



26 July 2017

Department of Planning, Transport and Infrastructure
Attention: Mr Daniel Pluck, *Planning Officer*
GPO Box 1815
ADELAIDE SA 5001

via email: daniel.pluck@sa.gov.au

Dear Daniel

DAC DEFERRAL RESPONSE

DA: 101/L002/15
Applicant: De Young Jamestown Pty Ltd
Proposal: Proposed kiosk, alfresco bar and dining area, toilet facilities, outdoor dining timber deck, boat pontoon and gangway and associated landscaping and temporary contractarian fence.
Subject land: Allotment 1001 Adelphi Terrace, Glenelg North

1.0 INTRODUCTION

Further to the resolution of the Development Assessment Commission (DAC) of 19 January 2017 to defer the above matter, the applicant has amended and updated the proposed development to address the matters raised.

Ben Green & Associates was appointed by the applicant, De Young Jamestown Pty Ltd – Mr Troy De Young, to undertake a thorough review of this project and in doing so assist in ensuring all matters raised by the DAC have been addressed, and also provide an overview of the additional information and amendments herein provided to obtain a favourable decision of the DAC.

In providing this correspondence, I can confirm that I have provided advice throughout the amendment and collation process and have reviewed with the most pertinent provisions of the Holdfast Bay Council Development Plan.

I provide my views and opinions on the matter below.



2.0 REASONS FOR DEFERRAL

At its meeting of 19 January 2017, the DAC resolved to defer Development Application 110/L002/15 to enable the applicant to address the following matters:

- *Clearer plans to demonstrate the interrelationship between the public realm and the development;*
- *Further details on the western façade of the Buffalo structure and any other alterations that are required for the development;*
- *Details around the resolution of the moat – landscaping, water quality;*
- *Identify the space, facilities and utilities that are located in the Buffalo structure that are integral to the wider development;*
- *Provide a consistent package of plans and documentation eg. Including resolution of noise attenuation, elevations of all facades and consistent plans.*

3.0 AMENDMENTS

In addressing the above points the applicant has made a series of amendments to the proposed plans and has also enlisted further expertise from qualified and experienced professionals in Town Planning, Landscape Architecture and various Engineer's to refine the merits of the proposal to ensure a quality outcome for such an iconic structure within such a prominent setting.

The following is a summary of the amendments:

- Updated existing and proposed floor plans and elevations of the Buffalo to outline the extent or works proposed.
- Updated 3D perspectives to align with current drawings.
- Inclusion of locality plan and lease area boundary to clearly confirm the extent of the proposed development.
- Clarify the extent of development on the northern side of the Buffalo structure relating to the outdoor dining deck and verandah.
- Relocation and amendment to outdoor amenities to accommodate DDA compliance with the Building Code of Australia.
- Increase paving levels to outdoor dining area to match existing levels to adequately accommodate essential services and infrastructure.
- Inclusion of glass acoustic barriers to upper level deck of Buffalo structure, and outdoor dining areas in accordance with advice from Acoustic Engineers.



- Upgrade of moat in accordance with engineering and landscaping specifications.
- Additional egress points provided to satisfy Building Code of Australia.
- Resolution of stormwater disposal and water quality outcomes for the moat to ensure compliance with relevant legislation.
- Provision of landscaping master plan together with indicative perspectives that highlights the structure post development in relation to with the surrounding public realm.

The following sections outline a response to each of the deferral points.

3.1 *Clearer plans to demonstrate the interrelationship between the public realm and the development*

The applicant engaged Outerspace Landscape Architects, who are recognised and leading professionals in this area with experience in other public realm projects throughout South Australia. Outerspace Landscape Architects have prepared a landscape master plan to demonstrate how the proposed development relates to the wider public realm including Wigley Reserve and the Patawalonga Lake.

The site plan and landscape master plan clearly now identifies the location of the lease boundary (being the interface between this development and the wider public realm) which is located to the north of the grassed area of Wigley Reserve between the Patawalonga Lake to the north-west and Adelphi Terrace to the east. The lease area encompasses the existing car park area adjacent to the Historic Tram Display incorporates the paved area around the curtilage of the Buffalo, and extends around the existing 'moat' structure. The lease area affords the opportunity for a pontoon to protrude on the north-western side of the Buffalo mid-way along this section of the moat.

The landscape master plan has been prepared considering the context in which the lease area sits within the wider public realm. Existing pedestrian linkages will be maintained through the site albeit reconfigured around the proposed built form and outdoor dining area.

The proposed kiosk building is orientated towards the broader public realm Wigley Reserve and the existing playground and amenities. This portion of the development is proposed to have feature concrete paving to define the transition between the public space and lease area.

Exposed aggregate concrete paving is proposed to define the point of entry to the Buffalo from the existing footpaths from the north and the car park.

Additional plantings are proposed to complement existing mature pine trees to improve the overall amenity of this space.

Outerspace Landscape Architects have also provided two photographic perspectives to illustrate how the site will appear post development and following the maturing of landscape species. Tree species proposed will be semi-mature when planted.



3.2 Further details on the western façade of the Buffalo structure and any other alterations that are required for the development

An outdoor dining deck area with pontoon is proposed on the north-western side of the Buffalo structure. The proposed deck area is to be constructed of hardwood timber and has been redesigned to be more sympathetic to the curved appearance of the Buffalo structure. The deck area is proposed to be covered by a hardwood verandah of simple form covered with a 5-degree corrugated steel roof so as not to dominate or detract from the main iconic Buffalo structure.

The outdoor dining deck area is proposed to be utilised in association with the restaurant space occupying the lower floor of the Buffalo and will have a consistent finished floor level between spaces.

In order to achieve access to the outdoor deck area from within the Buffalo, alterations to the existing window openings are required. Prior to preparing this response, the applicant sought advice on the overall proposed development from Building Certifiers, PBS Australia Pty Ltd which is attached to this correspondence.

PBS Australia Pty Ltd do not foresee there will be any complications with the proposal achieving the Building Code of Australia, and in their advice outlined a number of minor suggestions relating to access and egress of the Buffalo structure which have been incorporated into the design.

3.3 Details around the resolution of the moat – landscaping, water quality

The Applicant engaged the services of Russell Consulting Engineers to provide advice in relation to the resolution of the moat and water quality outcomes. It is proposed to fill the moat with suitable clean material compacted to an adequate standard in order to raise the moat to a finished level consistent with the existing paved area to provide an improved relationship between the Buffalo structure and the public realm.

The design measures required to achieve this include:

- Thickened raft slab (glass fibre reinforced) to be placed on existing surface level.
- Increase height of existing moat bund/sleeper wall to RL 2.500 AHD as per concourse level and above 100 year ARI and king tide level (2.36 AHD).
- Porous fill (no fines concrete, coarse rubble or similar granular fill) to underside of landscape zone, free draining back to lake.
- Reinforced concrete wall adjacent boat to support fill and prevent ingress of water under boat and around timber piles. Sump and pumps to remove any seepage or upwelling.

The stormwater system will be designed in accordance with relevant *Australian Standards*, *the EPA Code of Practice*, *Local Government's WSUD Urban Design Manual* and recognised engineering best practices. It is noted that the report previously presented to



the DAC included a recommended condition to this effect. The applicant does not object to a condition of this nature forming part of the approval.

The landscape master plan prepared by Outerspace Landscape Architects also proposes a planting and landscaping scheme around the curtilage of the Buffalo and within the existing moat area (now raised to existing ground level). It is proposed to install large planter boxes within the infilled moat for the placement of larger tree species which are to be interspersed with various shrubs and ground covers. The species selection includes various sand tolerant and native plantings that are expected to thrive within the coastal environment. The landscaping will wrap around the Buffalo to not only soften the appearance of the structure, but also to frame it. The master plan specifies that the planting will give rise to a 'forest' feel around the boat, by imbedding the Buffalo structure within the Wigley Reserve whilst retaining its long standing links with the Patawalonga River by way of introduction to the pontoon.

Also proposed within the landscaped moat area is a dry gravel creek and several boulders to wrap around the structure to reflect the rocks found along the wave breakers to the north along the eastern side of the Patawalonga Lake.

The outdoor dining space on the southern side of the Buffalo will be surrounding by landscaping, including some feature plants in pots and urns to create a sense of enclosure and soften the appearance of the acoustic barriers of the dining space.

3.4 *Identify the space, facilities and utilities that are located in the Buffalo structure that are integral to the wider development;*

It is the intention of the applicant to resume operation of the Buffalo as a restaurant with associated office. The resumption of operations are considered to be in keeping with the previous and pre-existing approved use of the structure.

The interior and exterior of the existing Buffalo structure is to be totally refurbished and reinstated to its former condition. A structural engineering assessment confirms that the Buffalo structure is generally sound, and any deficiencies are to be brought into compliance to meet current engineering and safety standards, which are not considered to constitute development.

The existing kitchen facilities are to be upgraded and will be utilised in association with both the northern outdoor deck area and the outdoor dining area on the southern side of the structure. The internal kitchen will also be supplemented by a pizza oven attached to the outside 'bar'.

Additional amenities and toilet facilities are provided within the outdoor dining area, which are to be utilised in conjunction with upgraded facilities within the Buffalo structure.



3.5 *Provide a consistent package of plans and documentation eg. Including resolution of noise attenuation, elevations of all facades and consistent plans.*

Plans and documents have been updated to ensure accuracy and consistency across all documentation.

Elevations have been updated and include the recommended noise attenuation measures referred to in the Acoustic Report prepared by Marshall Day Acoustics dated 14 October 2016.

The following plans and documents form the basis of the proposed development:

- Proposed Plans – Stuart J Patrick & Associates, July 2017
- Engineering Response – Russell Consulting Engineers, 3 July 2017
- Acoustic Report – Marshall Day Acoustics, 14 October 2016
- Landscape Master Plan – Outerspace Landscape Architects, 30 June 2017
- Landscape Perspectives – Outerspace Landscape Architects, 27 June 2017
- Building Code of Australia consideration – PBS Australia Pty Ltd, 6 July 2017



4.0 CONCLUSION

After careful consideration of the report presented to DAC at its meeting of 19 January 2017 and the matters discussed herein in response to the concerns raised, it is our view that the application is not 'seriously at variance' with the provisions of the Development Plan and as such warrants the issuing of Development Plan Consent.

The proposal would provide complementary retail and food outlets that would enhance and activate the Buffalo structure and surrounding recreational areas and reinvigorate one of the most iconic structures within the Glenelg Foreshore.

In our view, the development displays significant merit when assessed against the relevant criteria of the Development Plan and we concur with the assessment and recommendation of the previous report presented to the DAC.

Other than where varied herein by way of additional and/or supplementary information, the applicant does not object to the Reserve Matter or Planning Conditions outlined in the previous recommendation to the DAC.

We trust that the additional information supplied herein sufficiently responds to the matters for deferral to enable a favourable recommendation to the next available DAC meeting, which is scheduled for 10 August 2017.

Should you wish to discuss any matters herein, please do not hesitate contacting me on 0410 147 541.

