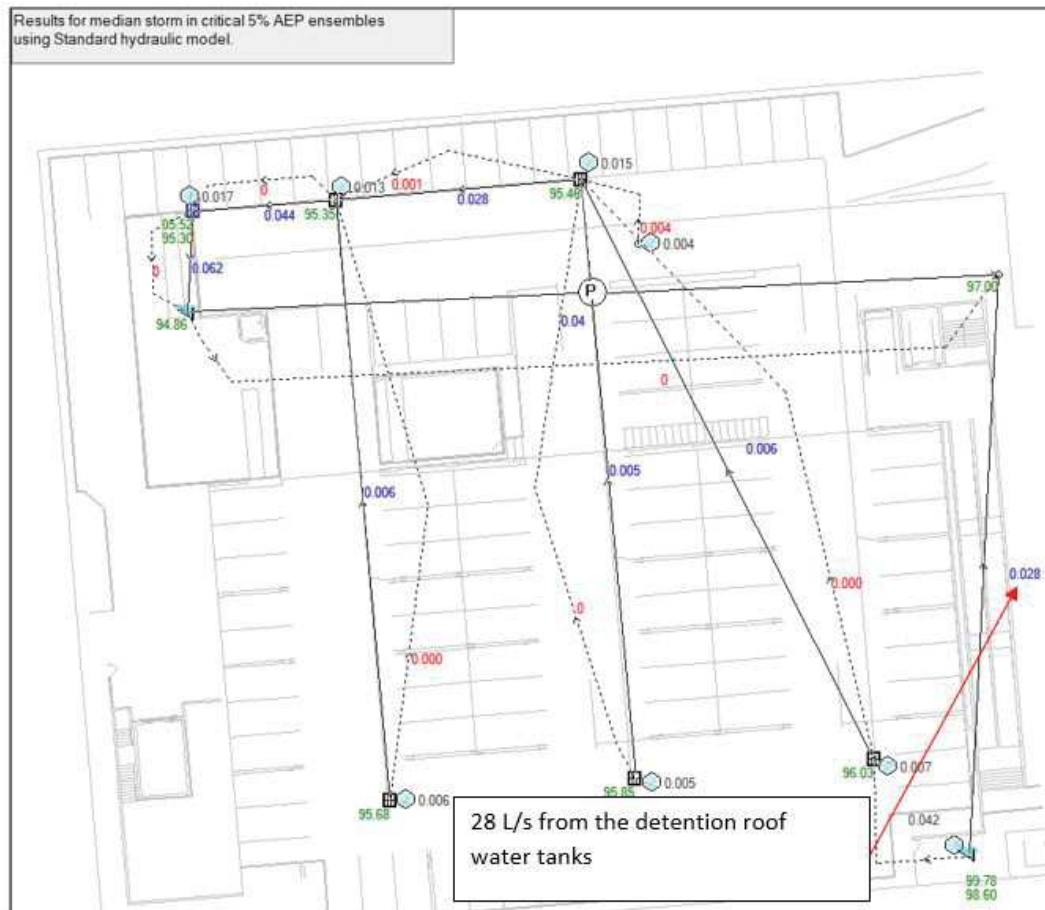


Figure 4 DRAINS modelling results during the 20-year ARI (5% AEP) storm event

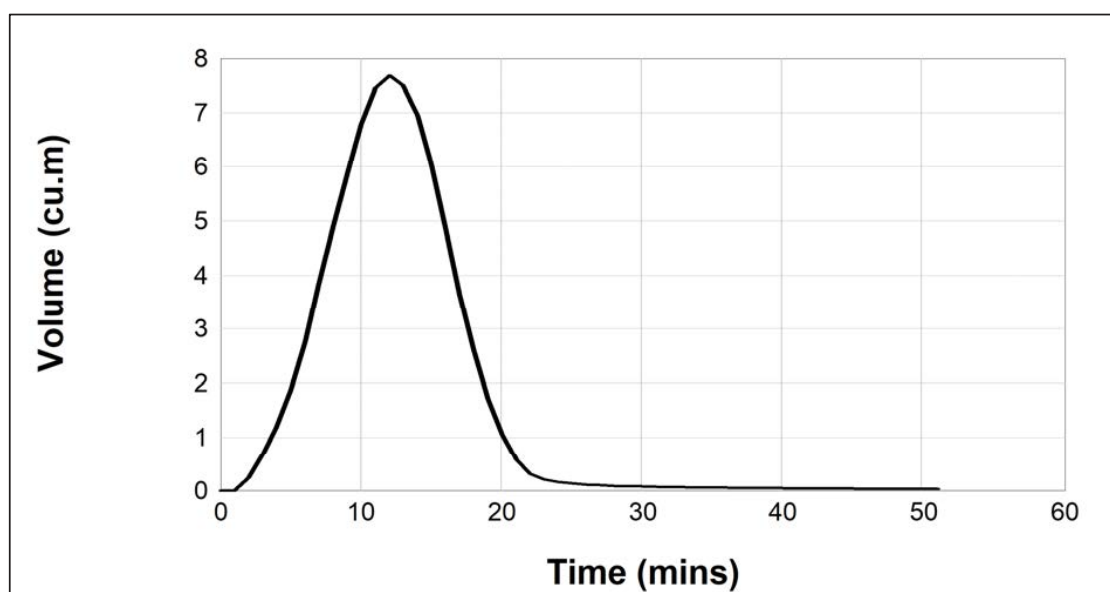


Figure 5 DRAINS storage relationship in the roofwater tank during the 20-year ARI (5% AEP) event

During the 100-year ARI storm event approximately 28 kL of underground storage will be detained in the underground tank such that no stormwater runoff will be diverted towards adjacent private property and the flows to the underground drainage network will be limited to the minor 5-year ARI storm event.

The results of the DRAINS modelling during the major 100-year storm event demonstrates that no stormwater will be diverted towards adjacent private property.

The results of the DRAINS modelling are shown in Figure 6.



Figure 6 DRAINS modelling results 100-year ARI (1% AEP) event

A 26 kL underground Tank will be required to store the 100-year ARI storm event, the storage relationship during the 100-year ARI storm event is shown in Figure 7.

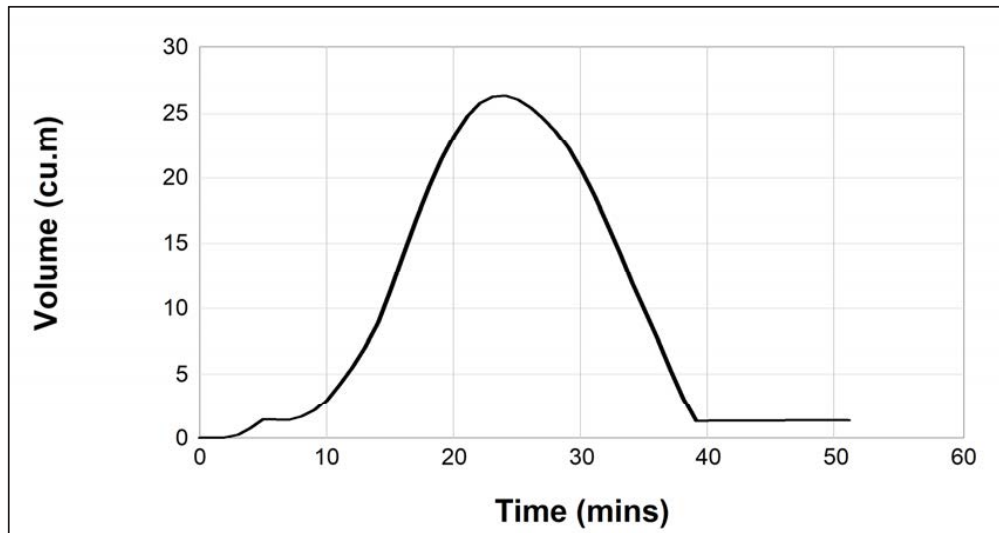


Figure 7 DRAINS 100-year ARI storage relationship

Stormwater quality

Councils development plan states:

'Development should include stormwater management systems to minimise the discharge of sediment, suspended solids, organic matter, nutrients, bacteria, litter and other contaminants to the stormwater system'.

It is proposed that stormwater runoff from the hardstand area will be treated via a proprietary gross pollutant trap (GPT) or similar approved device. The 3-month ARI estimated treatable flow rate from the hardstand area is 12 L/s. The Rocla First Defense FD-HCM will treat this flow rate.

Conclusion

This stormwater management plan was prepared prior to planning approval. This stormwater management plan was prepared in accordance with Council's development plan.

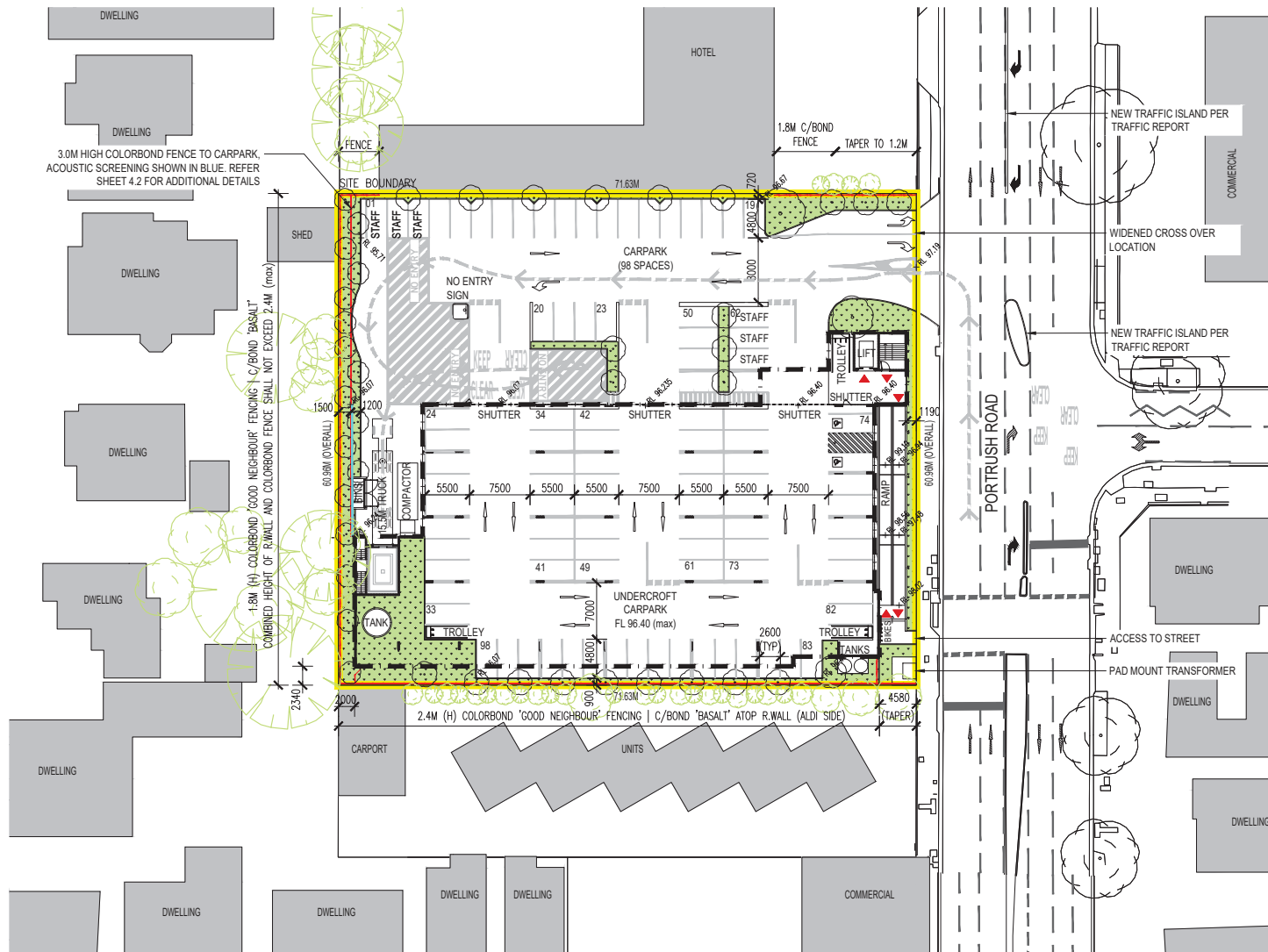
Under the proposed plan stormwater runoff will drain to the west of the site and be diverted to an underground tank and pumped to the existing side entry pit located in Portrush Road. Stormwater from the roof area will be diverted to above ground tanks and discharged to the existing side entry pit in Portrush Road at rates not exceeding the 5-year ARI pre-development peak flows.

Stormwater from the hardstand area will be treated by a gross pollutant trap or similar approved device.

This stormwater management plan demonstrates that the Council's general requirements for stormwater management may be met. It is recommended that Council provide feedback on the proposed stormwater management strategy prior to undertaking detailed civil design.

Appendix A

Site plan



NOTES

ORIGINAL A3

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01 LOWER SITE PLAN
1:500 PROPOSED

CLIENT
ALDI STORES



PROJECT
ALDI GLENUNGA
No. 498 PORTRUSH ROAD
GLENUNGA S.A. 5064

DRAWING
LOWER SITE PLAN

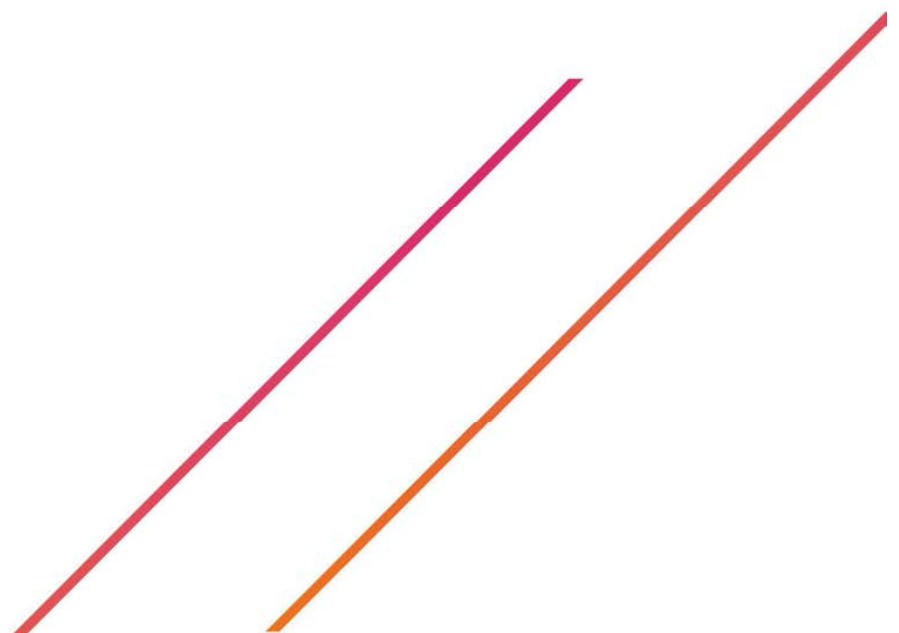
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1108 M4 Barker Road Stirling
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p: 08 8339 8008
f: 08 8339 2004
info@nielsenarchitects.com.au
www.nielsenarchitects.com.au

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Appendix B

Catchment plan





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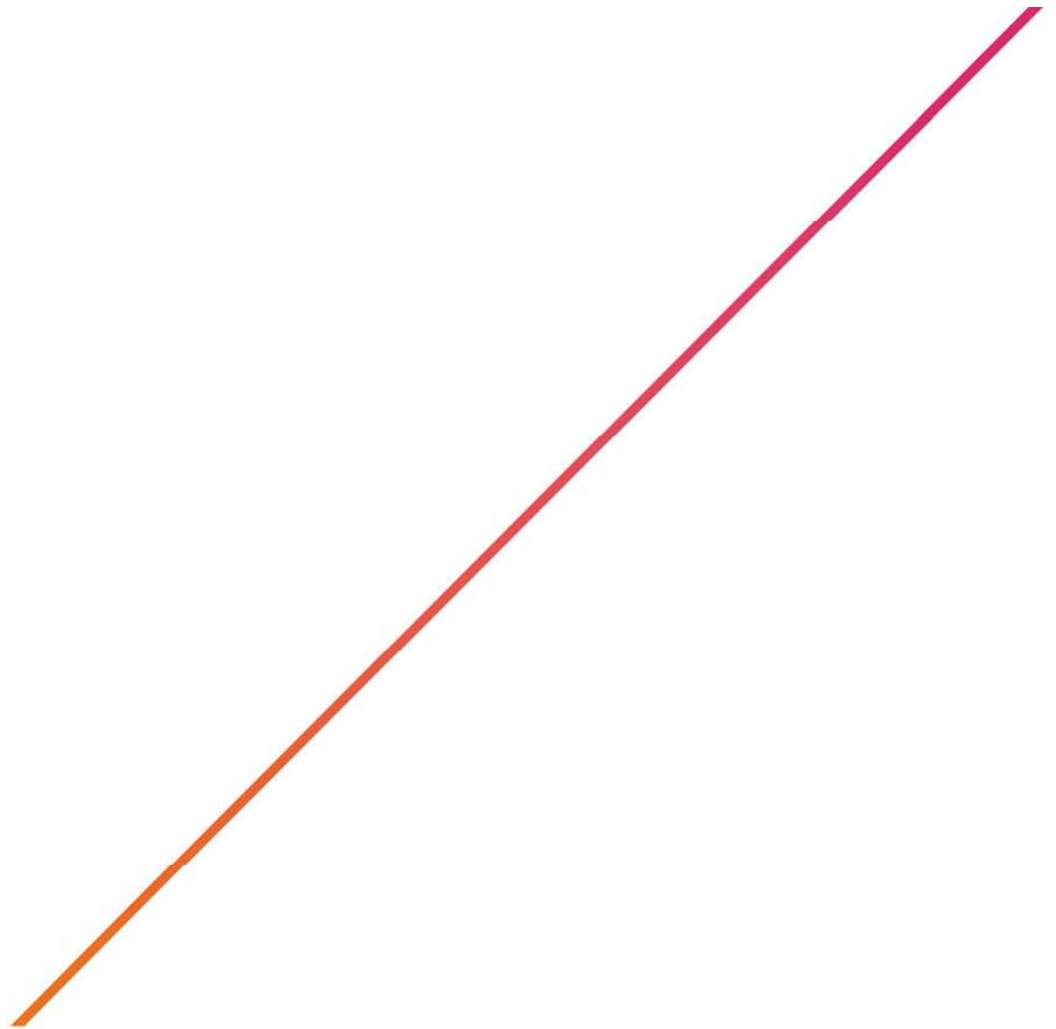
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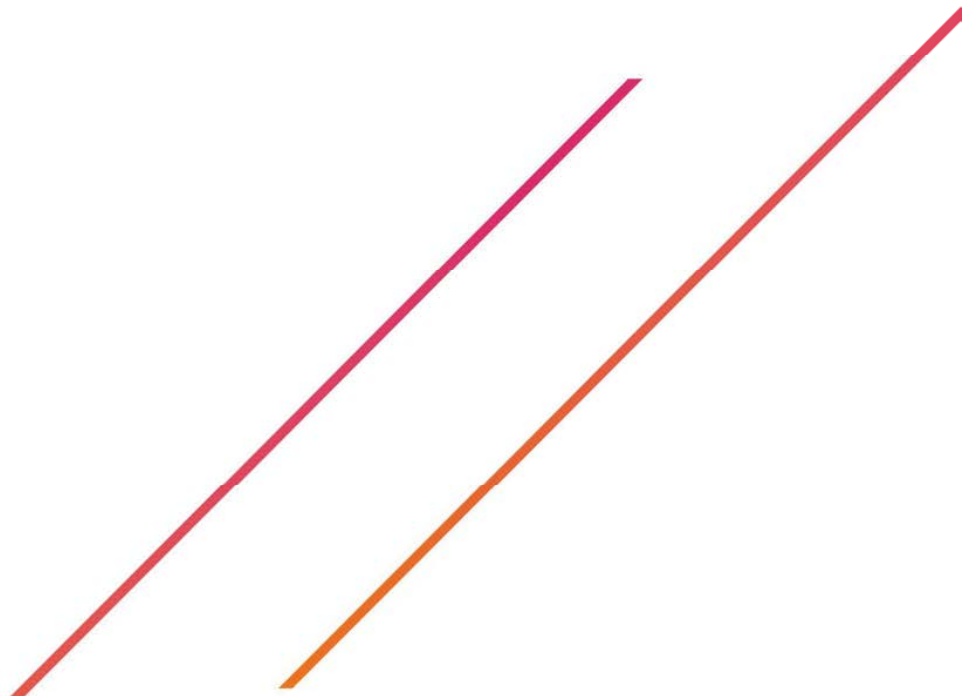
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ALDI STORES
PROJECT TITLE
ALDI GLENUNGA
SITE ADDRESS
499 Portrush Road,
GLENUNGA, SA 5064X
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CATCHMENT PLAN

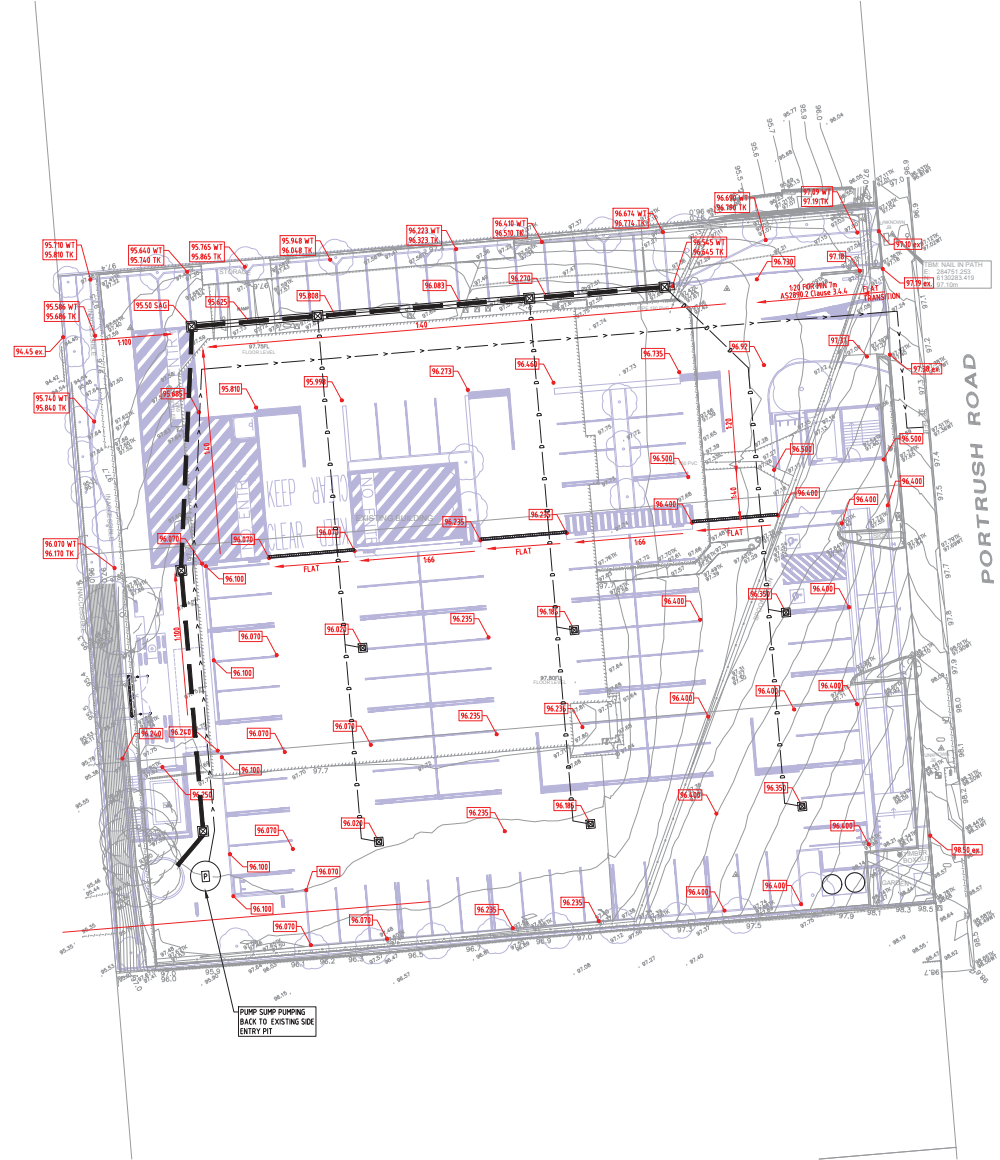
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Appendix C

Preliminary civil plans





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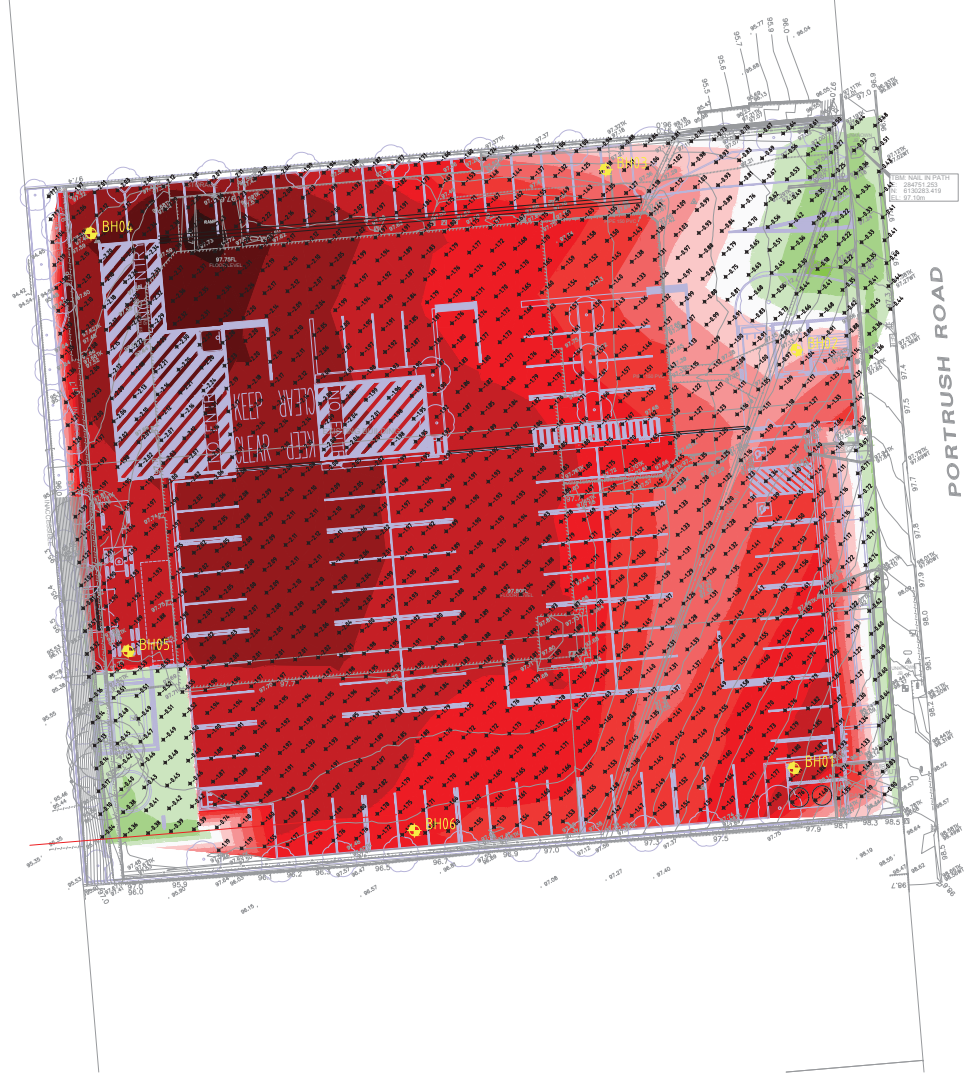
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CLIENT: ALDI STORES
PROJECT TITLE: GLENUNGA STORE
SITE ADDRESS: 499 PORTRUSH ROAD, GLENUNGA, SA
DRAWING TITLE: CONCEPT SITE GRADING

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FG	FG	CC
SCALE	1:200 @ A1	DATE EXPIRES
DATE	15.08.2019	01.08.2019
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LEGEND

BHO5 BOREHOLE LOCATION

DEPTH OF CUT(-) / FILL(+)			
Nr.	LOWER DEPTH (m)	UPPER DEPTH (m)	COLOR
1	-0.00	-0.40	
2	-0.40	-0.80	
3	-0.80	-1.20	
4	-1.20	-1.60	
5	-1.60	-2.00	
6	-2.00	-2.40	
7	-2.40	-2.80	
8	-2.80	-3.20	
9	-3.20	-3.60	
10	-3.60	-4.00	
11	-4.00	-4.40	
12	-4.40	-4.80	
13	-4.80	-5.20	
14	-5.20	-5.60	
15	-5.60	-6.00	

VOLUME NOTES:
APPROX VOLUME OF MATERIAL TO BE REMOVED (BASED ON PROPOSED SURFACE LEVELS LESS 30mm PAVEMENT THICKNESS ACROSS SITE) = 4750m³
COMPACTION/EXPANSION FACTORS AND STRIPPING OF EXISTING SITE HAVE NOT BEEN ACCOUNTED FOR.

SCALE 0 2 4 6 8 10 METRES

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PROJECT NO.
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PROJECT NO.
S45999-266918

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SK-C02

D



GRATED INLET ON ROULA FIRST
OFFICE VORTEX SEPARATOR SINCE OR
APPROVED EQUIV INSTALLED TO
MANUFACTURERS SPECIFICATIONS

PROVIDE 2M³ PRECAST UNDERGROUND
STORAGE TANK WITH PACKAGED PUMP
STATION DETAILS T.B.C.
PUMP TO HAVE 40L/A DUTY. LOCATION
T.B.C.

PUMP 40L/A TO EXISTING
SIDE ENTRY PIT

PROVIDE 8M³ OF ABOVE GROUND
DETENTION STORAGE WITH TWO
OFFICE CONNECTED TO U/G
STORMWATER NETWORK IN
PORTRUSH ROAD. USE 2 X 44L
AQUAPLATE TANKS OR SIMILAR
APPROVED.

LEGEND

→ DIRECTION OF SURFACE FALL

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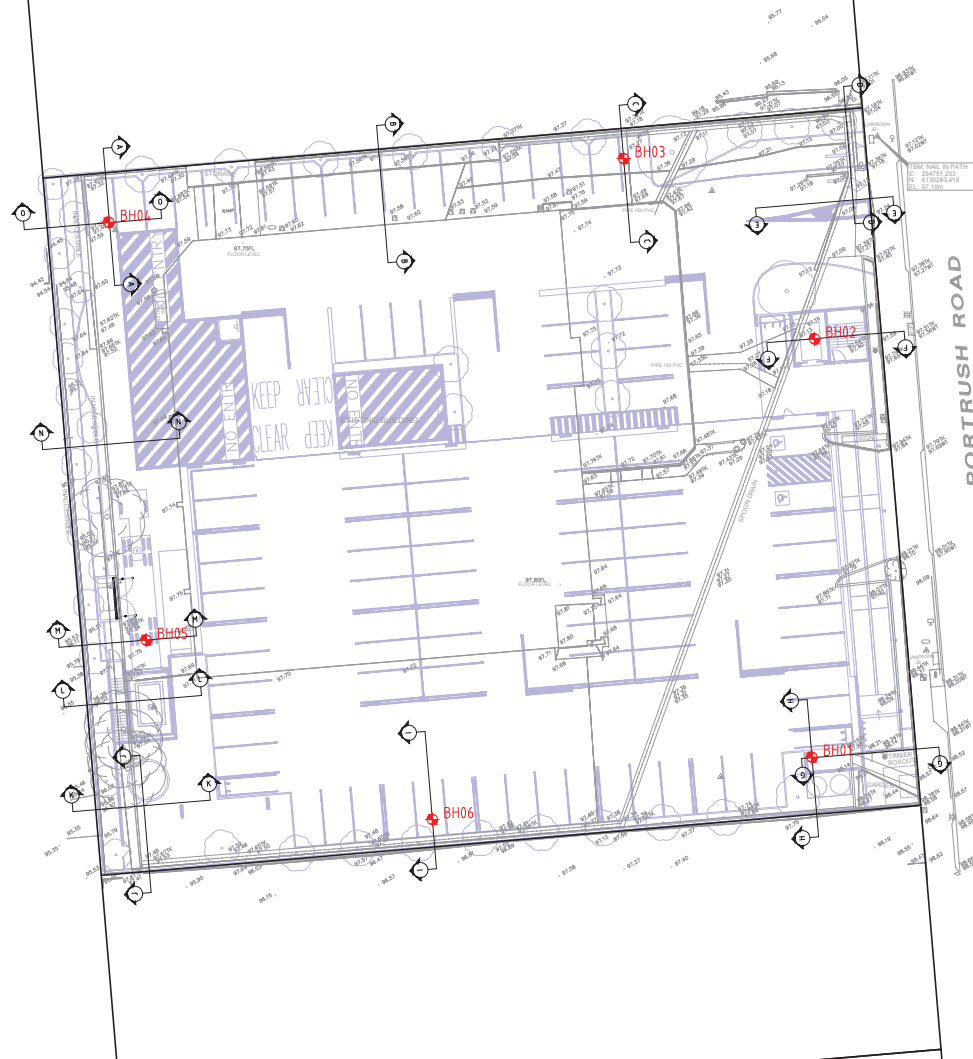
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CLIENT
ALDI STORES
PROJECT TITLE
GLENUNGA STORE
SITE ADDRESS
4/99 PORTRUSH ROAD
GLENUNGA, SA
DRAWING TITLE
STORMWATER MANAGEMENT

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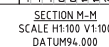
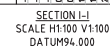
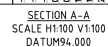
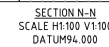
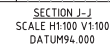
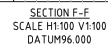
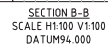
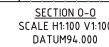
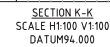
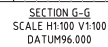
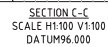
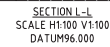
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ALDI STORES
PROJECT TITLE
GLENUNGA STORE
SITE ADDRESS
499 PORTRUSH ROAD
GLENUNGA, SA
DRAWING TITLE
CONCEPT SITE SECTION PLAN

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SCALE	1:200 @ A1	DATE EXPIRES	01.08.2019
DATE OF ISSUE	26.08.2019	ISSUED BY	SK-04
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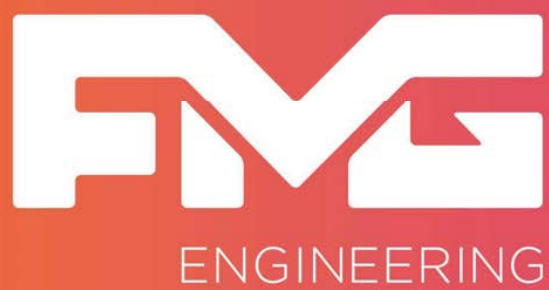
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Appendix 5. Environmental Noise Assessment (Sonus)

ALDI Glenunga

Environmental Noise Assessment

September 2019

S4217.30C5

sonus.

Chris Turnbull
Principal
Email: ct@sonus.com.au
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Document Title : ALDI Glenunga
Environmental Noise Assessment

Document Reference : S4217.30C5

Date : September 2019

Prepared By : Chris Turnbull, MAAS

Reviewed By : Jason Turner, MAAS

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INTRODUCTION

An environmental noise assessment has been made of the proposed ALDI supermarket to be located at 499 Portrush Road, Glenunga.

The subject site is located amongst existing commercial and residential land uses. The closest noise sensitive receivers are the residences, units, and hotel located immediately adjacent to the site. The subject site locality and nearby noise sensitive locations are shown in Appendix A.

The assessment considers noise levels at sensitive locations from activity at the proposed facility. Specifically, the following noise sources have been considered:

- General car park activity and vehicle movements;
- Delivery activity;
- Compactor operation;
- Operation of mechanical plant servicing the facility; and
- Rubbish collection.

The assessment has been based on the following:

- *Nielsen Architects* planning drawing set for "ALDI GLENUNGA", project number "2353SL1", dated September 2019, including drawings "DA01.1" through "DA06.3";
- Site inspection and background noise measurements conducted at the subject site from 22 to 27 August 2019;
- Implementation of all measures described in the ALDI SA, "Delivery & Loading Procedures" (Appendix D) to minimise noise. The procedure includes measures such as turning off the refrigeration and reversing beepers prior to entering the site, minimising truck movements on site, and turning off the truck during unloading; and
- The understanding that:
 - Trade will not occur at the site before 7:00am or after 10:00pm;
 - Delivery trucks will access the ALDI site at any time during the day or night;
 - All delivery trucks accessing the ALDI site will have down swept (low level discharge) exhausts, attenuated compressed air release and a noise level equivalent to that measured at the ALDI Derrimut distribution centre (Victoria, 2014); and

- The ALDI store's refrigeration units will operate at all times during the day and night, however air conditioning, fans and the compactor will not operate before 7:00am or after 10:00pm.

This assessment summarises the prediction of noise from the proposed ALDI, compares the predictions with the relevant criteria, and provides recommendations for acoustic treatment to ensure that the noise from the proposal does not detrimentally affect the amenity of the locality.

CRITERIA

Development Plan

The proposed site, northern hotel, and southern units are all located within the *Local Centre Zone* within the *Burnside (City) Council Development Plan* (the Development Plan). The residences immediately west of the site, and on the opposite side of Portrush Road are within the *Glenunga North Policy Area 24* and *Linden Park Policy Area 21*, respectively, of a *Residential Zone*. The Development Plan has been reviewed and the following provisions are considered relevant to the noise assessment.

Council Wide Section - Amenity

OBJECTIVE

- 20 *The amenity of localities not impaired by the appearance of land, buildings and objects, or by noise, light, emissions, traffic or any other quality, condition or factor.*

PRINCIPLES OF DEVELOPMENT CONTROL

- 51 *Development should not take place in a manner which will cause nuisance, or detract from the amenity of the locality, by:*
(a) the emission of noise, ...

Noise Generating Activities

- 60 *Development that emits noise (other than music noise) should include noise attenuation measures that achieve the relevant "Environment Protection (Noise) Policy" criteria when assessed at the nearest existing noise sensitive premises.*
- 61 *Development with the potential to emit significant noise (e.g. industry) should incorporate noise attenuation measures that prevent noise from causing unreasonable interference with the amenity of noise sensitive premises.*

Environment Protection (Noise) Policy 2007

Principle of Development Control 60 from the Development Plan references the Environment Protection (Noise) Policy 2007, which provides goal noise levels to be achieved at noise sensitive locations from general activity at a site and specific provisions for other activity such as rubbish collection.

The Policy is based on the World Health Organisation Guidelines to prevent annoyance, sleep disturbance and unreasonable interference on the amenity of an area. Therefore, compliance with the Policy is considered to be sufficient to satisfy all provisions of the Development Plan relating to environmental noise.

General Activity

The Policy provides goal noise levels to be achieved at noise sensitive locations based on the principally promoted land use of the Development Plan Zones in which the noise source (the development) and the noise receivers (the residences, hotel, and units) are located. Based on the land uses and the “development” nature of the project, the following goal noise levels are provided by the Policy to be achieved at noise receivers:

- Within the *Local Centre Zone*:
 - An average (L_{eq}) noise level of 57 dB(A) during the daytime (7am to 10pm); and,
 - An average (L_{eq}) noise level of 50 dB(A) at night (10pm to 7am).
- Within the *Residential Zone*:
 - An average (L_{eq}) noise level of 52 dB(A) during the daytime (7am to 10pm); and,
 - An average (L_{eq}) noise level of 45 dB(A) at night (10pm to 7am); and,
 - A maximum (L_{max}) noise level of 60 dB(A) at night (10pm to 7am).

When measuring or predicting noise levels for comparison with the goal noise levels of the Policy, penalties may be applied for each characteristic of tone, impulse, low frequency and modulation of the noise source, however this must be considered within the context the existing acoustic environment. The application of penalties is discussed further in the Assessment section of this report.

Rubbish Collection

The Policy deals with rubbish collection by limiting the collection hours to the least sensitive period of the day. Division 3 of the Policy requires rubbish collection to only occur between the hours of 9am and 7pm on

Sunday or public holiday, and between 7am and 7pm on any other day, except where it can be shown that the maximum (L_{\max}) noise level from such activity is less than 60 dB(A).

EXISTING ACOUSTIC ENVIRONMENT

The acoustic environment at the western boundary of the site has been measured with a noise logger over a period including weekdays and a weekend. The noise logging location, which is shown in Appendix A, represents the existing noise at the closest residence to the proposed loading area and compactor.

The noise logging was conducted from 22 to 27 August 2019. The results of the logging are provided in Appendix B, showing the background (L_{90}), average (L_{eq}) and maximum (L_{max}) noise levels in 15 minute intervals.

During the daytime, the noise level was controlled by local activity at the subject site such that the results are not considered representative of the acoustic environment at the nearest residences.

However, during the night time, the noise levels were controlled by traffic on Portrush Road. The lowest background noise level measured was 27 dB(A), average noise levels were more than 32 dB(A), and maximum noise levels were typically above 65 dB(A). The noise levels are indicative of an area dominated by intermittent traffic at night.

ASSESSMENT

General Activity

Carpark Activity and Vehicle Movements

The noise from car park activity and vehicle movements on the site has been predicted, based on noise measurements conducted at similar sites. These measurements account for the noise associated with:

- car park activity such as people talking as they vacate or approach their vehicles, the opening and closing of vehicle doors, vehicles starting, vehicles idling, and vehicles moving into and accelerating away from their park position; and
- typical vehicle movements through a carpark.

Mechanical Plant

As is typical at the development application stage, the proposed cool room and air conditioning plant units have not yet been designed or selected. The assessment of the mechanical plant has therefore been based on previous noise measurements and procured data at similar facilities. The subsequent overall sound power level data for the following mechanical plant components are summarised in Appendix C:

- 3x Daikin RX(Y)Q12TY1A air conditioning units;
- 1x Daikin RX(Y)Q18TY1A air conditioning units;
- 1x Daikin RX(Y)Q20TY1A air conditioning units; and
- 1x condenser unit.

The noise level and any acoustic treatment associated with mechanical plant should be reviewed during the detailed design phase, should the final equipment selections have different sound power levels or should a different number of units be proposed to those specified within this report.

Deliveries

The noise from deliveries to the ALDI supermarket has been predicted based on specific noise measurements of ALDI trucks for each of the following processes, conducted in accordance with the ALDI SA "Delivery & Loading Procedures" (refer Appendix D):

- driving into a site;
- reversing into a loading dock and stopping the engine;
- unloading; and
- starting the engine, accelerating and driving out of the site.

The predictions of noise from deliveries have been based on a single delivery in any 15-minute period of the day or night.

Compactor

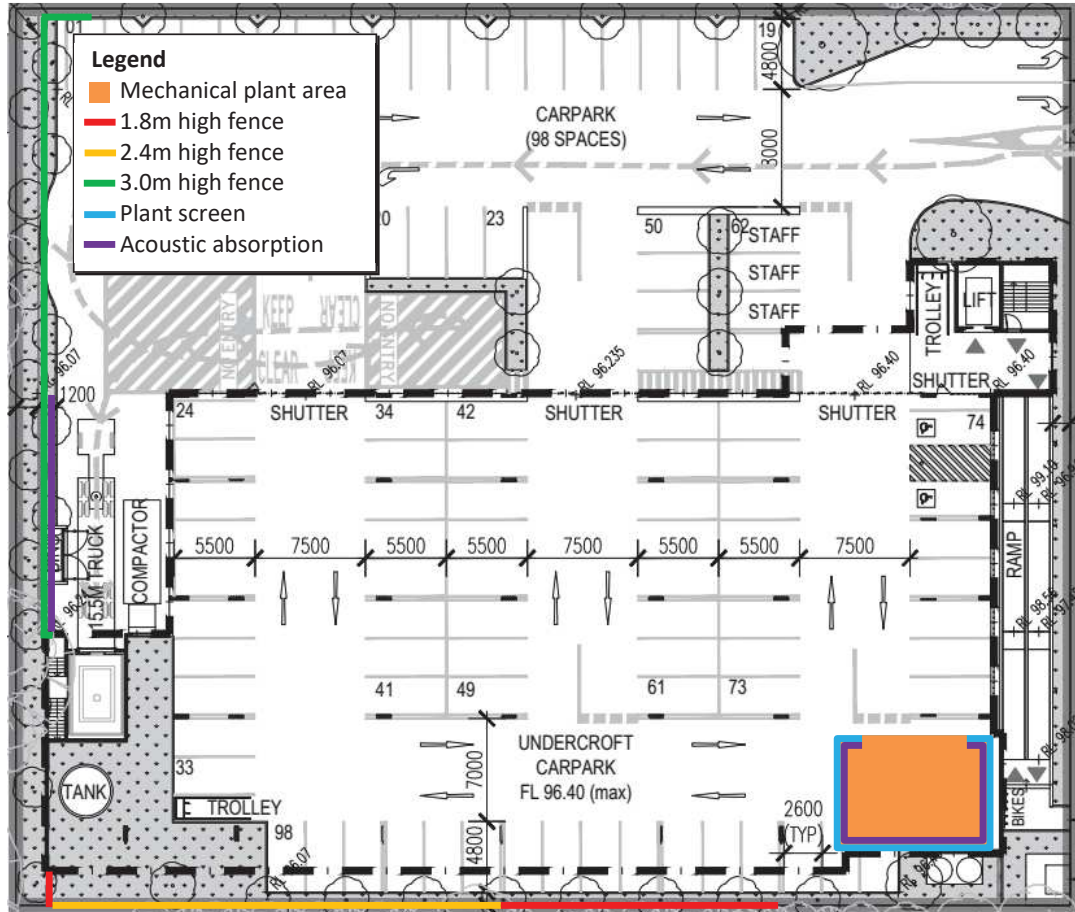
The noise from compactor has been determined based on measurements of an existing compactor operating at another ALDI store, conducted on 28 August 2019. The measured compactor had no specific acoustic attenuation and was measured continuously over a representative time period while operating. The assessment has considered the continuous operation of the compactor over any 15-minute period during the day or night.

Recommended Treatments

Based on the above, the goal noise levels of the Policy will be achieved with the following acoustic treatments:

- Construct fences which are 1.8m high, 2.4m high, and 3.0m high, for the extents shown in Figure 1 as **RED**, **YELLOW**, and **GREEN**, respectively. All fences should be constructed from a minimum of 0.42 BMT sheet steel ("Colorbond" or similar) or a material with the same or greater surface density (kg/m^2). An airtight seal should be achieved at all junctions, including at the ground, joins to other fences, and returns to the building. The heights specified for the 1.8m and 2.4m fences are to be achieved relative to the ground level of the adjacent residence on the boundary. The height of the 3.0m fence is to be relative to the level of the loading area.
- The fence along the northern boundary should be constructed to return to the adjacent masonry wall to provide an airtight seal at the junction;

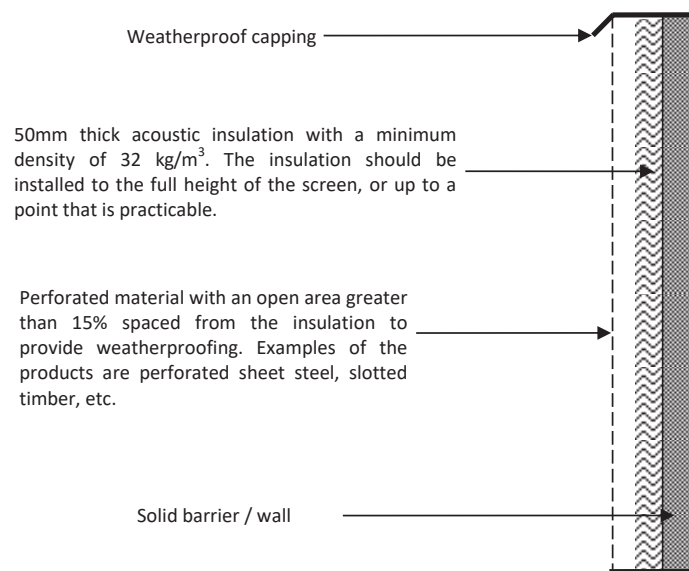
Figure 1: Site plan and recommended acoustic treatments.



- Locate all mechanical plant on the roof of the ALDI building within the designated area indicated in **ORANGE** on Figure 1;
- Construct the mechanical plant screens on the building roof and ensure a minimum height of 1.0m above the tallest unit for the extent shown in Figure 2 as **BLUE**. All screens should be constructed from a minimum of 0.42 BMT sheet steel ("Colorbond" or similar) or a material with the same or greater surface density (kg/m^2). The barrier should be sealed air tight at all junctions, noting that a small gap may be required at the roof join for drainage. The barrier may be open on the northern side for the extent shown for ventilation purposes.

- Install acoustic absorption material, such as 50mm thick polyester insulation with a minimum density of 32kg/m^3 in accordance with Figure 2, or a proprietary weather proof product with an “NRC” rating of 0.8 or greater (“Stratocell Whisper” or similar), to the full extent of the mechanical plant screen and fence, as indicated in **PURPLE** in Figure 1.

Figure 2: Barrier absorption construction detail.



- To ensure there is not unreasonable interference from noise from rubbish collection, it is recommended that the hours of rubbish collection from the site be restricted to the hours of Division 3 of the *Environment Protection (Noise) Policy 2007*. That is, only between the hours of 9:00am and 7:00pm on a Sunday or public holiday, and 7:00am and 7:00pm on any other day.

The current documentation has been reviewed and found to adequately incorporate all relevant acoustic treatment measures described above.

Combined Noise Levels

The predicted noise level from general activity at the facility operating concurrently has been determined and with the inclusion of the acoustic treatments detailed in this report will achieve the relevant requirements of the *Environment Protection (Noise) Policy 2007* during the day and night. The highest predicted noise levels at the closest noise-sensitive receivers are as shown in Table 1.

Table 1: Highest predicted average (L_{eq}) noise levels.

Zone	Predicted		Criteria	
	Day	Night	Day	Night
<i>Residential</i>	52 dB(A)	45 dB(A)	52 dB(A)	45 dB(A)
<i>Local Centre</i>	55 dB(A)	47 dB(A)	57 dB(A)	50 dB(A)

A penalty for modulating noise character from vehicles on the site is considered to be unwarranted at noise sensitive receivers which front Portrush Road given the likely high noise level from traffic. Vehicle movements on the public road corridor will occur more frequently, at higher speed, and at closer distances that onsite vehicle activity.

Additionally, a penalty for modulating noise character is not considered warranted at the units located to the south of the site on the basis that the predicted noise level at this location is controlled by constant noise sources such as rooftop mechanical plant, rather than modulating sources such as car parking activity.

Based on the low background noise level measured at the rear of the site, a 5 dB(A) penalty for modulating noise character from vehicle movements has been applied at all residences located to the west of the site which are separated from Portrush Road by a large distance, and in some circumstances, will be shielded by the new supermarket building and fence. This penalty has been included in all relevant noise predictions provided above.

Maximum Noise Level

The instantaneous maximum noise levels have been predicted for all residences within the Residential Zone. Predicted maximum noise levels have been based on measurements of the highest noise level produced by an ALDI delivery truck at a comparable ALDI site.

Existing maximum noise levels are in the range of 60 dB(A) to 70 dB(A) during the night time period and the predicted maximum noise level from the development is 63 dB(A). Therefore, the development will not contribute significantly to the existing maximum noise levels and will achieve the requirements of the Policy at all noise sensitive receivers in the vicinity.

CONCLUSION

An environmental noise assessment has been made of the proposed ALDI supermarket to be located at 499 Portrush Road, Glenunga.

The assessment considers noise at nearby sensitive locations from car park activity, vehicle movements, delivery activity, mechanical plant, and rubbish collection within the context of the surrounding acoustic environment to ensure the proposal does not adversely impact on the amenity of the locality.

The predicted noise levels from the development will achieve the relevant requirements of the *Environment Protection (Noise) Policy 2007* subject to the treatments in this report, comprising;

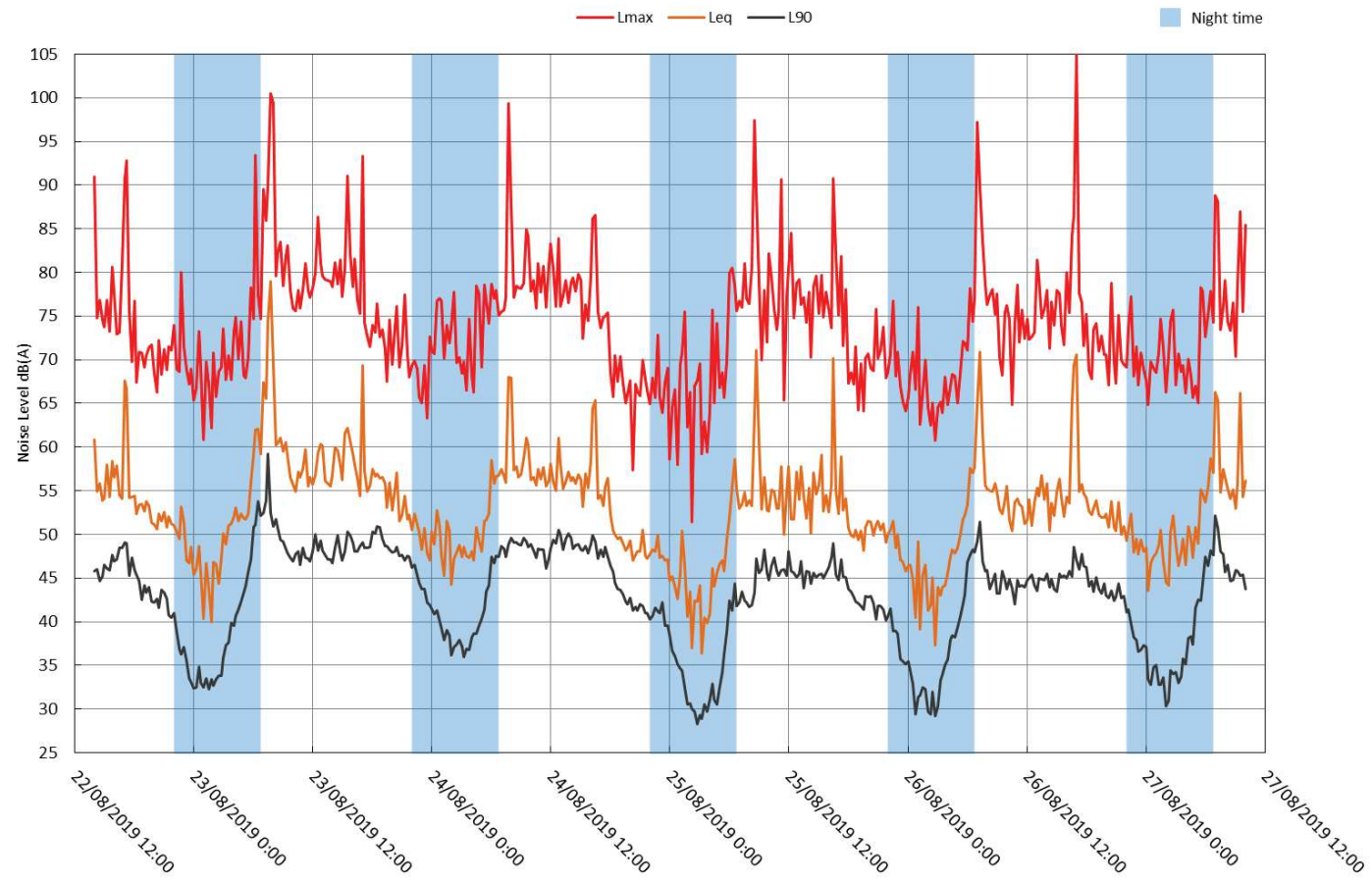
- specific fence and screen heights and constructions;
- Installation of acoustic absorption in specific locations;
- specific location of mechanical plant; and
- Restricting the times for rubbish collection.

It is therefore considered that the facility has been designed to *not take place in a manner which will cause nuisance, avoid unreasonable interference on amenity, and will not detrimentally affect the amenity of the locality by way of noise*, thereby achieving the relevant provisions of the Development Plan related to environmental noise.

APPENDIX A: Subject Site Locality and Nearby Noise Sensitive Locations



APPENDIX B: Existing Noise Levels 22 to 27 August 2019

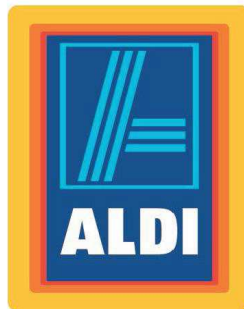


APPENDIX C: Noise Source Sound Power Level Data

Equipment/Activity		Sound Power Level
Carpark Activity and Vehicle Movements	Car movement	82 dB(A)
	General carpark activity	77 dB(A)
Mechanical Plant	Condenser	86 dB(A)
	Daikin RX(Y)Q12TY1A	68 dB(A)
	Daikin RX(Y)Q18TY1A	70 dB(A)
	Daikin RX(Y)Q20TY1A	71 dB(A)
Deliveries	Truck forward	98 dB(A)
	Truck reverse	97 dB(A)
Compactor	Compactor	81 dB(A)

APPENDIX D: ALDI Delivery & Loading Procedures

ALDI SOUTH AUSTRALIA



DELIVERY & LOADING PROCEDURES

ALDI DELIVERY & LOADING PROCEDURES

- ALDI own, operate and control all of its supply and logistics via its purpose built Distribution Centre. South Australia's Distribution Centre is under construction in Regency Park.
- ALDI's supply chain and logistics operates on a palletised system. All products are delivered to our Distribution Centre on pallets. The same product is then loaded onto delivery trucks; delivered to the store and in many cases the same pallet is then located on the retail floor. This streamlined system of operation enables ALDI to position itself as a discount supermarket quite differently to its competitors.
- With all ALDI supplies delivered directly to our Distribution Centre, ALDI then undertake consolidated store deliveries to each store via its own fleet of trucks/trailers and ALDI employed drivers.
- ALDI Stores do not undertake "night fill" or store replenishment outside retail trading hours therefore the stores close and staff leave very shortly after retail close. Deliveries are however enabled to undertaken throughout a full 24 hour period to any store given the Distribution Centre operates 24/7 and our own truck and drivers are able to access and unload to a store without the store being open or trading.
- With the dedicated ALDI supply chain direct from our Distribution Centre to a store, only a maximum of two ALDI deliveries are undertaken within a 24 period to each store. The only exception is one bread delivery from a bakery supplier, which is undertaken in a small rigid truck, once per day.
- The ALDI truck movement, on any site, typically takes between 1 to 2 minutes, with the truck moving onto the site, stopping, reversing and engaging with the purpose built loading dock.
- The prime mover is always turned off during unloading.
- Given the palletised system and dedicated dock connection the total delivery period is on average 15 minutes from the moment the truck is docked. The unloading is undertaken by only one person being the ALDI truck driver.
- All products are unloaded from within the trailer directly inside the building using a manually operated pallet jack. (No forklifts are used and no external activity occurs outside the truck). The trailers are sealed and connected to the building via a dedicated dock leveller and dock curtain.
- The ALDI prime movers and trailers are purpose built to ALDI specifications.
 - All trailers are built with a reversing camera which is connected to a driver display in the cabin of the prime mover.
 - All trailers are also able to be controlled from within the cabin to turn-off the reversing beepers (if required) and the refrigeration units (if required).
- All ALDI loading docks are fitted with motion sensors and automatic lighting to ensure that night time deliveries benefit from appropriate surveillance including safe reversing manoeuvres without reversing beepers (where required).
- All drivers are briefed and aware of each site and any site specific circumstances or restrictions that are applicable for that store.
- If an acoustic assessment requires any specific noise management then either or both the refrigeration and reversing beepers can be turned off including any requirement to do so whilst transiting past any noise sensitive areas.
- As a result of the above, where other retailers cannot, ALDI consistently can comply with any prescribed operational or acoustic requirements of either the Environmental Protection Authority and any relevant local statutory requirements when performing night time deliveries.
- Coupled with the above where additional on-site acoustic mitigation measures are required/recommended ALDI will construct these to further alleviate and mitigate any potential noise interface issues i.e. Acoustic screens, fences and/or gates.



Appendix 6. Traffic Impact Assessment (GTA)

ALDI 499 Portrush Road, Glenunga SA

Transport Impact Assessment

Prepared by: GTA Consultants (SA) Pty Ltd for ALDI Australia

on 13/09/19

Reference: S174580

Issue #: A

ALDI 499 Portrush Road, Glenunga SA

Transport Impact Assessment

Client: ALDI Australia

on 13/09/19

Reference: S174580

Issue #: A

Quality Record

Issue	Date	Description	Prepared By	Checked By	Approved By	Signed
A	13/09/2019	Final	Richard Frimpong	Paul Morris	Paul Morris	

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1. INTRODUCTION

1.1. Background

An ALDI Store is proposed on a site located at 499 Portrush Road in Glenunga. GTA Consultants were engaged by ALDI Australia to undertake a Transport Impact Assessment for the proposed development.

1.2. Purpose of this Report

This report sets out an assessment of the anticipated transport implications of the proposed development, including consideration of the following:

1. Existing traffic and parking conditions surrounding the site
2. Parking demand likely to be generated by the proposed development
3. Suitability of the proposed parking in terms of supply (quantum) and layout
4. Traffic generation characteristics of the proposed development
5. Proposed access arrangements for the site
6. Transport impact of the development proposal on the surrounding road network.

1.3. References

In preparing this report, reference has been made to the following:

- Burnside (City) Council Development Plan (consolidated 19 December 2017)
- Australian Standard/ New Zealand Standard, Parking Facilities, Part 1: Off-Street Car Parking AS/NZS 2890.1:2004
- Australian Standard, Parking Facilities, Part 2: Off-Street Commercial Vehicle Facilities AS 2890.2:2018
- Australian Standard / New Zealand Standard, Parking Facilities, Part 6: Off-Street Parking For People With Disabilities AS/NZS 2890.6:2009
- Plans for the proposed development prepared by Nielsen Architects dated September 2019
- Traffic surveys undertaken adjacent the site on Saturday 22 June 2019, Thursday 27 June 2019, Thursday 29 August 2019 and Saturday 31 August 2019
- Various technical data as referenced in this report
- An inspection of the site and its surrounds
- Other documents as nominated.

2. EXISTING CONDITIONS

2.1. Subject Site

The subject site is located in Glenunga on Portrush Road on the western side of the T-junction at Hay Road. It is currently occupied by a Mitre 10 store. The subject site has an area of approximately 4,366 sqm and has a frontage of approximately 63 metres to Portrush Road. The existing site has two access points on Portrush Road, with the southern access point operating as an ingress and the northern access point operating as an egress. The site is located within a Local Centre Zone, with the surrounding land uses being a mix of residential and commercial properties including a motel to the north.

The location of the subject site and the surrounding environs is shown in Figure 2.1.

Figure 2.1: Subject Site and its Environs



(PhotoMap courtesy of NearMap Pty Ltd)

2.2. Road Network

2.2.1. Adjoining Roads

Portrush Road

Portrush Road is a national highway managed and maintained by the Department of Planning, Transport and Infrastructure. It comprises dual carriageways aligned in a north-south direction and configured with two lanes in each direction. The northbound and southbound carriageway widths are 8.3 metres and 8.2 metres wide respectively.

EXISTING CONDITIONS

The central median is 3.0 metres (approx.) wide adjacent the site. The road reserve is approximately 25.2 metres wide adjacent the site. Kerbside parking is not permitted immediately adjacent the site with no stopping parking controls in place. Further from the site, parking is generally permitted on both sides of the road outside of clearway times (7:00am – 10:00am & 3:00pm – 7:00pm, Monday to Friday). A Pedestrian Actuated Traffic Signal Crossing is located on Portrush Road adjacent the site and to the south of Hay Road.

Portrush Road has a posted speed limit of 60km/h and carries approximately 39,800 vehicles per day¹.

Figure 2.2 considers Portrush Road looking south.

Figure 2.2: Portrush Road (adjacent subject site) – Looking south (site on right)



Hay Road

Hay Road is a local road managed by the Burnside City Council and is aligned in an east-west direction forming a T-junction with Portrush Road. It is a single carriageway two-way road configured with one lane in each direction. The carriageway width is 8.4 metres (approx.) and is set within an 18.6 metre wide road reserve. Kerbside parking is generally permitted on both sides of the street, however subject to time constraints and loading/permit restrictions. Traffic controls have been installed on Hay road in the form of road humps.

Hay Road provides access to Linden Park Primary School located to the east.

Hay Road has an urban default speed limit of 50km/h and carries approximately 1,500 vehicles per day.²

Figure 2.3 considers Hay Road looking east.

¹ Sourced from Location SA, which is based on DPTI counts undertaken for Portrush Road in 2017.

² Based on the peak hour traffic counts undertaken by HDS on Thursday 27 June 2019 and assuming a peak-to-daily ratio of 10%.

EXISTING CONDITIONS

Figure 2.3: Hay Road (adjacent subject site) – Looking east



2.2.2. Surrounding Intersections

The following intersections currently exist in the vicinity of the site:

- Portrush Road / Hay Road Intersection
- Portrush Road / Mitre 10 Ingress
- Portrush Road / Mitre 10 Egress

There is also a pedestrian actuated crossing (PAC) located immediately south of the Hay Road/Portrush Road Intersection.

2.2.3. Traffic Volumes

GTA commissioned turning movements surveys at the Portrush Road intersections with Hay Road and the Mitre 10 access point on:

- Thursday (27 June 2019) : 3:00pm – 6:00pm
- Saturday (22 June 2019): 10:00am – 1:00pm

The Thursday PM peak hour occurred between 5:00pm and 6:00pm while the Saturday peak hour occurred between 11:45am and 12:45am. The traffic volumes are shown in Figure 2.4 and Figure 2.5, respectively.

EXISTING CONDITIONS

Figure 2.4: Existing Thursday PM Peak Hour Traffic Volumes (5:00pm – 6:00pm)



Figure 2.5: Existing Saturday Peak Hour Traffic Volumes (11:45am – 12:45am)



EXISTING CONDITIONS

Based on the turning movement counts, Mitre 10 currently generates a Thursday PM Peak Hour volume of 20 movements and a Saturday Peak Hour volume of 129 trips.

2.2.4. Gap Analysis Survey

GTA Consultants also commissioned a gap survey analysis to be undertaken for Portrush Road during the Thursday PM Peak Period and Saturday Peak Period.

Gaps of 7 seconds or longer were recorded for both Portrush Road approaches to reflect a hypothetical scenario for vehicles to turn right out of the subject site onto Portrush Road (southbound). A follow up headway of 4 seconds was used to determine the number of vehicles which could turn right within the specified gap.

Gaps of 5 seconds or longer were recorded for the Portrush Road south approach (northbound) to reflect conditions to undertake a right turn into the subject site from Portrush Road (north approach). A follow up headway of 3 seconds was used to determine the quantity of vehicles which could theoretically turn right in within the allotted time.

The PAC was also monitored over this time period to establish a correlation between when the signals were triggered and the gaps which were available.

EXISTING CONDITIONS

Thursday PM Peak Hour (5:00pm – 6:00pm)

Figure 2.6: Gap Analysis – Thursday Peak Hour (Two Way) in seconds

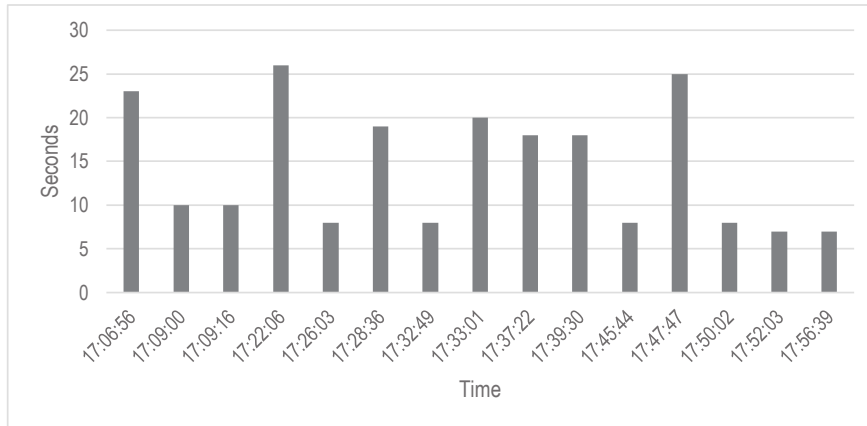
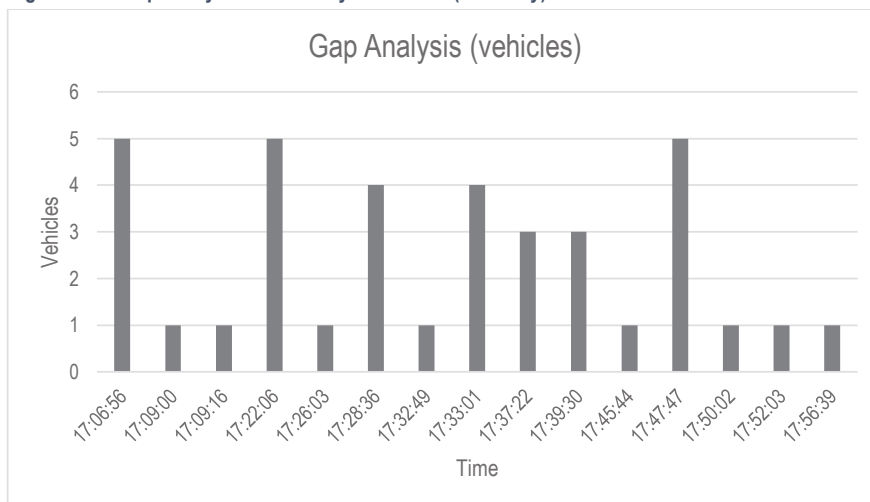


Figure 2.7: Gap Analysis – Thursday Peak Hour (Two Way) for vehicles



Based on the above:

- There were 15 separate opportunities during the Thursday Peak Hour where there was a critical gap of 7 seconds or more.
- Allowing for a follow up headway of 4 seconds, up to 37 vehicles could perform a right turn out onto Portrush Road during the peak hour.

EXISTING CONDITIONS

Figure 2.8: Gap Analysis – Thursday Peak Hour (One Way Northbound) in seconds

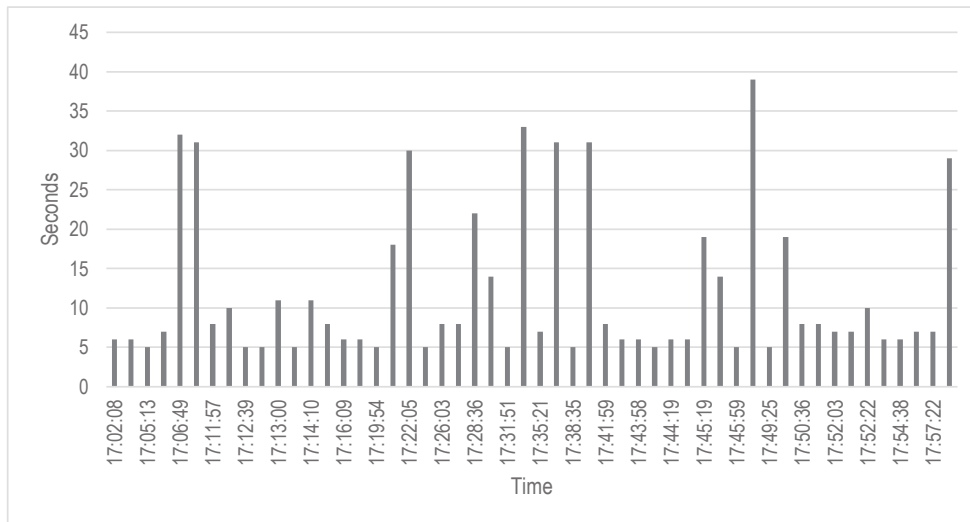
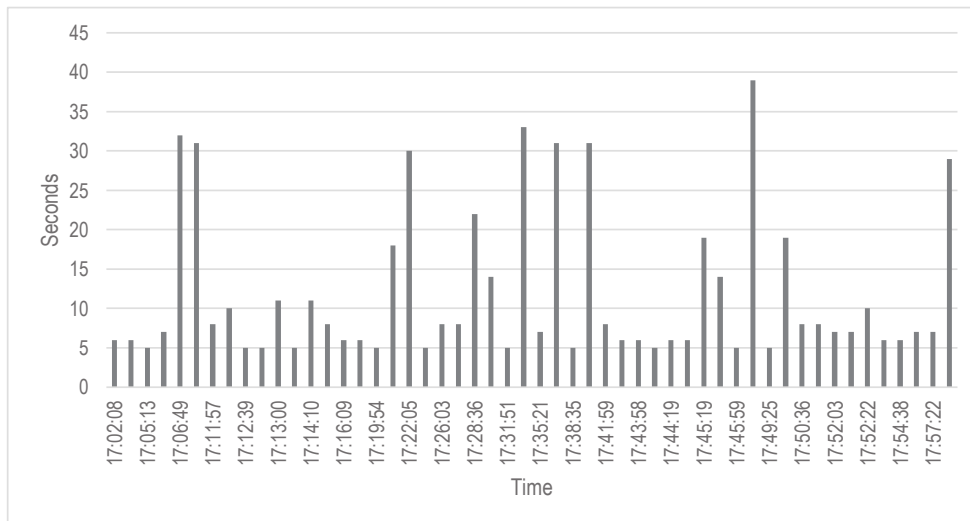


Figure 2.9: Gap Analysis – Thursday Peak Hour (One Way Northbound) for vehicles

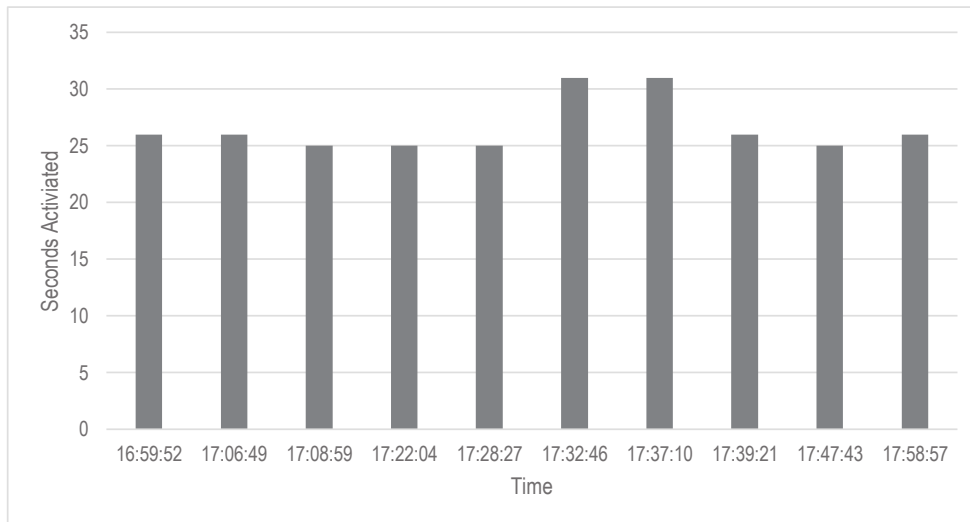


Based on the above:

- There were 72 separate opportunities during the Thursday Peak Hour where there was a critical gap of 5 seconds or more.
- Allowing for a follow up headway of 3 seconds, up to 157 vehicles can perform a right turn into ALDI during the peak hour.

EXISTING CONDITIONS

Figure 2.10: Thursday PM Peak PAC Activation (5:00pm – 6:00pm)



Based on the above, the PAC was triggered on ten (10) separate occasions during the Thursday PM Peak Hour, which in part contributed to some of the gaps in traffic on Portrush Road.

EXISTING CONDITIONS

Saturday Peak Hour (11:45am – 12:45pm)

Figure 2.11: Gap Analysis – Saturday Peak Hour (Two Way) in seconds

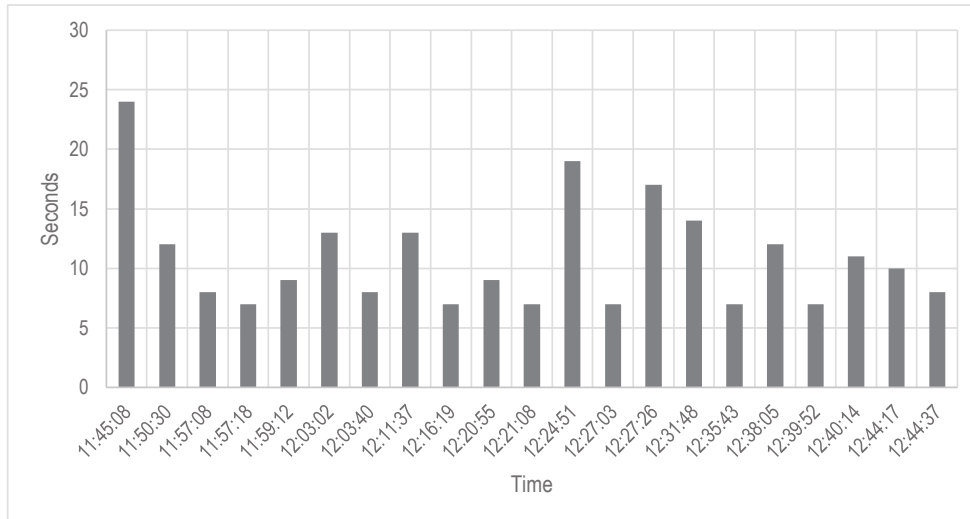
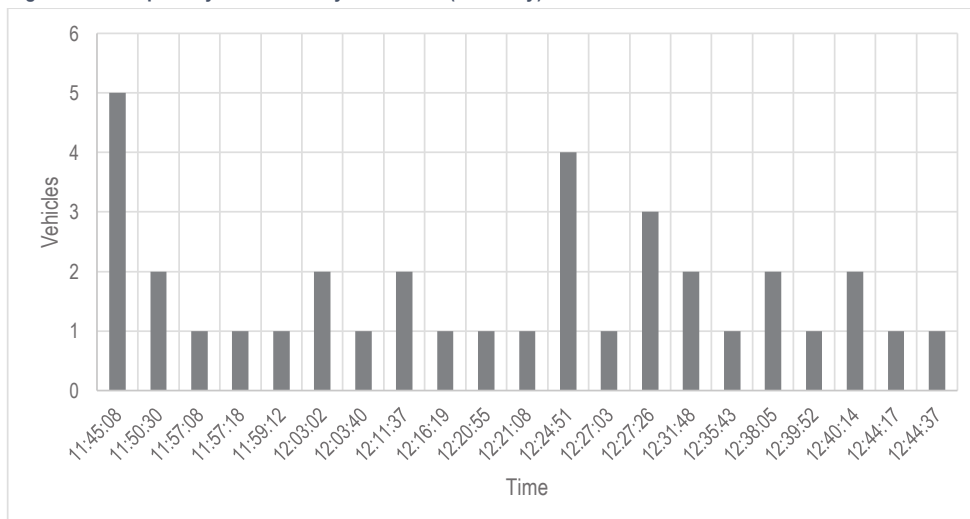


Figure 2.12: Gap Analysis – Saturday Peak Hour (Two Way) for vehicles



Based on the above:

- There were 21 separate opportunities during the Saturday Peak Hour where there was a critical gap of 7 seconds or more.
- Allowing for a follow up headway of 4 seconds, up to 36 vehicles can perform a right turn out onto Portrush Road during the peak hour.