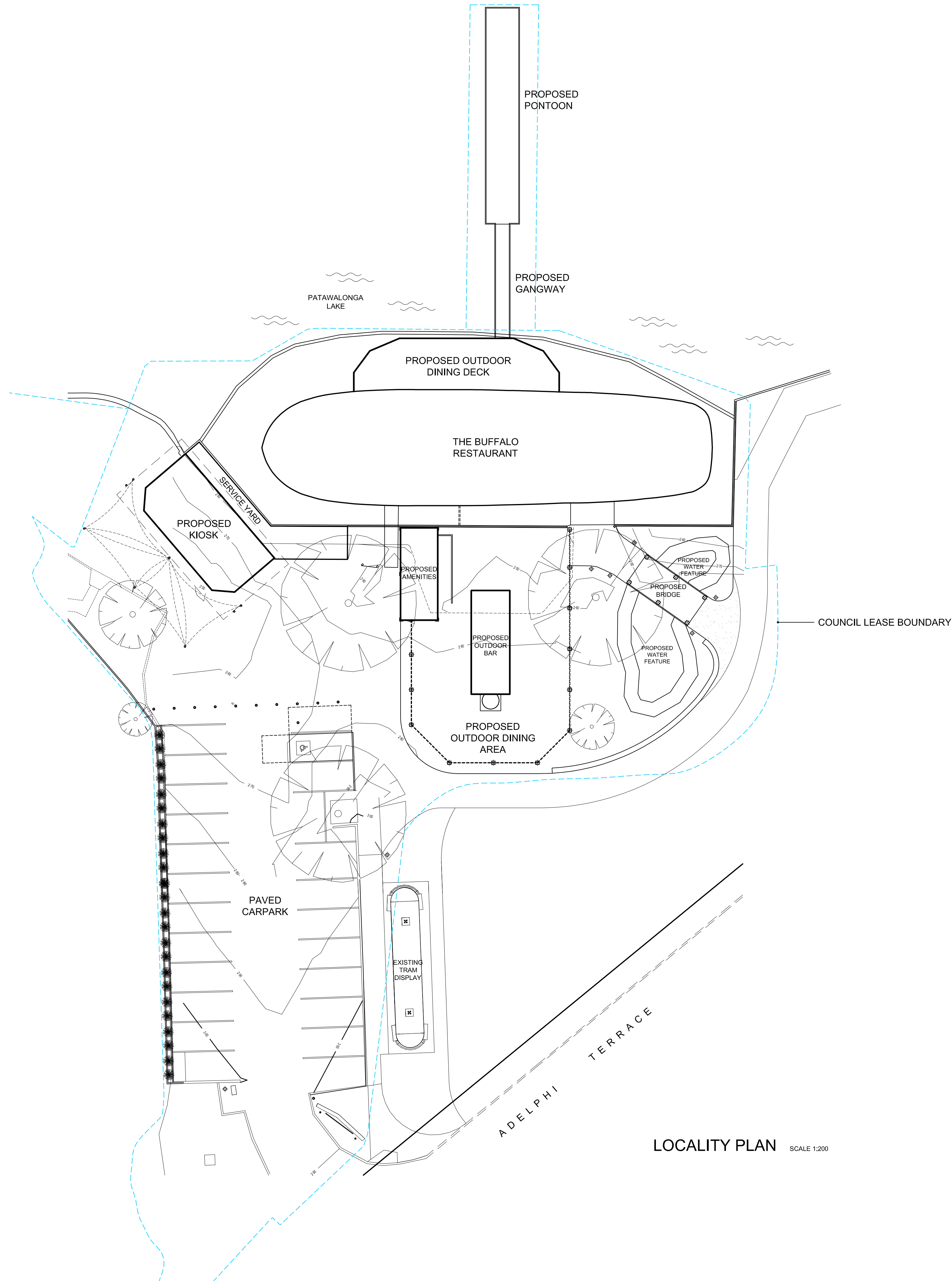
Yours faithfully

Ben Green & Associates

Ben Green, CPP MPA
bengreen@bengreen.com.au

Tom Gregory, CPP MPA
tomgregory@bengreen.com.au

cc: Mr Troy De Young, Mr Conrad Speight, Mr Stuart Patrick, Mr Angus Russell, Mr Patrick Graham
enc: Proposed Plans / Engineering Response / Acoustic Report / Landscape Master Plan / Design Perspectives / Building Code of Australia considerations



LOCALITY PLAN SCALE 1:200

NORTH APPROX.

Stuart J. Patrick
& ASSOCIATES PTY LTD
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DEVELOPMENT PLAN CONSENT APPLICATION
- NOT FOR CONSTRUCTION

PROPOSED RE-DEVELOPMENT
BUFFALO RESTAURANT
ADELPHI TCE, GLENELG

FILE NO: ADI/GLG/BUFFALO17
DATE: JULY 2017
DRAWN BY: MC / SP
SHEET: 1 OF 12
SCALE: AS SHOWN
ORIGINAL SHEET SIZE: A1

ALL WORKS SHALL COMPLY WITH S.A. DEVELOPMENT ACT AND REGULATIONS BUILDING CODE OF AUSTRALIA RELEVANT AUSTRALIAN STANDARDS OF CURRENT ISSUE AND ALL STATUTORY AUTHORITY REQUIREMENTS

CONTRACTORS SHALL VERIFY ALL DIMENSIONS, LEVELS AND DETAILS ON SITE BEFORE COMMENCING ANY WORK, OR PREPARATION OF SHOP DRAWINGS

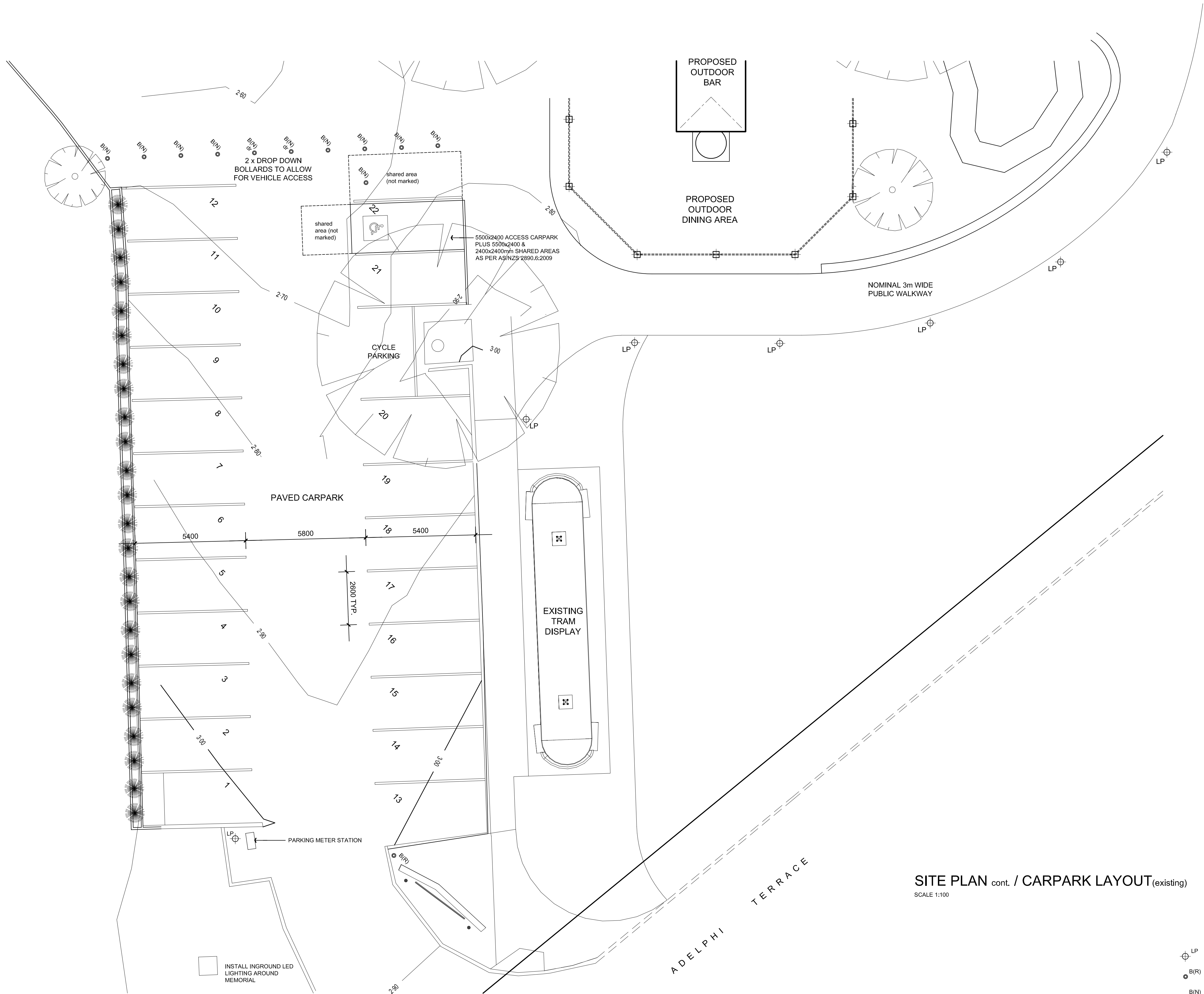
WRITTEN DIMENSIONS TAKE PRECEDENCE OVER SCALE

ANY DISCREPANCIES OF SIGNIFICANCE TO BE REPORTED TO THE DESIGNER OR SUPERVISOR IMMEDIATELY & RESOLVED BEFORE PROCEEDING FURTHER WITH THE WORKS

ANY BUILDER/CONTRACTOR PREPARING ESTIMATES FOR THE PROPOSED WORKS SHALL VISIT THE SITE AND, IN CONSULTATION WITH THE OWNER, CONFIRM THE NATURE AND EXTENT OF ALL WORKS

THE BUILDER IS TO ENSURE THAT ALL TRADES HAVE THE LATEST AND COMPLETE COORDINATION PROVIDED BY THE DESIGNER AND CONSULTANTS, AND THAT THE DOCUMENTATION IS CONSISTENT THROUGHOUT

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SITE PLAN cont. / CARPARK LAYOUT(existing)
SCALE 1:100

- LP LIGHT POLE
- B(R) EXISTING BOLLARD TO BE REMOVED
- B(N) NEW BOLLARD
- B(E) EXISTING BOLLARD
- B(N) dr NEW BOLLARD DROP DOWN BOLLARD

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DEVELOPMENT PLAN CONSENT APPLICATION
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BUFFALO RESTAURANT
ADELPHI TCE, GLENELG

FILE NO: ADI/GLG/BUFFALO17
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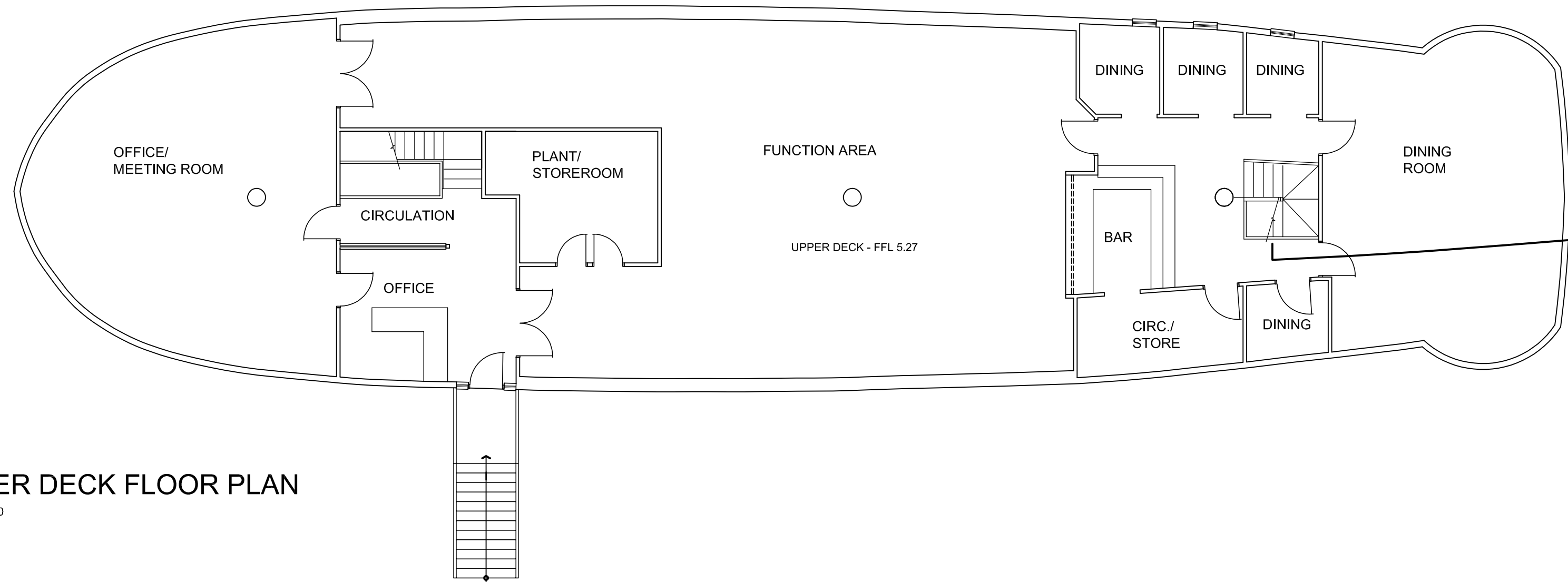
THE BUFFALO REDEVELOPMENT
PROPOSED 3D PERSPECTIVE IMAGES



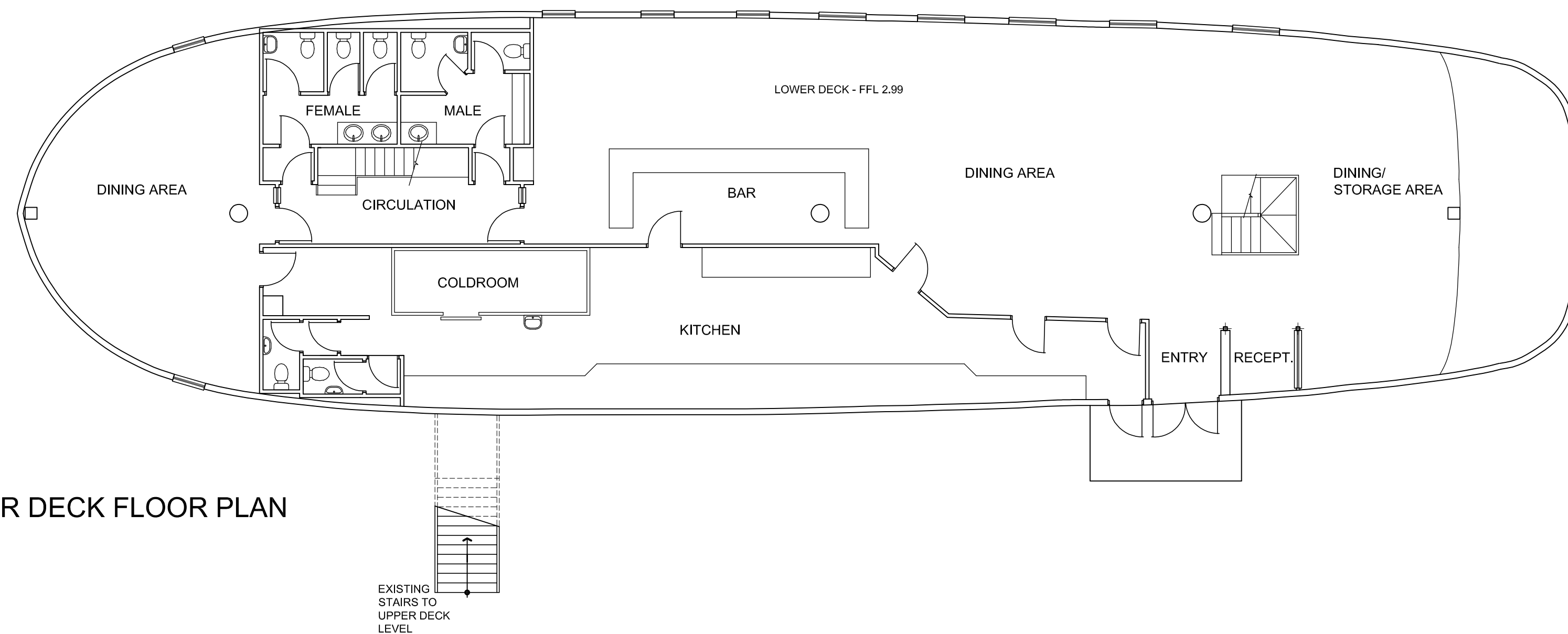
THE BUFFALO REDEVELOPMENT
PROPOSED 3D PERSPECTIVE IMAGES



THE BUFFALO REDEVELOPMENT
PROPOSED 3D PERSPECTIVE IMAGES

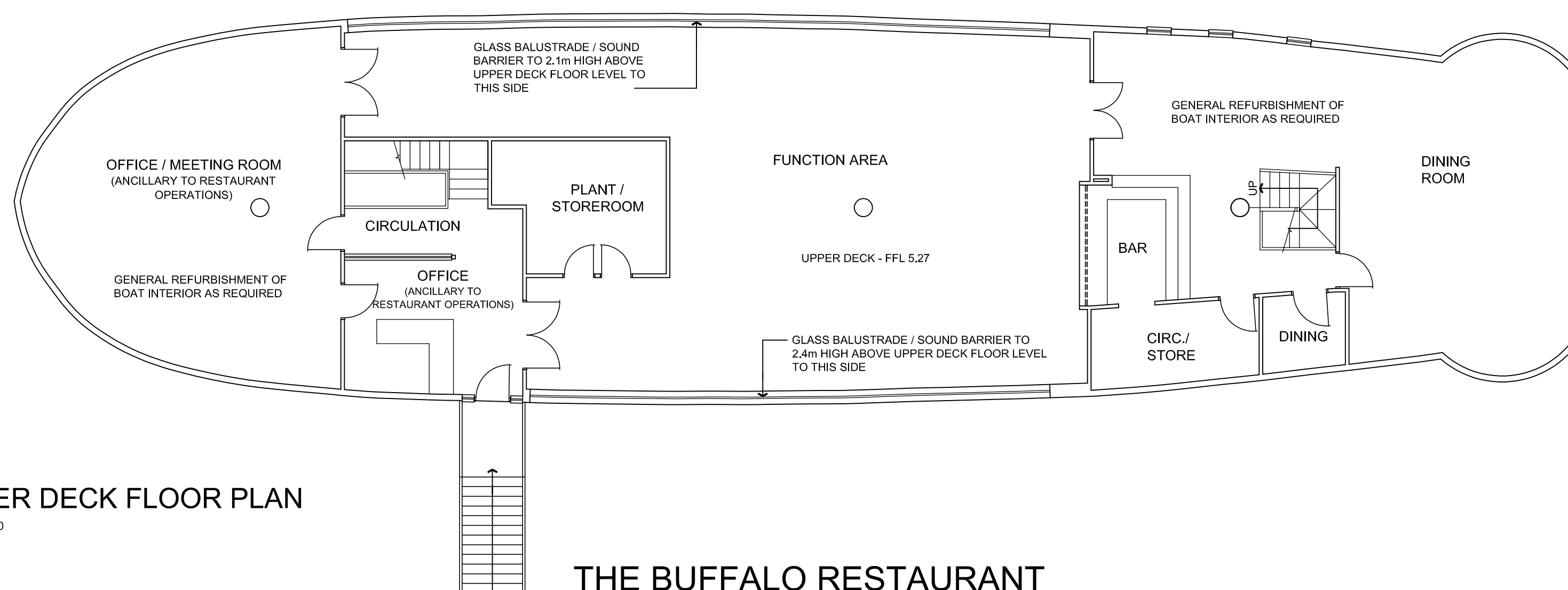


UPPER DECK FLOOR PLAN
SCALE 1:100



LOWER DECK FLOOR PLAN
SCALE 1:100

THE BUFFALO RESTAURANT - EXISTING -



UPPER DECK FLOOR PLAN
SCALE 1:100

THE BUFFALO RESTAURANT - PROPOSED -



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DEVELOPMENT PLAN CONSENT
APPLICATION
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PROPOSED RE-DEVELOPMENT
BUFFALO RESTAURANT
ADELPHI TCE, GLENELG

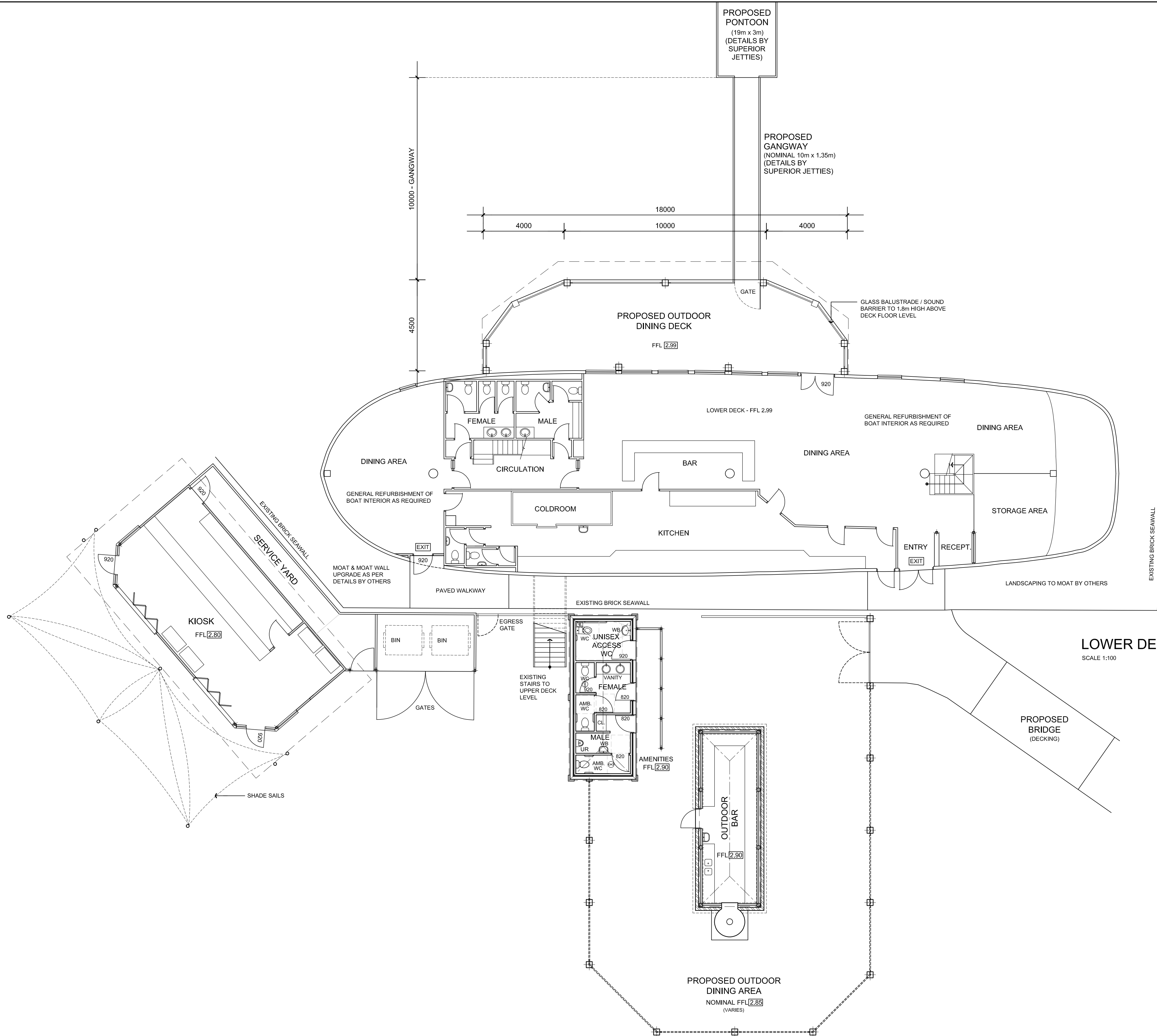
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THE BUFFALO RESTAURANT
- PROPOSED -