EXISTING CONDITIONS

Figure 2.13: Gap Analysis – Saturday Peak Hour (One Way Northbound) in seconds

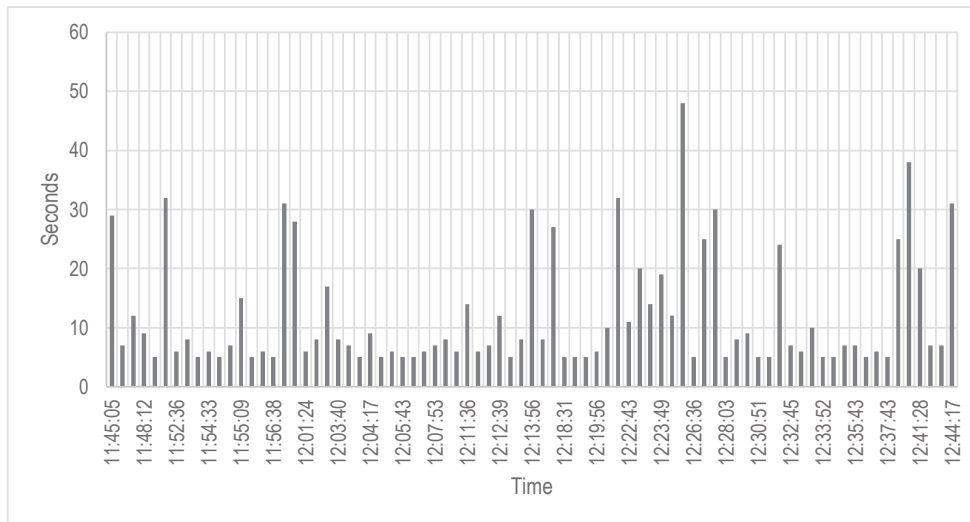
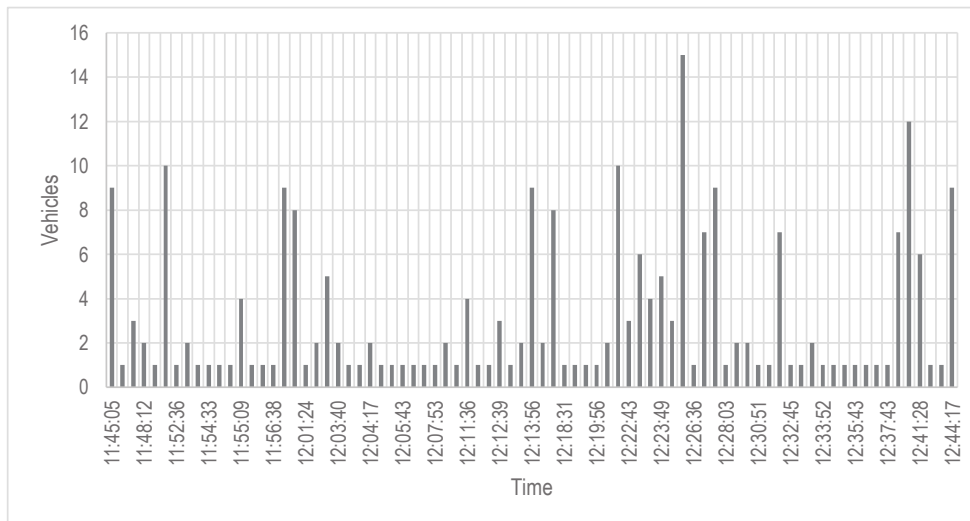


Figure 2.14: Gap Analysis – Saturday Peak Hour (One Way Northbound) for vehicles

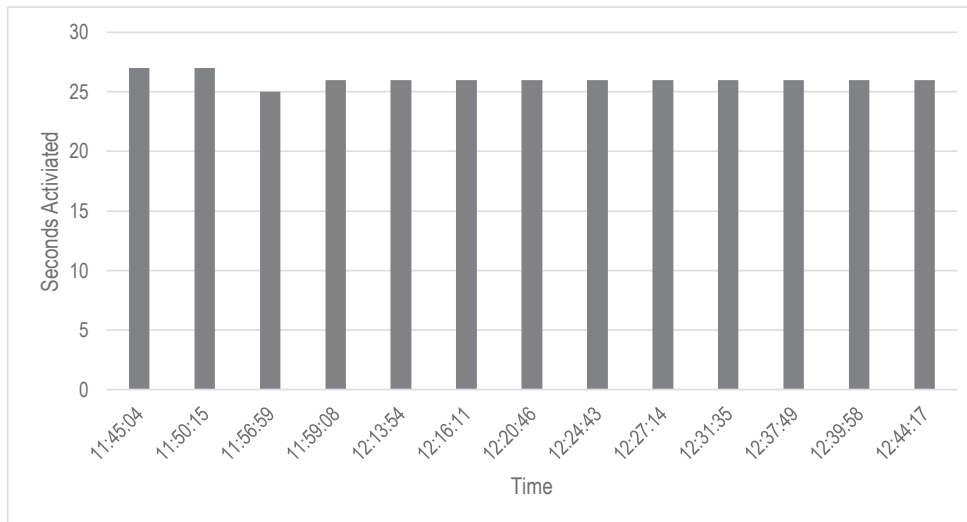


Based on the above:

- There were 79 separate opportunities during the Saturday Peak Hour where there was a critical gap of 5 seconds or more.
- Allowing for a follow up headway of 3 seconds, up to 241 vehicles can perform a right turn into ALDI during the peak hour.

EXISTING CONDITIONS

Figure 2.15: Saturday Peak Hour PAC Activation – (11:45 am – 12:45pm)



Based on the above, the PAC was triggered on thirteen (13) separate occasions during the Saturday Peak Hour, which in part contributed to some of the gaps in traffic on Portrush Road.

2.2.5. Queue Length Observation

GTA Conducted on-site observations on Thursday 29 August 2019 and Saturday 31 August between 3:00pm – 6:00pm and 11:45am – 12:45pm respectively. During this time, the right turn lane queue length was observed into Mariner Street, approximately 70 metres north of the site. The results are shown in Table 2.1.

Table 2.1: Right Turn Queue Length into Mariner Street

	Max Queue (veh)	95 th Percentile Queue (veh)
Thursday 29/8/19 (3:00pm – 6:00pm)	3	1
Thursday 29/8/19 (5:00pm – 6:00pm)	2	1
Saturday 31/8/19 (11:45am – 12:45pm)	1	1

Based on the above:

- A longer observation was undertaken on the Thursday to understand if there was any additional queuing due to pick-up activity associated with Linden Park Primary School between 3:00pm – 4:00pm. It was concluded there was little school related activity on Mariner Street.
- The maximum queue observed over any period was 3 vehicles.
- The 95th percentile queue length for the right turn lane in all periods was 1 vehicle.
- Generally, right turn activity into Mariner Street was infrequent and localised, with the operational capacity of the right turn lane not being exceeded during both periods. The occasional vehicle would perform a U-turn.

EXISTING CONDITIONS

2.2.6. General Observations

While on-site on Thursday 29 August 2019 and Saturday 31 August 2019, GTA Consultants undertook observations of the motel located to the north. The site has three (3) access points, with 18 entry/exit movements recorded between the three (3) access points during the Thursday PM Peak Hour. During the Saturday Peak Hour between 11:45am – 12:45pm, 14 movements were recorded. None of these movements involved a right turn in or right turn out.

2.2.7. Crash History

A review of the reported accident casualty history for the roads and intersections adjoining the subject site has been sourced from DPTI between 2013 and 2017. A summary of the road crashes for the last available five-year period is presented in Figure 2.16 and summarised in Table 2.2.

Figure 2.16: Crash History 2013-2017



Table 2.2: Crash History 2013-2017 in vicinity of the subject site

Location	No. of Crashes	Type of Crash
Midblock Portrush Road between Mariner Street and Hay Road	7	1x Rear End (Property Damage Only) 2x Side Swipe (Property Damage Only) 1x Right Angle (Minor Injury) 1x Roll Over (Minor Injury) 2x Hit Parked Vehicles (Property Damage Only)
Intersection of Hay Road and Portrush Road	5	3x Rear End (Property Damage Only) 1x Hit Fixed Object (Property Damage Only) 1x Side Swipe (Property Damage Only)
Midblock Portrush Road between Hay Road and Queen Street	6	2x Rear End (Property Damage Only) 2x Side Swipe (Property Damage Only) 1x Hit Pedestrian (Minor Injury) 1x Hit Parked Vehicle (Minor Injury)

Source: Data.gov.au crash history database

EXISTING CONDITIONS

Based on the above, the most common types of crashes occurred in vicinity of the site was 'rear end' and 'side swipe' crashes. A total of 4 crashes resulted in minor injuries, including one 'right angle' crash, one 'roll over' crash, one 'hit pedestrian' crash and one 'hit parked vehicle' crash.

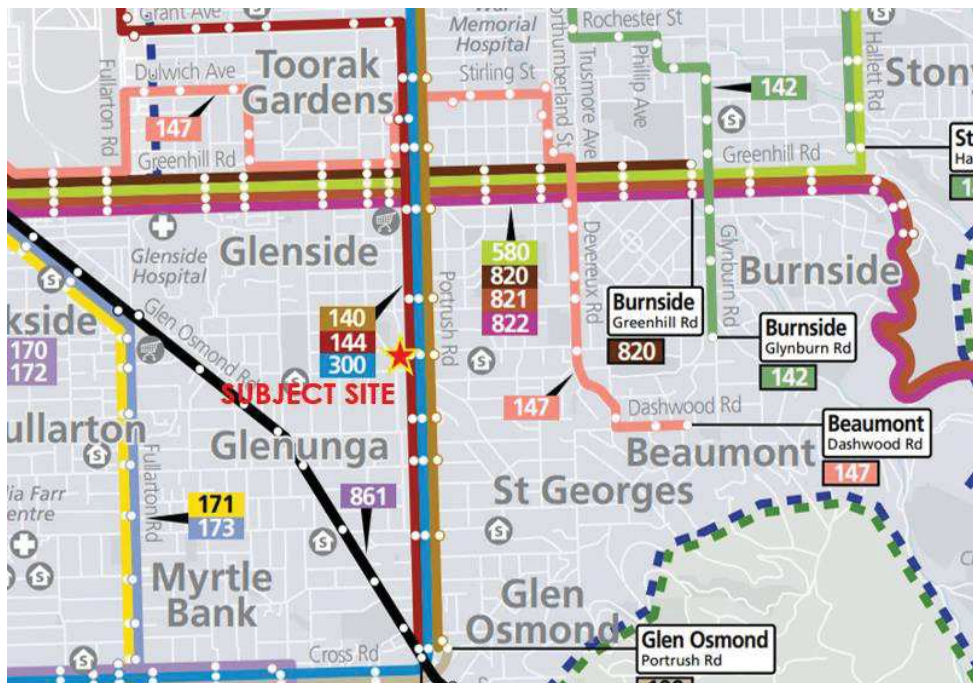
The hit pedestrian crash involved a car travelling straight ahead to the south hitting pedestrian at the PAC. The 'hit parked vehicle' crash involved a vehicle travelling straight ahead to the north hitting two parked vehicles on Portrush Road. The 'right angle' crash occurred when a vehicle heading east leaving the Mitre 10 driveway was hit by a vehicle travelling north on Portrush Road. The 'roll over' crash occurred when a cyclist travelling straight ahead to the north rolled over without a vehicle involved.

2.3. Sustainable Transport Infrastructure

2.3.1. Public Transport

Figure 2.17 shows the subject site in relation to existing public transport routes within its vicinity whilst Table 2.3 summarises the road based routes and major destinations that can be reached using these services.

Figure 2.17: Public Transport Map



Source: AdelaideMetro

EXISTING CONDITIONS

Table 2.3: Public Transport Provision

Route No's	Route Description	Frequency	Distance to nearest stops
140	Glen Osmond to City	Operates in peaks only Approximately every 30mins	30m
144	Glen Osmond to City	Approximately every 15mins in the peak and every 30mins interpeak	30m
300	Suburban connector loops to Arndale Centre Interchange	Approximately every 30 mins	30m

2.3.2. Pedestrian Infrastructure

There are sealed footpaths located on both sides of Portrush Road. A pedestrian actuated crossing (PAC) is located at the southern end of the site, which enables a safe crossing location across Portrush Road for pedestrians and cyclists

2.3.3. Cycle Infrastructure

There are no bicycle lanes on Portrush Road.

3. DEVELOPMENT PROPOSAL

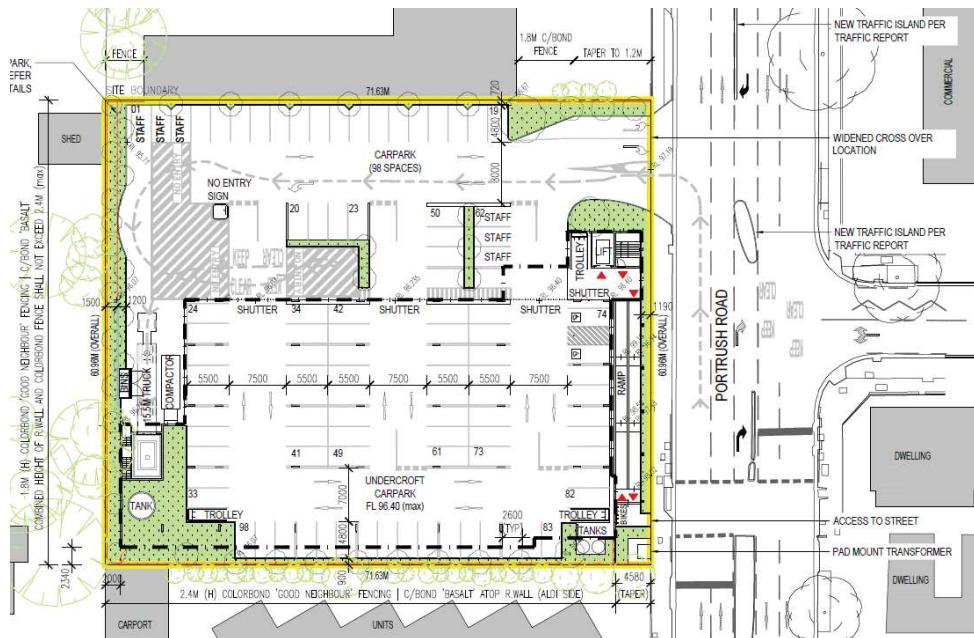
3.1. Land Uses

The proposed development includes:

- ALDI Supermarket of approximately 1,883 sq. m gross leasable floor area (GLFA)
- 98 off-street car parking spaces in a part under-croft/open-air car park
- A single access point located at the northern end of the site, with unrestricted turning movements to/from Portrush Road
- Loading facilities accommodating up to a 15.5 metre Semi Trailer.
- Pedestrian connectivity from Portrush Road to the main entrance.

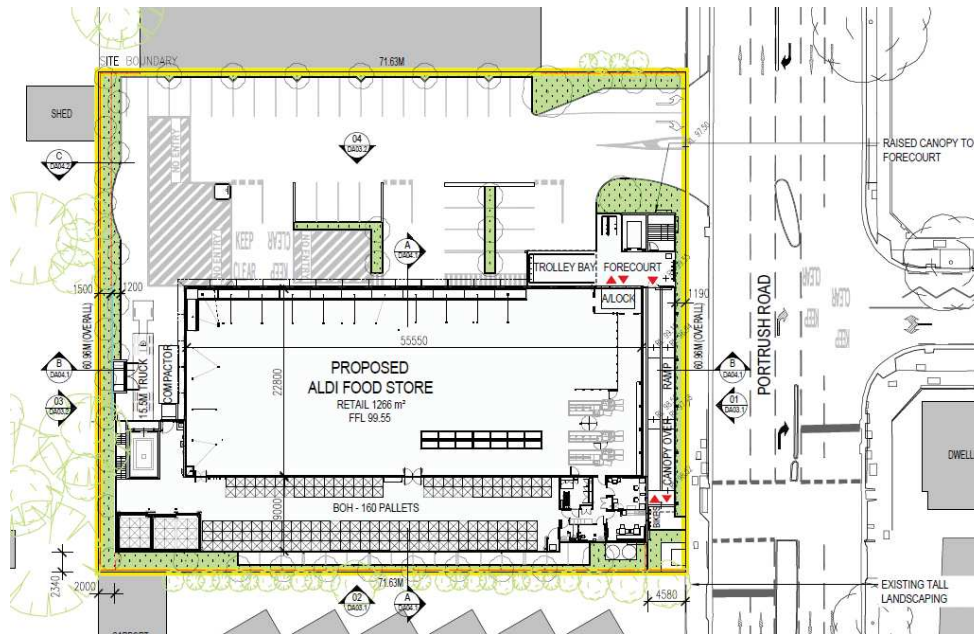
Figure 3.1 and Figure 3.2 considers the ground level and first floor layout.

Figure 3.1: Development Proposal – Ground Level



DEVELOPMENT PROPOSAL

Figure 3.2: Development Proposal – Level 01



4. CAR PARKING

4.1. Development Plan Car Parking Rates

Car parking rates for the proposed development have been sourced from Table Bur/5 of the Burnside City Council Development Plan. The most applicable rate within the development plan is a shop, with a development plan parking provision as follows:

Shop 7 spaces per 100 sq. m of total floor area

Based on a GLFA of 1,883 sq. m, this equates to development plan parking provision of 132 car parking spaces.

Based on the parking provision provided at other ALDI Stores in metropolitan Adelaide, a rate of 7 spaces per 100 sq. m is considered high.

4.2. Empirical Assessment

Parking demand surveys were undertaken by GTA Consultants at stand-alone ALDI Stores including surveys at the same sites on more than one occasion. Table 4.1 presents the results of the parking demand surveys for the most recent dates for each site.

CAR PARKING

Table 4.1: Parking Demand Surveys (Weekends) – ALDI Stores

Location	Retail Floor Area (sq.m)	Date	Peak Parking Demand (spaces per 100 sq.m)
Sunbury	1,274	17&18/02/2006	3.4
Hampton Park	1,291	17&18/02/2006	3.2
Carrum Downs	1,284	24&25/02/2006	4.6
Rosebud	1,454	24&25/02/2006	3.7
Corio	1,445	18&19/04/2008	5.3
Kangaroo Flat	1,285	18&19/04/2008	5.5
Morwell	1,424	2&3/05/2008	2.7
Sebastopol	1,434	2&3/05/2008	2.7
Warragul	1,447	18&19/04/2008	3.7
Ferntree Gully	1,274	15&31/07/2010	6.1
Pakenham	1,382	15&31/07/2010	3.5
Drysdale	1,400	12&13/09/2014	4.5
Hawthorn	1,566	24/05/2017	3.7
Hawthorn	1,566	25/05/2017	3.6
Hawthorn	1,566	27/05/2017	4.6
AVERAGE			4.1

Table 4.1 demonstrates that an ALDI store generates an average peak parking demand of 4.1 car parking spaces per 100 sq. m GLFA.

On this basis, the proposed ALDI Store of 1,883 sq. m would generate a peak parking demand of 77 car parking spaces.

4.3. Adequacy of Parking Supply

The proposed parking supply of 98 car parking spaces (5.2 spaces per 100 sq. m) will exceed the anticipated parking demands for an ALDI Store.

5. PARKING LAYOUT AND ACCESS

5.1. Vehicle Access

5.1.1. Portrush Road Access

A new access point is proposed along Portrush Road and will replace the existing Mitre 10 northern driveway. The existing southern access point opposite Hay Street will be removed. The proposed access point will be offset 2 metres from the northern property boundary, enabling pedestrian sight lines to be maintained. The driveway will also be 1.0 metres clear of an existing stobie pole located to the south.

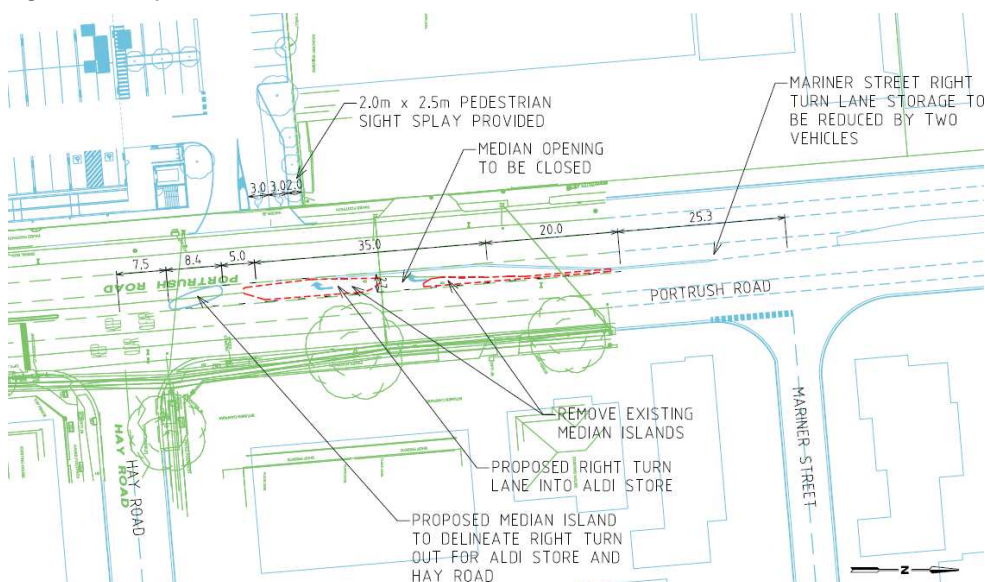
Vehicle movements will be unrestricted with left in, right in, left out and right turn out available for light vehicles. The right turn lane proposed into the site is 55 metres long including a 20 metre long taper and 35 metres of deceleration. The following has been noted:

- The existing motel to the north has a right turn out gap within the existing Portrush Road median, which will be closed.
- The length of the Mariner Street right turn lane will be reduced by 10 metres to create additional queuing storage for the right turn lane into ALDI.

A central median island has also been proposed south of the crossover to provide delineation between right turn movements out for both Hay Road and the ALDI Store.

The extent of intersection treatment is shown in Figure 5.1.

Figure 5.1: Proposed Intersection Treatment – Portrush Road



The back to back right turn lanes into the ALDI Store and Mariner Street will facilitate a queue length up to five (5) vehicles for the ALDI Site and (4) vehicles for Mariner Street. While the right turn lane into Mariner Street has been shortened, there will still be adequate capacity for the right turn lane to operate without overspilling onto Portrush Road based on queue lengths observed on-site.

5.2. Car Parking Layout

The car parking bays will be designed in accordance with AS/NZS2890.1:2004 & AS/NZS2890.6:2009 as follows:

- The car parking spaces are 2.6 metres wide x 5.4 metres long and is set within a minimum 7.0 metre aisle width, which exceeds the User Class 3A requirements for short term, high turnover parking.
- Columns within the under-croft car park are located outside the vehicle design envelope in accordance with the standard;
- In locations where aisles are adjacent walls or columns, an additional 300 mm clearance is provided;
- A 2.0 metres x 2.5 metre pedestrian sight splay is provided for the access onto Portrush Road; and
- Two (2) disabled car parking spaces will be located adjacent the lift with an associated shared space.

5.2.1. General Parking Layout Requirements

Further to the above, the grades within the parking area must conform to the following requirements (as per AS/NZS2890.1:2004, AS/NZS2890.6:2009 and AS2890.2:2018):

- Maximum grade of 1 in 20 (5%) across nature strip
- Maximum grade of 1 in 40 (2.5%) across any footpath
- Maximum grade of 1 in 20 (5%) for 10 metres into the site (where commercial vehicles use the driveway)
- A maximum grade of 1 in 6.5 (15.4%) along commercial vehicles circulation roads, the maximum grade shall be 1 in 8 (12.5%) where reverse manoeuvres are required
- A maximum grade of 1 in 20 (5%) measured parallel to the angle of parking
- A maximum grade of 1 in 16 (6.25%) measured in any other direction to the angle of parking.

6. SUSTAINABLE TRANSPORT INFRASTRUCTURE

6.1. Bicycle End of Trip Facilities

Bicycle parking rates for developments in the Local Centre Zone are not specified within the Burnside City Council Development Plan.

Observations of other ALDI stores indicates that the provision of 2 bicycle parking spaces would adequately cater for anticipated bicycle parking demands.

6.2. Walking and Cycling Network

A pedestrian connection will be provided between the footpath on Portrush Road and the proposed ALDI lifts and pedestrian ramp up to the store level. The connection will be located on the south-eastern corner of the site adjacent the PAC. This will provide convenient and safe access for people from all directions given the crossing facilities at the existing intersection and footpath network around the site. Pedestrians will not be required to use vehicle access points for access to the site.

6.3. Public Transport

The site is accessible to public transport for buses with Stop 13 Portrush Road located immediately adjacent to the store.

7. LOADING FACILITIES

7.1. Development Plan Requirements

Principle of Development Control (PDC) 93(c) in the 'Transportation System Facilities' section of the Burnside City Council Development Plan sets out the requirements for loading applicable to the proposed development. PDC 93(c) is as follows:

93 (c) "Development should provide for safe and convenient off-street loading, unloading and turning vehicles, likely to be required in association with the use of the land."

PDC 112 of the 'Transportation System Facilities' section also specifies the following:

112 "Loading and unloading areas and parking areas for service and commercial vehicles should have separate ingress and egress points from those for car parking areas, and should be screened from the general view"

7.2. Heavy Vehicle Demands

ALDI Stores typically generate the following heavy vehicle demands:

Deliveries	15.5 metre semi-trailer	2 per day
Waste Collection	Medium to Large Truck (Heavy Rigid Vehicle)	1 per week
Cardboard Bailer Collection	Medium Truck (Medium Rigid Vehicle)	1 per week
Bread Deliveries	Small Truck (Small Rigid Vehicle)	1 per day

ALDI utilises its own fleet vehicles for deliveries from its distribution centre in Regency Park. This enables ALDI to manage delivery times and sizes of vehicles as required for each store.

7.3. Proposed Loading Arrangements

A loading area is proposed to be located at the western end of the site.

As discussed in Section 3 of this report, the site will facilitate loading for a Semi Trailer up to 15.5 metres long which would occur on average 2 times per day.

Entry for the 15.5 m Semi-trailer will occur via a left turn into the site from Portrush Road as shown in Figure 7.1.

While direct access into the site is shared with light vehicles, the truck manoeuvre will take place at the rear of the site, with minimal interruption to light vehicle traffic circulating the aisle. The truck will perform a 180 degree turn within the chevron area until it's positioned 90 degrees to the loading dock. The truck will then reverse at a 90 degree angle to the dock as shown, maintaining adequate clearance to the building canopy. The internal manoeuvre is shown in Figure 7.2.

Exit will occur via a left turn from the site onto Portrush Road as per Figure 7.3.

The loading dock will be designed in accordance with ALDI's standard detail. A bin store and bailer will also be located in the loading dock area.

ALDI places a high level of safety for truck manoeuvres in the car park with trucks equipped with reversing cameras, which would alert drivers of other users and furniture within the vicinity, as well as wide angle mirrors on each side of the cabin. This facilitates safe deliveries for ALDI at each store as part of standard operating procedures. It is further noted that the site would only expect up to 2 semi-trailer deliveries per day.

LOADING FACILITIES

Figure 7.1: 15.5m Semi Trailer – Entry - ALDI

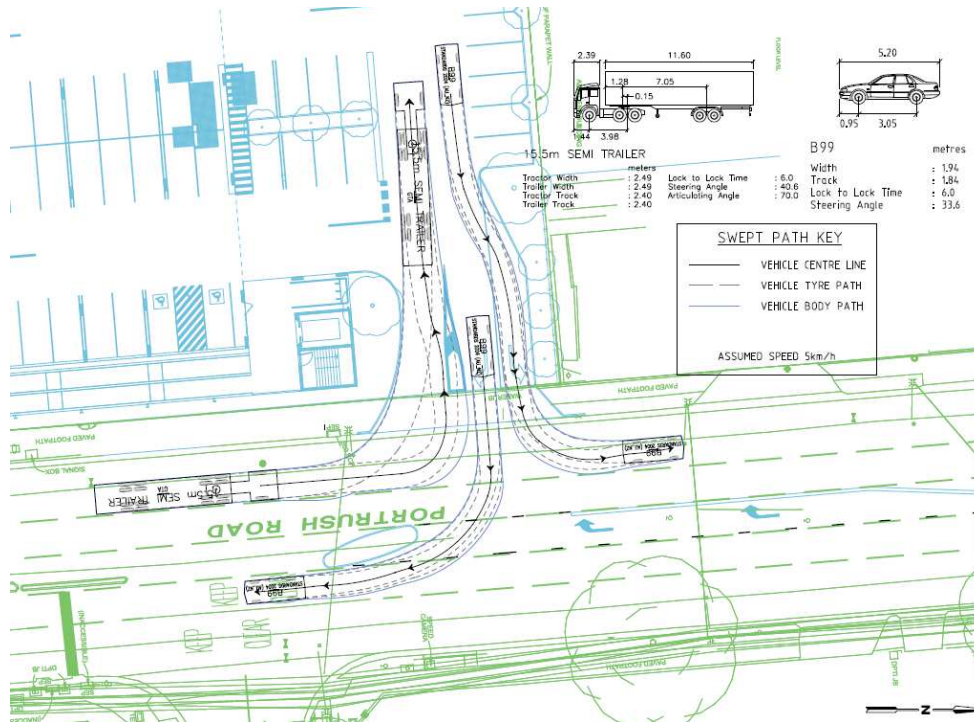
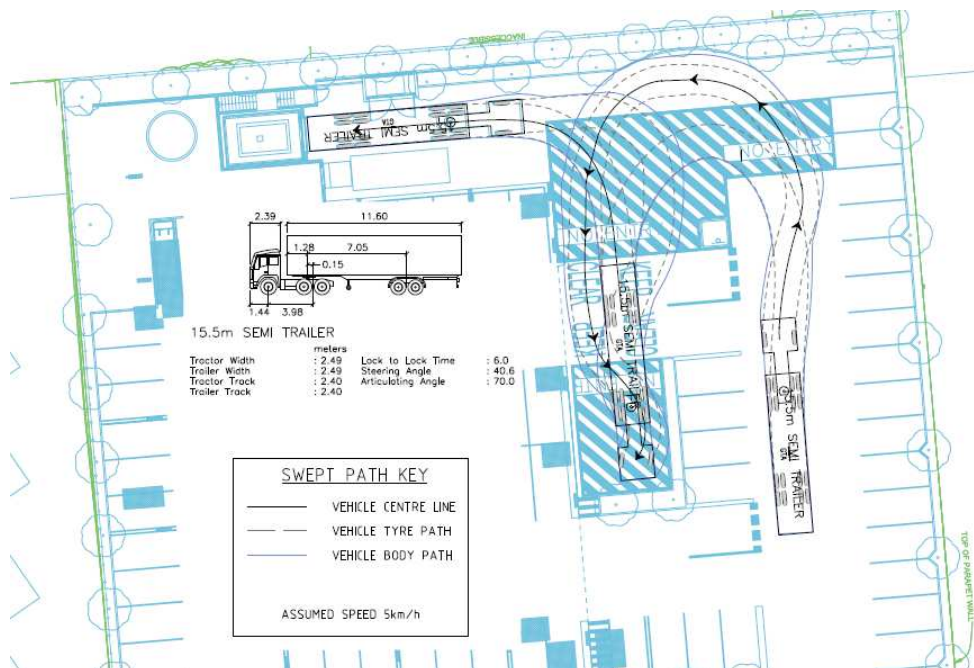


Figure 7.2: 15.5m Semi Trailer – Dock Access



8. TRAFFIC IMPACT ASSESSMENT

8.1. Traffic Generation

8.1.1. Empirical Design Rates

The proposed ALDI Store will comprise a Gross Leasable Floor Area (GLFA) of 1,883 sq. m. Table 8.1 presents the results of traffic generation surveys undertaken by GTA at standalone ALDI stores.

Table 8.1: Traffic Generation Surveys – ALDI Stores

Location	Retail Floor Area (sq.m)	Date	Traffic Generation (trips per 100sq.m)	
			Daily	PM Peak Hour
Sunbury	1,274	17/02/2006	136.5	13.5
Hampton Park	1,291	17/02/2006	126.2	14.6
Carrum Downs	1,284	24/02/2006	-	13.2
Rosebud	1,454	24/02/2006	-	10.7
Ferntree Gully	1,274	15&31/07/2010	-	27.5
Pakenham	1,382	15&31/07/2010	-	12.0
AVERAGE			131.0	15.3

Based on the traffic generation survey results the following traffic generation rates have been adopted:

Weekday (Daily): 131 trips per 100sq.m gross retail floor area
 Weekday (PM Peak Hour): 15.3 trips per 100sq.m gross retail floor area

Based on empirical traffic data for other ALDI Stores in Australia, the weekday PM Peak traffic generation is 15.3 trips per 100 sq. m. Notwithstanding, more recent survey data has been collected for the ALDI Store in Hawthorn, SA, indicating a peak rate of 10.7 trips per 100 sq. m during the Thursday PM Peak.

Albeit, the rate of 12.3 trips per 100 sq. m from the RTA Guidelines was used for this assessment as a conservative measure, which equates to 232 peak hour trips.

Based on traffic data obtained at the ALDI Hawthorn Store on a Saturday, the trip generation during the road network peak was recorded at 15.3 trips between 10:00am and 1:00pm. This rate was used to assess the Saturday impact, which equates to an additional 288 peak hour trips on the road network.

While passing trade is usually a key component for ALDI Stores, as a conservative assessment, no passing trade discount has been applied in this model.

8.1.2. Distribution and Assignment

The directional distribution and assignment of traffic generated by the proposed development will be influenced by a number of factors, including the:

1. Configuration of the arterial road network in the immediate vicinity of the site
2. Existing operation of intersections providing access between the local and arterial road network
3. Distribution of households in the vicinity of the site
4. Likely distribution of employee's residences in relation to the site
5. Configuration of access points to the site.

TRAFFIC IMPACT ASSESSMENT

Having consideration to the above, for the purposes of estimating vehicle movements, the following directional distributions for entry movements has been assumed:

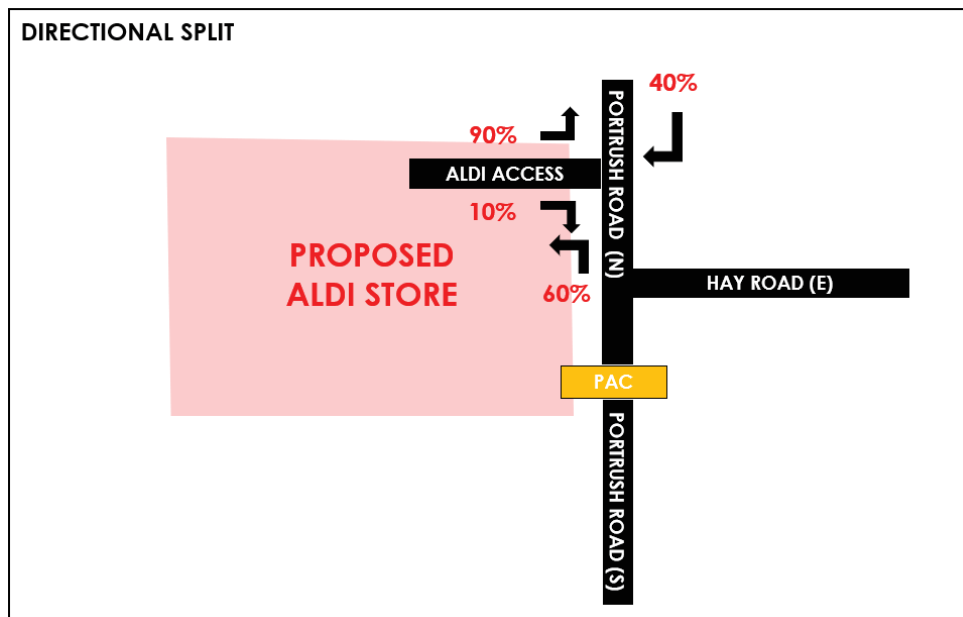
- Portrush Road (North) – 40%
- Portrush Road (South) – 60%

This is primarily based on the anticipated catchment area for the store and the proximity of other ALDI Stores.

A much higher proportion of vehicles were assumed to turn left out instead of right out, which is not uncommon during peak times when the queue length and delay times for right turns are higher than during other periods.

Figure 8.1 has been prepared to consider the directional split during both Thursday PM Peak and Saturday Peak.

Figure 8.1: Directional Split



In addition, the directional split of traffic (i.e. the ratio between the inbound and outbound traffic movements) is assumed to be 50:50.