LOWER DECK FLOOR PLAN
SCALE 1:100

EXTERIOR COLOUR SCHEDULE - THE BUFFALO	
EXISTING BOAT	PAINTED "BLACK" WITH "WHITE" DETAILS (AS PER EXISTING)

EXTERIOR COLOUR SCHEDULE - OUTDOOR DINING AREA	
PERIMETER FENCE	RECYCLED TIMBER POSTS AND BEAMS (NATURAL CLEAR FINISH TO ALL TIMBER) CLEAR GLASS PANELS WITH STAINLESS STEEL SUPPORTS

EXTERIOR COLOUR SCHEDULE - AMENITIES	
ROOF	TRUE OAK STEEL 'STEALTH' FINISH (MATTE BLACK)
POSTS & BEAMS	RECYCLED TIMBER POSTS AND BEAMS (NATURAL CLEAR FINISH TO ALL TIMBER)
WALLS	NATURAL LOCALLY SOURCED STONE


EXTERIOR COLOUR SCHEDULE - OUTDOOR BAR	
ROOF	TRUE OAK STEEL 'STEALTH' FINISH (MATTE BLACK)
POSTS & BEAMS	RECYCLED TIMBER POSTS AND BEAMS (NATURAL CLEAR FINISH TO ALL TIMBER)
WALLS	NATURAL LOCALLY SOURCED STONE

EXTERIOR COLOUR SCHEDULE - KIOSK	
ROOF	COLORBOND STEEL 'BASALT'
WALLS	COLORBOND STEEL 'WINDSPRAY'
WINDOWS	COLORBOND STEEL 'WINDSPRAY'
POSTS	'WHITE'
SHADE SAILS	'WHITE'

EXTERIOR COLOUR SCHEDULE - OUTDOOR DINING DECK	
ROOF	TRUE OAK STEEL 'STEALTH' FINISH (MATTE BLACK)
POSTS & BEAMS	RECYCLED TIMBER POSTS AND BEAMS (NATURAL CLEAR FINISH TO ALL TIMBER)
BALUSTRADE	CLEAR GLASS PANELS WITH STAINLESS STEEL SUPPORTS

NORTH APPROX.



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DEVELOPMENT PLAN CONSENT APPLICATION
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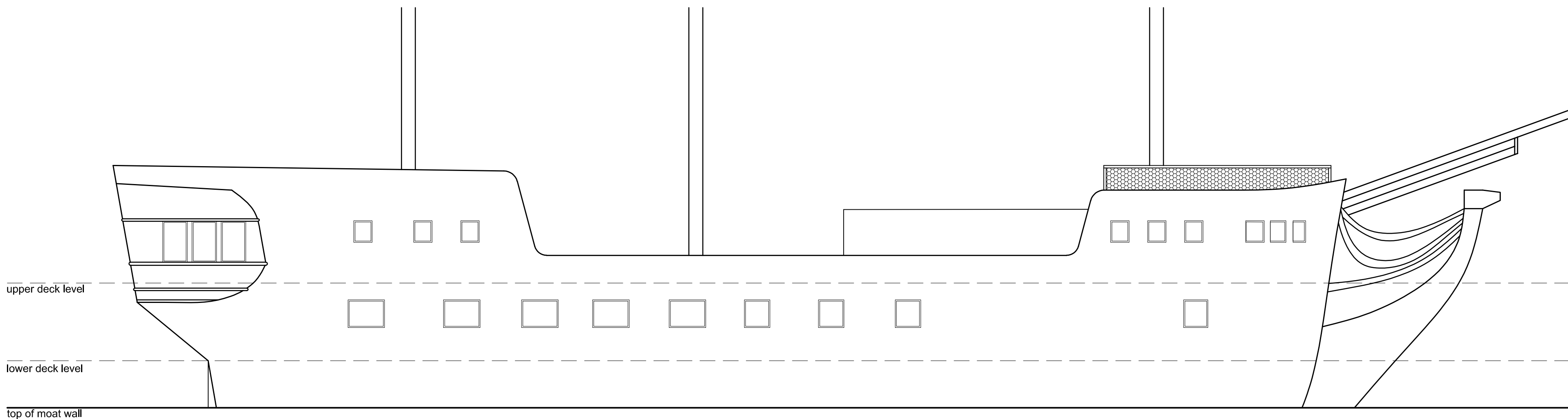
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BUFFALO RESTAURANT
ADELPHI TCE, GLENELG

FILE NO: ADI/GLG/BUFFALO17
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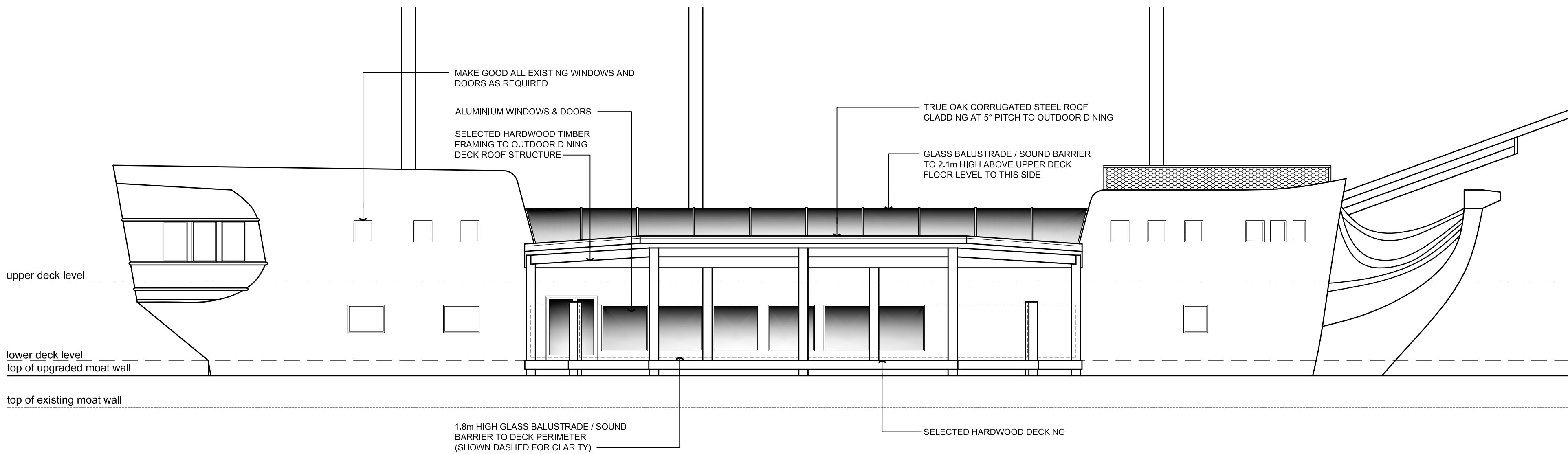
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THE BUFFALO RESTAURANT
- EXISTING NORTH-WEST ELEVATION -



THE BUFFALO RESTAURANT
- PROPOSED NORTH-WEST ELEVATION -



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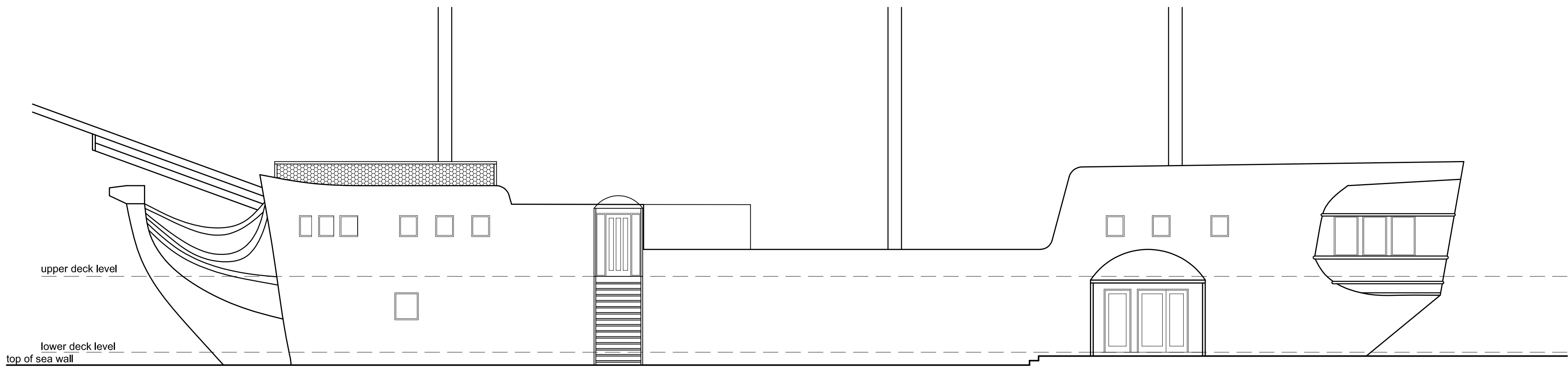
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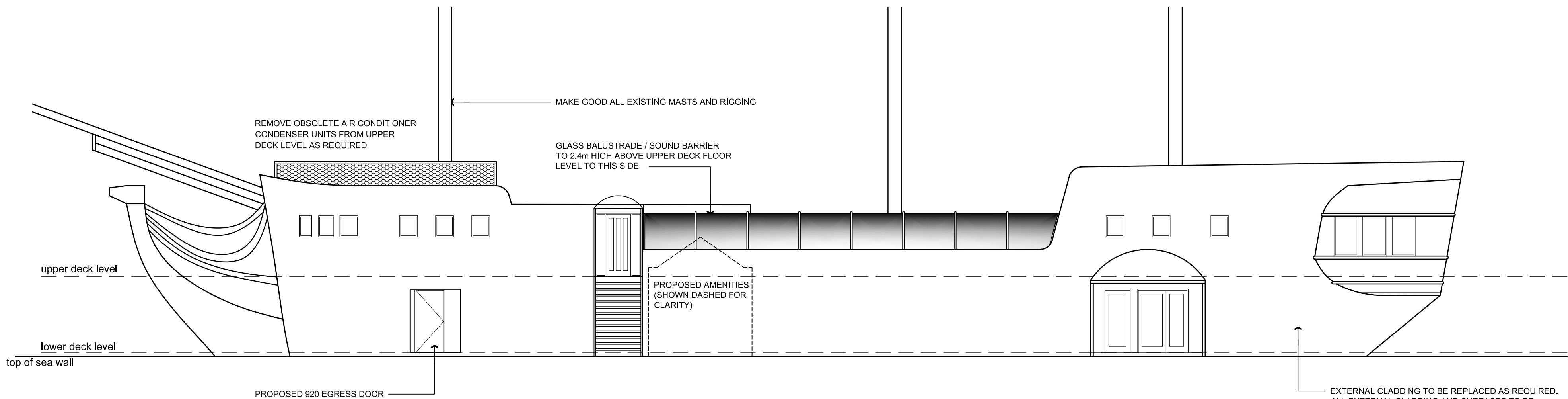
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THE BUFFALO RESTAURANT
- EXISTING SOUTH-EAST ELEVATION -



THE BUFFALO RESTAURANT
- PROPOSED SOUTH-EAST ELEVATION -



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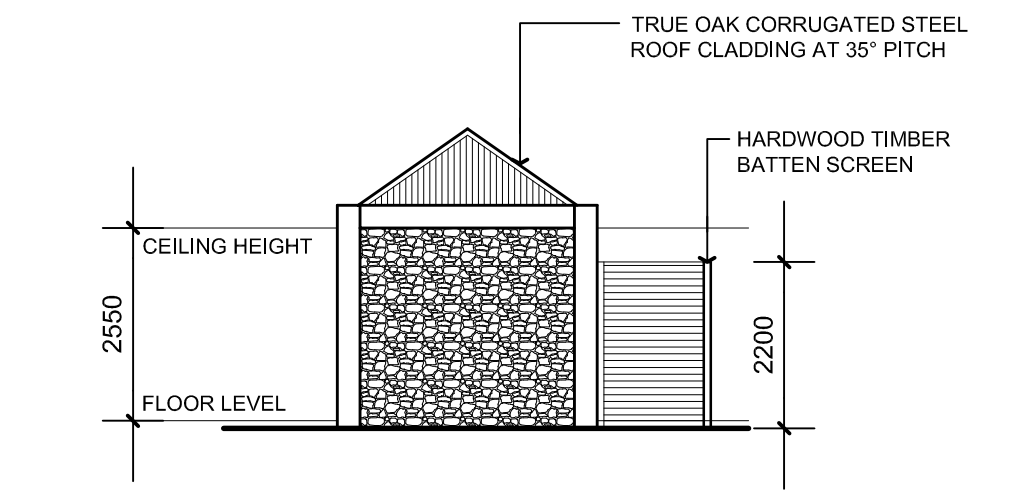
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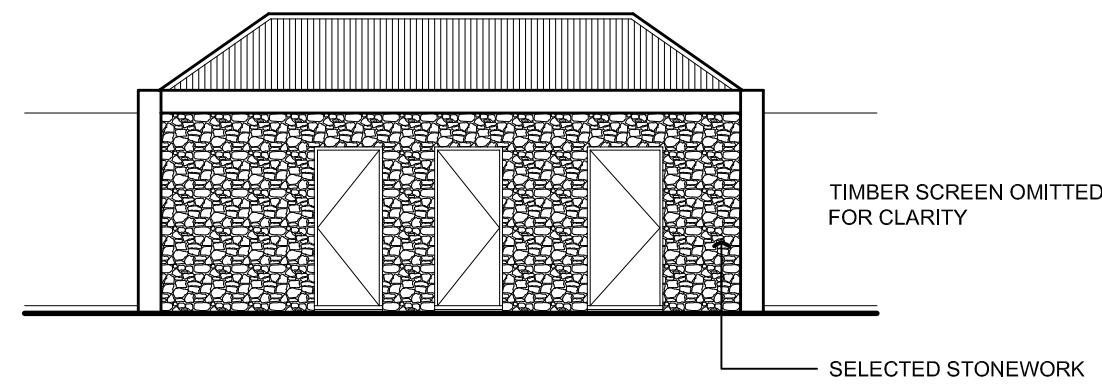
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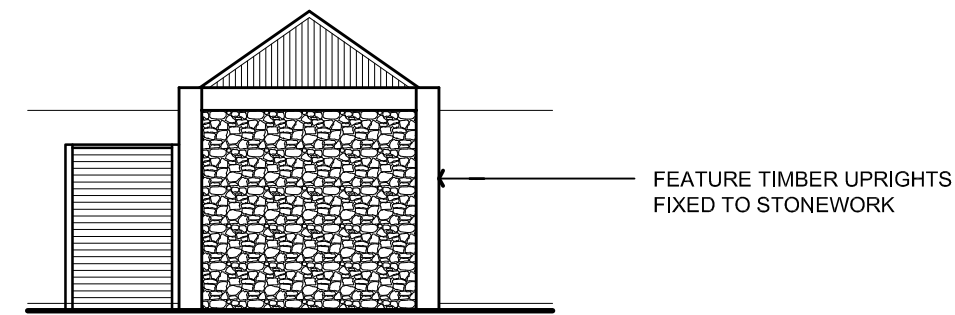