8.1.3. Post Development Traffic

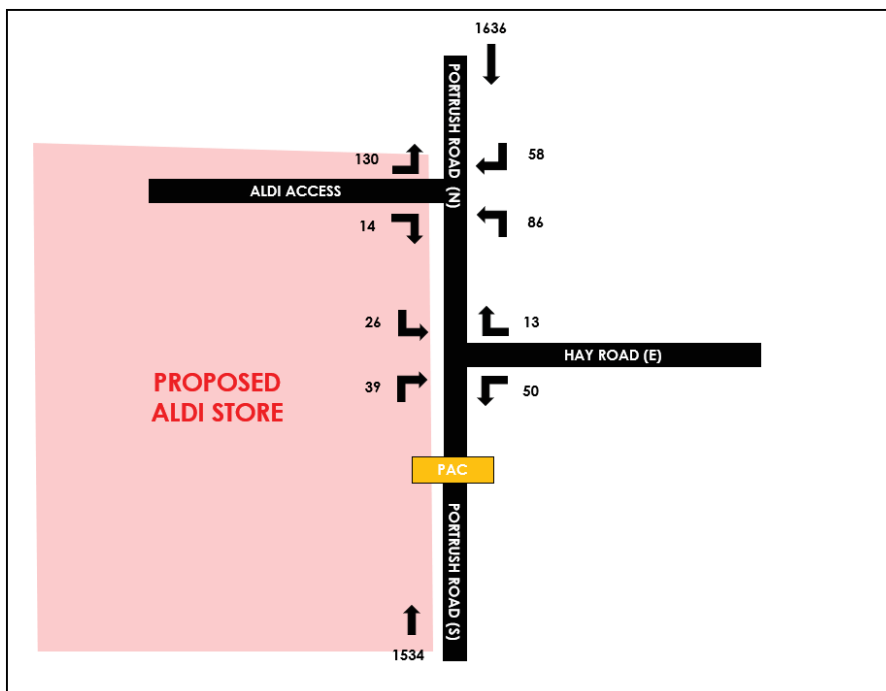
Figure 8.2 and Figure 8.3 have been prepared to show the expected traffic distribution from the ALDI store post development for both the Thursday PM and Saturday peak periods.

TRAFFIC IMPACT ASSESSMENT

Figure 8.2: Thursday PM Peak (5:00pm – 6:00pm) – Post Development Traffic



Figure 8.3: Saturday Peak (11:45am – 12:45pm) – Post Development Traffic



8.2. Intersection Analysis

The operation of the Portrush Road and ALDI Access Intersection has been assessed using SIDRA Intersection 7.0 under post development conditions during the Thursday PM Peak Hour and the Saturday Peak Hour.

The results for the Thursday PM Peak Hour is summarised in Table 8.2, while the results for the Saturday Peak Hour is described in Table 8.3.

Table 8.2: Portrush Road / ALDI Access – Thursday Peak Hour – Post Development

Movement Performance - Vehicles											
Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Portrush Road (S)											
1	L2	74	0.0	0.487	5.6	LOS A	0.0	0.0	0.00	0.04	30.6
2	T1	1828	2.9	0.487	0.1	LOS A	0.0	0.0	0.00	0.02	59.6
Approach		1902	2.8	0.487	0.3	NA	0.0	0.0	0.00	0.02	58.4
North: Portrush Road (N)											
8	T1	2115	3.2	0.541	0.1	LOS A	0.0	0.0	0.00	0.00	59.8
9	R2	48	0.0	0.760	117.9	LOS F	2.4	17.1	0.99	1.10	13.0
Approach		2163	3.2	0.760	2.7	NA	2.4	17.1	0.02	0.02	57.2
West: Aldi Access											
10	L2	109	0.0	0.284	9.5	LOS A	1.0	7.3	0.77	0.86	37.8
12	R2	13	0.0	2.105	1453.5	LOS F	8.2	57.1	1.00	1.60	1.2
Approach		122	0.0	2.105	158.9	LOS F	8.2	57.1	0.80	0.94	9.1
All Vehicles		4187	2.9	2.105	6.2	NA	8.2	57.1	0.03	0.05	52.9

Based on the above:

- The proposed right turn lane into the ALDI Store would operate at a 95th percentile queue length of 17.1 metres, which is fully contained within the proposed right turn lane. The average delay was higher, but this is not uncommon during the peak period. A gap analysis was undertaken which confirmed there would more than sufficient opportunities to accommodate the anticipated right turning vehicles into the site. Some of these gaps were attributed to the PAC activations, of which 10 occurred during the peak hour.
- The proposed right turn out would operate with higher queuing and delays, which is not dissimilar to other local road intersections with arterial roads during the PM Peak Hour. A gap analysis was undertaken during the Thursday PM Peak Hour, which confirmed that there were 15 separate gap opportunities and up to 37 vehicles can turn right against both directions of traffic during the peak hour.
- The remaining approaches are anticipated to operate at a LOS of A with no notable queuing or delays.

Table 8.3: Portrush Road / ALDI Access – Saturday Peak Hour – Post Development

Movement Performance - Vehicles											
Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Portrush Road (S)											
1	L2	91	0.0	0.436	5.6	LOS A	0.0	0.0	0.00	0.06	30.5
2	T1	1615	2.4	0.436	0.1	LOS A	0.0	0.0	0.00	0.03	59.6
Approach		1705	2.3	0.436	0.4	NA	0.0	0.0	0.00	0.03	57.9
North: Portrush Road (N)											
8	T1	1722	2.2	0.438	0.1	LOS A	0.0	0.0	0.00	0.00	59.9
9	R2	61	0.0	0.563	55.5	LOS F	1.8	12.5	0.97	1.05	21.3
Approach		1783	2.1	0.563	2.0	NA	1.8	12.5	0.03	0.04	57.8
West: Aldi Access											
10	L2	137	0.0	0.280	7.0	LOS A	1.1	7.6	0.71	0.79	39.6
12	R2	15	0.0	2.456	1761.6	LOS F	10.5	73.2	1.00	1.65	1.0
Approach		152	0.0	2.456	177.6	LOS F	10.5	73.2	0.73	0.87	8.3
All Vehicles		3640	2.1	2.456	8.5	NA	10.5	73.2	0.05	0.07	50.5

Based on the above:

- The proposed right turn lane into the ALDI Store with a 95th percentile queue length of 12.5 metres is fully contained within the proposed right turn lane. Onsite observations confirmed there were more than sufficient gaps available on Portrush Road to enable a vehicle to turn right in. Some of these gaps were attributed to the PAC, which was triggered 13 times during the peak hour.
- The proposed right turn out would operate with higher queuing and delays, which is not dissimilar to other local road intersections with arterial roads during the PM Peak Hour. A gap analysis was undertaken during the Saturday Peak Hour, which confirmed that there were 21 separate gap opportunities and up to 36 vehicles can turn right against both directions of traffic during the peak hour.
- The remaining approaches are anticipated to operate at a LOS of A with no notable queueing or delays.

8.3. Traffic Impact

The proposed ALDI Store is envisaged to generate 232 trips during the Thursday PM Peak Hour and 288 trips during the Saturday Peak Hour. When considering this against the existing volumes for Mitre 10 (20 trips during the Thursday PM Peak Hour and 129 trips during the Saturday Peak Hour), this would equate to an additional 212 and 159 trips on the road network during the Thursday PM Peak Hour and Saturday Peak Hour respectively.

The proposed right turn lane into ALDI is expected to accommodate more than the anticipated volumes based on the SIDRA Intersection Modelling output as well as from the gap surveys undertaken on-site. It is also noted that shortening of the right turn lane to Mariner Street, will not adversely affect queues on this approach. Based on observations, the 95th Percentile Queue Length for both peak periods is one (1) vehicle, which will easily be accommodated in the modified right turn lane.

While it is envisaged that there would be higher queuing associated with the right turn movement, the gap analysis survey and PAC activation count identified an adequate amount of opportunities for vehicles to turn out.

As such, the traffic generated by the proposed ALDI Store would not adversely impact on the road network.

9. CONCLUSIONS

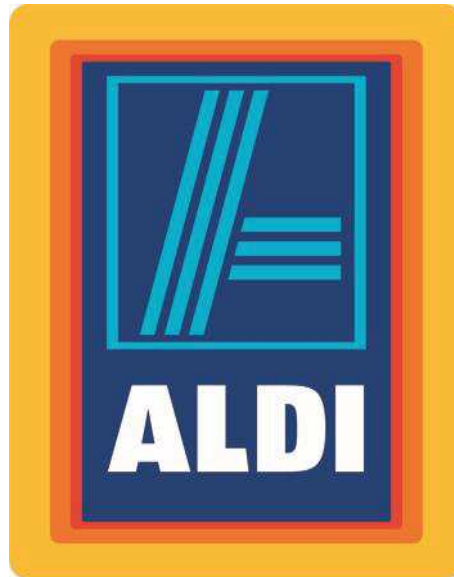
Based on the analysis and discussions presented within this report, the following conclusions are made:

1. The proposed development will generate a Development Plan Parking Requirement 132 car parking spaces, although this is considered high when compared to other ALDI Stores.
2. The proposed supply of 98 car parking spaces (5.2 spaces per 100 sq. m) exceeds the empirical rate for ALDI Stores (4.1 spaces per 100 sq. m).
3. The proposed car parking layout is consistent with the dimensional requirements set out in the Australian/New Zealand Standard for Off-Street Car Parking (AS/NZS2890.1:2004) and the Australian Standard for Parking for People with Disabilities (AS2890.6:2009).
4. GTA considers the provision of two (2) bicycle parking spaces suitable in meeting anticipated demand, given no guidance is provided in the Development Plan.
5. A new access point is proposed at the north-east of the site which will enable unrestricted turning movements to/from the site.
6. The loading facility for the ALDI Store will accommodate truck access up to a 15.5 metre Semi Trailer, with trucks entering via a left turn in and exiting via a left turn out. Access for recycling and waste collection vehicles will also be suitable.
7. The proposed development is predicted to generate up to 232 and 288 two-way vehicle movements during the Thursday PM Peak and Saturday Peak Hour respectively. Given the Mitre 10 site currently generates 20 movements during the Thursday PM Peak Hour and 129 movements during the Saturday Peak Hour, this equates to an additional 212 and 159 trips onto the road network respectively.
8. An analysis of the additional traffic by the proposed development during peak periods indicates the proposed right turn lane will accommodate the anticipated additional traffic, while the reduced right turn lane length into Mariner Street won't adversely impact on this approach.
9. Right turns out of the site will experience higher queue lengths and delays, however based on gap analysis and PAC activations, there would be more than sufficient opportunities to facilitate the anticipated demands.
10. There is adequate capacity in the surrounding road network to cater for the traffic generated by the proposed development.



Appendix 7. ALDI Loading Procedures

ALDI SOUTH AUSTRALIA



DELIVERY & LOADING PROCEDURES

ALDI DELIVERY & LOADING PROCEDURES

- ALDI own, operate and control all of its supply and logistics via its purpose built Distribution Centre. South Australia's Distribution Centre has been constructed in Regency Park.
- ALDI's supply chain and logistics operates on a palletised system. All products are delivered to our Distribution Centre on pallets. The same product is then loaded onto delivery trucks; delivered to the store and in many cases the same pallet is then located on the retail floor. This streamlined system of operation enables ALDI to position itself as a discount supermarket quite differently to its competitors.
- With all ALDI supplies delivered directly to our Distribution Centre, ALDI then undertake consolidated store deliveries to each store via its own fleet of trucks/trailers and ALDI employed drivers.
- Deliveries can be undertaken throughout a full 24 hour period to any store given the Distribution Centre operates 24/7 and our own truck and drivers are able to access and unload to a store without the store being open or trading.
- With the dedicated ALDI supply chain direct from our Distribution Centre to a store, only a maximum of two ALDI deliveries are undertaken within a 24 period to each store. The only exception is one bread delivery from a bakery supplier, which is undertaken in a small rigid truck, once per day.
- The ALDI truck movement, on any site, typically takes between 1 to 2 minutes, with the truck moving onto the site, stopping, reversing and engaging with the purpose built loading dock.
- The prime mover is always turned off during unloading.
- Given the palletised system and dedicated dock connection the total delivery period is on average 30 minutes from the moment the truck is docked. The unloading is undertaken by only one person being the ALDI truck driver.
- All products are unloaded from within the trailer directly inside the building using a manually operated pallet jack. (No forklifts are used and no external activity occurs outside the truck). The trailers are sealed and connected to the building via a dedicated dock leveller and dock curtain.
- The ALDI prime movers and trailers are purpose built to ALDI specifications.
 - All trailers are built with a reversing camera which is connected to a driver display in the cabin of the prime mover.
 - All trailers are also able to be controlled from within the cabin to turn-off the reversing beepers (if required) and the refrigeration units (if required).
- All ALDI loading docks are fitted with motion sensors and automatic lighting to ensure that night time deliveries benefit from appropriate surveillance including safe reversing manoeuvres without reversing beepers (where required).
- All drivers are briefed and aware of each site and any site specific circumstances or restrictions that are applicable for that store.
- If an acoustic assessment requires any specific noise management then either or both the refrigeration and reversing beepers can be turned off including any requirement to do so whilst transiting past any noise sensitive areas.
- As a result of the above, where other retailers cannot, ALDI consistently can comply with any prescribed operational or acoustic requirements of either the Environmental Protection Authority and any relevant local statutory requirements when performing night time deliveries.
- Coupled with the above where additional on-site acoustic mitigation measures are required/recommended ALDI will construct these to further alleviate and mitigate any potential noise interface issues i.e. Acoustic screens, fences and/or gates.

ALDI DELIVERY & LOADING PROCEDURES



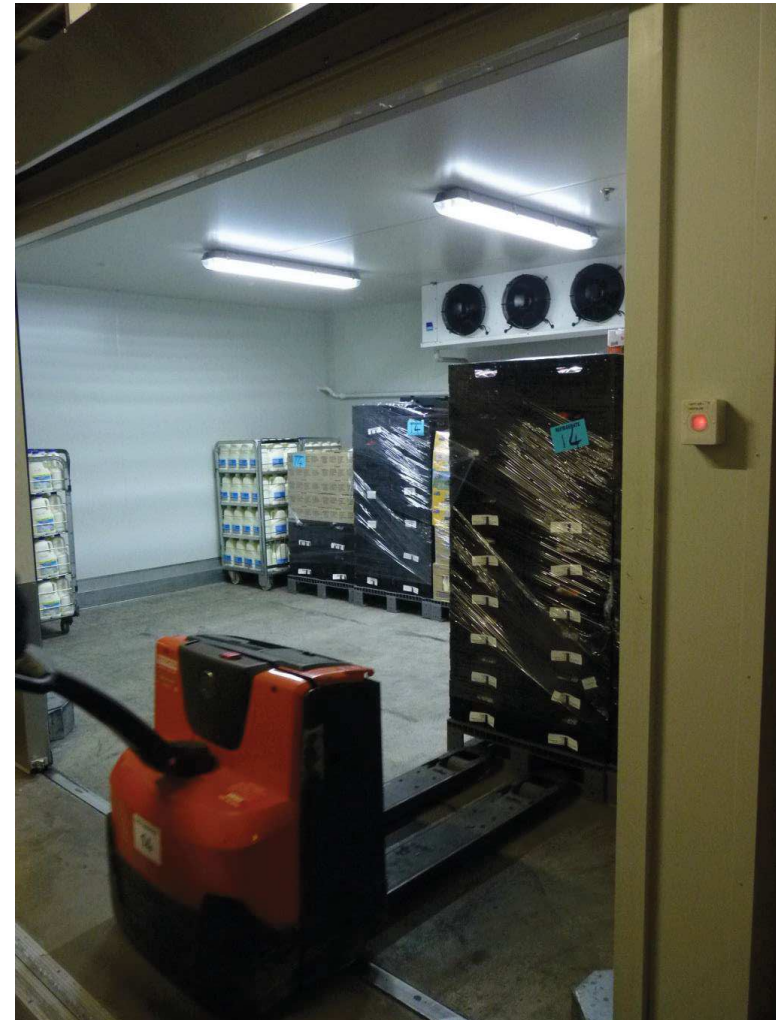
ALDI DELIVERY & LOADING PROCEDURES



ALDI DELIVERY & LOADING PROCEDURES



ALDI DELIVERY & LOADING PROCEDURES





Appendix 9. Legal Advice (Botten Levinson)

Our ref: JAL/219217

26 August 2019

Mr J Hallion
State Co-ordinator General
Level 9, 131 Grenfell Street
ADELAIDE SA 5000

Dear Mr Hallion,

Proposed Aldi store - 499 Portrush Road, Glenunga

We act for the Applicant. This letter explains why the proposal is a "consent on merit" development under section 35 of the *Development Act* 1993.

The Proposal

The proposal is depicted in a set of drawings entitled 'ALDI Glenunga Planning Pack' containing 12 drawings prepared by Nielsen Architects and relevantly includes

1. A multi level structure with a new ALDI Food Store at the upper level;
2. Access to the site from Portrush Rd leading to open car-parking spaces, a loading dock and ramp to an undercroft car park;
3. At basement level, car-parking in an undercroft below the ALDI Food Store.
4. The finished floor level (FFL) of the undercroft carpark is 96.4m AHD.

Development Plan

PDC 9 for the Local Centre Zone lists as non-complying:

"...building containing one floor level directly above another floor level except where the lower floor level is for the provision of car parking and is set at a level of not less than 1m below the predominant ground level of the site..." (my underlining)

The phrase "predominant ground level" only appears in the Local Centre Zone non-complying list and is not defined.

The survey information

The site has a frontage to Portrush Road and the top of the kerb (water table) along Portrush Road varies from about 97m AHD at the northern end to about 98.5m AHD at the southern end.

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There is an existing bulky goods outlet on the site. Alexander Symonds Surveyors (**AS**) have undertaken a thorough survey of the site including the ground level of the existing building on the land. The carpark and surrounding area occupies about 60% of the site and the hardware building about 40%. It has a floor level of 97.7m.

The car park and access area is at a level of about 97.4m AHD – 97.7m AHD.

The mean level is 97.49m AHD, the median level is 97.62m AHD and the mode level is 97.8m AHD.

Predominant ground level

The phrase is not defined in the Development Plan (nor the Development Act or Regulations) and is therefore given its ordinary English meaning. According to the Macquarie Dictionary "*predominant*" relevantly means "*prevailing*".

Based on the AS survey the carpark and surrounds occupies about 60% of the site. As such the prevailing (predominant) level being the car park area, ranges from 97.41m AHD – 97.7m AHD. A very cautious figure is the lower end of the range for the car park (97.41m AHD). Clearly, the mean of 97.49m AHD means that at least half of the site is above 97.41m AHD.

Not non-complying

The proposed development meets the exception in PDC 9 because -

1. It comprises a building;
2. One floor level (the main store area) is directly above another floor level (the car park undercroft);
3. The lower level is for the provision of car parking; and
4. The lower level is set 1.01 metres below the cautiously calculated predominant ground level of 97.41m AHD.

Accordingly, the proposal is for a consent on merit form of development under the Act.

Yours faithfully



James Levinson
BOTTEN LEVINSON
 Mob: 0407 050 080
 Email: jal@bllawyers.com.au



Appendix 10. Retail Demand and Impact (Deep End)

4 September 2019

Nigel Uren
Property Director
ALDI Stores
84 Gallipoli Drive,
Regency Park SA 5010

Dear Nigel,

Proposed ALDI store - 499 Portrush Road, Glenunga

Further to your request, this letter sets out a brief supply and demand analysis of supermarket floorspace and other need considerations relevant to the proposed ALDI store at 499 Portrush Road, Glenunga.

ALDI South Australia

ALDI opened its first stores in South Australia in February 2016. With the latest openings in 2019 at Port Adelaide and Newton, there are now 31 stores in the state including 24 in the Greater Adelaide region and seven in regional areas (refer Figure 1). Other committed stores to open in this year are at Blackwood and Mt Gambier.

By the end of 2019, ALDI will have 33 stores operating in the state or about two-thirds of its planned network of 45-50 stores.

ALDI has filled a gap in the discount market and been well received by South Australians. It currently serves over 200,000 customers every week.

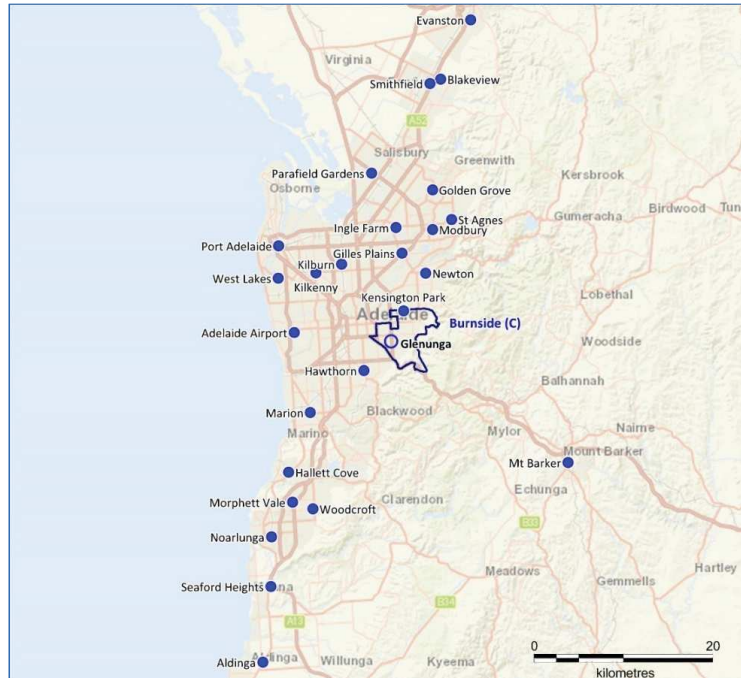
Its low grocery prices and unique weekly specials draw customers from wide catchment areas. Notwithstanding its growth, ALDI has less than 6% market share of 'Supermarkets and Grocery store' turnover in South Australia¹ indicating further opportunities to develop under serviced catchments and compete with the long-established national chains and other major operators.

Adelaide's affluent eastern suburbs have high average spending levels on food and groceries but are generally underserved with supermarkets with low average rates of supermarket floorspace per capita.

ALDI currently services the eastern suburbs with two stores at Kensington Park and Newton and with a third to open in 2020 at Norwood. Based on current customer patterns, the business has identified an opportunity to provide a fourth store to service customers in the southern areas of the City of Burnside.

¹ Source: Australian Bureau of Statistics Retail Trade, Deep End Services.

Figure 1—ALDI stores & Glenunga site



Location & proposed development

The site of the proposed ALDI store is currently developed with a small Mitre 10 hardware store of approximately 1,435 sqm and surface parking. The 4,366 sqm site is zoned Local Centre under the City of Burnside Development Plan and is part of a broader shopping and commercial area under the Local Centre zone, extending about 450 metres along the west side of Portrush Road (refer Figure 2). In earlier years, the site had a Bi Lo supermarket operating from it.

The Local Centre Zone is central to the large residential area south of Greenhill Road and east of Glen Osmond Road, containing the suburbs of Glenunga, Glenside, Frewville, Linden Park, St Georges, Glen Osmond, Hazelwood Park, Beaumont and Burnside. These suburbs range between 1,000 and 3,000 people with a combined population of over 19,000.

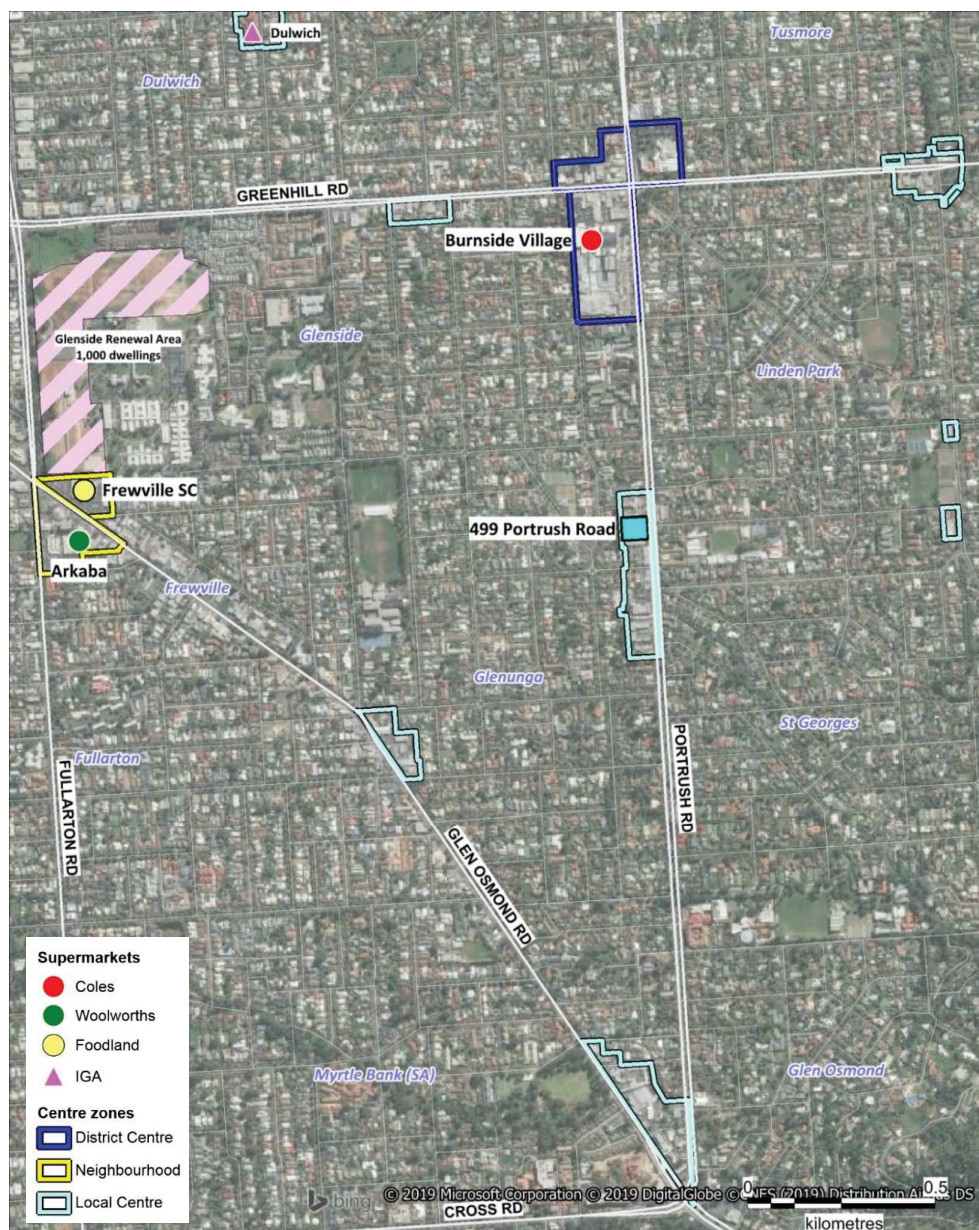
The area has had recorded low but steady population growth in the last 10 years however population levels will be boosted by the former Glenside Hospital redevelopment with 1,000 townhouses and apartments over the next 8-10 years.

The site is 550 metres south of the popular Burnside Village Centre which has a Coles supermarket and up-market fashion, homewares and eateries. Supermarkets on Glen Osmond Road at Frewville and Fullarton are about 1.6 km west of the site.

Portrush Road is a busy north-south arterial traversing the south-eastern and eastern suburbs between the Princes Highway and Payneham Road. It carries 34,000 vpd through Glenunga.

Plans show a freestanding, elevated ALDI store (2,070 sqm GFA) with undercroft and open area parking for 98 cars. A crossover at the northern end of the site enables full turning movements to and from the site. Southbound traffic on Portrush Road can turn right into the site via a dedicated turning lane. Cars turning right from the site onto Portrush Road have the protection of the central turning aisle.

Figure 2—Site location



Local context

The site and pattern of uses within and surrounding the Local Centre Zone is shown in Figure 3. It shows the zone is developed with a broad range of retail and commercial uses including:

- Multiple groups of shops and small commercial premises.
- A medical centre.
- Zee's eatery and adjoining specialist 'fine food' grocer known as 'Tony & Marks' which has been extended and appears to be at least 800 sqm GFA.
- A Dan Murphy's liquor store in excess of 1,000 sqm GFA.
- Several other restaurants, a motel and service station.

Another groups of shops, opposite the site, on the east side of Portrush Road is within the Residential Zone.

The proposed ALDI (2,070 sqm GFA) is considered to be a small supermarket. It is compatible with the range of existing shops and services including the specialist grocer and Dan Murphy's, the latter operating with a broad catchment extending through the eastern suburbs.

Figure 3—Proposed site and land use in the Local Centre zone



Source: Nearmap, Google, Deep End Services

ALDI network in eastern suburbs

ALDI has developed a network plan for Adelaide based on one store for approximately 30,000-40,000 people.

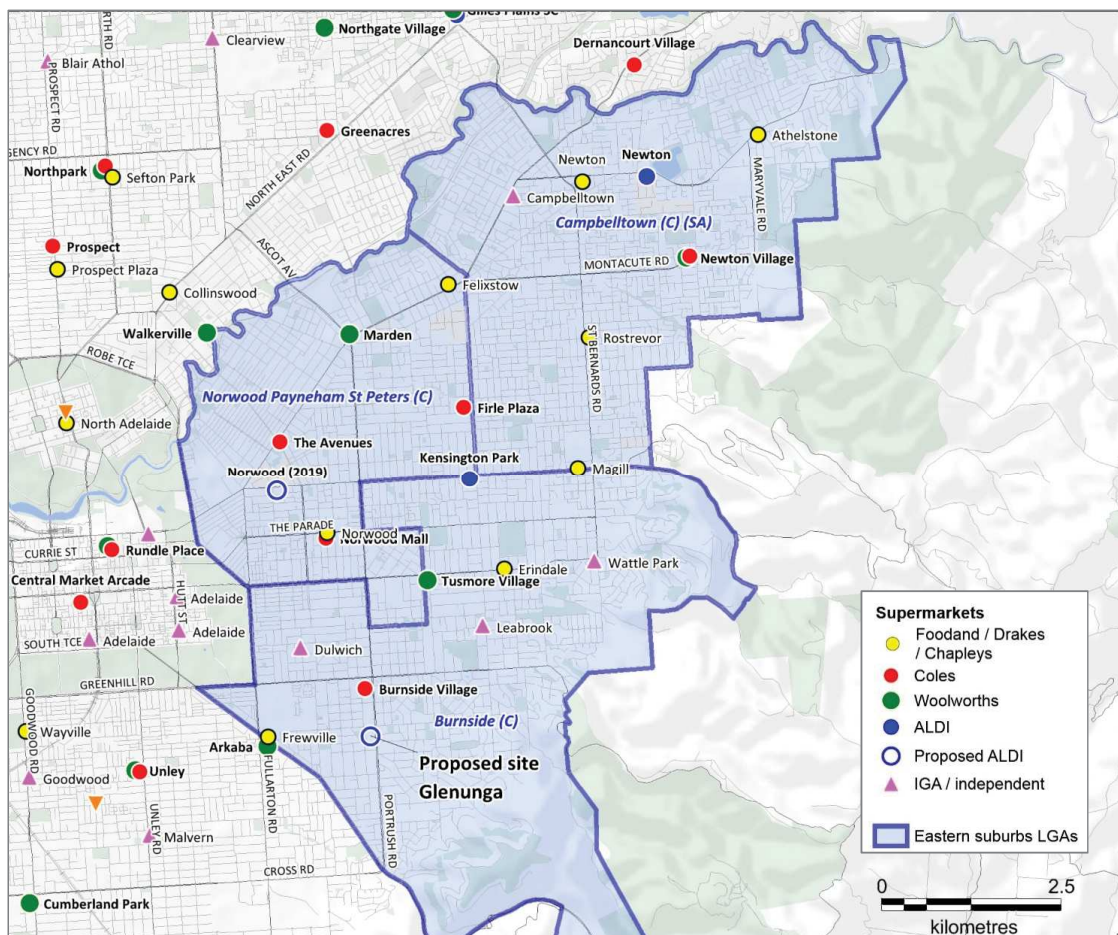
The distribution of existing supermarkets and ALDI stores (existing and proposed) in the eastern suburbs is shown in Figure 4.

ALDI's two existing stores in the eastern suburbs are:

- ALDI Kensington Park, a freestanding store situated in the Local Centre Zone (City of Burnside) on Magill Road that opened in September 2017. The underlying zone and range of shops and services along Magill Road Kensington Park is similar to the local centre on Portrush Road, Glenunga.
- ALDI Newton, a freestanding store on Gorge Road within the Urban Employment Zone (City of Campbelltown). The store opened in May 2019.

A third ALDI store is under construction on Magill Road, Norwood, located within the Urban Corridor Zone (City of Norwood Payneham St Peters).

Figure 4—Eastern suburbs distribution of ALDI stores and supermarkets



The proposed Portrush Road site would result in a 4-store network across the eastern suburbs, noting that the Glenunga store will also trade into areas west of Glen Osmond Road. The estimated 2018 population of the three local government areas (shaded blue in Figure 4) is 133,961 people – or an average of one ALDI store per 33,490 people. This is an efficient distribution and density of stores.

The Glenunga site appears to be an optimal distance from the Kensington Park store. Data provided to me by ALDI on the distribution of customers by postcode shows that less than 10% of shoppers at Kensington Park travel to the store from south of Greenhill Road. This indicates a large and underserved catchment centred on Glenunga and other southern suburbs of the City of Burnside.

Competition

There are 23 existing supermarkets across the three local government areas covering Adelaide's eastern suburbs.

The independent sector including the Foodland banners and larger operators such as Drake's, Romeo's and Chapley's have a combined 8 stores or 38% of the 48,538 sqm of supermarket floorspace (refer Table 1).

Woolworths and Coles combined have 8 of the 23 supermarkets and 50% of the supermarket floorspace.

ALDI, with just two existing stores at Kensington Park and Newton, has 7% of supermarket floorspace in the region. The proposed ALDI Norwood store will increase ALDI's share of floorspace in the region to 10% while the Glenunga store (assuming no other changes to competition by 2021) will grow ALDI's share to approximately 14%. This is still well below the other banner groups in store numbers and floorspace share.

Table 1—Eastern Suburbs supermarkets

Supermarket banner	No. stores	Floorspace (sqm GLA)	%
Woolworths	3	9,440	19%
Coles	5	15,278	31%
Foodland / Drakes / Chapley / Romeo's	8	18,354	38%
IGA / Other Independents	5	2,111	4%
ALDI	2	3,355	7%
Total	23	48,538	100%

Source: Deep End Services

In relation to more direct local supermarket competition to Glenunga:

- Coles at Burnside Village is a large (4,032 sqm) and long-established supermarket which has been operating from the centre since 1988. It appears to trade strongly from the successful, up-market complex where it has little competition in the immediate area.
- The popular and industry-awarded Foodland Frewville is a 1.6 km west of the site. The 2,900 sqm store has a good position with convenient parking on the

out-bound side of Glen Osmond Road. Its premium food and grocery range is well-suited to the high income profile of the catchment.

- Woolworths Arkaba (opposite Foodland Frewville) appears to be a lower-volume supermarket competing with Foodland and supermarkets at Unley, a short distance west.

Elsewhere, in Glenunga's surrounding residential catchment, there are several small centres under the Local Centre Zone. The only food and grocery offer is a small convenience store on Devereux Road, Linden Park.

Catchment area

The apparent success of ALDI Kensington Park since opening indicates there is a very strong need for value-driven supermarkets, even in areas of high relative affluence.

ALDI Glenunga will appeal to a range of geographic and market segments including:

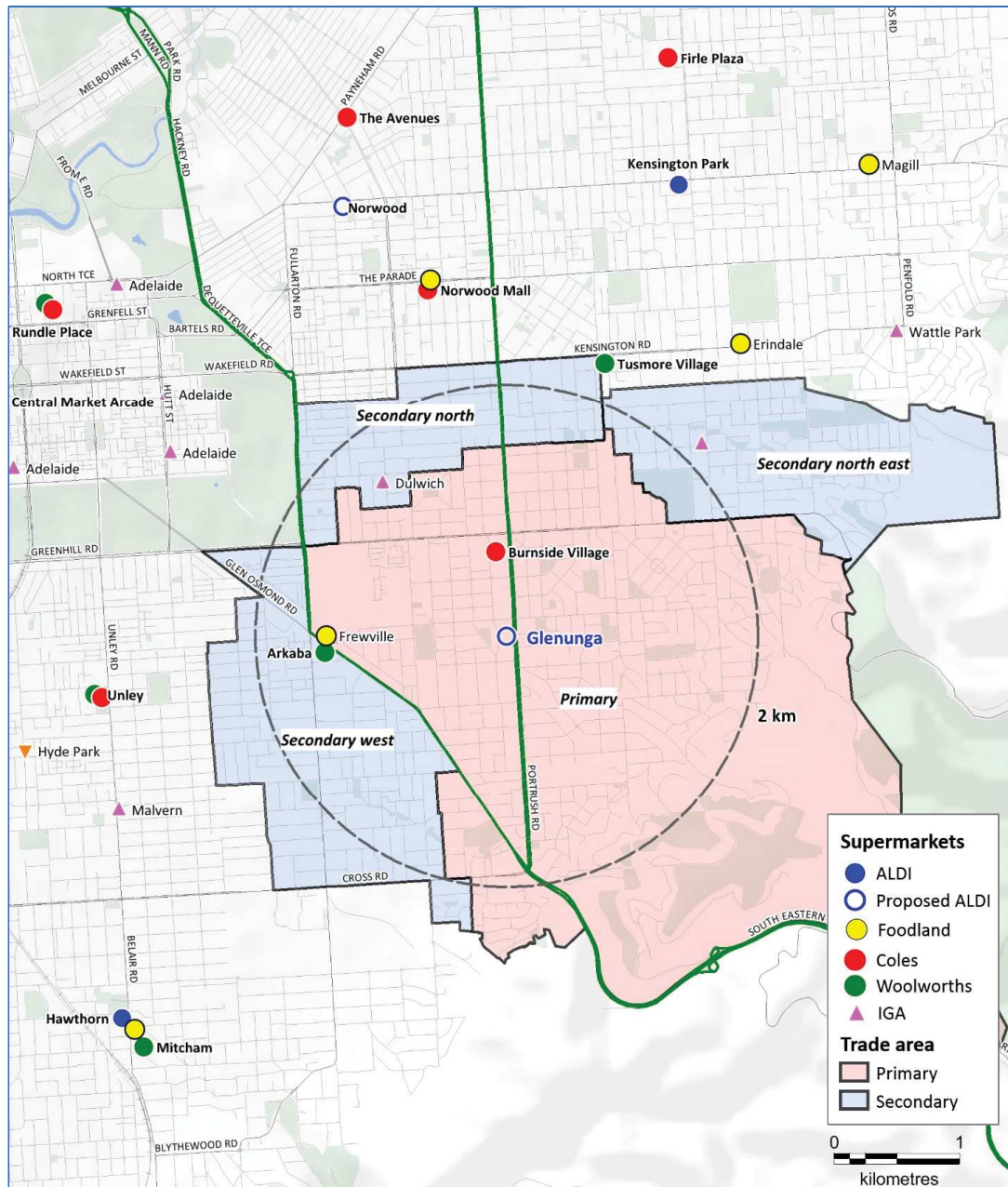
- A local primary catchment where residents have close and convenient access to the store and will compare ALDI's prices and merchandise range against supermarkets at Burnside and Frewville.
- Three secondary catchments where other centres and supermarkets are, in some cases, closer than Glenunga however ALDI's unique product offering and competitive prices will draw people on a less frequent, but still regular basis.
- Commuters and other passing traffic on Portrush Road which is used as a major cross-city by-pass road and route through to the Adelaide Hills. The visible and convenient parking at Glenunga will be in contrast to the busy and congested deck and basement / undercroft parking at Burnside Village.
- The small workforce in the Glenunga local centre who will have a convenient option to purchase groceries during or after work hours.

The local and broader secondary catchments where I expect ALDI will draw a high proportion of its sales from is shown in Figure 5.

The local or **primary catchment** is influenced by the strong north-south function of Portrush Road, the residential areas through to the hills face with no supermarket and the barrier effect of Glen Osmond Road and the Frewville / Arkaba supermarkets to the west. Most of the primary catchment falls within 2km of the site.

Two **secondary** north and north-east catchments are shallow extensions of the primary catchment and are influenced by the existing ALDI store at Kensington Park and proposed store at Norwood. The secondary west (west of Glen Osmond Road) still has good access to Glenunga although other supermarkets and ALDI Hawthorn will influence shopping patterns.

Figure 5—ALDI Glenunga trade area & competition



Population

The primary trade area had a 2016 Census population of 22,444 people. Between 2011 and 2016 it grew by approximately 1,000 people or 0.9% per annum - a rate higher than the Adelaide average (0.6% per annum) in the same period (refer Table 2).

The primary will grow at higher rates in the next 10 years as the former Glenside Hospital site is developed, accommodating up to 2,500 new residents within 1.6 km of the Glenunga site. Along with underlying growth, the primary is expected to conservatively add over 3,000 people in the next 10 years. The first stages of residential building approvals at the Glenside renewal site can be seen in the elevated residential building approvals in the primary sector in 2017/18 (refer Figure 6).

The three secondary catchments make up just over 20,000 people resulting in a total catchment area of 42,640 (2016) increasing to over 46,000 by 2025.

Annual spending on food & grocery (F&G) items by primary catchment residents is modelled at **\$146.6 million** per annum in 2021 and in the total catchment **\$269.4m**. Average per capita spending levels on F&G is well above the Adelaide average in all sectors, due to the high average income profile.

Table 2—Trade area population and food & grocery spending

Sectors	Population				F&G spend (\$m)		Per cap var'n from avge
	2011	2016	2021	2025	2016	2021	
Primary	21,446	22,444	24,045	25,565	115.7	146.6	+11.6%
Secondary sectors							
• north	4,812	4,809	4,861	4,901	25.0	29.9	+12.5%
• north east	4,958	5,012	5,075	5,115	26.4	31.6	+13.9%
• west	10,247	10,376	10,452	10,552	51.5	61.3	+7.4%
Total secondary	20,017	20,197	20,388	20,568	102.8	122.8	+10.2%
Total trade area	41,463	42,641	44,433	46,133	218.6	269.4	+11.0%
Avg annual growth		0.6%	0.8%	0.9%			
Adelaide avge			1.1%				\$4,832

Figure 6—Dwelling approvals by trade area sector

Source: ABS

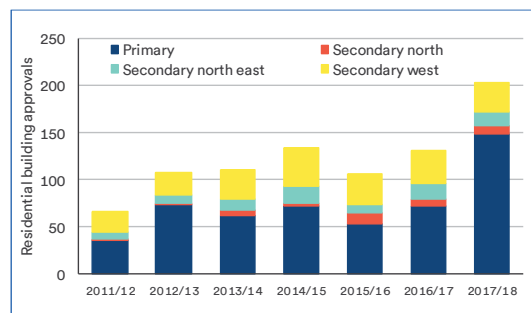


Table 3 summarises the income and household characteristics of the trade area. The catchment covers some of Adelaide's most affluent suburbs.

All trade area sectors have income levels well above the Adelaide average. Personal income levels in the primary sector are 45% above the Adelaide average, rising to 55-60% above average in the north sectors.

Families with children make up a similar proportion to the Adelaide average with average household sizes also close to average. The age structure is slightly older than average which is also reflected in high levels of home ownership.

**Table 3—
Demographic
profile (2016)**

Sectors	Income variation from avge		Avge h'hold size	Avge age	Aust. born	Home own'ship	Families with children
	Personal	H'hold					
Primary	+45.2%	+35.3%	2.46	43.2	68%	78%	42.0%
Secondary sectors							
• north	+61.4%	+52.1%	2.48	40.7	74%	71%	41.5%
• north east	+56.7%	+51.7%	2.49	43.5	71%	82%	42.8%
• west	+38.5%	+30.3%	2.35	42.7	76%	70%	39.6%
Total secondary	+48.5%	+40.6%	2.42	42.4	74%	73%	40.9%
Total trade area	+47.5%	+37.8%	2.44	42.8	71%	76%	41.4%
<i>Adelaide avge</i>	<i>\$44,252</i>	<i>\$83,748</i>	<i>2.46</i>	<i>39.8</i>	<i>72%</i>	<i>70%</i>	<i>42.1%</i>

Supermarket floorspace provision

A general measure of the rate of supermarket floorspace provision in a catchment or other defined area is made by dividing the resident population into the total supply of floorspace (sqm) and expressing this as a rate (sqm) per person. When compared with city or state-wide benchmarks, the current or projected rate for a given area is a simple guide as to whether retail floorspace supply levels are relatively high or low, before and after a proposed development.

City of Burnside

An analysis of our data base of all supermarkets in Adelaide yields an average metropolitan rate of provision of **0.42 sqm** of supermarket floorspace per capita. Figure 7 charts the averages for all Adelaide local government areas against the city average.

The City of Burnside has an estimated 13,278 sqm of supermarket floorspace for its 45,706 residents (2018) yielding an average rate of **0.29 sqm** per capita. This is the equal lowest rate of Adelaide's 20 municipalities. The adjoining City of Unley has a similar rate (0.29 sqm) exacerbating the generally low provision of supermarkets in the area.

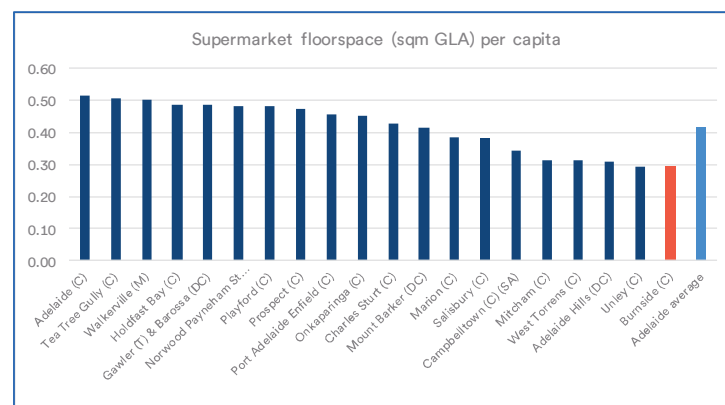
The effect of the proposed ALDI store at Glenunga is to increase the rate of provision (at 2018 population levels) to **0.34 sqm** per capita - a rate still 20% below the Adelaide average.

The potential effects from very low rates of supermarket floorspace provision are:

- A lack of choice and competition in the market which can result in higher prices for consumers.

- Residents travelling further and longer to access basic necessities.
- Congestion and parking difficulties in peak periods at centres or supermarkets.
- Lost employment opportunities.
- Lost opportunities to underpin or support local and neighbourhood centre retailers with a strong supermarket tenant.

**Figure 7—
Supermarket
floorspace provision
rates**



Glenunga catchment area

While the city-wide provision rates provide a high-level analysis, an assessment of the defined Glenunga trade area (which crosses municipal boundaries) can determine whether there is any under or oversupply of supermarket floorspace at the catchment or regional level.

Table 4 calculates rates of supermarket floorspace provision for the Glenunga trade area adopting 2018 small-area population estimates and 2019 supermarket floorspace.

Based on a 2018 population of 43,100 people and 12,028 sqm of supermarket space, the current rate of supermarket floorspace provision in the trade area is **0.28 sqm per capita** – a rate 34% below the Adelaide average of 0.42 sqm per capita.

If the proposed Glenunga ALDI is factored in, the rate increases to 0.33 sqm, still 22% below the Adelaide average.

On either the municipal or trade area based spatial area, the Glenunga catchment area has very low rates of supermarket floorspace provision. This deficiency is more acute given the high income levels and higher average spending on food and groceries which can support higher-than-average levels of supermarket floorspace supply.

Table 4—Glenunga trade area supermarket floorspace provision

Glenunga trade area	2019
Primary catchment	(sqm)
• Coles Burnside	4,032
• Foodland Frewville	2,900
Secondary catchment	
• IGA Dulwich	326
• IGA Leabrook	320
• Woolworths Tusmore Village (50% GLA)	1,350
• Woolworths Arkaba	3,100
Total Supermarket floorspace	12,028
Trade area population	43,100
Supermarket floorspace provision (sqm capita)	0.28
<i>Adelaide Average (sqm capita)</i>	<i>0.42</i>

Source: Deep End Services

Impacts

Analysis of ALDI stores in Adelaide and other metropolitan areas shows that ALDI tends to generate a low market share of food and grocery spending over a wide area - as opposed to very high, dominant market shares of local areas that larger full-range supermarket chains tend to generate.

ALDI's low market penetration - often resulting from customers cross-shopping at a range of supermarkets depending on their needs - supports the argument that ALDI's impacts on surrounding supermarkets (and centres) in a competitive market are generally quite low.

The expected impacts are also low because:

- The ALDI store at 2,070 sqm (or 1,266 sqm of retail area) is about half the size of a major supermarket and represents a 16% increase in supermarket floorspace across the City of Burnside - an area of 45,700 people in 2018.
- Supermarket provision rates are very low and when combined with high average incomes and high F&G spending levels, it suggests existing supermarkets are probably trading at well above average trading levels. In this context, supermarkets at Burnside Village and Frewville can easily withstand small, or even moderate, trading impacts.
- Some of ALDI's sales at Glenunga will be transferred from other ALDI stores, including Kensington Park and Hawthorn.
- A high proportion of ALDI's retail floorspace is dedicated to 'special buys'. These are mainly clothing, household and other general merchandise lines which are unique to ALDI and not generally sold in large quantities Coles, Woolworths or independent supermarkets.
- The effect of Portrush Road with high levels of through and commuter traffic will be to draw customers passing through the area who will divert small levels of spending from a wide range of centres and supermarkets, potentially some distance away.

- The proposal is a stand-alone store with no specialty shops or other retailing. The small impacts are therefore confined to the large well-resourced supermarkets in the area.

Since ALDI has been operating in South Australia for over three years, there are no instances that I am aware of where a small or large supermarket has closed due to ALDI's presence.

In overall terms, impacts on existing supermarkets in the area are likely to be relatively low and almost negligible on centre trading levels. The expected small sales re-allocations from strong-performing supermarkets across the catchment are likely to be within the tolerance levels of a normal competitive environment where retail turnover naturally fluctuates with changes in economic and market conditions.

Benefits

ALDI has been well-received in South Australia. It will bring obvious benefits to low income and other price-conscious families in an area where, notwithstanding the high average income levels, many grocery buyers and families are drawn to better value products and pricing.

The eastern and inner southern suburbs have very low levels of supermarket floorspace provision. The introduction of ALDI improves the range and choice of supermarkets (and products) and helps address the current deficiency. Given the planning, development and ownership constraints at existing centres, there are very few opportunities to viably develop an ALDI store in other parts of the City of Burnside.

The Glenunga site sets up an efficient regional network of stores extending from Newton to Kensington Park and then south to Glenunga and across to Hawthorn. Glenunga fits a gap between Kensington Park and Hawthorn where most residents will be no further than 2km from an ALDI store.

The site is central to the Local Centre Zone and is similar to the establishment of ALDI Kensington Gardens in the Local Centre Zone on Magill Road (City of Burnside). There is nothing to suggest that the Kensington Park store is incompatible with the nature of its adjoining retail and commercial uses in the zone. On the contrary, it has brought vehicle and foot traffic to the centre that would otherwise go elsewhere.

Various benefits can be anticipated at Glenunga. The proposal re-installs a supermarket back on to the site where a small supermarket (Bi-Lo) operated for many years.

ALDI customers drawn to the site may frequent other businesses in the strip. In the longer term, ALDI's presence could see new businesses establish or adjoining properties redevelop to leverage the improved profile of the centre.

I am advised that the existing Mitre 10 store was expected to close once the site was placed on the market by the current business and property owner. ALDI therefore guarantees 15-20 employment positions will be replaced on the site. The new jobs to the Glenunga Centre are unlikely to result in job losses elsewhere.

In all respects, the proposed ALDI store will result in a range of benefits to the local community, the broader catchment and the Glenunga Centre.

Should you have any questions please contact the undersigned.

Kind regards



Chris Abery
Director

In reply please quote 2019/00292, Process ID: 596269
Enquiries to Reece Loughron
Telephone (08) 7109 7876
E-mail dpti.luc@sa.gov.au



Government of South Australia

Department of Planning,
Transport and Infrastructure

**TRANSPORT PLANNING AND
PROGRAM DEVELOPMENT**

Transport Assessment

GPO Box 1533
ADELAIDE SA 5001

ABN 92 366 288 135

29 October 2019

State Commission Assessment Panel
C/- Ms Yasmine Alliu
Department of Planning, Transport and Infrastructure
GPO Box 1815
ADELAIDE SA 5001

Dear Ms Alliu

SCHEDULE 8 - REFERRAL RESPONSE

Development No.	180/E012/19
Applicant	Nielsen Architects
Location	499 Portrush Road, Glenunga (Lots 27, 28 and 29 in FP 16174)
Proposal	Demolition of all existing structures and staged construction of a freestanding supermarket(shop – 1266m ²) with associated earthworks, retaining wall, car park, advertising displays, fencing, solar panels and landscaping

I refer to the above development application forwarded to the Commissioner of Highways (CoH) in accordance with Section 37 of the *Development Act 1993*. The proposed development involves development adjacent a main road as described above.

The following response is provided in accordance with Section 37(4)(b) of the *Development Act 1993* and Schedule 8 of the *Development Regulations 2008*.

CONSIDERATION

The subject site abuts Portrush Road, an arterial road and national highway under the care, control and management of the CoH. The adjacent section of Portrush Road is identified as a Major Traffic Route, Primary Freight Route, Major Cycling Route, High Activity Pedestrian Area and Standard Frequency Public Transport Corridor under the Department of Planning, Transport and Infrastructure's (DPTI's) '*A Functional Hierarchy for South Australia's Land Transport Network*' and is gazetted for 26.0 metres B-Double vehicles. At this location, Portrush Road carries approximately 37,800 vehicles per day (7.5% commercial vehicles) and has a posted speed limit of 60 km/h.

Access and Road Safety

It is proposed that the supermarket will be accessed via a single two-way access on Portrush Road adjacent to the northern property boundary, with the existing entry point located opposite Hay Road being closed. Modifications to the central median on the adjacent section of Portrush Road are proposed to enable vehicles waiting to turn right into the site to store in a sheltered lane and thus not interfere with the free flow of traffic on Portrush Road. The

modifications are shown in 'Figure 5.1: Proposed Intersection Treatment – Portrush Road' in the Traffic Impact Assessment (TIA) prepared by GTA Consultants dated 13 October 2019. The right turn lane proposed into the site is 55 metres long, including a 20 metres long taper and 35 metres of storage (approximately 6 vehicles). It is noted that the right turn out median opening for the existing motel to the north is proposed to be closed and that the length of the Mariner Street right turn lane will be reduced by 10 metres to create additional queuing storage for the right lane into the proposed development. In principle, DPTI supports the proposed access and median modifications.

Delivery Vehicles

The TIA indicates that the largest vehicle expected to enter and exit the site is a 15.5 metres long articulated vehicle and that these vehicles will generally approach the site from the south and undertake left turn in and left turn out movements from and to Portrush Road. Figures 7.1 to 7.3 show that this vehicle can enter and exit the site in a forward direction and that the truck manoeuvres will take place at the rear of the site, with minimal interruption to light vehicle traffic circulating the aisle. These arrangements are supported.

Signage

Illuminated signage is proposed on this site as shown on Nielsen Architects Signage Plan, Project No. 2353SL1, Drawing No. DA05.1 dated September 2019. DPTI has released the '*Advertising Signs - Assessment Guidelines for Road Safety*' to assist with the review and assessment of advertising signs abutting the arterial road network. DPTI has reviewed the proposed signage against the above guidelines and recommends that appropriate conditions be applied to any approval in the interests of road safety.

ADVICE

The planning authority is advised to attach the following conditions to any approval:

1. Access to the site shall be in general accordance with Nielsen Architects Lower Site Plan, Project 2353SL1, Drawing No. DA02.3 dated September 2019.
2. The proposed modifications to the central median on the adjacent section of Portrush Road, as shown in 'Figure 5.1: Proposed Intersection Treatment – Portrush Road' in the Traffic Impact Assessment (TIA) prepared by GTA Consultants dated 13 October 2019, shall be designed in accordance with the relevant Austroads Guides, Australian Standards and to DPTI's requirements. All costs associated with the design and construction of the proposed median modifications (including, but not limited to, project management and any necessary public consultation, road drainage, lighting upgrades etc.) shall be borne by the applicant. The applicant shall enter into a Developer Agreement with DPTI regarding these works and shall contact DPTI's Traffic Services Section, Network Planning Engineer, Ms Teresa Xavier on (08) 8226 8389 or via email at Teresa.Xavier@sa.gov.au to obtain approval and discuss any technical issues regarding the proposed median modifications. The proposed median modifications shall be completed prior to the proposed development becoming operational.
3. All vehicles shall enter and exit the site in a forward direction.
4. All off-street parking shall be designed in accordance with AS/NZS 2890.1:2004 and AS/NZS 2890.6:2009. Additionally, clear sightlines, as shown in Figure 3.3 'Minimum Sight Lines for Pedestrian Safety' in AS/NZS 2890.1:2004, shall be provided at the Portrush Road property line to ensure adequate visibility between vehicles leaving the site and pedestrians on the adjacent footpath.

5. All off-street commercial vehicle facilities shall be designed in accordance with AS 2890.2:2018.
6. The largest vehicle permitted on-site shall be restricted to a 15.5 metres long articulated vehicle.
7. A Traffic Management Plan for the construction period of the development shall be produced to the satisfaction of DPTI and Council prior to the commencement of construction. This plan shall detail the types, volumes and distributions of traffic and how they will be managed.
8. Illuminated signage that is visible from the adjacent roads shall not contain any element that flashes, scrolls, moves or changes, or imitates a traffic control device and shall be limited to a low level of illumination (i.e. $< 150\text{Cd/m}^2$).
9. Any flood lighting shall be located and shielded in order to minimise the potential for driver distraction and discomfort.
10. Stormwater run-off shall be collected on-site and discharged without jeopardising the integrity and safety of the adjacent road network. Any alterations to drainage infrastructure required to facilitate this shall be at the applicant's cost.

Yours sincerely



A/MANAGER, TRANSPORT ASSESSMENT
for **COMMISSIONER OF HIGHWAYS**

A copy of the decision notification form should be forwarded to dpti.developmentapplications@sa.gov.au

31 October 2019

State Commission Assessment Panel
GPO Box 1815
ADELAIDE SA 5001

Sent via email: scapadmin@sa.gov.au

Dear Sir/Madam

Decision Notification

Development Application:	180\1006\19
Applicant:	Nielsen Architects
Location:	Mitre 10 499 Portrush Road GLENUNGA SA 5064
Nature of Development:	Schedule 10 development - Demolition of all existing structures and staged construction of a freestanding supermarket (ALDI)

Thank you for referring the above application to the City of Burnside for consideration and comment pursuant to Section 33(1) of the Development Act 1993 (the Act).

Council is generally supportive of the proposal to establish an Aldi supermarket in this location based largely on the nature and scale of the existing land use, however consideration should be given to design modifications aimed at reducing the size, scale and visual massing of the proposed building such to ensure it is more in keeping with the "small scale" character envisaged by the Zone provisions and avoids adverse impacts on the surrounding Residential Zone.

The subject land is located within the Local Centre Zone (LCZ) in the Burnside City Development Plan, which envisages *"a range of shops, offices, business, health and community service providers all being of a small-scale suited to servicing the needs of the local community"* (my underlining).

The LCZ anticipates a range of shops, offices, business, health and community service providers of a small-scale suited to servicing the needs of the local community. Further, buildings containing more than one level, unless the lower floor level is for provision of car parking and is set at a level of not less than one metre below the predominant ground level of the site, are not encouraged within the LCZ. Portions of the proposed building exceed a typical two storey building height.

The locality does include buildings situated close to the street frontage, however these generally have a lesser building height so to present to the public realm at a comfortable human scale. In comparison, the proposed building is larger, taller and closer to external boundaries than the existing Mitre 10 building and therefore has greater potential to be a visually dominant feature within the streetscape, to the detriment of the envisaged small scale character of the Zone.

Consideration should therefore be given to increasing the front boundary set-back from that provided to off-set the scale and height of the building and providing greater space for landscaping in front of the building that is more proportionate to the scale of the building.

The Development Plan also seeks to protect and enhance visual amenity of the local area by ensuring a high standard of design in respect of the appearance of development.

It is anticipated that the siting of the proposed building close to the southern boundary will alter the outlook for occupants of the units at 505 Portrush Road, Glenunga, while the reduced building set-back to the western rear boundary is anticipated to have an adverse visual impact on the outlook from rear facing habitable rooms and private open space areas on adjoining residential properties to the west, particularly those at 78, 80, 82 and 84 Sydney Street.

Consideration should be given to increasing the building set-backs from adjoining residential properties to the south and west, such that the visual scale and massing of the proposed building are more consistent with small scale character envisaged for the Zone and to enable the establishment of landscaping that is more proportionate to the scale of the building. Appropriate choice of plant species in areas which will be frequently shaded by the proposed building should also be given due consideration.

With regard to operational matters, there are many factors associated with the proposed land use, which have the potential to impact on the amenity of surrounding sensitive land uses. Accordingly, the following should be given further consideration:

- Appropriateness of the level of illumination of the proposed signage, and the potential impact on the amenity of surrounding sensitive land uses and motorists;
- Appropriately conditioning operating hours, delivery time and service arrangements so as to not detract from residential amenity of adjoining properties; and
- Appropriately conditioning the waste storage area to be maintained so to not be unsightly from public view and to not result in unpleasant smells that could be experienced from adjoining allotments.

With regard to traffic and stormwater management, Council is generally satisfied with the merits of the proposed development, subject to approval being sought for any and all alterations to the existing verge as a separate process under Section 221 of the Local Government Act 1993, noting that the existing redundant portion of driveway/gutter crossing must be removed and reinstated to kerb and gutter upon the completion of the proposed altered gutter crossing.

Lastly, it is recommended that the following information be requested of the Applicant to ensure a full and thorough assessment of all relevant planning matters relating to this form of development be considered in detail:

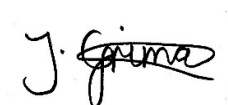
- An Outdoor Lighting Plan identifying location and design of all security lighting, lighting within parking areas, lighting of vehicles used to service the site outside of operating hours, and illumination levels (LUX detail);
- A detailed Landscaping Plan including detailing species, location, spacing, planting heights and mature heights; and

- Identification and consideration of any regulated and/or significant trees on adjoining land in proximity to the subject land.

Thank you again for the opportunity for Council to provide feedback in relation to this matter.

If you require further information or clarification, please do not hesitate to contact me.

Yours faithfully



Jessica Grima
Development Officer – Planning

Phone: 8366 4170

Email: jgrima@burnside.sa.gov.au

**South Australian
DEVELOPMENT ACT, 1993
REPRESENTATION ON APPLICATION – CATEGORY 2**

Applicant: Nielson Architects
Development Number: 180/E012/19
Nature of Development: Demolition of all existing structures and staged construction of a freestanding supermarket (ALDI), incorporating an undercroft carpark
Development Type: Merit
Zone / Policy Area: Local Centre Zone
Subject Land: 499 Portrush Road Glenunga
Contact Officer: Yasmine Alliu
Phone Number: 08 71097076
Close Date: 22 October 2019

My Name: Anita Derbali **My phone number:** 0430 350 013

Primary method(s) of contact: Email: derbali@apt.net.au
Postal Address: _____ **Postcode:** _____

You may be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.

My interests are:
(please tick one)

- ☐ owner of local property
- ☒ occupier of local property
- ☒ a representative of a company/other organisation affected by the proposal
- ☐ a private citizen

The address of the property affected is: 6/476 Portrush road Linden Park **Postcode** 5065

My interests are:
(please tick one)

- ☐ I support the development
- ☒ I support the development with some concerns
- ☐ I oppose the development

The specific aspects of the application to which I make comment on are: My concerns are the proposed change to the road. I may affect the flow of traffic into the row of shops. It appears provision is made for traffic to enter Aldi but not 476 Row of 5 shops all SA owned small businesses

I: ☐ wish to be heard in support of my submission
(please tick one) ☒ do not wish to be heard in support of my submission
(Please tick one)

By: ☐ appearing personally
(please tick one) ☐ being represented by the following person
(Please tick one)

Signature: A. Derbali

Date: 15/10/19

Alliu, Yasmine (DPTI)

Subject: FW: Emailing: PDF Low Res001.PDF, PDF Low Res.PDF
Attachments: Emailing: PDF Low Res001.PDF, PDF Low Res.PDF

From: derbali@aapt.net.au [<mailto:derbali@aapt.net.au>]
Sent: Wednesday, 16 October 2019 2:43 PM
To: DPTI:State Commission Assessment Panel <scapadmin@sa.gov.au>
Subject: Fwd: Emailing: PDF Low Res001.PDF, PDF Low Res.PDF

Hi Yasmine

Please find attached concerns over the road changes for the new Aldi development.

Could we please have the access to our row of shops be considered during and after the process of development. We are very concerned as we have heard of so many sad stories of small businesses in South Australia that have had to close their doors due to road works affecting their trade.

Thankyou

Kind regards

Anita Derbali

South Australian
DEVELOPMENT ACT, 1993
REPRESENTATION ON APPLICATION – CATEGORY 2

Applicant: Nielson Architects
Development Number: 180/E012/19
Nature of Development: Demolition of all existing structures and staged construction of a freestanding supermarket (ALDI), incorporating an undercroft carpark
Development Type: Merit
Zone / Policy Area: Local Centre Zone
Subject Land: 499 Portrush Road Glenunga
Contact Officer: Yasmine Alliu
Phone Number: 08 71097076
Close Date: 22 October 2019

My Name: Paul Derbali **My phone number:** 0412 652190

Primary method(s) of contact: **Email:** derbali@adpt.net.au
Postal Address: _____ **Postcode:** _____

You may be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.

My interests are:
(please tick one)

☐ owner of local property
☒ occupier of local property
☐ a representative of a company/other organisation affected by the proposal
☐ a private citizen

The address of the property affected is:

4/476 Portrush Road Linden **Postcode** 5065

My interests are:
(please tick one)

☐ I support the development
☒ I support the development with some concerns
☐ I oppose the development

The specific aspects of the application to which I make comment on are:

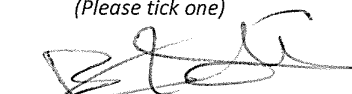
Not Clear on the flow of traffic into 476 row of shops.

I: ☐ wish to be heard in support of my submission
(please tick one) ☒ do not wish to be heard in support of my submission
(Please tick one)

By: ☐ appearing personally
(please tick one) ☐ being represented by the following person
(Please tick one)

Signature:

Date:


15-10-19

South Australian
DEVELOPMENT ACT, 1993
REPRESENTATION ON APPLICATION – CATEGORY 2

RECEIVED

17 OCT 2019

Applicant: Nielson Architects
Development Number: 180/E012/19
Nature of Development: Demolition of all existing structures and staged construction of a freestanding supermarket (ALDI), incorporating an undercroft carpark
Development Type: Merit
Zone / Policy Area: Local Centre Zone
Subject Land: 499 Portrush Road Glenunga
Contact Officer: Yasmine Alliu
Phone Number: 08 71097076
Close Date: 22 October 2019

My Name: Selina Zhou My phone number: 0426937267

Primary method(s) of contact: Email: ZHOUCHANG_ZC@hotmail.com
Postal Address: 493 Portrush Road Postcode: 5064
Glenunga

You may be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.

My interests are:
(please tick one)

☐ owner of local property
☒ occupier of local property
☐ a representative of a company/other organisation affected by the proposal
☐ a private citizen

The address of the property affected is:

493 Portrush Road, Glenunga Postcode 5064

My interests are:
(please tick one)

☐ I support the development
☐ I support the development with some concerns
☒ I oppose the development

The specific aspects of the application to which I make comment on are: These whole process of demolition and rebuild the new market will be very noisy, that would influence my clients staying in our Hotel.

I: ☒ wish to be heard in support of my submission
(please tick one) ☐ do not wish to be heard in support of my submission
(Please tick one)

By: ☐ appearing personally
(please tick one) ☐ being represented by the following person
(Please tick one)

Signature: Selina Zhou
Date: 14/10/2019

**South Australian
DEVELOPMENT ACT, 1993
REPRESENTATION ON APPLICATION – CATEGORY 2**

Applicant: Nielson Architects
Development Number: 180/E012/19
Nature of Development: Demolition of all existing structures and staged construction of a freestanding supermarket (ALDI), incorporating an undercroft carpark
Development Type: Merit
Zone / Policy Area: Local Centre Zone
Subject Land: 499 Portrush Road Glenunga
Contact Officer: Yasmine Alliu
Phone Number: 08 71097076
Close Date: 22 October 2019

My Name: Calvin Chin **My phone number:** Chin 0416 228 007

Primary method(s) of contact: **Email:** chiuskids@gmail.com
Postal Address: _____ **Postcode:** _____

You may be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.

My interests are:
(please tick one)

- ☒ owner of local property
- ☐ occupier of local property
- ☐ a representative of a company/other organisation affected by the proposal
- ☐ a private citizen

The address of the property affected is:

78 Sydney St, Glenunga **Postcode** 5064

My interests are:
(please tick one)

- ☐ I support the development
- ☐ I support the development with some concerns
- ☒ I oppose the development

The specific aspects of the application to which I make comment on are: Please refer to attached page

I: ☒ wish to be heard in support of my submission
(please tick one) ☐ do not wish to be heard in support of my submission
(Please tick one)

By: ☒ appearing personally
(please tick one) ☐ being represented by the following person
(Please tick one)

Signature: chi

Date: 21 / 10 / 2019

These are the comments on different aspects of the development from Calvin Chiu of 78 Sydney Street, Glenunga, SA 5064:

1. Noise

- a. It is expected that there will be more frequent car park activities and vehicle movements than existing Mitre 10 throughout the day. Thus it is reasonable to control the noise generated to a lower level than existing.
- b. It is unacceptable to allow a semi-trailer doing deliveries, loading and unloading on a 24/7 basis which can cause great disturbance particularly in the midnight. However, in Figure 6.4 (or Appendix A - Site Plan), the Acoustic Screening is not to be installed at the northern part of the western boundary of the subject site. The time allowed for goods deliveries should also be limited.

2. Overshadowing

In Appendix 8 - Landscaping Concept Plan, it proposed that there will be medium trees *Pyrus calleryana* 'Chanticleer' to be planted at the western boundary of the subject site. This affects the sun light entering to my backyard and dwelling, as well as the solar PV generation which the system was recently installed. It is not expected that the new Fencing or Trees, whichever higher, should exceed the height of existing fencing.

3. Odour / Waste bin

In the plan section 6.9.3 Odour, it mentioned that "The waste storage area will be located approximately 20 metres from the nearest residential dwelling located to the west". Yet it should be measured against the boundary of my backyard as my children usually play there. Having odour will definitely affect the use of backyard and its amenities. It is not tolerable on any odour noticeable at the playground/backyard.

4. Rats / Pests Problem

There is nothing written in the plan to regularly control Rats and Pests (particularly cockroaches), which are not uncommon to find in grocery shops, infesting the surrounding areas.

**South Australian
DEVELOPMENT ACT, 1993
REPRESENTATION ON APPLICATION – CATEGORY 2**

Applicant: Nielson Architects
Development Number: 180/E012/19
Nature of Development: Demolition of all existing structures and staged construction of a freestanding supermarket (ALDI), incorporating an undercroft carpark
Development Type: Merit
Zone / Policy Area: Local Centre Zone
Subject Land: 499 Portrush Road Glenunga
Contact Officer: Yasmine Alliu
Phone Number: 08 71097076
Close Date: 22 October 2019



My Name: MAGGIE & BOB SCHAPEL **My phone number:** 0409282181

Primary method(s) of contact: **Email:** bobschapel@optusnet.com.au
Postal Address: 80 SYDNEY STREET
GLENUNGA, SOUTH AUSTRALIA **Postcode:** 5064

You may be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.

My interests are:
(please tick one)

☒ owner of local property
☒ occupier of local property
☐ a representative of a company/other organisation affected by the proposal
☐ a private citizen

The address of the property affected is:

80 SYDNEY STREET, GLENUNGA, SOUTH AUSTRALIA **Postcode** 5064

My interests are:
(please tick one)

☐ I support the development
☐ I support the development with some concerns
☒ I oppose the development

The specific aspects of the application to which I make comment on are: NOISE, VISUAL EFFECTS,
WASTE, PLANS APPEAR INCORRECT / MISLEADING, NON COMPLIANCE

I: ☒ wish to be heard in support of my submission
(please tick one) ☐ do not wish to be heard in support of my submission
(Please tick one)

By: ☒ appearing personally
(please tick one) ☐ being represented by the following person
(Please tick one)

Signature: R Schapel
Date: 21/10/19

/

Re ALDI at 499 Portrush Road: Maggie and Bob Schapel 80 Sydney Street, Glenunga

The plans obviously do not comply with the regulations for the zone. Amenity of the neighbouring properties will be seriously damaged under these plans. As we are retired, our home and backyard are heavily used by us, our children and grand-children. The following, are the main reasons we strongly oppose the development.

GENERAL

- The building will dominate the "amenity". At over 550 sq.m. more area and 2.4 metres greater height than Dan Murphy's and Tony and Mark's, It will dwarf the two Shops which the plans use for comparison. These other shops are also at least 23 metres further from residential boundaries. In addition, ALDI will be elevated on portion of existing land-filling at the rear. It would be an enormous building with a dominant façade.
- The plans do not comply and the purpose is not needed in our district. We have Coles, Woolworths, Foodland and Drakes all within a short distance. Other stores, such as Tony and Mark's are also close by. **The proposal omits the Glenunga "Drakes" from their (figure 6.2) local area map!** That is another supermarket of approximately 800 sq.m.
- Traffic on Portrush Road is already very heavy. Semi-trailers, trucks and customer cars will make the area even busier. ALDI will attract customers from outside the intended local district, including other ALDI stores. A Mitcham Resident told me that ALDI caused increased traffic problems at that location. Local residential streets will become busier.

NOISE

- Semi-trailers and other **trucks moving and unloading only a couple of metres behind our back fence at any time 24/7**. A resident from behind Mitcham ALDI told me: "I hear screeching sounds *inside* the building during loading." "Loading takes a long time because they are big trucks." "Residents had to notify Council because of noise before 7am." She, and an ALDI staff member both told me the Mitcham Council had a curfew on loading.
- In comparison, staff at Dan Murphy's told me they only accept deliveries between 9am and 5 pm. I measured Dan's rear delivery doors at 40 metres from the residential fence.
- Proposed Sound absorption materials will not sufficiently stop noise reflection from the tall opposite wall, or sound travelling through the fence further north of that material.
- Plans state that truck **reversing beepers and air conditioners** are turned off "where required." **No indication is given as to who does that "requiring"**. Mitcham ALDI plans state they are turned off outside normal trading hours. We object to these noises at any time. I have been told it is illegal to turn reverse beepers off.
- **Compactor noise is 81 dBA** according to the plans. Our petrol lawnmower is marked 75 dBA. 81 dBA is unacceptable, especially if used several times a day, 7am to 10 pm. A compactor should not be allowed outside. Plans don't say how often it will be used or emptied by yet another truck. (Mitcham ALDI does not have a compactor. It has a bailer inside.)
- Noise from rubbish bin being emptied by a truck.
- Noise of PA system. The Mitcham resident said that was sometimes an issue.
- Expected noise levels are only "averages". **Maximum dBA should also be limited**. Short, sudden loud noises don't increase the average dBA by much, yet are very disruptive, especially at night. If overnight loading is allowed, it is quite feasible that residents could often be woken by short loud noises even though "average" noise level appears acceptable.

- Plans clearly do not conform to PDC 28 which states that outer storage, loading and service areas should be sited away from sensitive land uses. It is only 1.5 metres away! The plan calls these "closest noise sensitive receivers." Our back yard is VERY special to us and **ambience is very noise sensitive!**
- Noise levels quoted in the Plans **appear to be right on the 45 dBA night limit** allowed by EPA. **However, they will exceed that limit because the "measurement place" does not match EPA policy Part 3, 12 (4).** Sonus quotes night noise *at* the back windows but this location only applies, "If the Authority determines that it is not practical or relevant to locate the measuring place outside" (Measurement at windows is obviously for situations such as high-rise buildings which do not have outside yards.)

Sonus, on Ekistics behalf, have measures current noise levels near the rear boundary. **Unlike the plan suggestions, current noise is only during trading hours**, however, levels are well above what they were meant to be. In 1997, an oversight by Burnside Council omitted the following quoted commitments from the Mitre 10 approval requirements. This has allowed levels well above the plan's "promises", and well above what was expected by residents.

Letter from Bone and Tonkin Planners (Alan Rumsby) to Burnside City Manager (Rodney Donne), 15 Jan 1997 (Their ref. 6522.let) Page 5 clearly states, "No goods handling will take place along the western residential boundary."

Report 5185 from Director of Development Services (R Green), to the Planning and Building Committee, 10 Jan 1997 Page 3 clearly states, "No goods deliveries will take place along the western elevation." and "No goods handling will take place along the western residential boundary."

Copies of some relevant pages from those plans are in APPENDIX

VISUAL ISSUES

- The enormous height of the building in general, particularly behind 82 Sydney Street where the planned wall is closer to the boundary, is bad for amenity. The proposed walls at 9.4 metres are double the height of the current 4.5 metre walls (which I have measured myself).
- A 3 metre fence is not high enough to fully shield a semi-trailer or the rubbish bin when lifted during collection. Mitcham ALDI loading bay floor is about 3.3 metres below fence top. It also has a watercourse with established heavy foliage as buffer between it and neighbouring properties. Our situation is worse as our property is much closer.
- 1.5 metres of landscaping is not enough to shield such a huge building.

WASTE STORAGE

- Plans state that the waste bin will be 20 metres from nearest residence. But it is only 1.5 metres from our very heavily used back yard! When the rubbish truck is emptying the bin, it will be elevated well above the three metre fence, in full view and likely to emanate odours, dust and debris, particularly when there is an easterly wind. Our grand-children's "Fairy-Garden" is against our back fence!