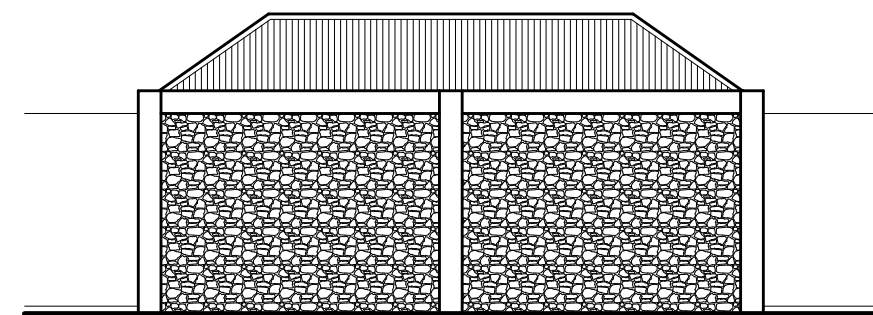
ELEVATION 1
SCALE 1:100



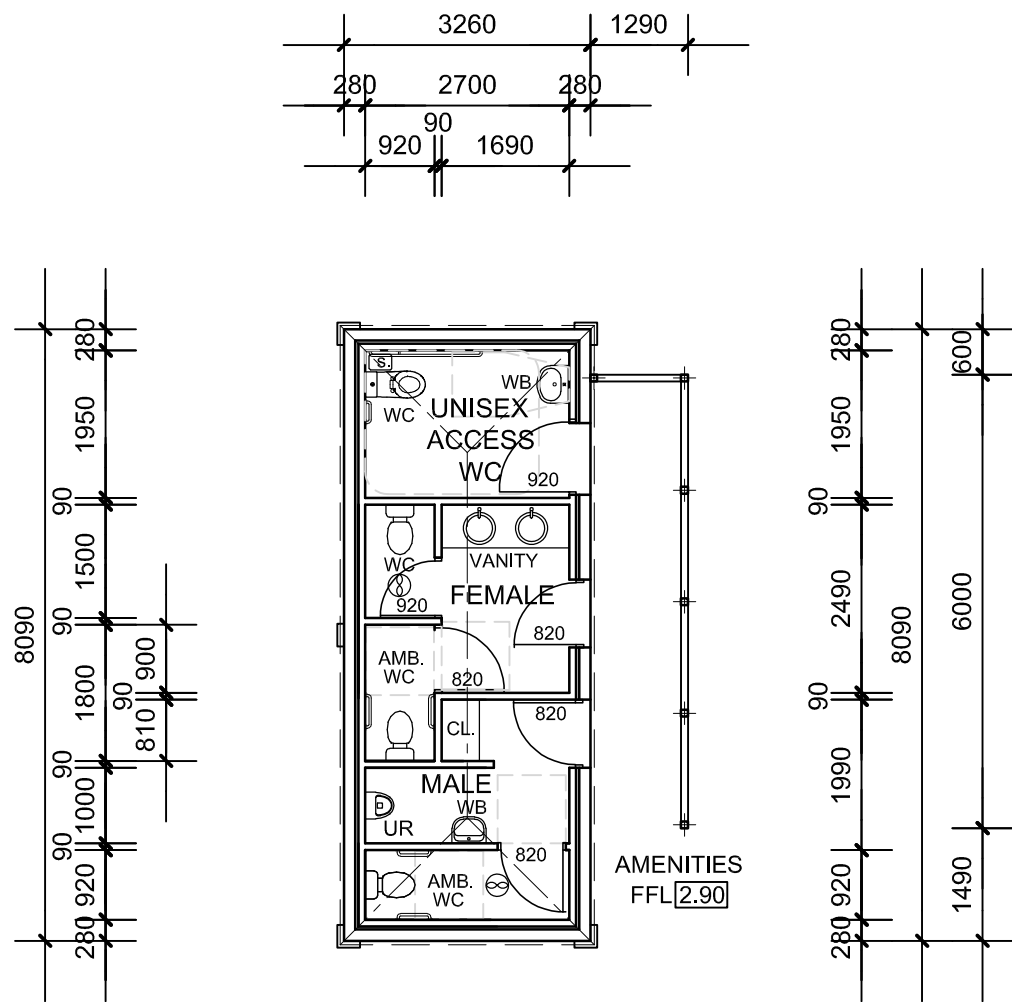
ELEVATION 2
SCALE 1:100



ELEVATION 3
SCALE 1:100



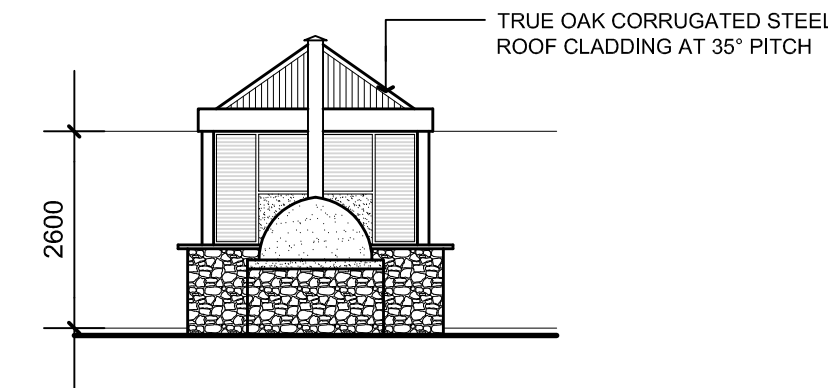
ELEVATION 4
SCALE 1:100



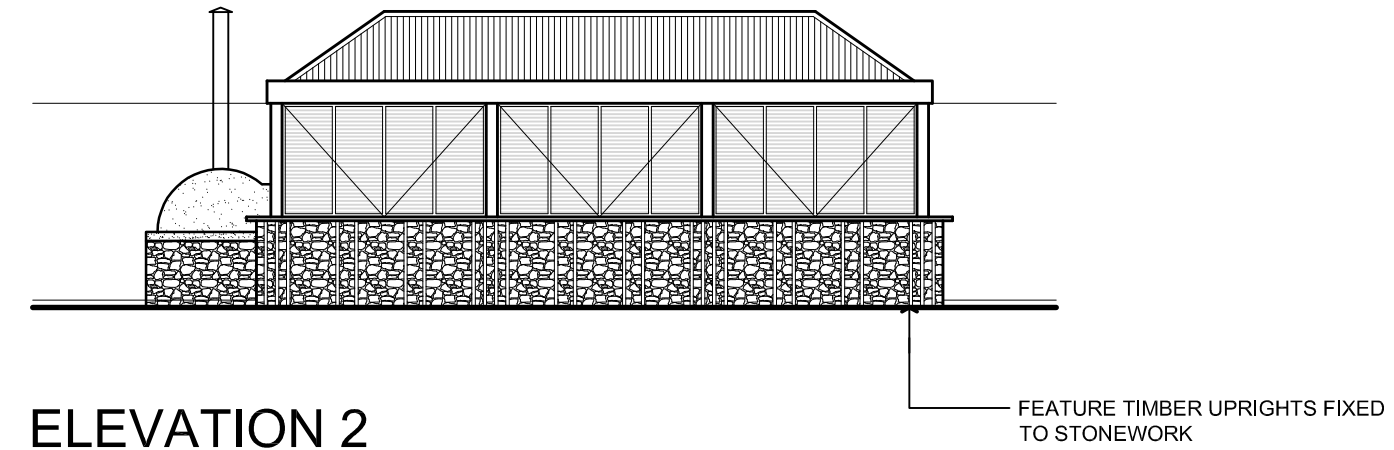
FLOOR PLAN
SCALE 1:100

PROPOSED AMENITIES

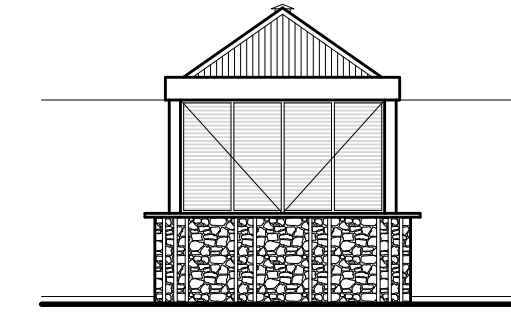
AMENDED - MC/SP 15.05.17



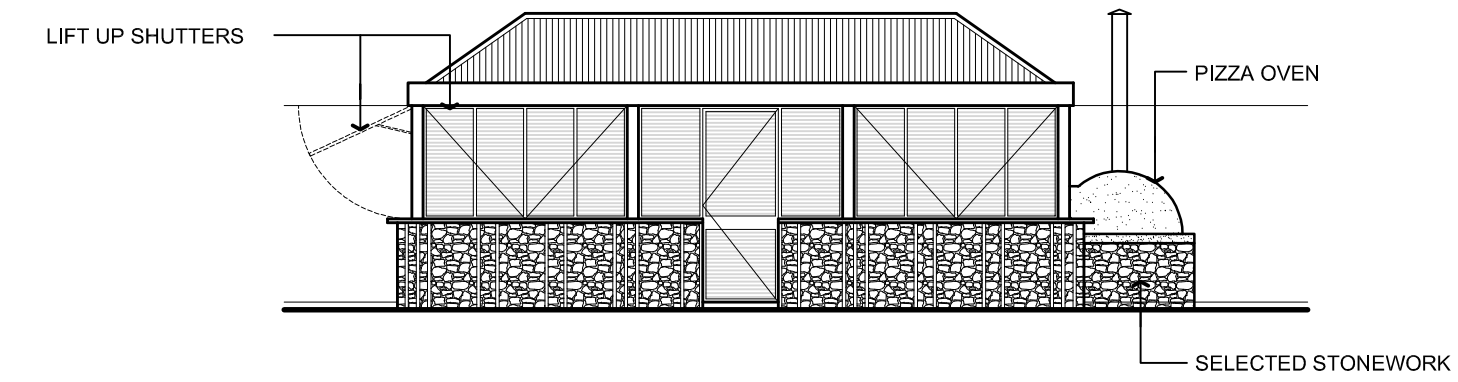
ELEVATION 1
SCALE 1:100



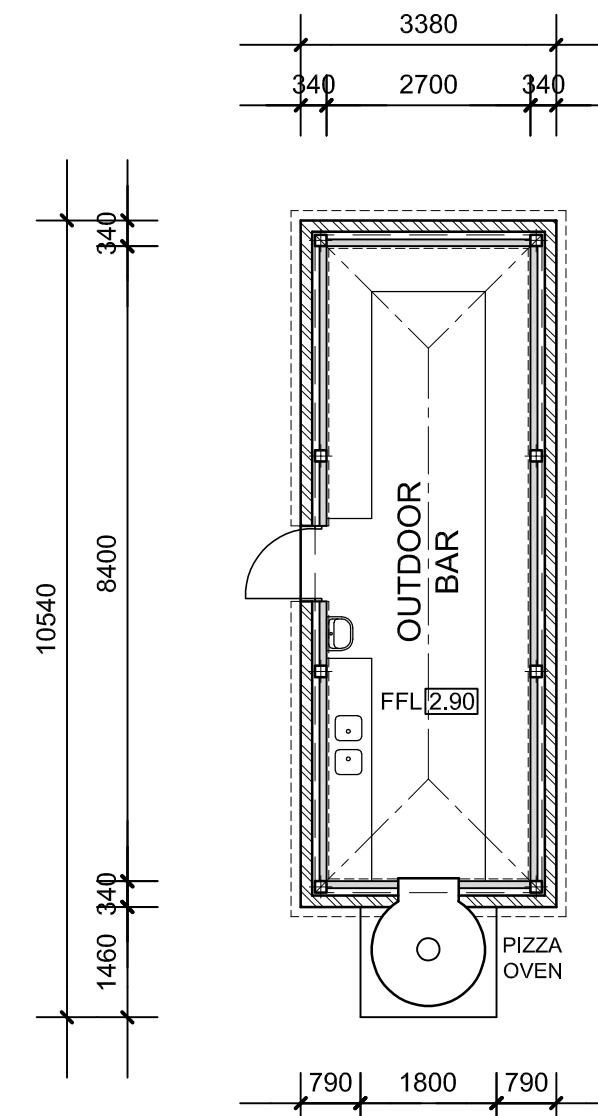
ELEVATION 2
SCALE 1:100



ELEVATION 3
SCALE 1:100

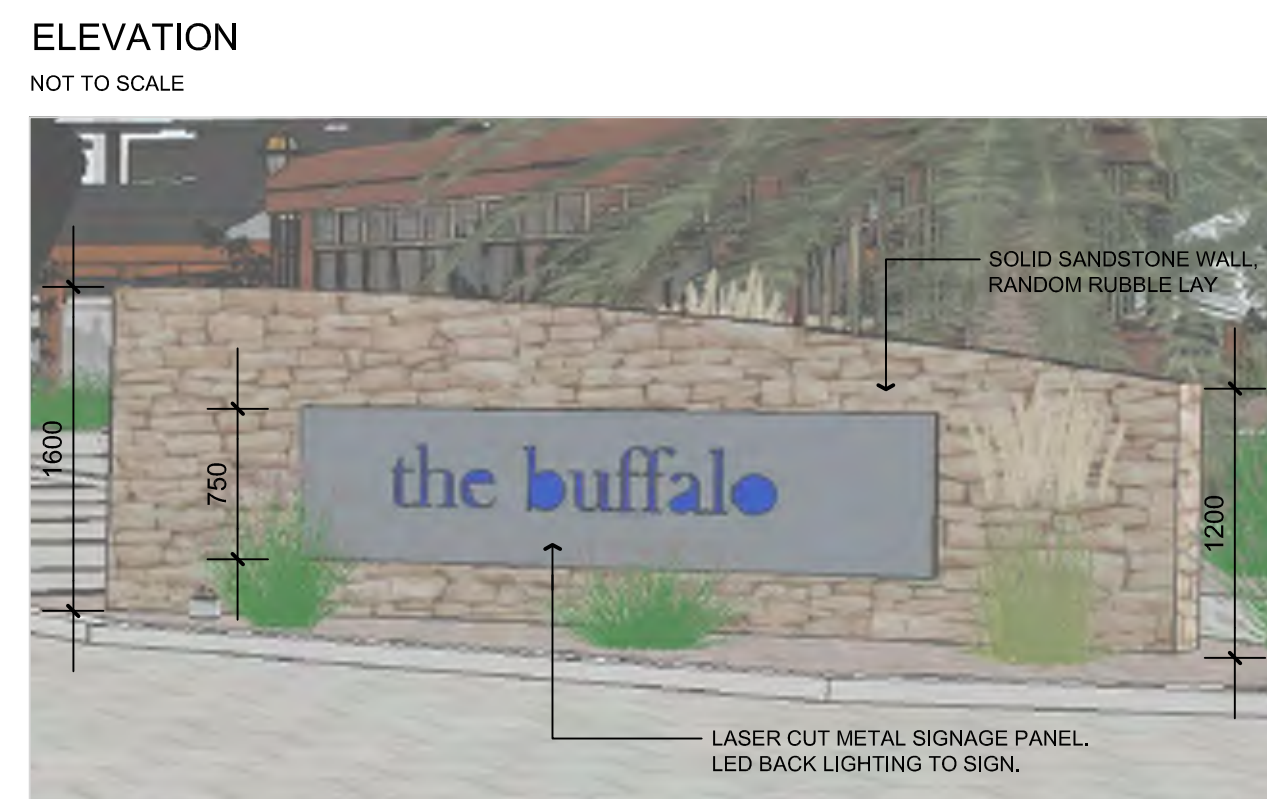


ELEVATION 4
SCALE 1:100

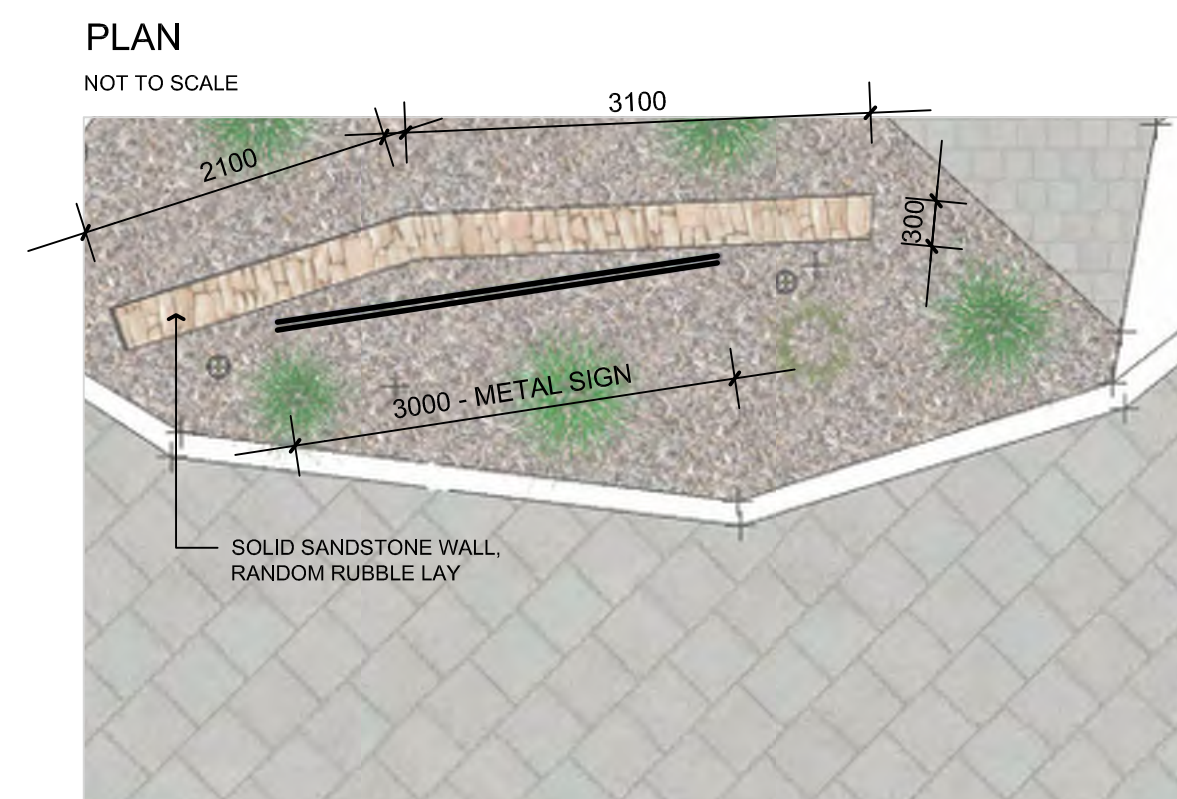


FLOOR PLAN
SCALE 1:100

PROPOSED OUTDOOR BAR



SIGNAGE DETAIL
CARPARK ENTRANCE
NOT TO SCALE



ANCHOR DETAIL (EXISTING ANCHOR)
NOT TO SCALE

AREAS (m²)	
AMENITIES -	23.47m²
OUTDOOR BAR -	35.62m²



DEVELOPMENT PLAN CONSENT APPLICATION
- NOT FOR CONSTRUCTION

PROPOSED RE-DEVELOPMENT
BUFFALO RESTAURANT
ADELPHI TCE, GLENELG

FILE NO: ADI/GLG/BUFFALO17
DATE: JULY 2017
DRAWN BY: MC / SP
SHEET: 11 OF 12
SCALE: AS SHOWN
ORIGINAL SHEET SIZE: A1

ALL WORKS SHALL COMPLY WITH S.A. DEVELOPMENT ACT AND REGULATIONS BUILDING CODE OF AUSTRALIA RELEVANT AUSTRALIAN STANDARDS OF CURRENT ISSUE AND ALL STATUTORY AUTHORITY REQUIREMENTS CONTRACTORS SHALL VERIFY ALL DIMENSIONS, LEVELS AND DETAILS ON SITE BEFORE COMMENCING ANY WORK, OR PREPARATION OF SHOP DRAWINGS WRITTEN DIMENSIONS TAKE PRECEDENCE OVER SCALE ANY DISCREPANCIES OF SIGNATURE TO BE REPORTED TO THE DESIGNER OR SUPERVISOR IMMEDIATELY & RESOLVED BEFORE PROCEEDING FURTHER WITH THE WORKS ANY BUILDER/CONTRACTOR/PREPARATION FEE ESTIMATES FOR THE PROPOSED WORKS SHALL VISIT THE SITE AND, IN CONJUNCTION WITH THE OWNER, CONFIRM THE NATURE AND EXTENT OF ALL WORKS THE BUILDER IS TO ENSURE THAT ALL TRADERS HAVE THE LATEST AND COMPLETE DOCUMENTATION PROVIDED BY THE DESIGNER AND CONSULTANTS, AND THAT THE DOCUMENTATION IS CONSISTENT THROUGHOUT

MEMBER OF BUILDING DESIGNERS ASSOCIATION OF SA INC.





AREAS (m ²)	
PROPOSED KIOSK	70m ²
SERVICE YARD	36.18m ²

NORTH APPROX.



PO BOX 455
WILLUNGA S.A. 5172
T: 08 8557 1968
E: design@stuartpatrick.com.au

DEVELOPMENT PLAN CONSENT
APPLICATION
- NOT FOR CONSTRUCTION

PROPOSED RE-DEVELOPMENT
BUFFALO RESTAURANT
ADELPHI TCE, GLENELG

FILE NO: ADI/GLG/BUFFALO17
DATE: JULY 2017
DRAWN BY: MC / SP
SHEET: 12 OF 12
SCALE: AS SHOWN
ORIGINAL SHEET SIZE: A1

ALL WORKS SHALL COMPLY WITH S.A. DEVELOPMENT ACT AND REGULATIONS; BUILDING CODE OF AUSTRALIA, RELEVANT AUSTRALIAN STANDARDS (OF CURRENT ISSUE) AND ALL STATUTORY AUTHORITY REQUIREMENTS. CONTRACTORS SHALL VERIFY ALL DIMENSIONS, LEVELS AND DETAILS ON SITE BEFORE COMMENCING ANY WORK, OR PREPARATION OF SHOP DRAWINGS. WRITTEN DIMENSIONS TAKE PRECEDENCE OVER SCALE. ANY DISCREPANCIES OF SIGNIFICANCE TO BE REPORTED TO THE DESIGNER OR SUPERVISOR IMMEDIATELY & RECTIFIED BEFORE PROCEEDING FURTHER WITH THE WORK.

THE BUILDER IS TO ENSURE THAT ALL TRADES HAVE THE LATEST AND COMPLETE DOCUMENTATION PROVIDED BY THE DESIGNER AND CONSULTANTS, AND THAT THE DOCUMENTATION IS CONSISTENT THROUGHOUT.

MEMBER OF BUILDING DESIGNERS
ASSOCIATION OF SA INC.



Tom Gregory
Ben Green & Associates
53a Broadway,
Glenelg South, SA

3 July 2017
Job No. 842

Dear Tom,

**RE: HMS Buffalo Redevelopment, Glenelg
DAC Request for Further Information – Engineering Response**

In response to the DAC request for further information, as per minutes of meeting 19 January 2017, please find the following response to the relevant structural and civil engineering queries;

Supply the following information: details around the resolution of the moat - landscaping, water quality.

Moat

Required Specifications

- Limit earthworks and removal of any significant depth of the residual soils and sediments.
- Provide a working platform for temporary works, scaffolds and the like to facilitate repair and/or replacement of the external cladding during construction.
- Provide a foundation of sufficient rigidity to provide stability to landscaping either side of the deck and around the full perimeter of the boat. Prevent any differential settlements.
- Provide a substrate to elevate landscaping to above king tide level and the 100 year ARI flood level.
- Protect the boat from tides and protection against minor storm surges such that the boat remains fully serviceable (note, tides controlled by entrance gates, not subject to ocean tides).