PLANS APPEAR INCORRECT or MISLEADING

Maggie & Bob Schapel

- In the Plans, day-time noise estimates are at our back boundary, while night time noise estimates are unjustly at our back windows. This gives a false impression of noise reduction during the night.
- Plans state that the current building wall height is about 7 metres. (Ekistics page 7) I have measured it with a tape measure at 4.5 metres (4.8 at ridge). We note that, plans include no elevation comparing the height of the proposed store with the height of the current store. Yet there are plan views clearly comparing the footprints of the two buildings.
- Plans appear to be incorrect as they do not show the current retaining wall at the bottom of the embankment near the western boundary. We are concerned that the building will be even higher than stated. Retaining wall height behind #80 is about a metre. Stated land level might be inaccurate because of the debris which has come down the slope since the building was done over 40 years ago. The original fence is still intact on our rear boundary. The fence is high on our side but looks much lower from the Mitre 10 side. This shows there is a lot of debris between it and the current retaining wall, giving a false impression of real land level.
- Plans say 4 single storey dwellings at rear but in fact one is two storey. (Ekistics page 11.)
- Plans say ALDI will increase the product range in the region, but elsewhere quotes Aldi carrying 1,500 products compared to most supermarkets 20,000 to 30,000.
- Plans have the 3 metre fence 1.5 metres from boundary (Ekistics page 20 and 54) but elsewhere state that the bin will be 1 metre from boundary. (Ekistics page 19) and the western wall in line with that fence is 2 metres from boundary (Ekistics P18). Which is correct?
- "Battered embankment on west" is actually in very good condition except for weeds!
- Details of truck to pick up compacted waste are not mentioned.

OTHER CONCERNS

- We have a very tall chain-wire trellis and creeper screening, 250mm inside our boundary. We are concerned about damage to this due to encroachment. Its preservation will be most needed if the ALDI plans go ahead.
- A future ALDI policy change or government legislation, could suddenly mean we have reversing beepers, refrigeration and idling diesel noise at any time 24/7.
- **The ALDI plan makes many self-limiting statements attempting to make the development look more acceptable. If they are not ALL included in the conditions, ALDI can ignore them, just as Mitre 10 ignored many of its statements in the 1997 plans.** As mentioned above, the Mitre 10 1997 plan application did not allow handling or loading of goods on the western elevation/boundary. Mitre 10 later ignored their own words because Council did not include them in their final stipulations. We are worried that this type of thing could happen with ALDI. We might have to endure reverse beepers, truck air-con, idling diesels, longer loading and truck movement times, night time rubbish collection and daily bin emptying of putrescible waste.

Amenity of neighbouring residential properties will suffer severely under the ALDI plans. Residents should not be subjected to conditions which do not fit the LOCAL CENTRE ZONE conditions and affect quality of life and property values. Our back yard is heavily used by us, family and friends, particularly as we are retired. We have been here since we married FORTY-ONE years ago and plan to be here for many more years. Our property is the hub of our extended family.

Maggie & Bob Schapel

APPENDIX

BOB + MAGGIE
SCHAPPEL

15 January, 1997

AMR:sw
Our Ref:6522.1et

Mr Rodney Donne
City Manager
Corporation of the City of Burnside
401 Greenhill Road
TUSMORE SA 5065

Attention: Ms Colleen McDonnell

Dear Ms McDonnell,

Colleen
**RE: Barrow and Bench Mitre 10 Shop Redevelopment
499 Portrush Road, Glenunga**

1.0 Background

We confirm that we have been engaged by the Applicant, Ian Stewart Nominees, to evaluate the proposed Barrow and Bench, Mitre 10 redevelopment of the former Bi-Lo Supermarket building to accommodate their relocation and expansion from nearby premises on Portrush Road.

It is proposed to occupy existing substantial shop premises (for which no approval is required) and to make changes to the administration/staff areas of the building, and externally to the loading bay and also to the car parking areas with the establishment of a garden centre. It is also proposed to increase the front fascia which is to be signwritten, and to change the form and display area of the existing free standing sign on Portrush Road.

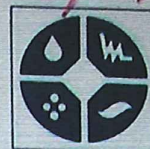
There will be a reduced trading intensity with the proposed change in retail trade, from a supermarket to a hardware store and garden centre. We consider the proposal has merit and warrants support given the minor and positive nature of changes to the existing lawful trading conditions on the subject property.

Hence, as we are able to support this application, in this instance, we report briefly on the representations (which is to be treated as the Applicant's response) as well as on the relative merits of this proposal.

2.0 Proposal

No approvals are needed to occupy the shop building with a hardware/garden centre. In this instance however, further works are proposed, in particular:

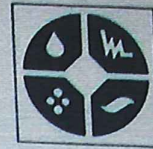
- (i) establishing a garden centre linked with the front of the building. This area is to be fenced and partly covered (but not roofed) with a tubular steel frame and shade cloth canopy. The garden centre, of some 125 square metres in area, necessitates modification to the existing parking and circulation areas on-site;



Bone & Tonkin Planners Pty Ltd
A.C.N. 007 755 277

Town & Country Planners
17 Hutt St, Adelaide, SA 5000
Telephone (08) 223 4866
Facsimile (08) 223 7347

SCANNED



Bone & Tonkin Planners Pty Ltd

- most, if not all deliveries, will occur from Monday to Friday, and occasionally of a Saturday morning during business hours but outside the peak Thursday evening and weekend trade;

no goods handling will take place along the western, residential, boundary; and

- the collection of goods by customers from the loading bay by passenger vehicle (with or without trailers) constitutes a relatively small proportion of the business and the movement of such vehicles should not give rise to, materially, to any additional nuisance. Any noise from goods loading and unloading will be suitably screened with the elevated colourbond fence at the end of the loading bay.

Rear Screening Treatment

A number of representors ask that a solid fence be placed across the whole of property along the top, next to the rear driveway.

The applicant proposes, to further plant the top bank with screening trees particularly in a small area where they have not flourished. A 1.8 metre high lattice timber screen is also to be placed in the interim (refer attached plan) until those trees are able to provide effective screening, and in any event for a period of no less than three years. In addition it should also be noted that a solid sheet metal fence (to be replaced with colourbond) is placed across the top of the site screening the end of the loading bay from the residential properties. The top bank is generally extensively planted affording a good visual screen, supplemented also along the neighbouring residential properties.

It is not considered necessary to create a second fenceline as suggested across the entire site as there will be no increase in servicing demand or goods movement along that rear driveway given the reduced business intensity of this proposal. Moreover, it will not be helpful from a security and maintenance view point to create a strip of land fenced and screened from all sides at the rear of the abutting residential properties, even with an access gate, as recommended by some representors. The proposed tree planting and lattice screen together with the existing fence barrier will suitably protect privacy and screen noise.

Hardware Doesn't Serve Local Community

Elspeth Reid was particularly concerned that, as an owner of one of the neighbouring flats at 505 Portrush Road, the change in retail tenancy would not be as appealing to her prospective future tenants nor generally to local residents.

Irrespective of whether that view is shared in the wider community (which our client strongly contests) it is irrelevant and beyond the scope of the planning scheme. Bi-Lo has already vacated the premises and any other shop use can, without any consent, occupy the former supermarket premises.

This proposed hardware/garden centre will better serve a highly motivated and house proud community.

Less Incentive to Hardware to Maintain Premises

All retailers need to attract custom and provide a pleasing and convenient shopping experience. We doubt whether there is any greater incentive, as Elspeth Reid

SCANNED

TO : PLANNING & BUILDING COMMITTEE
FROM : R GREEN - DIRECTOR, DEVELOPMENT SERVICES
SUBJECT : DEVELOPMENT ASSESSMENT
DATE : 10 JANUARY 1997
REPORT NO. : 5185

5185.1 **IAN STEWART NOMINEES**- Development Application No. 180/959/96/CA

Location: 499 Portrush Road, Glenunga

Proposal: Change of use to retail and service trade premises and additions and alterations

Relevant Development Plan Provisions:

Local Centre Obj: 1
 PDC: 1

Burnside(City) Obj: 5, 8, 17, 28(a)
 PDC: 37(b), (d), 39, 51, 53(a), 55, 56, 57, 63, 64, 67, 68, 103,
 104, 105

Metro. Adelaide Obj: 11, 17, 41, 42, 43, 44
 PDC: 17, 19, 36, 37, 40

SUMMARY

ISSUES

Noise; hours of operation and service/goods delivery; carparking

ENVIRONMENTAL IMPACT

Low

COMMENTS

Five (5) objections

RECOMMENDATION

Conditional approval

Description Of Proposal

The site contains an existing building with a floor area of 1285 square metres which until recently was used as a supermarket. The proposed development takes advantage of the existing use rights for retail use, with some minor additions and alterations including an outdoor garden centre to the front of the building (which necessitates some alterations to the carparking layout and numbers), erection of a canopy over the existing service area, and a replacement outdoor advertisement.

The use has been considered a combination of both retail (ie "shop") with a minor "service trade premise" component (approximately one third) where the building will be used for the storage of bulky goods such as timber and cement.

PUBLIC NOTIFICATION

Type: Category 2

Closing date: 7 January 1997

Number of submissions received: Five (5) objections

Summary of submissions

R & M Schapel, 80 Sydney Street, Glenunga

- opposed to the proposed development;
- concerned with rear driveway use, Sunday trading and rear fencing;
- request that 1.8 metre fence be constructed along rear of driveway.

E Reid, 124 Grange Road, Westbourne Park

- concerned with 7 day trading;
- hardware shop not likely to be of benefit to local residents;
- more planting should take place along Portrush Road.

L Gibbs, 11/486 Portrush Road, Linden Park





- opposed to the proposed development;
- believe site should remain as a supermarket to avoid travelling to Burnside Village.

V Michielin, 82 Sydney Street, Glenunga

- concerned with 7 day trading and noise.

Applicant's response (Bone & Tonkin Planners on behalf of Ian Stewart Nominees)

- it is proposed to occupy existing substantial shop premises;
- no approvals are needed to occupy the shop building with a hardware/garden centre, however further works are proposed;
- whilst the proposed hardware and garden centre occupies a building of a larger scale and will continue to serve the needs beyond an immediate local catchment it:-
 - enhances the building form to Portrush Road;
 - reduces the existing lawful trading intensityand hence traffic and parking demand;

- increases the sites' service yard and makes other changes which improve the privacy to the rear residential properties; and
- maintains the vitality of the local centre.
- proposed carparking equates to 5.3 spaces per 100 square metres and although less than the 7 per 100 anticipated....we consider the most appropriate parking standard for this use to be between 3.5 and 4.0 spaces per 100 square metres of floor area.
- in relation to Sunday trading limitation, the Shop Trading Hours Act should determine the trading hours of all retail establishments;
- in relation to servicing and impacts on adjoining residents, the proposal seeks to modify existing service yard such that it precludes all pedestrian and vehicle access at the rear of the building, except for pickups or servicing vehicles;
-  no goods deliveries will take place along western elevation with all  unloading taking place at the loading bay;
- rear driveway will only accommodate trailered vehicles and small rigid body commercial vehicles up to eleven metres in length....large vehicles will reverse back to the loading dock from front;
- most deliveries will occur Monday to Friday;
-  no goods handling will take place along the western, residential  boundary;
- applicant proposes to further plant the top bank with screening vegetation and to construct a lattice;

PLANNING ASSESSMENT

Site and Locality

The site is located on the western side of Portrush Road. It is a large site which currently contains a substantial building with existing use rights for retail use.

The site is set down slightly from Portrush Road. There is also another change in levels between the end of the bitumen paved driveway at the rear and the adjoining properties. The embankment is quite steep with a grade of around 1:1 which is currently quite bare and unattractive.

The site is flanked by residential to the west, residential units to the south within the Local Centre Zone, a motel to the north and residential and retail development across Portrush Road to the east.

The locality has a low to moderate level of amenity due to the noise, traffic and fumes from Portrush Road and visual qualities.

Zoning and Land Use

The site is located within the Local Centre Zone. The additional service trade premises use is neither complying nor non-complying in this Zone.

**South Australian
DEVELOPMENT ACT, 1993
REPRESENTATION ON APPLICATION – CATEGORY 2**

Applicant: Nielson Architects
Development Number: 180/E012/19
Nature of Development: Demolition of all existing structures and staged construction of a freestanding supermarket (ALDI), incorporating an undercroft carpark
Development Type: Merit
Zone / Policy Area: Local Centre Zone
Subject Land: 499 Portrush Road Glenunga
Contact Officer: Yasmine Alliu
Phone Number: 08 71097076
Close Date: 22 October 2019

My Name: KOVACIC SUPER NOMINEES PTY LTD **My phone number:** 0412 528 038

Primary method(s) of contact: **Email:** marc@constructionglazing.com.au
Postal Address: PO Box 899 VUEY BC SA **Postcode:** 5061

You may be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.

My interests are:
(please tick one)

☒ owner of local property
☐ occupier of local property
☐ a representative of a company/other organisation affected by the proposal
☐ a private citizen

The address of the property affected is:

82 SYDNEY STREET, GLENUNGA **Postcode** 5064

My interests are:
(please tick one)

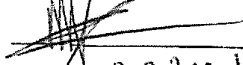
☐ I support the development
☐ I support the development with some concerns
☒ I oppose the development

The specific aspects of the application to which I make comment on are:

PLEASE SEE ATTACHED LETTER FROM HILDITCH LAWYERS
AND ACCOMPANYING CORRESPONDENCE FROM
RESONATE ACOUSTICS

I: ☒ wish to be heard in support of my submission
(please tick one) ☐ do not wish to be heard in support of my submission
(Please tick one)

By: ☐ appearing personally
(please tick one) ☒ being represented by the following person JAMES HILDITCH OF
(Please tick one) HILDITCH LAWYERS

Signature: 
Date: 22/10/2019



HILDITCH LAWYERS

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Adelaide SA 5000

GPO Box 11010
Adelaide SA 5001
www.hilditchlawyers.com

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Fax 08 8231 8323
lawyers@hilditchlawyers.com

21 October 2019

The Secretary
State Commission Assessment Panel
GPO Box 1815
ADELAIDE SA 5001
scapreps@sa.gov.au

Dear Secretary

499 Portrush Road, Glenunga (DA No. 180/E012/19) – Statement of Representation

We act for Kovacic Super Nominees Pty Ltd which is the owner of the land located at 82 Sydney Street, Glenunga ("**our client's land**").

We are instructed to make a written representation on behalf of our client in relation to DA No. 180/E012/19 which proposes a development described by the SCAP as "*Demolition of all existing structures and staged construction of a freestanding supermarket (ALDI), incorporating an undercroft car park*" ("**the proposed development**") on land known as 499 Portrush Road, Glenunga ("**the subject land**").

Our client is opposed to the proposed development and submits that the SCAP should refuse planning consent.

Our Client's Land

The eastern boundary of our client's land at 82 Sydney Street adjoins the western boundary of the subject land.

Our client's land comprises an existing dwelling. The private open space of the dwelling adjoins the development site.

The Development Plan

The subject land is located in the Local Centre Zone ("**the LCZ**") and our client's land is located in the Residential Zone, more specifically, in Residential Policy Area 24 Glenunga (North).

Liability limited by a scheme approved under Professional Standards Legislation.

Hilditch Lawyers Pty Ltd ACN 145 516 276

Summary of Concerns

It is of paramount importance to recognise that the subject land is located within the LCZ and on the boundary of the Residential Zone.

Council Wide Objective 70 envisages a hierarchy of centres. In accordance with the usual convention, Local Centres are identified at the bottom of this hierarchy. Above them in the hierarchy we find Neighbourhood Centres, District Centres, Regional Centres and then the Central Business Area of the City of Adelaide.

The specific provisions for the LCZ firmly place it at the bottom of the hierarchy in a number of ways. Importantly, the objective for the LCZ envisages a zone which accommodates a range of developments "of a small-scale suited to servicing the needs of the local community". PDC 1 for the LCZ further entrenches the idea that development within it should be of a small-scale. PDC 2 for the LCZ then works together with objective 1 and PDC 1 in a complementary way by specifying that shop development "should generally comprise a maximum gross leasable floor area in the order of 450 square metres".

This all leads to the obvious observation that the proposed development is well beyond the size and scale of development envisaged in this zone. The proposed development in numerical terms alone is more than four times the size or scale of any shop development generally envisaged in the zone.

By way of example, in the matter of *Eastern Building Group Pty Ltd v The Barossa Council* [2005] SAERDC 26, the ERD Court considered the meaning of the term "small-scale" and concluded at [33] that *"the proposed development could not be reasonably regarded as being small in scale. In my view, a building having an area of 650 square metres... is not, in scale terms, a development that appears to be contemplated..."*

Another more recent example can be found in the matter of *Shahin Enterprises P/L v Development Assessment Commission & Ors* [2018] SAERDC 31. In this matter the ERD Court made the following remarks with reference to a proposed service station complex comprising a control building of 211 square metres and a canopy: *"...a proposal of these dimensions, on an allotment of this size, attracting this much vehicular traffic and consequent custom and operating during the hours proposed, in this locality, is not a small-scale development"*.

It is our contention that it is not open to the planning authority to conclude that the proposal meets objective 1, PDC 1 or PDC 2 for the LCZ. It is quite clearly not of a "small-scale" and the proposed building is quite clearly well above the maximum gross leasable floor area envisaged in the Zone provisions.

The approval of a shop development of a clearly inappropriate scale will not only have a consequence for the centres hierarchy, noting that it can clearly be expected to service a catchment well beyond the "local community". The proposed supermarket can obviously be expected to have an operational intensity which goes well beyond the kind of small-scale shops envisaged in the LCZ. It will have a fundamental impact on the character of the locality.

Furthermore, the proposed supermarket building will be of a scale and appearance which is completely at odds with the type of building envisaged by the Zone provisions. The large new building will be elevated to accommodate under-croft parking and this will exacerbate its visual impact, as will the two "tower" components at each end of the building which will "clearly identify the ALDI brand" (see Ekistics report dated 16 September 2019 at page 18).

In every sense, the proposed development goes well beyond the type of shop envisaged in the zone and our client is very concerned about the external impacts which will inevitably arise.

The existence of the smaller Mitre 10 store is no answer to the above concerns, particularly because:

1. The proposed development will be significantly larger in scale and height (it will be many metres taller than the existing building, noting that my client does not accept that the existing Mitre 10 building has a wall height of approximately 7 metres). It will introduce a new and undesirable character to the existing residential interface;
2. The proposed use (supermarket) is different to the existing use (bulky goods outlet);
3. The proposed development can be expected to service a very broad catchment as outlined in the applicant's own retail analysis and can be expected to bring about a significant change to the nature and character of the locality;
4. The proposed development will apparently operate for longer hours and will generate greater external impacts for longer periods; and
5. The proposed development will invariably have a greater operational intensity and will change the function of the LCZ. For example the current site comprises approximately 67 parking spaces whereas the proposed development involves the provision of almost 100 spaces (30 at-grade and 68 under-croft parks) and the predicted increase in traffic movements is very significant (see GTA report dated 13 September 2019 in which it is stated that the Mitre 10 site currently generates 20 movements during the Thursday PM peak hour and 129 movements during the Saturday peak hour and yet the proposed development is predicted to generate up to 232 and 288 two-way vehicle movements during the Thursday PM peak and Saturday peak hour respectively which equates to an additional 212 and 159 trips onto the road network respectively – see conclusion at point 7).

The assessment of the proposed development does not, in any event, involve a mechanical comparison between the existing hardware store and the proposed supermarket to the exclusion of other relevant considerations. It primarily involves an assessment of this significant new form of development against the relevant provisions in the Development Plan having regard to the present level of amenity.

The only conclusion to be reached in this matter is that the proposed development will move the use and operation of the site even further away from the objectives for the Zone. The approval of the proposal will entrench an even larger and even less appropriate form of development on the subject land. There is no justification whatsoever for such an outcome having regard to the Development Plan or the existing state of development within the LCZ.

The applicant has attempted to identify "other examples of large floor plate commercial operations within the Local Centre Zone, including 'shops' which exceed 450m²". However, the proposed development, having a floor area of over 2000 square metres, is significantly larger than any of the other examples which the applicant can point to. This exercise only tends to emphasise the point that the proposed development will apparently be the largest shop by far in this particular LCZ. Once again, neither the Development Plan provisions nor the existing state of development in the LCZ can be called upon in support of the proposed supermarket which is more akin to a District or Regional Centre type use. With respect to the location of existing ALDI stores in centres, it is our understanding that the vast majority of existing ALDI stores are located within Regional and District Centres with some also located in Neighbourhood Centres.

The Applicant's Retail Analysis

The Applicant's own "supply and demand analysis" prepared by Deep End Services and dated 4 September 2019 only serves to emphasise the point that this will not be a proposal which is of a "*small scale suited to servicing the needs of the local community*". The report contains the following statements which are of significant concern to my client:

- The success of ALDI Kensington Park indicates there is a very strong need for value-driven supermarkets (page 7 of 14).
- ALDI Glenunga (the proposal) will appeal to a range of geographic and market segments summarised on page 7. It will service extensive primary and secondary trade areas (see diagram on page 8).
- ALDI has developed a planned network of stores for Adelaide based on one store for approximately 30,000 to 40,000 people (page 5 of 14).
- The proposed ALDI will "fit" a gap between Kensington Park and Hawthorn and will improve the profile of the centre (page 13 of 14).
- The proposed ALDI will not only "result in a range of benefits to the local community" but will also result in benefits to "the broader catchment and the Glenunga Centre".

The Deep End Report confirms that the proposed development will go far beyond a use which is small-scale and suited to servicing the needs of the local community. It will service a catchment well beyond the local community and is therefore an inappropriate use in the LCZ.

The concern is magnified by the fact that ALDI is able “to position itself as a discount supermarket quite different to its competitors” (see Ekistics report dated 16 September 2019 at page 15).

In the matter of *Shahin Group of Companies v City of Burnside & G Walshe* [2002] SAERDC 22, a full bench of the ERD Court considered a proposal for an integrated service station complex in a Local Business Zone in Beulah Park. The proposed development was to occupy 2,500 sqm and those portions of the site covered by buildings (including canopy) were to be about 760 sqm in area. This was a zone which comprised policies which envisaged “small-scale” developments “servicing the needs of the local community”. The Court made the following remarks in its judgment at [25]:

“Furthermore, we think it appropriate to add that we construe the words “servicing the needs of the local community” as meaning “servicing principally the needs of the local community”. Thus, as we construe the first Objective and first Principle of the zone, a facility which, whilst servicing the needs of the local community also has a district or regional function, is not envisaged by the Objective and Principle”.

The Court concluded at [31] that *“the proposed development, particularly the car wash facility and perhaps the convenience store, is likely to have a greater than local function, indeed, even greater than a neighbourhood function. We think that its function may approach that of district level”.*

The Court ultimately concluded at [33] that *“what is here proposed goes well beyond what might be reasonably described as “small-scale” and “servicing the needs of the local community”.* The Court concluded at [37] that the proposed development did not constitute an appropriate use of the development site.

In short, the simple fact that the proposed development may be frequented by members of the “local community” is not to the point at all. For so long as the facility will have a district or regional function as well (such as a supermarket on the western side of Portrush Road in this location) it will go well beyond the kind of development envisaged in the Zone.

Specific Impacts

Whilst my client contends the proposed development is fundamentally inappropriate on this site having regard to the provisions in the relevant Development Plan and should be refused for this reason alone, I am instructed to note a number of potential impacts of concern to my client:

1. My client estimates that the proposed ALDI building will reach a height of approximately 12.5 metres above the level of its dwelling floor level and rear private open space. My client estimates that the building will project about 8 metres above the height of the common boundary fence. It will be a dominant visual element which will also give rise to overshadowing and unwelcome solar reflection of the western sun in my client’s opinion. By way of comparison, I am instructed the existing Mitre 10 building is not visible from my client’s land.

I enclose a photograph which I am instructed is taken from Sydney Street (taken at head height from the eastern side of the street and looking in an easterly direction toward the dwelling at number 82 Sydney Street which has gum trees in its back yard). My client is concerned that a significant portion of the background depicted in this photograph will be consumed by the proposed ALDI building.

2. My client does not accept that any assumption should be made that ALDI Delivery and Loading Procedures will be followed. The idea that my client will not suffer from noise impacts because truck reversing alarms and refrigeration units will be turned off, or because prime movers will be turned off during unloading, is very concerning to my client. These procedures will be impossible to police and enforce. They are completely impractical and no assumption whatsoever should be made that these procedures will be effective in any acoustic assessment.
3. My client is concerned that no assessment appears to have been made in relation to the noise arising from rubbish collection.
4. There seems to be a clear possibility that significant noise disturbance could arise outside normal business hours and the maximum noise of the compactor will be a significant industrial noise arising at a residential interface.

I am instructed to attach a report prepared by Resonate Acoustics dated 21 October 2019 which summarises potential noise impacts from their perspective.

Summary and Conclusion

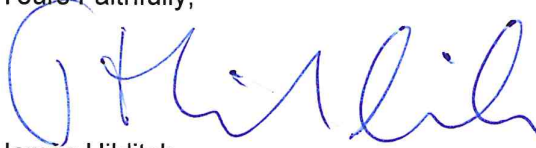
The proposed development does not have adequate regard to the relevant zone provisions. It will be a completely inappropriate development in the LCZ and will have a range of consequences and impacts which would not be expected to arise from development of the much smaller scale envisaged by the zone. The acoustic assessment is incomplete and makes a number of unreliable assumptions about the management and operation of the proposal, including the way in which delivery truck drivers will operate their vehicles on the site.

Whilst the subject land may be an appropriate site from the applicant's perspective in a strategic sense in terms of its visibility, surrounding catchment and visibility on a major arterial road, these features and benefits cannot be permitted to trump the relevant provisions in the Development Plan which clearly discourage a proposal of this scale and intensity from taking root in the LCZ.

The proposed development clearly does not warrant development authorisation for the above reasons.

Please contact me if you have any queries.

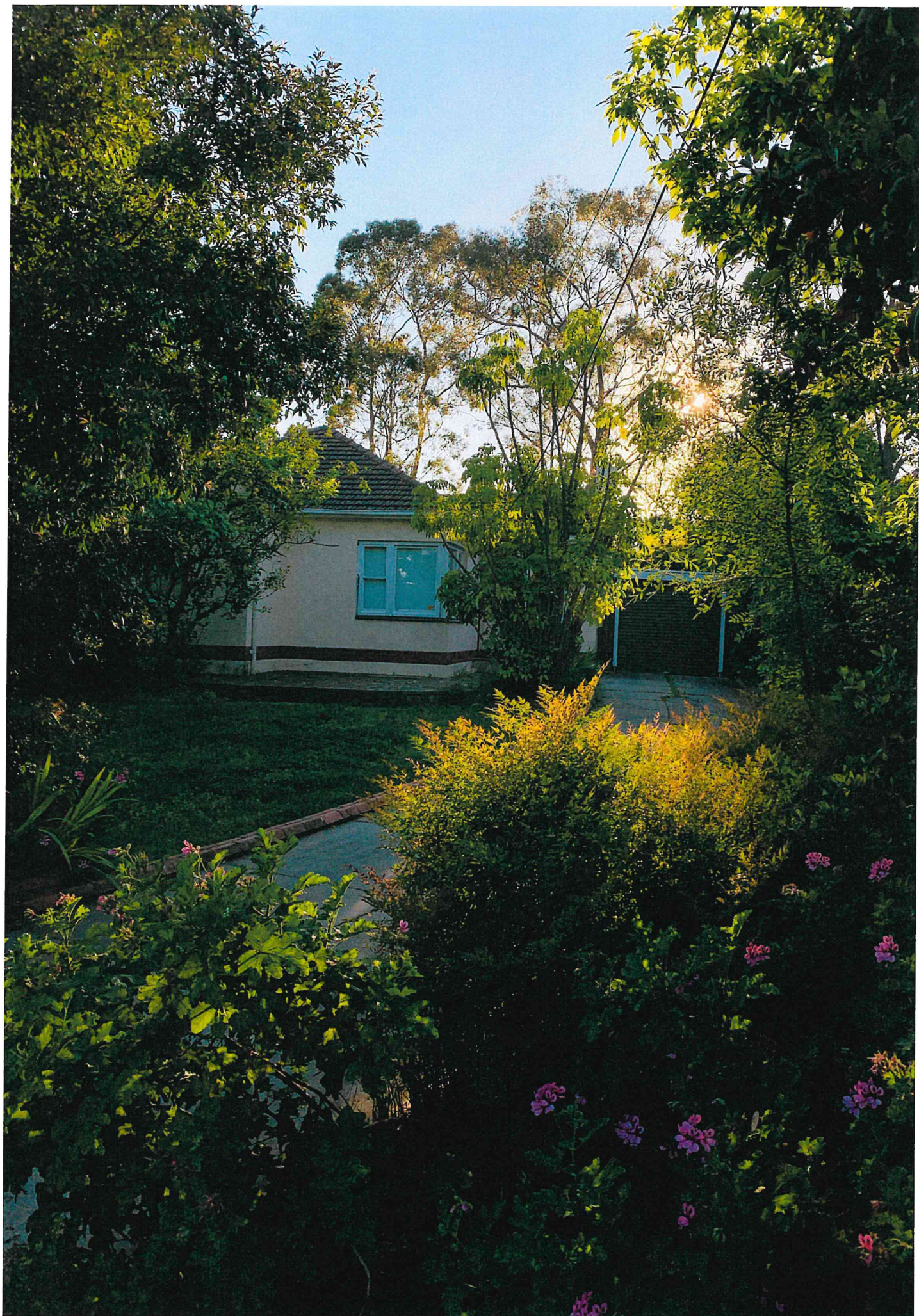
Yours Faithfully,

A handwritten signature in blue ink, appearing to read 'J Hilditch', with a large, stylized initial 'J'.

James Hilditch

james@hilditchlawyers.com

Our Ref: JRH:000699
Your Ref:



Monday, 21 October 2019

Project number: A190886
Reference: A190886LT2

Marc Kovacic
PO Box 899
Unley SA 5061

Dear Marc,

ALDI Glenunga (499 Portrush Road)
Acoustic Review

This letter provides our preliminary opinion on the acoustical aspects associated with the proposed ALDI store to be located at 499 Portrush Road, Glenunga. This review has considered the information generally available to the public in relation to the proposal.

Our comments on the Sonus acoustic assessment are provided below.

1. We are generally in agreement with the assessment approach and predicted levels carried out by Sonus with exception of the following:
 - a. The Sonus assessment relies on the truck reversing alarms and refrigeration units to be turned off when "transiting past any noise sensitive areas" in accordance with the *ALDI Delivery and Loading Procedures*. In practice, this would require the truck driver to turn off the refrigeration unit and reversing alarms prior to entering the proposed ALDI site. We believe this noise mitigation approach relies too heavily on driver diligence in combination with the policing of ALDI, which is unlikely to occur in practice. Therefore, in our opinion, the predicted noise levels inclusive of the truck reversing alarm and refrigeration unit should be undertaken with additional mitigation measures identified as required.
 - b. The Sonus assessment also relies on the prime mover being turned off during unloading (in accordance with the *ALDI Delivery and Loading Procedures*). We note that the ALDI procedure assumes that the average delivery period is 15 minutes. We believe shutting down the engine of a prime mover for a period of 15 minutes also relies on driver diligence. Therefore, in our opinion, the predicted noise levels inclusive of the truck idling should also be undertaken, with additional mitigation measures identified as required.
 - c. The Noise EPP considers rubbish collection to have an adverse impact if the maximum noise level exceeds 60 dB(A) at noise-affected premises. Provisions apply if it is found that the noise has an adverse impact on amenity (refer Noise EPP Division 3). The Sonus report has not presented the noise level results for rubbish collecting activities. Therefore, in our opinion, the prediction of noise from rubbish collection activities on the noise-affected premises should be undertaken to determine if the noise level results in a potential adverse impact. If so, all reasonable and practicable measures must be taken to minimise noise.
2. In light of the above comments, we believe there is risk of non-compliance with the Noise EPP. Furthermore, the location of the truck delivery and compactor area (i.e. the potentially most annoying noise sources of the ALDI operation) are a few meters from the residents furthest from Portrush Road (i.e. with quieter ambient noise), which is ignorant of PDC 28. Further consideration of this aspect should be undertaken by the applicant.
3. Noise from the ALDI PA system has not been assessed by Sonus. We believe this is unlikely to contribute to a noise compliance risk, however the relevance of this aspect should be investigated and assessed if required to confirm.

In summary, we believe the proposal as it stands (with the currently proposed noise mitigation) is at risk of not complying with the Noise EPP. In our opinion, further consideration of the noise impact risks as noted above should be undertaken by the applicant.

Please let me know if you have any further questions.

Yours sincerely,



Darren Jurevicius
Managing Director
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m+61 408 229 272
darren.jurevicius@resonate-consultants.com

South Australian
DEVELOPMENT ACT, 1993
REPRESENTATION ON APPLICATION – CATEGORY 2



Applicant: Nielson Architects
Development Number: 180/E012/19
Nature of Development: Demolition of all existing structures and staged construction of a freestanding supermarket (ALDI), incorporating an undercroft carpark
Development Type: Merit
Zone / Policy Area: Local Centre Zone
Subject Land: 499 Portrush Road Glenunga
Contact Officer: Yasmine Alliu
Phone Number: 08 71097076
Close Date: 22 October 2019

My Name: NEDWELL PTY LTD My phone number: 0412 528 038
(SOLE DIRECTOR - MARC KONACIC)

Primary method(s) of contact: Email: marc@constructionglazing.com.au
Postal: PO BOX 899
Address: Unley BC SA Postcode: 5061

You may be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.

My interests are:
(please tick one)

- ☒ owner of local property
☐ occupier of local property
☐ a representative of a company/other organisation affected by the proposal
☐ a private citizen

The address of the property affected is:

84 SYDNEY STREET, GLENUNGA Postcode 5064

My interests are:
(please tick one)

- ☐ I support the development
☐ I support the development with some concerns
☒ I oppose the development

The specific aspects of the application to which I make comment on are:

PLEASE SEE ATTACHED LETTER FROM HILDITCH
LAWYERS AND ACCOMPANYING CORRESPONDENCE
FROM RESONATE ACOUSTICS

I: ☒ wish to be heard in support of my submission
(please tick one) ☐ do not wish to be heard in support of my submission
(Please tick one)

By: ☐ appearing personally
(please tick one) ☒ being represented by the following person
(Please tick one)

JAMES HILDITCH OF
HILDITCH LAWYERS

Signature: [Signature]

Date: 22/10/2019



HILDITCH LAWYERS

Level 1, 24 Grote Street
Adelaide SA 5000

GPO Box 11010
Adelaide SA 5001
www.hilditchlawyers.com

Tel 08 7325 5900
Fax 08 8231 8323
lawyers@hilditchlawyers.com

21 October 2019

The Secretary
State Commission Assessment Panel
GPO Box 1815
ADELAIDE SA 5001
scapreps@sa.gov.au

Dear Secretary

499 Portrush Road, Glenunga (DA No. 180/E012/19) – Statement of Representation

We act for Nedwell Pty Ltd which is the owner of the land located at 84 Sydney Street, Glenunga and Mr Marc Kovacic who is a sole director of this company and an occupant of the dwelling located at 84 Sydney Street, Glenunga ("**our client's land**").

We are instructed to make a written representation on behalf of our client in relation to DA No. 180/E012/19 which proposes a development described by the SCAP as "*Demolition of all existing structures and staged construction of a freestanding supermarket (ALDI), incorporating an undercroft car park*" ("**the proposed development**") on land known as 499 Portrush Road, Glenunga ("**the subject land**").

Our client is opposed to the proposed development and submits that the SCAP should refuse planning consent.

Our Client's Land

The north-eastern corner of our client's land at 84 Sydney Street adjoins the south-western corner of the subject land.

Our client's land comprises an existing dwelling. The private open space of the dwelling adjoins the development site.

The Development Plan

The subject land is located in the Local Centre Zone ("the LCZ") and our client's land is located in the Residential Zone, more specifically, in Residential Policy Area 24 Glenunga (North).

Summary of Concerns

It is of paramount importance to recognise that the subject land is located within the LCZ and on the boundary of the Residential Zone.

Council Wide Objective 70 envisages a hierarchy of centres. In accordance with the usual convention, Local Centres are identified at the bottom of this hierarchy. Above them in the hierarchy we find Neighbourhood Centres, District Centres, Regional Centres and then the Central Business Area of the City of Adelaide.

The specific provisions for the LCZ firmly place it at the bottom of the hierarchy in a number of ways. Importantly, the objective for the LCZ envisages a zone which accommodates a range of developments "of a small-scale suited to servicing the needs of the local community". PDC 1 for the LCZ further entrenches the idea that development within it should be of a small-scale. PDC 2 for the LCZ then works together with objective 1 and PDC 1 in a complementary way by specifying that shop development "should generally comprise a maximum gross leasable floor area in the order of 450 square metres".

This all leads to the obvious observation that the proposed development is well beyond the size and scale of development envisaged in this zone. The proposed development in numerical terms alone is more than four times the size or scale of any shop development generally envisaged in the zone.

By way of example, in the matter of *Eastern Building Group Pty Ltd v The Barossa Council* [2005] SAERDC 26, the ERD Court considered the meaning of the term "small-scale" and concluded at [33] that "*the proposed development could not be reasonably regarded as being small in scale. In my view, a building having an area of 650 square metres... is not, in scale terms, a development that appears to be contemplated...*"

Another more recent example can be found in the matter of *Shahin Enterprises P/L v Development Assessment Commission & Ors* [2018] SAERDC 31. In this matter the ERD Court made the following remarks with reference to a proposed service station complex comprising a control building of 211 square metres and a canopy: "*...a proposal of these dimensions, on an allotment of this size, attracting this much vehicular traffic and consequent custom and operating during the hours proposed, in this locality, is not a small-scale development*".

It is our contention that it is not open to the planning authority to conclude that the proposal meets objective 1, PDC 1 or PDC 2 for the LCZ. It is quite clearly not of a "small-scale" and the proposed building is quite clearly well above the maximum gross leasable floor area envisaged in the Zone provisions.

The approval of a shop development of a clearly inappropriate scale will not only have a consequence for the centres hierarchy, noting that it can clearly be expected to service a catchment well beyond the "local community". The proposed supermarket can obviously be expected to have an operational intensity which goes well beyond the kind of small-scale shops envisaged in the LCZ. It will have a fundamental impact on the character of the locality.

Furthermore, the proposed supermarket building will be of a scale and appearance which is completely at odds with the type of building envisaged by the Zone provisions. The large new building will be elevated to accommodate under-croft parking and this will exacerbate its visual impact, as will the two "tower" components at each end of the building which will "clearly identify the ALDI brand" (see Ekistics report dated 16 September 2019 at page 18).

In every sense, the proposed development goes well beyond the type of shop envisaged in the zone and our client is very concerned about the external impacts which will inevitably arise.