- Provide an environment to maximise the life of existing timber piles and boat superstructure. Prevent water accumulating under boat. Moat infill to be independent to boat and new deck structure to allow for settlement of moat infill.

Design Measures (refer drawing 842-003, attached)

- Thickened raft slab (glass fibre reinforced) to be placed on existing surface level.
- Increase height of existing moat bund/sleeper wall to RL 2.500 AHD as per concourse level and above 100 year ARI and king tide level (2.36 AHD). Refer O'Donnell Griffith 2003 flood level survey.
- Porous fill (no fines concrete, coarse rubble or similar granular fill) to underside of landscape zone, free draining back to lake.
- Reinforced concrete wall adjacent boat to support fill and prevent ingress of water under boat and around timber piles. Sump and pumps to remove any seepage or upwelling.

Water Quality

The stormwater system design (as noted in recommended condition 9 of the previous meeting agenda) shall be in accordance with relevant *Australian Standards, the EPA Code of Practice, Local Government's WSUD Urban Design Manual* and recognised engineering best practices. A detailed stormwater management plan will be supplied to Council to satisfy their "Statement of Requirements" and development consent conditions during the normal building rules approval process.

Typically, water quality is addressed via the following;

- Removal of gross pollutants;
- Removal of suspended solids;
- Removal of oil & grease;
- Reduction in nutrient loading of total phosphorous and total nitrogen to recommended levels.

We note that all surface runoff from Adelphi Tce side entry pits, existing carpark and concourse/footpaths drains directly into the Patawalonga Lake with no screening or treatment. The existing open deck areas of the boat drain directly into the moat. The moat currently forms an inaccessible trap for a great deal of rubbish.

It is proposed to connect the existing outlets from the upper boat deck to the in-ground (treated) stormwater system. The acoustic screens will prevent patrons dropping waste directly into the Lake. Paved areas will drain to sumps connected to treatment devices. The landscape architect has provided plant selection that doesn't require the use of heavy watering, herbicides and fertilisers. Additionally, landscaping is to be located in planters to minimise leaching and provide bioretention of any such herbicides and nutrients. A regular maintenance regime will be programmed to ensure the system operates as designed. These measures will substantially improve the quality of pre-development discharges to that required.

A sediment control plan will be implemented before any site clearing is performed and during construction.

Yours Sincerely,



Angus Russell
CPEng MIEAust

Attachments: Moat Infill Plan & Section (Rev P2)



MARSHALL DAY
Acoustics 

BUFFALO REDEVELOPMENT
NOISE ASSESSMENT

Rp 001 R02 2016306AL | 14 October 2016

Project: **BUFFALO RESTAURANT REDEVELOPMENT**

Prepared for: **DeYoungs**
PO Box 104
O'Sullivans Beach SA 5166
Australia

Attention: **Mr Troy DeYoung**

Report No.: **Rp 001 R02 2016306AL**

Disclaimer

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Document Control

Status:	Rev:	Comments	Date:	Author:	Reviewer:
final	-	Issued	5/8/2016	A. Morabito	P. Heinze
final	R01	Minor amendments	23/9/2016	A. Morabito	P. Heinze
final	R02	Revised following DAC review	14/10/2016	A. Morabito	P. Heinze

TABLE OF CONTENTS

1.0	INTRODUCTION	1
2.0	SITE DESCRIPTION	1
2.1	Location	1
2.2	Proposed Operations	2
2.2.1	Outdoor alfresco dining.....	2
2.2.2	Outdoor deck	2
2.2.3	Outdoor function space.....	2
2.3	Planning Considerations	3
3.0	ASSESSMENT CRITERIA	4
3.1	Music Noise	4
3.1.1	Derived music noise criteria	4
3.2	Patron Noise	5
4.0	RECOMMENDATIONS	6
5.0	MUSIC NOISE ASSESSMENT	7
5.1	Music Noise Levels	7
5.1.1	Background music.....	7
5.1.2	Acoustic music	7
6.0	PATRON NOISE ASSESSMENT	8
6.1	Patron Noise Data	8
6.2	Predicted Patron Noise Levels	9
7.0	SUMMARY	11

APPENDIX A GLOSSARY OF TERMINOLOGY

APPENDIX B CITY OF HOLDFAST BAY DEVELOPMENT PLAN

APPENDIX C ACOUSTIC TREATMENT DETAILS

1.0 INTRODUCTION

The Buffalo ship, moored in the Patawalonga River in Glenelg's Wigley Reserve previously operated for many years as a restaurant. In 2015, the former owner sold the replica ship to the DeYoung Group.