The existence of the smaller Mitre 10 store is no answer to the above concerns, particularly because:

1. The proposed development will be significantly larger in scale and height (it will be many metres taller than the existing building, noting that my client does not accept that the existing Mitre 10 building has a wall height of approximately 7 metres). It will introduce a new and undesirable character to the existing residential interface;
2. The proposed use (supermarket) is different to the existing use (bulky goods outlet);
3. The proposed development can be expected to service a very broad catchment as outlined in the applicant's own retail analysis and can be expected to bring about a significant change to the nature and character of the locality;
4. The proposed development will apparently operate for longer hours and will generate greater external impacts for longer periods; and
5. The proposed development will invariably have a greater operational intensity and will change the function of the LCZ. For example the current site comprises approximately 67 parking spaces whereas the proposed development involves the provision of almost 100 spaces (30 at-grade and 68 under-croft parks) and the predicted increase in traffic movements is very significant (see GTA report dated 13 September 2019 in which it is stated that the Mitre 10 site currently generates 20 movements during the Thursday PM peak hour and 129 movements during the Saturday peak hour and yet the proposed development is predicted to generate up to 232 and 288 two-way vehicle movements during the Thursday PM peak and Saturday peak hour respectively which equates to an additional 212 and 159 trips onto the road network respectively – see conclusion at point 7).

The assessment of the proposed development does not, in any event, involve a mechanical comparison between the existing hardware store and the proposed supermarket to the exclusion of other relevant considerations. It primarily involves an assessment of this significant new form of development against the relevant provisions in the Development Plan having regard to the present level of amenity.

The only conclusion to be reached in this matter is that the proposed development will move the use and operation of the site even further away from the objectives for the Zone. The approval of the proposal will entrench an even larger and even less appropriate form of development on the subject land. There is no justification whatsoever for such an outcome having regard to the Development Plan or the existing state of development within the LCZ.

The applicant has attempted to identify "other examples of large floor plate commercial operations within the Local Centre Zone, including 'shops' which exceed 450m²". However, the proposed development, having a floor area of over 2000 square metres, is significantly larger than any of the other examples which the applicant can point to. This exercise only tends to emphasise the point that the proposed development will apparently be the largest shop by far in this particular LCZ. Once again, neither the Development Plan provisions nor the existing state of development in the LCZ can be called upon in support of the proposed supermarket which is more akin to a District or Regional Centre type use. With respect to the location of existing ALDI stores in centres, it is our understanding that the vast majority of existing ALDI stores are located within Regional and District Centres with some also located in Neighbourhood Centres.

The Applicant's Retail Analysis

The Applicant's own "supply and demand analysis" prepared by Deep End Services and dated 4 September 2019 only serves to emphasise the point that this will not be a proposal which is of a "*small scale suited to servicing the needs of the local community*". The report contains the following statements which are of significant concern to my client:

- The success of ALDI Kensington Park indicates there is a very strong need for value-driven supermarkets (page 7 of 14).
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“Furthermore, we think it appropriate to add that we construe the words “servicing the needs of the local community” as meaning “servicing principally the needs of the local community”. Thus, as we construe the first Objective and first Principle of the zone, a facility which, whilst servicing the needs of the local community also has a district or regional function, is not envisaged by the Objective and Principle”.

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In short, the simple fact that the proposed development may be frequented by members of the “local community” is not to the point at all. For so long as the facility will have a district or regional function as well (such as a supermarket on the western side of Portrush Road in this location) it will go well beyond the kind of development envisaged in the Zone.

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Whilst my client contends the proposed development is fundamentally inappropriate on this site having regard to the provisions in the relevant Development Plan and should be refused for this reason alone, I am instructed to note a number of potential impacts of concern to my client:

1. My client does not accept that any assumption should be made that ALDI Delivery and Loading Procedures will be followed. The idea that my client will not suffer from noise impacts because truck reversing alarms and refrigeration units will be turned off, or because prime movers will be turned off during unloading, is very concerning to my client. These procedures will be impossible to police and enforce. They are completely impractical and no assumption whatsoever should be made that these procedures will be effective in any acoustic assessment.
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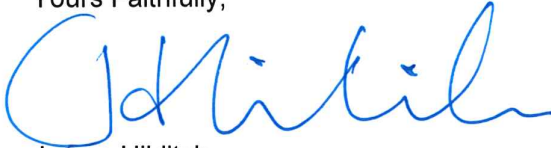
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Whilst the subject land may be an appropriate site from the applicant's perspective in a strategic sense in terms of its visibility, surrounding catchment and visibility on a major arterial road, these features and benefits cannot be permitted to trump the relevant provisions in the Development Plan which clearly discourage a proposal of this scale and intensity from taking root in the LCZ.

The proposed development clearly does not warrant development authorisation for the above reasons.

Please contact me if you have any queries.

Yours Faithfully,

A handwritten signature in blue ink, appearing to read 'J Hilditch', with a large, stylized initial 'J'.

James Hilditch

james@hilditchlawyers.com

Our Ref: JRH:000699

Monday, 21 October 2019

Project number: A190886
Reference: A190886LT2

Marc Kovacic
PO Box 899
Unley SA 5061

Dear Marc,

**ALDI Glenunga (499 Portrush Road)
Acoustic Review**

This letter provides our preliminary opinion on the acoustical aspects associated with the proposed ALDI store to be located at 499 Portrush Road, Glenunga. This review has considered the information generally available to the public in relation to the proposal.

Our comments on the Sonus acoustic assessment are provided below.

1. We are generally in agreement with the assessment approach and predicted levels carried out by Sonus with exception of the following:
 - a. The Sonus assessment relies on the truck reversing alarms and refrigeration units to be turned off when "transiting past any noise sensitive areas" in accordance with the ALDI *Delivery and Loading Procedures*. In practice, this would require the truck driver to turn off the refrigeration unit and reversing alarms prior to entering the proposed ALDI site. We believe this noise mitigation approach relies too heavily on driver diligence in combination with the policing of ALDI, which is unlikely to occur in practice. Therefore, in our opinion, the predicted noise levels inclusive of the truck reversing alarm and refrigeration unit should be undertaken with additional mitigation measures identified as required.
 - b. The Sonus assessment also relies on the prime mover being turned off during unloading (in accordance with the ALDI *Delivery and Loading Procedures*). We note that the ALDI procedure assumes that the average delivery period is 15 minutes. We believe shutting down the engine of a prime mover for a period of 15 minutes also relies on driver diligence. Therefore, in our opinion, the predicted noise levels inclusive of the truck idling should also be undertaken, with additional mitigation measures identified as required.
 - c. The Noise EPP considers rubbish collection to have an adverse impact if the maximum noise level exceeds 60 dB(A) at noise-affected premises. Provisions apply if it is found that the noise has an adverse impact on amenity (refer Noise EPP Division 3). The Sonus report has not presented the noise level results for rubbish collecting activities. Therefore, in our opinion, the prediction of noise from rubbish collection activities on the noise-affected premises should be undertaken to determine if the noise level results in a potential adverse impact. If so, all reasonable and practicable measures must be taken to minimise noise.
2. In light of the above comments, we believe there is risk of non-compliance with the Noise EPP. Furthermore, the location of the truck delivery and compactor area (i.e. the potentially most annoying noise sources of the ALDI operation) are a few meters from the residents furthest from Portrush Road (i.e. with quieter ambient noise), which is ignorant of PDC 28. Further consideration of this aspect should be undertaken by the applicant.
3. Noise from the ALDI PA system has not been assessed by Sonus. We believe this is unlikely to contribute to a noise compliance risk, however the relevance of this aspect should be investigated and assessed if required to confirm.

In summary, we believe the proposal as it stands (with the currently proposed noise mitigation) is at risk of not complying with the Noise EPP. In our opinion, further consideration of the noise impact risks as noted above should be undertaken by the applicant.

Please let me know if you have any further questions.

Yours sincerely,



Darren Jurevicius
Managing Director
p+61 8 8155 5888
m+61 408 229 272
darren.jurevicius@resonate-consultants.com

25 November 2019

REF No.: 00835-003

State Commission Assessment Panel
GPO Box 1815
ADELAIDE SA 5001

Attention: Yasmine Alliu, Planning Officer

By Email: Yasmine.alliu@sa.gov.au

Dear Yasmine,

RE: DA 180/E012/19 – ALDI GLENUNGA – RESPONSE TO REPRESENTATIONS AND COUNCIL

We refer to the proposed development of an ALDI Store at 499 Portrush Road, Glenunga.

Ekistics has been engaged by the Applicant to respond to the Category 2 representations received during public notification and to the comments provided by the City of Burnside ("the Council").

We address these matters under separate headings below.

1. Overview of Representations and Referrals

Category 2 public notification resulted in the following seven (7) representations being submitted.

Table 1.1 *Summary of Representations*

Name	Affected Property	Oppose/Support	Wish to be Heard
Anita Derbali	6/476 Portrush Road	Support with Concerns	Not Heard
Paul Derbali	4/476 Portrush Road	Support with Concerns	Not Heard
Calvin Chiu	78 Sydney Street	Oppose	Heard
Bob and Maggie Schapel	80 Sydney Street	Oppose	Heard
Selina Zhou	493 Portrush Road	Oppose	Heard
Kovacic Super Nominees Pty Ltd.	82 Sydney Street	Oppose	Heard
Nedwell Pty Ltd.	84 Sydney Street	Oppose	Heard

Def: E-KIS-TICS [noun] : The Science of Human Settlements ...

Level 1/16 Vardon Ave, Adelaide SA 5000 **p** 08 7231 0286 **e** contact@ekistics.com.au **w** ekistics.com.au ABN 34 918 250 862

The relevant planning related issues and concerns raised by the Representors can be summarised as follows:

- Concerns with the **scale and intensity** of the proposed land use, including:
 - » The scale of the development is contrary to the **objectives of the Local Centre Zone** and will disrupt the 'Centres Hierarchy';
 - » The development will have an adverse impact on the **character of the locality**; and
 - » The development is of greater **intensity** vis-à-vis the existing Mitre 10, in terms of land use, hours of operation, customer catchment, and trip generation;
- **Visual amenity and building design**, in terms of floor plate, building height, and setback of the building from the western boundary;
- Insufficient screening of **servicing activities** to the west;
- **Noise** generated by the development and the adequacy of noise attenuation measures, particularly as they relate to adjoining residential properties to the west;
- Proximity of the **waste area** to the western boundary and potential for odour and dust impacts, as well as management of rats and vermin;
- **Overshadowing** concerns to the west; and
- **Traffic impacts** for the adjacent shops to the eastern side of Portrush Road.

A written response is provided to the Representations within Section 3 below.

In addition to the Representations, we are also in receipt of:

- Referral comments from **Traffic Operations** of the Department of Planning, Transport and Infrastructure (DPTI) dated 29 October 2019, who advised they have no objection to the proposal, subject to a number of conditions. We thank the Department for their supportive comments, and we confirm the Applicant is fully aware of the recommended conditions.
- Referral comments from the **City of Burnside**, dated 31 October 2019, which advised that Council is generally supportive of the proposal, subject to a number of considerations (summarised) as follows:
 - » Potential reduction in the size, scale and visual massing of the building as it relates to the Residential Zone and streetscape (Portrush Road);
 - » Potential increase in the front boundary setback and additional landscaping forward of the building;
 - » Consideration to increasing the setback of the building from the southern and western boundaries;
 - » Establishment of landscaping that is more proportionate to the scale of the building and selection of plants that are appropriately shade tolerant;
 - » Demonstration that signage illumination levels have appropriate regard to motorists and surrounding residential sites;
 - » Appropriate conditions be applied to ensure management/control of hours of operation, servicing arrangements, and waste management practices; and

- » Request that additional information be provided in the form of an 'outdoor lighting plan', 'detailed landscaping plan', and identification of any significant/regulated trees on adjoining sites.

A written response to the issues raised by the Council are provided within Section 4 below.

2. Additional Plans and Amendments

Concurrently with our review of the various issues raised, we wish to highlight the following minor amendments and additional information as follows:

- **Appendix 1** – Minor amendments to the Outerspace Landscape Plan (Drawing No. OS680_CP01D dated 14/11/19) to provide additional and more substantial tree planting along the western boundary and additional screening shrubs adjacent the northern 'tower';
- **Appendix 2** – Nielsen Architects have made minor amendments to the following suite of drawings, and 'clouded' the applicable changes/amendments:
 - » DA02.3 – To reflect the amended landscape scheme along the western boundary;
 - » DA02.4 – To stipulate the alignment of sectional drawings provided in Sheet DA04.1;
 - » DA03.1 – To identify that boundary tree plantings along the southern boundary reflect the 'juvenile' heights of the proposed tree species;
 - » DA03.2 – To identify that boundary tree plantings along the western boundary reflect the 'juvenile' heights of the proposed tree species;
 - » DA03.3 – Amended streetscape drawings to reflect the 'mature' heights of proposed tree species;
 - » DA04.1 – Extension of sectional drawings to better reflect the adjoining sites to the south and west, clarify treatment of the existing retaining wall to the north; and illustrate 'mature' landscape heights; and
 - » DA04.2 – To provide a clearer understanding of the visual perspective from the west, including fence details, illustration of 'mature' landscape heights, clarification of western setback distances, and treatment to the existing retaining wall to the west.
- **Appendix 3** – Nielsen Architects have prepared the following additional drawings to assist with an overall understanding of site development and the relationship of the proposed ALDI Store to the existing Mitre 10 store. These include:
 - » DA04.3 – Illustrating the southern fence elevation, and providing a pre-development and post-development comparison;
 - » DA04.4 – Simplified sectional drawings to highlight the extent of 'cut' and 'fill' across the site and its relationship to the proposed ALDI Store; and
 - » DA04.5 – Elevations of the Store from the south and west, overlayed with the existing Mitre 10 store.

- **Appendix 4** – Nielsen Architects have prepared additional overshadow diagrams to evidence the impact of the proposal on 21 June to those adjoining properties to the west (Drawing No. SA01.3);
- **Appendix 5** – Sonus Acoustic Engineers have prepared a supplementary submission in response to the Representations received; and
- **Appendix 6** – Arborman have assessed the impact of the proposal on the trees on the adjoining site to the west.

These amendments and inclusions will be discussed further within Sections 3 and 4 below.

3. Response to Representations

3.1 Scale and Intensity of Land Use

While ‘land use suitability’ was discussed at some length within Section 6.2 of our original Planning Statement, it is important to reinforce that the subject site has a long and established history in supporting large format retail. We understand that from the late 1970s/early 1980s, the site was occupied by a ‘Half Case Warehouse’ supermarket, which was then followed by a ‘Bi-Lo’ supermarket. Similarly, the site is presently occupied by a ‘Mitre 10’ Bulky Goods Outlet, which is also a form of shop.

The existing building has a roofed area of approximately 1,435m², as well as an outdoor display area of approximately 225m².

Accordingly, the site has, for a sustained period, supported large scale retail activities, which share similar ‘ancillary’ activities and externalities to that which are proposed by ALDI, such as loading/unloading, servicing arrangements and customers moving to and from the site.

While we do not suggest the proposed ALDI should simply be measured against the land use activities and built form arrangements of the ‘Half Case Warehouse’, ‘Bi-Lo’ or ‘Mitre 10’, it undoubtedly provides useful context in understanding the history of the site and how it has traditionally supported a scale of retail which is more than ‘small-scale’, and certainly larger than that contemplated by the Local Centre Zone.

Further, and as outlined in our Planning Statement, we maintain the view that the proposed scale of retail activities are appropriate within the context of the locality where other large floor plate commercial operations are present (also within the Local Centre Zone), including Dan Murphy’s Liquor Merchant, Tony and Mark’s Produce Market/Zeea’s Eatery and the adjoining Best Western Granada Motor Inn, which are all significantly larger than the floor plate ‘cap’ envisaged by the Local Centre Zone. Having frontage to an arterial road lends itself to the establishment of such a scale and type of activity and we note this is a commonplace arrangement across many areas of Adelaide.

It is also important to reinforce that the subject site comprises three (3) separate allotments. It is an entirely reasonable position to consider that each of these allotments could be developed separately, and each support a ‘shop’ with a floor area in the order of 450m² (as expressly contemplated by the Local Centre Zone). This

would result in a combined floor area of 1350m², which is comparable to the overall floor area encapsulated by the proposed development (1,266m² retail / 2,006m² total net area).

Importantly, the proposed ALDI Store represents a contemporary and more efficiently designed premises for the subject site. As discussed within our Planning Statement and further expanded below, not only will the ALDI Store significantly enhance the visual amenity of the site, it will improve the interface with adjoining residential sites to the south and west, through improved loading/unloading procedures, waste management and servicing practices, as well as noise attenuation measures. Further, the development will consolidate and significantly improve access/egress to Portrush Road, which will be of benefit both in terms of safety and efficiency.

To reinforce the appropriateness and benefits of the Store for this locality, we once again highlight the analysis undertaken by Deep End Services which was submitted with the Development Application. Deep End noted:

- The Glenunga catchment has a 'very low' rate of supermarket floorspace provision, which, in terms of impacts to local residents can result in lack of choice, higher consumer prices, longer travel distances, congestion and parking difficulties;
- The expected external impacts of the ALDI Store are 'low' given ALDI is a relatively 'small' supermarket (when compared to much larger brands such as Coles and Woolworths) with negligible anticipated impacts on existing 'centre' trading levels; and
- The ALDI Store will deliver a range of benefits to the locality and community more generally including increased customer choice, employment opportunities, as well as serving as a catalyst to attract further new businesses to the area.

We also take this opportunity to highlight that the Deep End analysis did include a consideration of the small grocery and fresh food store located on Glen Osmond Road, as queried by the Representors Bob and Maggie Schapel. We note the shop has recently been re-branded as 'Drakes' Foodland.

Having regard to the expert findings of Deep End and given the absence of supporting evidence provided by the Representors, we conclude the development will not detract from the function of other centre zones or disrupt the centres hierarchy more generally.

Accordingly, we remain of the view that the scale and intensity of land use are entirely reasonable for the subject site, locality and the Local Centre Zone.

3.2 Visual Amenity and Building Design

We note the concerns raised in relation to the bulk, scale and height of the building, as well as the setback of the building from the western boundary.

There is considerable commentary within our earlier Planning Statement regarding the 'design and appearance' of the proposed ALDI Store (refer Sections 4.2 and 6.3 of the Planning Statement). Further, the supporting architectural package, prepared by Nielsen Architects, included elevation drawings, streetscape perspectives and 3D renders which clearly illustrate the overall appearance and high standard of presentation of the ALDI Store.

Notwithstanding, Nielsen Architects have prepared additional drawings to further demonstrate the overall approach to site development including the extent of site excavation; the relationship of the proposal to adjoining sites; and by offering a contextual understanding vis-à-vis the existing Mitre 10 arrangement (refer **Appendix 3**).

We recognise the development will deliver a different built form arrangement to that which presently exists on the site. However, this approach is to be embraced noting that:

- The building has been sited towards the southern side of the site to allow for provision of safe, functional and convenient vehicle access/egress to the northern side of the site, located as far as possible from the Hay Road intersection and Pedestrian Actuated Crossing;
- The building has been ‘cut’ into the southern side of the site which reduces the overall height of the building when viewed from the street and adjoining properties to the south and west;
- The building has been sited close to the Portrush Road frontage to activate the street, and deliver a sense of urban enclosure which responds to the established setback pattern of many existing commercial developments within the locality;
- The building delivers a high quality presentation and level of visual interest to the public realm including varied roof line pattern, defining ‘tower’ elements which are located away from side property boundaries, and a range of design elements, materials and colours which will all enhance the visual presentation of the site;
- The under-croft car park will largely be screened from view through a combination of design elements, landscape treatments and ‘setting’ down of the car park level, as desired by the Development Plan;
- Plant equipment and solar panels will be integrated into the overall design of the building; and
- The servicing and loading areas have been located away from the main frontage of the building, in a suitably screened and noise attenuated area.

Accordingly, we remain of the view that the proposal will deliver a high design standard, contribute positively to the character of the streetscape, and has appropriate regard to adjoining residential sites, which satisfies the key design provisions of the Development Plan.

3.3 Overshadowing

Section 6.3.2 of our Planning Statement detailed overshadowing impacts to the south which highlighted that the proposal satisfied the overarching intent for management of overshadowing impacts contained within the Development Plan. Accordingly, we do not discuss overshadow impacts to the south within this response.

To illustrate potential overshadowing impacts to the west, Nielsen Architects have prepared additional overshadowing diagrams for 21 June (refer **Appendix 4**).

The overshadowing diagrams clearly highlight that the western shadow cast by the proposed ALDI Store, is limited to the period between 8.00am and 11.00am. The shadow profile does not reach any of those adjacent residential dwellings and is limited to portions of the rear yard areas only.

Accordingly, the ALDI Store will not unreasonably restrict access to sunlight for those adjoining dwellings to the west, with limited overshadowing of outdoor areas only. On that basis, the proposal accords with the relevant overshadowing provisions of the Development Plan.

3.4 Screening of Servicing Area

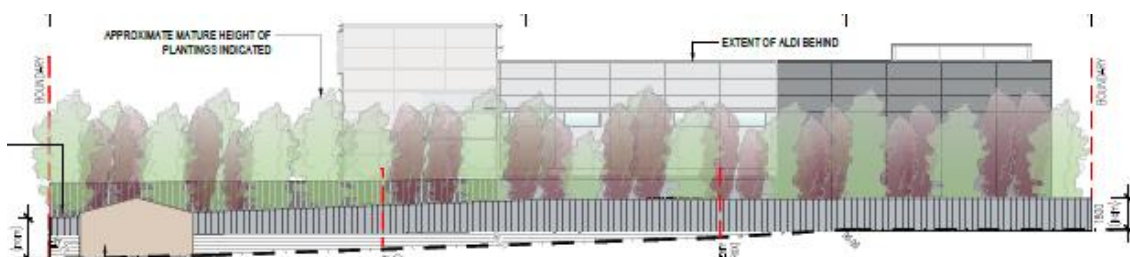
We note concerns regarding the adequacy of the western boundary fencing and landscape treatments to screen servicing activities from those adjoining residential properties to the west.

The original landscape proposal for the western boundary included the planting of tall, upright trees intended to form a dense 'green screen' when viewed from the west. In addition to this, and as illustrated in the amended Landscape Plan (refer **Appendix 1**), a second, slightly smaller, upright tree species ('Oakville Crimson Spire') is now proposed to be planted on an alternate basis. The 'Crimson Spire' grows to be around 2m wide and 6m tall, and this will add further density, variation and ensure there are no 'gaps' in the western facing landscape treatment.

The proposed tree species have moderate to fast growth rates and have been selected as they are hardy, adaptable, with low maintenance requirements while offering an attractive and dense foliage type. Importantly, no understorey planting is proposed to ensure maintenance access is achievable along the length of the western boundary, with entry to the landscape space available via the under-croft car park.

The effectiveness of the landscape 'screen' is illustrated in Figure 3.1 below, which has been prepared by Nielsen Architects (refer DA04.2 in **Appendix 2**) to demonstrate the mature height of the chosen tree species relative to the ALDI Store.

Figure 3.1 Western Elevation – Mature Tree Heights



Accordingly, we are of the opinion that the proposed western landscape treatment is appropriate and in the long term, the ALDI Store and all ancillary servicing arrangements will be significantly screened from view when viewed from those adjoining sites to the west.

In addition, and set behind the dense screen of trees, is the proposed 3-metre-high acoustic fence which is to be finished in Colorbond 'Basalt' grey. While primarily installed for noise attenuation purposes, this fence will also screen the majority of day to day servicing activities, as well as fully screening the dedicated waste bin. While we understand there will be some activities that will not be entirely screened by the fence alone (for example the top portion of the delivery truck, or the lifting of the waste bin while it is being collected), such occurrences will be infrequent and for short periods of time only.

In any event, these activities will ultimately be screened by the landscape screen and we note this is a significant improvement relative to the existing situation, whereby the rear servicing and loading area of the Mitre 10 is screened by an approximate 2-metre-high, non-acoustically treated fence only.

3.5 Noise Impacts

At the time of lodgement, the Development Application was supported by detailed acoustic advice prepared by Sonus Acoustic Engineers (refer Appendix 5 of our initial Planning Statement), as well as a clear explanation of ALDI's Delivery and Loading Procedures (refer Appendix 7 of our initial Planning Statement).

However, we note the concerns raised regarding potential noise impacts and the appropriateness of the proposed noise attenuation measures and operational procedures, particularly as they relate to the adjoining residential sites to the west. We also recognise that two (2) of the Representations included a review of the proposal by Resonate Acoustics.

The Representor's concerns have been reviewed in consultation with Sonus Acoustic Engineers and their response is provided in **Appendix 5**.

A summary of the Sonus response is provided below:

- ALDI has adopted a consistent approach to the management and implementation of their Delivery and Loading Procedures which applies across all ALDI Stores within South Australia. ALDI has invested in specific trucks and training of their drivers to ensure that all loading and operating procedures are consistently followed. In particular, ALDI trucks are fitted with;
 - » Low-level exhausts to maximise the effectiveness of noise barriers;
 - » Reversing cameras to enable reversing alarms to be switched off; and
 - » Control of refrigeration and reversing alarms from within the driver's cab.

ALDI truck drivers unload their own trucks and therefore the truck is not left idling while unloading occurs;

- Waste is collected on Mondays, Wednesdays and Fridays using a truck fitted with a low-level exhaust. These collections occur between the hours of 7:00am to 7:00pm to achieve the requirements of the *Environment Protection (Noise) Policy 2007*. In addition, the noise barriers proposed in the vicinity of the loading dock will significantly reduce the noise during the day;
- The noise from waste collection has been measured at ALDI West Lakes. Based on this noise measurement, it is predicted that the maximum instantaneous noise level in the rear yard of the closest residence would be lower than the expected noise at the front facade of the residence from Council rubbish collection. The average noise level (LAeq) in the rear yard is expected to be 52 dB(A). This level achieves the compliance level of the *Environment Protection (Noise) Policy 2007* for activity which might occur continuously all day;
- ALDI does not operate a Public Address (PA) system; and

- The Sonus assessment of noise generated within the Loading Dock included consideration of delivery activity, rubbish collection and operation of the compactor. The proposed noise attenuation measures were carefully considered and assessed and have been designed to reduce the noise to suitable levels in accordance with the *Environment Protection (Noise) Policy 2007*.

We also highlight the unique design of the ALDI Loading Dock which, as illustrated in Figure 3.2 below, includes a 'shroud' around the loading dock. Accordingly, when an ALDI truck is connected to the Loading Dock, the 'shroud' further assists to reduce noise associated with loading/unloading of the truck.

Figure 3.2 Loading Dock 'Shroud' (ALDI, Hawthorn)



Accordingly, we remain of the opinion that the proposed development has been appropriately assessed and designed from a noise management perspective. Importantly, the submission includes clear and detailed operational procedures which are practical and enforceable and offer certainty and confidence to both the Relevant Authority and the Representors.

Finally, in relation to noise associated with demolition and construction activities, we note that such activities will be limited to the period between 7.00am and 7.00pm Monday to Saturday, and all reasonable and practicable measures will be taken to minimise noise at all times in accordance with the *Environment Protection Act 1993*.

3.6 Waste Impacts and Management of Vermin

Several Representors raised concern with the proposed location of the waste bin area and the potential for odour and dust impacts, as well as the management of rats and vermin.

The dedicated waste bin and compactor are proposed to be located within the loading dock area. This is a practical and appropriate location for such service activities, both from an operational and visual perspective and accords with the intent of the Development Plan to ensure that such areas are accessible and screened from view.

As discussed above, the waste area (and loading dock area in general) will be screened from view to the west by proposed landscape and fencing treatments. Further, the waste storage area is to be sited approximately 20 metres from the nearest residential dwelling.

The ALDI Store will have a dedicated 1.5 cubic metre bin which is equipped with a hinged lid (i.e. it remains closed when not in use) which is stored within a screened bin enclosure. Figure 3.3 below illustrates a similar arrangement from the ALDI Blackwood Store, which highlights the appropriateness of this arrangement.

In addition, the ALDI Glenunga waste bin enclosure will be fitted with a covered roof.

Figure 3.3 *Waste Bin Enclosure (ALDI Blackwood)*



The collection of the bin is undertaken by a third-party contractor who will attend the Store three (3) times per week to collect the contents of the bin utilising a 10.5m front load truck, an example of which is illustrated in Figure 3.4 below. As discussed, above, waste collection occurs during 'daytime' hours and the waste truck will be onsite for approximately 3 minutes.

Figure 3.4 *Example of 10.5m Front Load Waste Truck*



Section 4.8 of our Planning Statement highlighted that ALDI have a consistent and environmentally sound approach to waste management which they implement across all Stores. The ALDI Waste Management strategy includes (but is not limited to):

- Concerted effort to reduce the amount of waste sent to landfill;
- Ongoing consultation with suppliers to improve and limit product packaging;
- Comprehensive education and ongoing training of staff members; and
- Monitoring of key performance indicators related to waste management, including receipt of data on current recycling services from appointed waste and recycling contractors such as audits of quantities from different waste streams, rates of contamination and weights of each collection. Analysis of this data ensures improvements can be communicated and implemented to further improve procedures.

In terms of the types of waste generated and managed onsite – we note:

- Minimal putrescible waste is generated onsite as no food is cooked or prepared within the Store (given there is no onsite butcher bakery, deli etc.);
- Waste disposed in the general/waste bin is limited to waste generated by staff on their lunch breaks (food packaging) and office-based type waste;
- Organic waste, comprising fruit, vegetables and other meat that are nearing the product's best before date are collected and re-distributed by ALDI's partner companies – Foodbank and Secondbite. They are not disposed into the waste bin and approximately 99% of organic waste is re-distributed in this manner; and
- Cardboard and plastic wrappings are securely stored, collected and taken offsite for recycling.

On this basis, we note there are little opportunities for the proposed ALDI Store to generate odour or encourage rats and other vermin. Notwithstanding, and recognising that it is important to their overall brand and image, ALDI ensure all internal and external areas of their Stores are maintained in a neat, tidy and presentable condition. Further, ALDI employ a proactive approach to pest management which includes placement of 'rat trap' boxes which are checked and maintained on a regular basis.

Having regard to the screened nature of the waste/service area; the distance to the nearest residential dwelling; the types of wastes stored; the frequency of waste collection; the waste bin collection times; and the management of vermin, we remain of the opinion that there are no unreasonable odour, dust, noise or health related amenity impacts associated with the proposed waste management arrangements.

Notwithstanding all of the above, we note that in the unlikely event that odour was to be an issue, the Council has powers to manage unreasonable odour levels under the *Local Nuisance and Litter Control Act, 2016*.

3.7 Traffic Impacts

We note the concerns raised regarding possible traffic impacts to the adjacent shops to the east at 476 Portrush Road.

As outlined in our initial Planning Statement, it is recognised the proposed development will result in some increase in traffic at the subject site and on Portrush Road adjacent the site. However, the overall design and associated traffic impacts were assessed by GTA Traffic Consultants who concluded *“there is adequate capacity in the surrounding road network to cater for the traffic generated by the proposed development”*.

In addition, the proposed access/egress arrangements and resultant traffic impacts have been reviewed by DPTI (Traffic) as part of their formal response provided under Section 37 of the *Development Act 1993*. DPTI offered *in-principle support* for the proposed access arrangements, median modifications and manoeuvring associated with delivery vehicles.

Accordingly, we note there will be no impact on the existing shops at 476 Portrush Road with their existing access arrangements (left-in from Portrush Road) unaffected by the proposed development.

4. Response to Council

4.1 Building Design and Setbacks

The ALDI Store has been deliberately sited close to the Portrush Road frontage to activate the street and create a sense of urban enclosure and intimacy to the public realm. The building benefits from a range of design elements intended to provide visual interest and reduce its overall bulk, including the use of large windows overlooking the street, canopy and frame projections and pedestrian switch back ramp access to clearly identify the entrance and primary frontage of the store.

In addition, the incorporation of the wrap around perforated tower screen, defines the northern corner elevation and adds further visual interest when viewed from Portrush Road.

The majority of the building has a height of approximately 9.4 metres, while the ‘taller’ elements relate to the feature ‘towers’ which are appropriately located away from property boundaries. These ‘taller’ elements act as important features to add visual interest to the building and enhance its overall appearance.

The proposed overall height of the building is driven by the need to ensure that sufficient onsite car parking is provided to support the use. Under-croft parking is contemplated within the Local Centre Zone and has been designed to integrate with the design of the building and is screened from Portrush Road by the switch back ramp, landscape treatments and by the setting down of the car park level below the front property boundary which all enhance the appearance of the development.

As discussed above, the height of the ALDI Store, relative to the public realm and adjoining properties, is reduced by the proposed excavation works and the ‘cutting’ of the building into the site.

The Streetscape Elevations prepared by Nielsen Architects (refer DA03.3 in **Appendix 2**) illustrate that the height of the building is contextually responsive and is compatible with nearby and adjoining sites along the western side of Portrush Road.

In terms of setbacks to the south and west, we note that both sites presently have established landscape/vegetated areas immediately adjacent the subject site. These existing vegetated areas presently offer a reasonable level of screening and separation from the subject site, and, other than removal of the existing embankment to the west (rear), the proposed development will not alter these existing arrangements.

In addition, and as discussed above, the proposed boundary landscape treatments will form a dense 'green screen' which will further soften the appearance of the building when viewed from the south and west.

Accordingly, and aligned with our response above, we remain of the view that the proposal will deliver a high design standard, contribute positively to the character of the streetscape, and has appropriate regard to adjoining residential sites.

4.2 Landscaping

Further to our discussion regarding the western boundary landscape treatment, we note the Council raised concerns regarding the adequacy of landscape treatments in front of the ALDI Store, as well as the shade tolerance of those species located to the south and west.

The landscape plan (refer **Appendix 1**) has been amended to include additional planting and shrubs around the northern tower, adjacent Portrush Road, which will further enhance the presentation of the street frontage and the internal amenity as customers enter the car park.

In terms of shade tolerance, all plant species selection has been undertaken in consultation with Outerspace Landscape Architects and all trees located along the southern boundary and western boundary (between the fences) are tolerant to partial and full shade.

Finally, we understand that the Council response included a preference that a detailed Landscape Plan be provided to support the development. However, it is not uncommon for these matters to be addressed during the detailed design phase of a development, and we are of the view that the conceptual landscape plan prepared by Outerspace is of sufficient quality and detail, and future establishment of landscaping can be appropriately managed via reasonable conditions of consent if required.

4.3 Signage Illumination and Outdoor Lighting

Section 6.6 of our Planning Statement considered the appropriateness of proposed advertising signage. In particular, it highlighted that illuminated ALDI signage has been designed and located to ensure there is no discomfort to approaching drivers or to create difficulty in the driver's perception of the road.

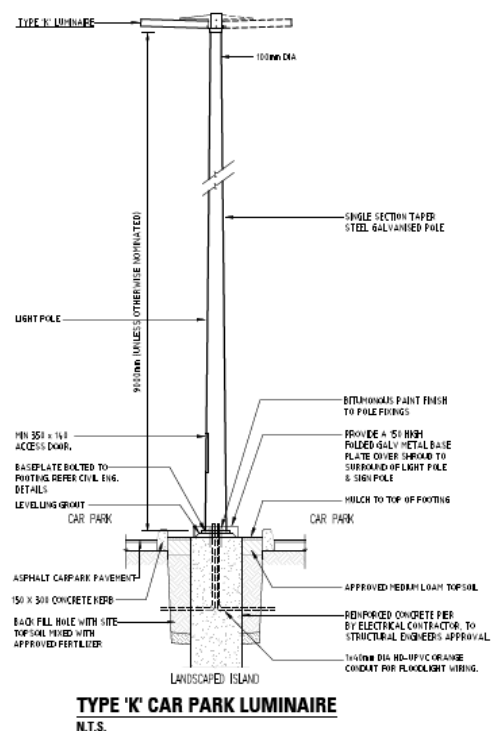
Furthermore, we note the proposed signage illumination arrangements were considered by DPTI (Traffic) in their referral response to the State Commission Assessment Panel. DPTI note that matters relating to illumination levels can be appropriately managed through conditions attached to any forthcoming Development

Plan Consent. ALDI understands and accepts DPTIs recommended condition and maintains that the signage will not cause discomfort or distraction.

In terms of outdoor lighting, we confirm that all lighting will be designed to comply with the requirements of *Australian Standard 4282 – 1997 'Control of the obtrusive effects of outdoor lighting'*.

We highlight that the lighting design used for most ALDI carparks involves the installation of marine grade die-cast aluminium alloy floodlights mounted on steel columns (heights vary). An image of the light design is illustrated below.

Figure 4.1 Light Design



Low-lux lighting is to be installed which will not be particularly visible beyond the site (no more brightness than street lights) and as mentioned above, will comply with relevant Australian Standards.

The Australian Standards are designed to ensure surface areas are appropriately lit without adversely impacting upon neighbouring properties due to unreasonable glare or light spill. It is therefore entirely appropriate to refer to the Standards in the design of lighting.

While we note Council's request that an Outdoor Lighting Plan be provided, it is not uncommon for these matters to be addressed during the detailed design phase of the development and we maintain the view that light spill and glare can be appropriately managed via reasonable conditions of consent which require lighting to be designed in accordance with relevant Australian Standards.

4.4 Trees on Adjoining Sites

Council requested that possible Regulated/Significant Trees on adjoining sites be identified and considered.

We confirm there are no Regulated or Significant Trees on the subject site, nor on the adjoining sites to the north and south.

Regarding the large tree located within the rear yard of No. 82 Sydney Street, which adjoins the site to the west, Arborman Tree Solutions were engaged to assess the possible impact of the development on the tree (refer **Appendix 6**).

The tree will not be directly affected by the proposed development, and, having regard to the 'drop' in level between the subject site and No. 82 (over 2 metres) there is not expected to be any intrusion into the root zone of the tree.

Accordingly, Arborman concluded:

"The site topography and the tree's distance from the proposed works, indicate that any works undertaken within the site will not impact the tree in any way. Additionally, no pruning of the tree is required to accommodate the redevelopment of the site".

4.5 Appropriate Conditions to Manage Interface Impacts

While we have addressed some of these matters above, we note that Council expressed a desire that appropriate conditions be applied to any Development Plan Consent that may be granted to ensure potential interface impacts are appropriately managed.

As we have confirmed, ALDI are supportive of reasonable and appropriate conditions being applied to signage and outdoor lighting illumination levels; the requirement to establish and maintain landscaping, and the screening and management of waste/service areas.

However, we are of the view that ALDI seeks consent for reasonable and practical hours of operation, delivery times and overall servicing arrangements which are cognisant of the site, adjoining land uses and the locality more generally. This position is supported by the plans and reports that have been submitted with the application and further supplemented within this response.

Accordingly, we see no requirement for additional and/or more onerous/restrictive conditions to be applied to any forthcoming Development Plan Consent.

5. Conclusion

Thank you for the opportunity to respond to these matters and we trust this submission offers a constructive response to the issues raised.

We confirm our desire to be heard and/or answer any questions raised at the State Commission Assessment Panel meeting when the application is determined.

Please do not hesitate to contact the undersigned should you require any further clarification in relation to this submission.

Yours Sincerely

A handwritten signature in grey ink, appearing to read 'C. Carrey', is positioned above the printed name.

Chris Carrey
Planning Consultant

Appendix 5. Acoustic Supplementary Response (Sonus)



Ekistics
Level 1, 16 Vardon Ave
ADELAIDE SA 5000

S42177.30C6

Attention: Chris Carrey

16 November 2019

Dear Chris,

**ALDI GLENUNGA
RESPONSE TO REPRESENTATIONS**

The representations relating to environmental noise for Development Application 145/3114/2018 have been reviewed.

One of the representations includes a review by Resonate. Resonate agrees with the Sonus general assessment approach and its predicted noise levels concur with the Sonus predictions. However, the review raises concern that the loading procedures will not be followed, that trucks will remain idling during unloading and suggests that the noise from the collection of rubbish has not been properly considered. The review also raises concern regarding the "ALDI PA System" and the location of the loading area with respect to PDC 28.

Delivery Procedures

ALDI has been using the *Delivery and Loading Procedures* at required sites since it began operating in South Australia. It has invested in trucks and training of drivers to ensure that all of the procedures are followed. The trucks are all fitted with:

- low level exhausts to maximise the effectiveness of noise barriers;
- reversing cameras to enable reversing alarms to be switched off;
- control of refrigeration and reversing alarms from the driver's cab.

Together, these features allow the procedures to be diligently followed.

ALDI truck drivers unload their own trucks and therefore there is no reason why trucks would remain idling. For a truck to remain idling, the driver would need to leave the truck cab unlocked with the keys in the ignition while unloading the truck from inside the store.

Rubbish Collection

The *Environment Protection (Noise) Policy 2007* separates the noise from rubbish collection from the noise from other sources. The Policy requires that unless the noise is lower than sleep disturbance criteria, it be limited to the day. It also requires that all reasonable and practicable measures be taken to minimise noise during the day.

ALDI collects waste on Mondays, Wednesdays and Fridays using a truck with a low level exhaust. A commitment has been made that these collections occur between the hours of 7:00am to 7:00pm to achieve the requirements of the *Environment Protection (Noise) Policy 2007*. In addition, the noise barriers proposed in the vicinity of the loading dock will significantly reduce the noise during the day. These noise barriers, in combination with the low level exhaust, represent all reasonable and practicable measures.

The noise from waste collection has been measured at ALDI West Lakes. Based on this noise measurement, it is predicted that the maximum instantaneous noise level in the rear yard of the closest residence would be lower than the expected noise at the front facade of the residence from Council rubbish collection. The average noise level (L_{Aeq}) in the rear yard is expected to be 52 dB(A). This level achieves the compliance level of the *Environment Protection (Noise) Policy 2007* for activity which might occur continuously all day.

ALDI PA System

ALDI does not operate a Public Address (PA) system.

Location of loading area

Resonate references Council Wide PDC 28. This PDC is under the general heading of "Design and Appearance" and does not appear to relate to environmental noise. Notwithstanding, the noise from activity at the loading dock including deliveries, rubbish collection and the compactor has been carefully considered and noise barriers have been designed to reduce the noise to suitable levels in accordance with the *Environment Protection (Noise) Policy 2007*.

sonus.

If you have any questions or require clarification, please call me.

Yours faithfully
Sonus Pty Ltd

A handwritten signature in black ink, appearing to read 'Chris Turnbull'.

Chris Turnbull
Principal

+61 417 845 720
ct@sonus.com.au



Reference Number:
ATS5735-ALDIGlenunga

Friday, 22 November 2019

ALDI C/- Nielsen Architects
Attn: Trent Burns
108 Mt Barker Road
Stirling SA 5152

Dear Trent,

Re: ALDI Glenunga, 499 Portrush Road, Glenunga – *Corymbia citriodora* (Lemon Scented Gum)

I have inspected the tree within the rear neighbour's property on Wednesday 20 November 2019. This tree has been assessed as a redevelopment of the site is proposed and concerns were raised regarding the potential for the redevelopment to impact the tree or the tree's root system.

The site topography and the tree's distance from the proposed works, indicate that any works undertaken within the site will not impact the tree in any way. Additionally, no pruning of the tree is required to accommodate the redevelopment of the site.

Thank you for the opportunity to provide this information. Should you have any questions or require further information, please contact me and I will be happy to assist.

Yours sincerely

JASON WILLIAMS
Consulting Arboriculturist
Graduate Certificate in Arboriculture
Diploma of Arboriculture
Australian Arborist Tier 1 License AL-2703
Arboriculture Australia - Registered Consulting Arborist
International Society of Arboriculture – Tree Risk Assessment (TRAQ)

Tree Location



Photos



Alliu, Yasmine (DPTI)

From: Chris Carrey <ccarrey@ekistics.com.au>
Sent: Tuesday, 3 December 2019 5:29 PM
To: Alliu, Yasmine (DPTI)
Cc: luke@nielsenarchitects.com.au; Nigel.Uren@aldi.com.au; Richard Dwyer
Subject: RE: SCAP invite ALDI c/- Ekistics - 180/E012/19
Attachments: Cut and Fill Depths.pdf

Hi Yasmine

Thanks for your query regarding the western and southern presentation and impact of the proposed ALDI Store at Glenunga. As requested, we provide the following details to assist with compilation of your SCAP Report, which is further to our formal response dated 19 November.

Western Elevation

As illustrated in the 'Western Elevation' drawing in Figure 1 below, and further illustrated within the 'cut/fill concept depths' drawing attached as Appendix 1, the development includes excavation across the majority of the land, with the ALDI Store to be 'cut' into the site. The overall strategy of 'cutting' the development into the site will ensure that the basement level car park is screened from view from Portrush Road and reduces the overall height of the building when viewed from the public realm and from adjoining properties.

While the level of cut/excavation varies in response to existing site and boundary levels, the extent of excavation along the western boundary is broadly summarised as follows:

- The existing battered embankment between the rear fence of the Mitre 10 and the 'true' western boundary will be removed. The proposed level of 'cut' will range from approximately zero (0) metres in the south-west corner, up to around 1.3 metres in the north-west quadrant. To support this approach, a retaining wall of varying height up to a maximum of 1.3 metres will be required along the 'true' western boundary which will be visible from those adjoining residential properties to the west.

A portion of the building, measuring approximately 16 metres in length, is to be setback 2 metres from the rear (western) boundary, in the south-west corner of the site. The remainder of the ALDI Store will be generously setback from the rear (west) boundary by approximately 10 metres.

As the extent of 'cut' is relatively moderate in the south-west quadrant, when viewed from the rear boundary of the adjoining residential property (82 Sydney Street), the building will exhibit a wall height of 9.9m, including the upper parapet.

However, this relatively short section of the building, is located more than 25 metres from the dwelling of 82 Sydney Street, screened by established vegetation within the rear yard of No. 82, and, as illustrated in Figure 1 below, will be significantly screened by the proposed fencing and (mature) landscape treatments along the western boundary.

While this portion of the building, slightly exceeds that of a 'traditional' two-storey development, it is appropriately setback from the boundary; does not result in unreasonable amounts of overshadow to the west; and will not result in extended loss of views from those adjoining residential properties to the west, in particular No. 82 Sydney Street.

Further, and as illustrated in Figure 2, the proposed site configuration delivers a more balanced outcome when contrasted with the existing situation and site levels whereby the Mitre 10 building exhibits an equivalent building height of around 7, to over 8 metres, for much of the western elevation, when viewed from those adjoining properties to the west.

Figure 1 – Western Elevation – Fence and Landscape

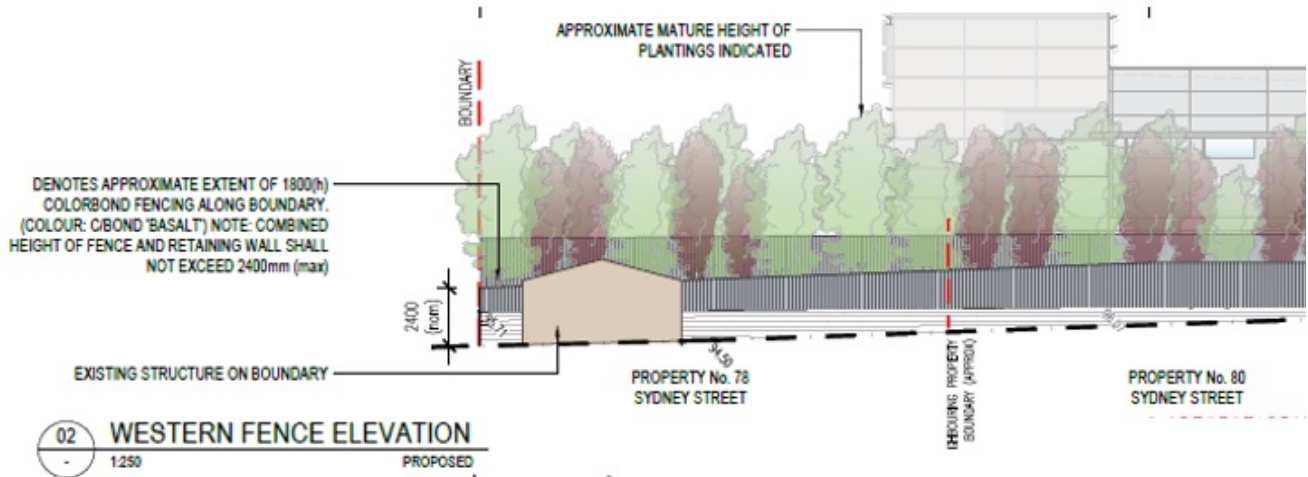
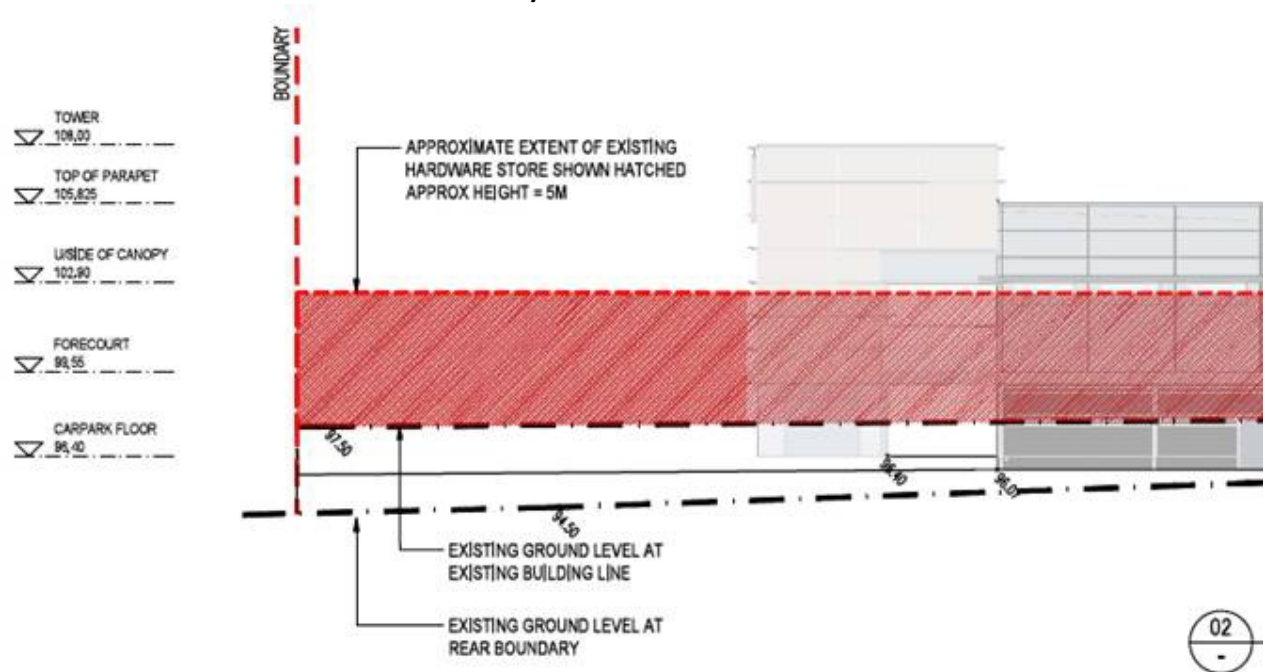


Figure 2 – Western Elevation with Mitre 10 Overlay



Southern Elevation

The ALDI Store is proposed to be located 2.34m from the southern boundary (at its closest point).

The extent of proposed excavation along the southern boundary is broadly summarised as follows:

- The proposed level of 'cut' will range from approximately zero (0) metres in the south-west corner to a maximum height of approximately 1.75 metres, with a taper to around 900mm in the south-east corner. The existing

retaining wall along the southern boundary will be removed and replaced with a new retaining wall which responds to the associated levels of excavation, noting that the new retaining wall will only be visible from the subject site.

This strategy will reduce the height of the ALDI Store when viewed from the south, with the southern element of the building exhibiting a maximum height of around 9.9m (close to the western boundary), to a height of around 8m (closer to Portrush Road).

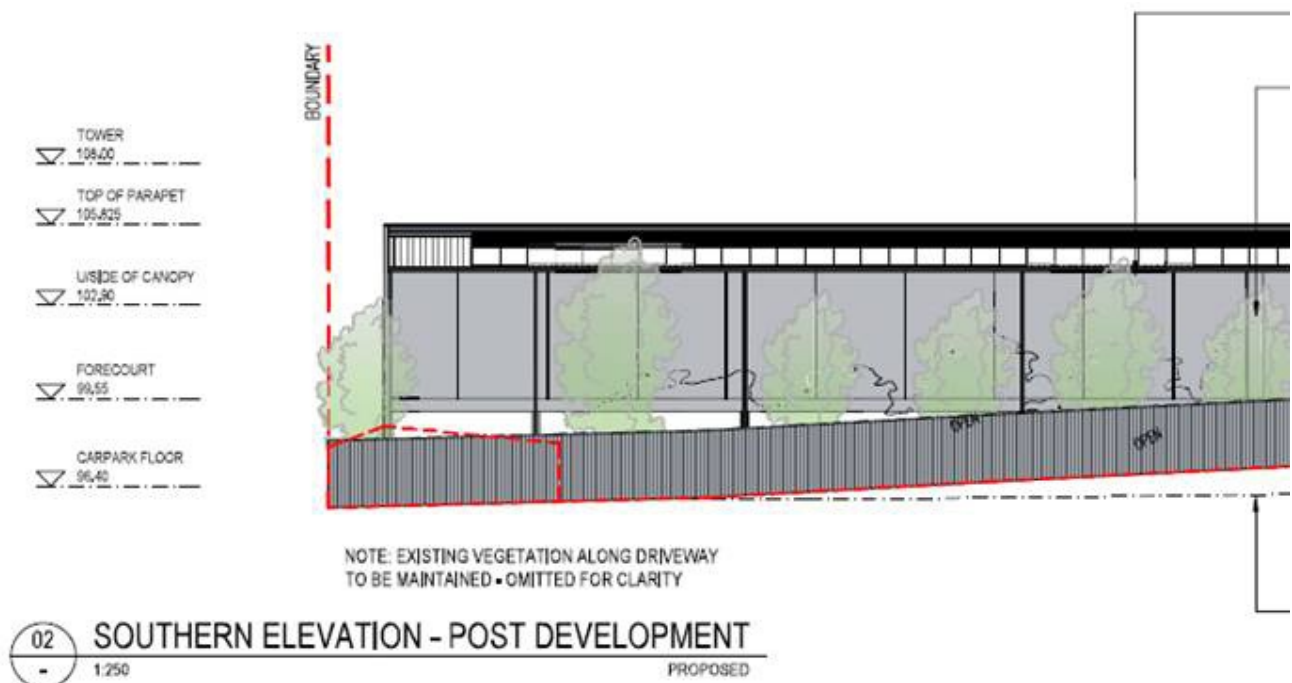
Similarly, the southern 'tower' will exhibit a height of around 8.5m when viewed from the south.

Accordingly, when viewed from the south, the ALDI Store exhibits an overall height comparable with that of a two-storey development, with the 'taller' portions located to the rear of the site, away from the residential units.

Importantly, the building will be screened by existing vegetation (to be retained) on the adjoining site to the south, as well as proposed (mature) boundary landscaping treatments along the southern boundary, as illustrated in Figure 3 below.

Further, and as previously discussed, the development does not result in unreasonable overshadowing impacts to the south, with clear and undisturbed solar access available to the adjoining southern units between 11.00am and 2.00pm on 21 June, which exceeds the more onerous 'residential' overshadowing requirements of the Development Plan.

Figure 3 – Southern Elevation



Yasmine, we trust this addresses your immediate queries. If we can assist with anything further, please do not hesitate to contact me on 0434 484 046.

Regards

Chris

Chris Carrey
Planning Consultant



Please note that the Ekistics office will be closed for the festive season from midday, Friday 20 December 2019 and will re-open Monday 6 January 2020.
Wishing you all the best for a Merry Christmas and a Happy New Year.

Level 1, 16 Vardon Ave, Adelaide SA 5000

p> 08 7231 0286

m> 0434 484 046

w> ekistics.com.au

From: Alliu, Yasmine (DPTI) <Yasmine.Alliu@sa.gov.au>
Sent: 03 December 2019 17:22
To: Chris Carrey <ccarrey@ekistics.com.au>
Subject: SCAP invite ALDI c/- Ekistics - 180/E012/19

Dear Chris

The State Commission Assessment Panel (SCAP) will meet to consider the above application on 12 Dec 2019. The SCAP meeting will be held in the Ground Floor meeting room, ODASA, 28 Leigh St, Adelaide at 9.30 am.

As the applicant, you are invited to appear personally or by counsel, solicitor or agent before the SCAP. Please be advised that SCAP meetings are now open to the public and therefore members of the public and the media may be in attendance.

Please notify of this office by telephone or email (with a copy to Jessie.Surace@sa.gov.au) to confirm your intention to attend.

General enquiries regarding the SCAP hearing can also be directed to Jessie Surace, SCAP Secretary
Jessie.Surace@sa.gov.au.

Your advice must be received by COB 10 Dec 2019. Should you not advise the SCAP by this date it will be assumed that you do not wish to attend.

Kind regards

Yasmine Alliu
Planning Officer
Strategic Development Assessment
Planning and Development (Performance Support Services, People and Business)
Department of Planning, Transport and Infrastructure
T 08 7109 7076 • E yasmine.alliu@sa.gov.au
Level 5, 50 Flinders Street, Adelaide SA 5000 PO Box 1815, Adelaide SA 5001 • DX967 • www.dpti.sa.gov.au



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We acknowledge and respect Aboriginal peoples as South Australia's first peoples and nations, we recognise Aboriginal peoples as traditional owners and occupants of land and waters in South Australia and that their spiritual, social, cultural and economic practices come from their traditional lands and waters; and they maintain their cultural and heritage beliefs, languages and laws which are of ongoing importance; We pay our respects to their ancestors and to their Elders.

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City of Burnside Development Plan Consolidated - 19 December 2017

COUNCIL WIDE

General

OBJECTIVES

Objective 8: A rational distribution and arrangement of land uses to avoid incompatibility between activities, and permit efficient use of land within the metropolitan area.

PRINCIPLES OF DEVELOPMENT CONTROL

Development Generally

3 Development should be undertaken in a manner that is consistent with the intended use and character of the relevant zone and policy area, and that would not interfere with the use of any other land.

Design and Appearance

OBJECTIVES

Objective 11: Development of a high design standard and appearance that responds to and reinforces positive aspects of the local environment and built form.

PRINCIPLES OF DEVELOPMENT CONTROL

14 Buildings should reflect the desired character of the locality while incorporating contemporary designs that have regard to the following:

- (a) building height, mass and proportion;
- (b) external materials, patterns, colours and decorative elements;
- (c) (d) façade articulation and detailing; and
- (e) verandahs, eaves, parapets and window screens.

15 Where a building is sited on or close to a side or rear boundary, the boundary wall should minimise:

- (a) the visual impact of the building as viewed from adjoining properties; and
- (b) overshadowing of adjoining properties and allow adequate sunlight access to neighbouring buildings

16 The external walls and roofs of buildings should not incorporate highly reflective materials which will result in glare to neighbouring properties, drivers or cyclists.

17 Structures located on the roofs of buildings to house plant and equipment should be screened from view and should form an integral part of the building design in relation to external finishes, shaping and colours.) roof form and pitch;

16 The external walls and roofs of buildings should not incorporate highly reflective materials which will result in glare to neighbouring properties, drivers or cyclists.

Overshadowing

21 The design and location of buildings should enable direct winter sunlight into adjacent dwellings and private open space and minimise the overshadowing of:

- (a) windows of main internal living areas;
- (b) upper-level private balconies that provide the primary open space area for a dwelling; and
- (c) solar collectors (such as solar hot water systems and photovoltaic cells).

Relationship to the Street and Public Realm

23 Buildings (other than ancillary buildings, group dwellings or buildings on allotments with a battle axe configuration) should be designed so that the main façade faces the primary street frontage of the land on which they are situated.

24 Buildings should be designed and sited to avoid extensive areas of uninterrupted walling facing areas exposed to public view.

25 Building design should emphasise pedestrian entry points to provide perceptible and direct access from public street frontages and vehicle parking areas.

Outdoor Storage and Service Areas

28 Outdoor storage, loading and service areas should be:

- (a) screened from public view by a combination of built form, solid fencing and/or landscaping;
- (b) conveniently located and designed to enable the manoeuvring of service and delivery vehicles;
- (c) sited away from sensitive land uses.

Building Setbacks from Road Boundaries

29 All setbacks from the road frontage should be additional to the road widening setback established under the *Metropolitan Adelaide Road Widening Plan Act 1972*.

Environmental Protection

PRINCIPLES OF DEVELOPMENT CONTROL

Energy Conservation

30 Development should be designed to take advantage of renewable sources of energy, especially solar energy, and minimise use of non-renewable sources of energy, including fossil fuels, in construction, and for heating, cooling, lighting and other purposes.

Water Sensitive Design

35 Water discharged from a development site should:

- (a) be of a physical, chemical and biological condition equivalent to or better than its pre-developed state; and
- (b) not exceed the rate of discharge from the site as it existed in pre-development conditions.

36 Development should include stormwater management systems to protect it from damage during a minimum of a 1-in-100 year average return interval flood.

37 Development should have adequate provision to control any stormwater over-flow runoff from the site and should be sited and designed to improve the quality of stormwater and minimise pollutant transfer to receiving waters.

38 Development should include stormwater management systems to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure the carrying capacities of downstream systems are not overloaded.

39 Development should include stormwater management systems to minimise the discharge of sediment, suspended solids, organic matter, nutrients, bacteria, litter and other contaminants to the stormwater system.

Waste

45 Development should be sited and designed to prevent or minimise the generation of waste (including wastewater) by applying the following waste management hierarchy in the order of priority as shown below:

- (a) avoiding the production of waste;
- (b) minimising waste production;
- (c) reusing waste;
- (d) recycling waste;
- (e) recovering part of the waste for re-use;
- (f) treating waste to reduce the potentially degrading impacts; and
- (g) disposing of waste in an environmentally sound manner.

46 The storage, treatment and disposal of waste materials from any development should be achieved without risk to health or impairment of the environment.

49 Development should include appropriately sized area to facilitate the storage of receptacles that will enable the efficient recycling of waste.

50 Development that involves the production and/or collection of waste and/or recyclable material should include designated collection and storage area(s) that are:

- (a) screened and separated from adjoining areas;
- (b) located to avoid impacting on adjoining sensitive environments or land uses;
- (c) designed to ensure that wastes do not contaminate stormwater or enter the stormwater collection system;
- (d) located on an impervious sealed area graded to a collection point in order to minimise the movement of any solids or contamination of water;
- (e) protected from wind and stormwater and sealed to prevent leakage and minimise the emission of odours; and

Amenity

OBJECTIVES

Objective 20: The amenity of localities not impaired by the appearance of land, buildings and objects, or by noise, light, emissions, traffic or any other quality, condition or factor.

Objective 21: Protection and enhancement of visual amenity by ensuring a high standard of design in respect of the appearance of development, and by the conservation and establishment of vegetation, including trees.

PRINCIPLES OF DEVELOPMENT CONTROL

General

51 Development should not take place in a manner which will cause nuisance, or detract from the amenity of the locality, by:

- (a) the emission of noise, vibration, odour, fumes, smoke, vapour, steam, soot, ash, dust, grit, oil, waste water, waste products, electrical interference, radioactivity, electro-magnetic radiation, reflection or light;
- (b) the drainage of stormwater or run-off from the land;

Visual Amenity

55 The design of buildings should be of a high standard and related to adjacent buildings and other features which contribute to streetscape quality.

56 Except in the Urban Corridor Zone, to maintain the harmony of built-form character within a streetscape, buildings should:

- (a) seek to maintain the continuity of vistas and existing building set-backs;
- (b) not be set-back a lesser distance than the nearby buildings with frontage to the same road unless such distance is consistent with the minimum set-back prescribed in the relevant zone and policy area; and
- (c) be set-back a greater distance if the proposed building is of greater bulk or height than other buildings fronting the same road, unless the taller or bulkier portion of a building is positioned towards the rear of its site, or the building is effectively screened, so that it will not dominate views from the road.

Noise Generating Activities

60 Development that emits noise (other than music noise) should include noise attenuation measures that achieve the relevant *Environment Protection (Noise) Policy* criteria when assessed at the nearest existing noise sensitive premises.

Landscaping

66 Development should incorporate appropriate landscaping which is designed to:

- (a) assist in visual enhancement of the development and its locality;
- (b) promote optimal micro-climate conditions in and around buildings (for example, by use of deciduous plants to allow penetration of heat from the sun in winter, and provide shade in summer);
- (c) shade, define and create windbreaks for pedestrian paths and open spaces;
- (d) screen, shade and enhance the appearance of car parking areas;
- (e) screen service yards, loading areas and outdoor storage areas;
- (f) promote conservation of water and biodiversity;
- (g) minimise risk, and not hinder protection from bushfires;
- (h) be durable and minimise the need for maintenance;
- (i) allow appropriate levels of surveillance of car parking areas, service areas and entrances to development to minimise opportunities for crime;
- (j) ensure safe traffic movement by not obstructing or obscuring the views of motorists and/or pedestrians; and
- (k) maintain the integrity of buildings, structures or infrastructure.

67 Landscaping should be adequately protected from damage by vehicles or pedestrians.

68 The selection of plant species should, where appropriate, include:

- (a) species indigenous to the local area, or others which will reinforce a sense of place and contribute to the objectives for the relevant zone and policy area;
- (b) trees in scale with buildings on the land;
- (c) taller tree species in localities where such trees are absent or scarce; and
- (d) species selected and located to minimise debris and leaf fall entering the nearby drainage system.

Movement and Parking of Vehicles

OBJECTIVES

Transportation System and Facilities

Objective 29: Provision for the safe, convenient and efficient movement of people and goods having regard to the road hierarchy, including arterial roads for major traffic movements, shown on Map Bur/1 (Overlay 1).

Objective 31: Encouragement of walking and cycling by provision of:

- (a) safe, convenient and legible movement networks to points of attraction; and
- (b) secure bicycle parking.

Objective 33: A form of development adjoining arterial roads which:

- (a) ensures traffic can move efficiently and safely;
- (b) discourages commercial ribbon development;
- (c) prevents high traffic-generating uses outside designated centre zones, or the Office or Urban Corridor Zone;
- (d) provides for adequate off-street parking; and
- (e) provides limited and safe points of access and egress.

Parking of Vehicles

Objective 35: Adequate parking for vehicles.

PRINCIPLES OF DEVELOPMENT CONTROL

Transportation System and Facilities

93 Development should provide for safe and convenient:

- (a) access for pedestrians, cyclists and vehicles, including emergency and essential service vehicles;
- (b) off-street parking to an extent and in a manner which will minimise interference with the flow of traffic on roads adjoining the site of development; and
- (c) off-street loading, unloading and turning of vehicles, including service vehicles, likely to be required in association with the use of the land.

94 The number, location and design of points of access to and egress from a road should be such as will minimise:

- (a) interference with the free flow of traffic on roads in the locality;
- (b) interference with the function of any intersection or device for the control of traffic;
- (c) queuing on the road;
- (d) right turn movements;
- (e) the gradient of any exit driveway;

96 Development on land abutting a Primary Arterial Road, or a Secondary Arterial Road, shown on Map Bur/1 (Overlay 1), should be designed to ensure that all vehicles can enter and leave the site of that development in a forward direction.

97 No point of vehicular access or egress from a road to a non-residential development site should be nearer to another access or egress point than six metres, nor be located within 10 metres of the carriageway of an intersection.

101 Development should cater for the safety and convenience of the users of land by providing, where appropriate, pedestrian pathways, ramps and car parking spaces located and designed to foster ease of movement by aged or disabled persons and persons accompanied by children.

104 Development should not take place until the carrying out of traffic control works, or other works or facilities associated with vehicular movement, including the construction and restoration of crossovers, required as a direct result of that development is assured.

Parking of Vehicles

106 Spaces for off-street car parking should be provided with development at the rates prescribed in Table Bur/5 and Table Bur/6, subject to the extent to which other principles of development control concerning provisions for car parking are met, and except where the prescribed rates are in conflict with those prescribed in any part of a zone in which case the provisions for the part of a zone take precedence.

107 Development should include the provision of space for off-street car parking sufficient to accommodate the likely needs for such space generated by the development, taking into account:

- (a) the scale and nature of the development and the activities of those who would use the land concerned;
- (b) the availability of space for car parking on land other than the site of the development; and
- (c) the likely use of public and/or non-motorised transport in travel to and from the site of the development.

108 Development should provide appropriately for the parking of vehicles by disabled persons.

109 Where appropriate, development should provide safe and secure facilities for the parking of bicycles.

110 An area for car parking and the manoeuvring of vehicles should:

- (a) incorporate landscaping, including substantial trees and shrubs both on the perimeter and within the parking area, for shade and visual enhancement;
- (b) be set-back not less than two metres from the boundary of a road except in the Urban Corridor Zone;
- (c) be designed, surfaced and graded to facilitate stormwater infiltration and treatment (for example, by surfacing open car parking spaces with materials which allow stormwater infiltration and/or diverting runoff from paved areas to soft landscaped areas, or stormwater infrastructure specifically designed to accommodate on-site infiltration or treatment);
- (d) be located and designed to ensure safe and convenient pedestrian access to and from the parked vehicles and between parked vehicles and the facilities served by the car parking area; and
- (e) be located and designed to ensure safe and convenient traffic circulation, with minimal conflict with service access and functions; and
- (f) include adequate provision

112 Loading and unloading areas and parking areas for service or commercial vehicles should have separate ingress and egress points from those for car parking areas, and should be screened from general view.

Retailing and Centres

OBJECTIVES

Retailing

Objective 68: Shopping, administrative, cultural, community, entertainment, educational, religious, and recreational facilities located in integrated centres and corridor zones.

General

Objective 69: Centres established and developed in accordance with a hierarchy based on function, so that each type of centre provides a proportion of the total requirement of goods and services commensurate with its role.

Objective 70: A hierarchy of centres located in centre zones or areas.

Objectives 68, 69 and 70 apply to the groupings of facilities into centres and the location of those centres.

The grouping of a wide range of facilities in integrated centres will benefit the community by encouraging economic and shared use of facilities, providing a meeting place for communities, and encouraging ready access by both public and private transport.

The hierarchy of centres is based on the principle that each type of centre provides a proportion of the total community requirement for goods and services commensurate with its role.

Centres within the area of metropolitan Adelaide are of the following types:

- (a) The Central Business Area of the City of Adelaide;
- (b) Regional Centre;
- (c) District Centre;
- (d) Neighbourhood Centre; and
- (e) Local Centre.

The degree to which the various facilities can be located within a centre will depend, among other things, upon the size of the centre, the specific policies relating to the centre, the implications of competing centres for the population being served, and the characteristics of the population to be served. Each development proposal for a centre should be evaluated against the defined roles in the centre hierarchy of that centre and other centres.

New development in centres or corridor zones should result in the expansion of the total range of retail goods and services available to the population to be served, have regard to the location and role of other existing and proposed centre zones, and be of a size and type which would not demonstrably lead to the physical deterioration of any existing centre zone.

The identification of each zone in a hierarchy of centres should be such as to:

- (a) cater for the existing and future population's shopping and community needs;
- (b) provide a degree of choice in the location of centre facilities;
- (c) be safely and readily accessible to the population to be served, particularly by public transport, and obviate the need for unscheduled large-scale traffic and transport works;
- (d) have minimal adverse impact on residential areas;
- (e) concentrate development on one side of an arterial road, or one quadrant of an arterial road intersection, and have minimal adverse impact on traffic movement on arterial roads. Linear extension of centre zones or areas along arterial roads is to be minimised;
- (f) reflect the potential to rehabilitate or extend centre zones or areas, and make effective use of existing investment in public infrastructure, utilities and transport, any costs involved being offset by benefits to the population being served;
- (g) be of a size and shape suitable for their functions, and provide car parking facilities;
- (h) have regard to the maintenance of retail employment levels in the area; and
- (i) have regard to the degree to which existing centres satisfy the above objectives.

Objective 74: Local centres and corridor zones to include shopping and local facilities and services to serve the day-to-day needs of the local community.

Objective 76: Retailing development not consistent with facilities envisaged in a centre located and operated so as not to adversely affect any designated centre, commercial, business or residential zones, or areas, or traffic movements on nearby roads.

The diversification of locations for retailing providing goods and services not compatible with the grouping of facilities envisaged for regional, district and neighbourhood centres (including corridor zones) may be considered so long as the integrity of the centre hierarchy is not compromised and the development is compatible with land uses in the locality.

Retail development of this kind should be evaluated having regard to:

- (a) its locational and operational compatibility with existing shopping, business and commercial zones, or areas, including the nature of the goods and materials to be stocked, and the noise levels of vehicles and plant used on, and servicing, the site;
- (b) its effect on adjacent residential development;
- (c) the increased use of local and arterial roads;
- (d) the adequacy of vehicular access and car parking; and
- (e) compliance with building and site development standards for centres.

PRINCIPLES OF DEVELOPMENT CONTROL

General

236 Development located within any centre zone should be designed and located to minimise its impact on existing or potential dwellings in an adjacent zone due to:

- (a) vehicular access, egress and circulation within the site of the development;
- (b) the location and arrangement of service, refuse and waste storage and collection facilities;
- (c) the scale, height and bulk of buildings where located close to the boundaries of the zone;
- (d) unreasonably adverse visual impact when viewed from the site of any dwelling;
- (e) loss of privacy or overshadowing; and
- (f) the generation of noise, odour, light or particulate matter.

237 Development in a centre zone should:

- (a) accord with the intended role of the centre within the centres hierarchy, and provide for any future expansion needs commensurate with that role;
- (b) provide for multiple use of facilities, sharing of utility spaces, and consolidated, coordinated and convenient parking;
- (c) ensure appropriate integration of facilities and public transport;
- (d) be concentrated and designed for pedestrian convenience, avoiding conflict between pedestrian and vehicular traffic movement;
- (e) produce a close relationship between shops in a “lively” setting;
- (f) enhance the amenity of the site and locally through unified design of buildings and signs, with landscaping forming an integral part of centre design and used to foster human scale, provide shade and visual relief, define spaces, reinforce paths and edges, and screen utility areas; and
- (g) not cause congestion, detract from the safety of users of adjoining roads, or cause a need for a significant expenditure on transport and traffic works or facilities in or outside the locality.

Shopping Development

238 The development of shops should be as follows:

- (a) a shop or group of shops with a gross leasable area of greater than 250 square metres should be located in the District Centre Zone, a Neighbourhood Centre Zone, or the Urban Corridor Zone;
- (b) a shop or group of shops with a gross leasable area of 250 square metres or less should not be located on an arterial road (as shown on Map Bur/1 (Overlay 1)), except within the District Centre Zone, a Neighbourhood Centre Zone, a Local Centre Zone, or the Urban Corridor Zone;
- (c) a shop or group of shops located outside the District Centre Zone, a Neighbourhood Centre Zone, a Local Centre Zone, or the Urban Corridor Zone should:
 - (i) not hinder the development or function of any of those zones (or a designated centre in any other Council area);
 - (ii) provide retail services of a strictly local nature; and

- (iii) not diminish the amenity of the locality;
- (d) development in the form of retail showrooms trading in bulky goods merchandise, should provide adequate manoeuvring and circulation areas in order to accommodate truck and trailer movements and safe access points; and
- (e) shopping development should include designated parking space for disabled persons.

Outdoor Advertisements

246 Advertisements and advertising displays should:

- (a) convey in simple, clear and concise language, symbols, print style, layout and a small number of colours, the owner/occupier and/or generic type of business, merchandise or services being advertised;
- (b) be few in number to avoid proliferation of advertisements or advertising displays;
- (c) be of a form, style, scale, height and size and in locations which:
 - (i) are appropriate to the character of the locality;
 - (ii) preserve reasonable exposure to the public for all adjacent sites; and
 - (iii) will not obscure a driver's view or distract drivers where demands on concentration are high such as intersections or bends in a road.
- (d) not have an adverse impact on the amenity of adjacent premises and areas from which they are visible;
- (e) rectify or improve existing unsatisfactory signage;
- (f) ensure that pole and other structures with the sole purpose of supporting an advertisement (g) not encroach beyond the boundaries of the subject property and if road widening is applicable, the proposed property boundary realignment;
- (h) be unified in colour, style, placement and proportion to present a co-ordinated theme and design for a site, centre or building;
- (i) be designed, made and presented in a high quality manner, and be maintained in good repair and condition at all times; and
- (j) be designed and located to avoid damage, pruning or lopping of on-site landscaping or street trees. are concealed from view, or of simple design and visually unobtrusive;

248 An advertisement or advertising display attached to a building should:

- (a) be of appropriate colour, scale and proportion, coordinated with and complementing the architectural form and design of the building that it is attached to; and
- (b) not be located so as to extend in a horizontal direction beyond the walls or other vertical surface of a building, unless the advertising display is appropriately designed to form an integrated and complementary extension of the existing building.

LOCAL CENTRE ZONE

Introduction

The objective and principles of development control which follow apply in the Local Centre Zone shown on Maps Bur/3 to 7 and 9. They are additional to those expressed for the whole of the council area.

OBJECTIVE

Objective 1: A zone accommodating a range of shops, offices, business, health and community service providers all being of a small-scale suited to servicing the needs of the local community.

PRINCIPLES OF DEVELOPMENT CONTROL

1 Development undertaken in the Local Centre Zone should generally consist of small-scale shops, offices, consulting rooms and commercial premises suited to meeting the needs of the local community.

2 Shop development should generally comprise a maximum gross leasable floor area in the order of 450 square metres.

5 Buildings should not contain one floor level directly above another floor level except where the lower floor level is for the provision of car parking space and the level of that floor is located not less than one metre below the predominant natural ground level for the site.

6 Undercroft vehicle parking areas should be reasonably screened from view from public roads by the use of landscaping or other means which enhance the external appearance of the land and buildings.

7 Vehicle parking should be provided in accordance with the applicable rates as set out in Table Bur/5 - Off-Street Vehicular Parking Requirements or Table Bur/6 - Off-street Vehicle Parking Requirements for Designated Areas (whichever applies).