The DeYoung Group now propose to redevelop the Buffalo, and include an outdoor alfresco dining area, bar/deck, kiosk, and upper deck function space.

Marshall Day Acoustics Pty Ltd (MDA) has been commissioned by The DeYoung Group to undertake a noise assessment of the proposed redevelopment.

The following report details the proposal, locality, relevant noise criteria and an assessment of noise to the nearest residences.

Acoustic terminology used throughout the report is provided in 0.

2.0 SITE DESCRIPTION

2.1 Location

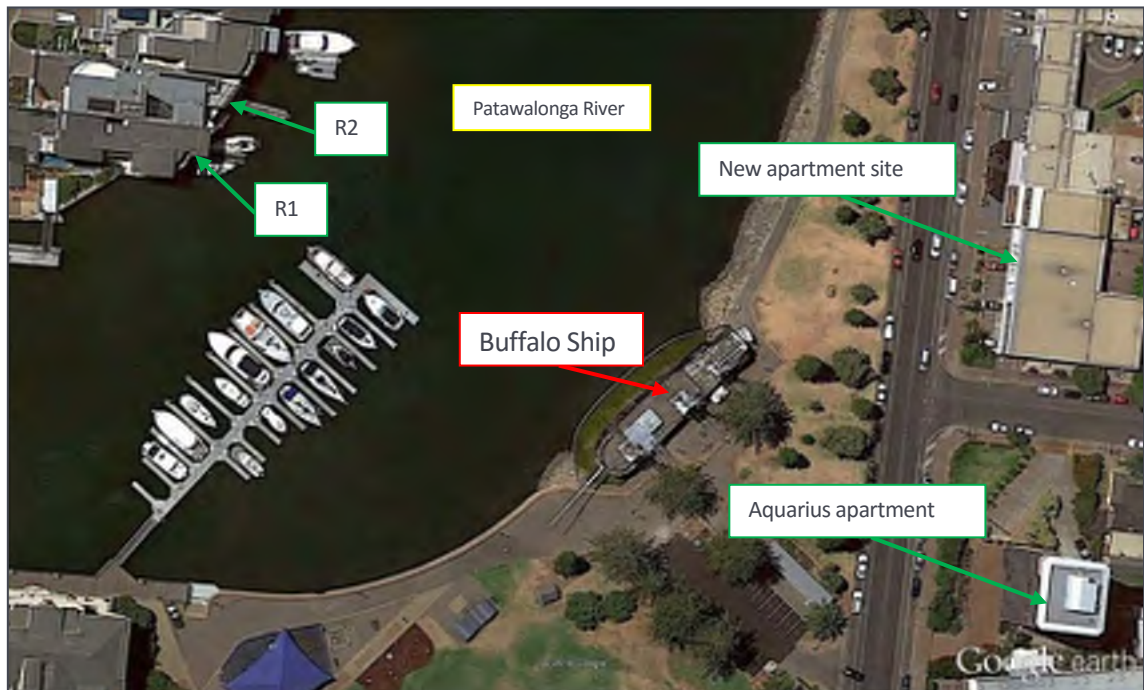
The Buffalo ship is located adjacent Glenelg's Wigley Reserve, at 1 Adelphi Terrace, Glenelg, shown in Figure 1. The site is bounded by the following:

- Wigley Reserve to the east and south. Adelphi Terrace is on the east side of Wigley Reserve, with existing apartments and a new approved hotel development on the east side of Adelphi Terrace
- Patawalonga River to the west and north. Existing residential properties are located on the west side of Patawalonga River

The nearest noise affected premises to the Buffalo ship are identified as follows:

- Existing Aquarius apartments on the eastern side of Adelphi Terrace, approximately 80 m east south east of the Buffalo
- Approved 12 storey apartment complex to be built on the eastern side of Adelphi Terrace, approximately 70 m east north east of the Buffalo
- Existing 3 storey residential properties on the western side of the Patawalonga River, approximately 110 m north west of the Buffalo

Figure 1: Buffalo Ship location (Google Earth)



2.2 Proposed Operations

The Buffalo redevelopment site plan is provided in Figure 2. The current License for the premise, limits the overall capacity to not exceed 250 persons at any one time.

In terms of external late night operations, with the potential to cause noise, the following areas and operations are proposed.

2.2.1 Outdoor alfresco dining

The outdoor alfresco dining area located on south east side of the Buffalo Ship, is proposed to operate until 2am, 7 days a week, and cater for up to 100 patrons.

Music in this dining area will be limited to background music only and 2 piece live acoustic music, including guitar and a singer.

2.2.2 Outdoor deck

The outdoor deck located on west side of the Buffalo Ship, is proposed to operate until midnight, and cater for up to 70 patrons, 7 days a week.

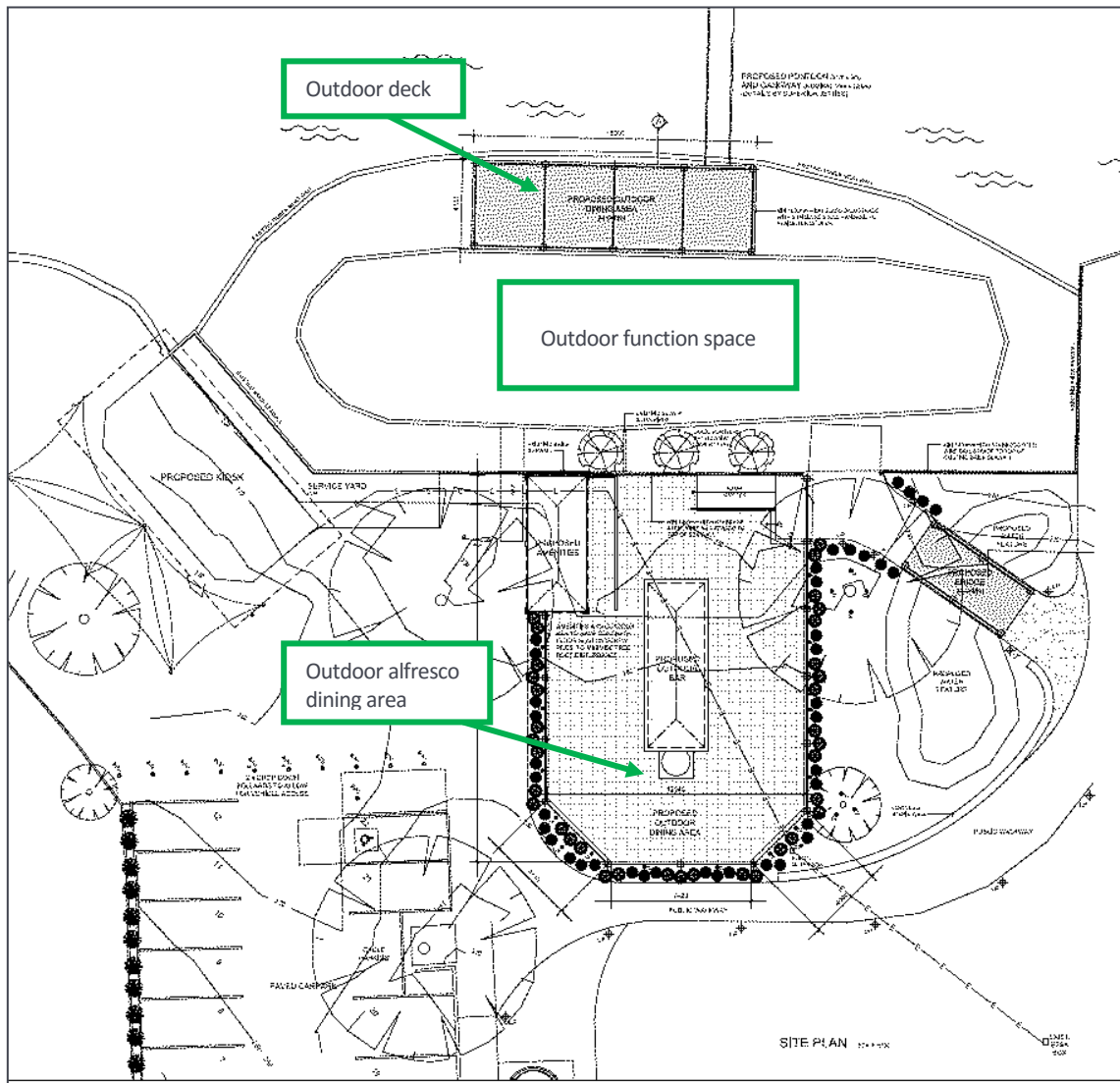
Music on the deck will be limited to background music only.

2.2.3 Outdoor function space

The outdoor function space is located on the upper deck of the Buffalo Ship is proposed to operate until midnight, 7 days a week for events such as corporate functions and weddings. The current license for the upper deck including the internal and outdoor areas is for 150 people. The outdoor function space can cater for up to 80 patrons.

Music in this outdoor space will be limited to background music only and 2 piece live acoustic music, including guitar and a singer.

Figure 2: Buffalo redevelopment site plan



2.3 Planning Considerations

The Buffalo Ship and surrounds are located within the City of Holdfast Bay.

With reference to the City of Holdfast Bay Development Plan consolidated 2 June 2016, the Buffalo site is within the Glenelg Foreshore and Patawalonga (GFP) Zone.

Residents on the east side of Adelphi Terrace are within the Residential Character (RC) Zone, and residents on the west side of the Patawalonga River are within the Residential High Density (RHD) Zone, both specifically within 'Policy Area 15 Urban Glenelg'.

The land use zoning map applicable to the site is provided in Appendix B.

The site land use category is for the purpose of this assessment is 'commercial' while the land use category for the nearest noise-affected premises to the east and west of the site is of a 'residential' nature.

3.0 ASSESSMENT CRITERIA

3.1 Music Noise

The City of Holdfast Bay Development Plan details applicable noise criteria in the form of the following principle of development control for venues proposing music noise:

10 Development proposing music should include noise attenuation measures that achieve the following desired noise levels:

Noise level assessment location	Desired noise level
Adjacent existing noise sensitive development property boundary	Less than 8 dB above the level of background noise ($L_{90,15min}$) in any octave band of the sound spectrum and Less than 5 dB(A) above the level of background noise ($L_{A90,15min}$) for the overall (sum of all octave bands) A-weighted level
Adjacent land property boundary	Less than 65dB(Lin) at 63Hz and 70dB(Lin) in all other octave bands of the sound spectrum or Less than 8 dB above the level of background noise ($L_{90,15min}$) in any octave band of the sound spectrum and 5 dB(A) overall (sum of all octave bands) A-weighted level

The “desired” music noise level is not explicitly defined in the Development Plan. The above principle of development control is however consistent with music noise criteria generally used in South Australia. Music noise in South Australia is typically assessed with reference to the L_{10} level, the noise level exceeded for 10% of the measurement period. Accordingly, we expect the “desired noise level” referred in the above principle of development control relates to the L_{10} noise metric.

3.1.1 Derived music noise criteria

In order to derive applicable music noise criteria, in accordance with the City of Holdfast Bay principles of development control, the background noise at the noise affected premise must be established.

Long term noise measurements of the background noise were undertaken at the north end of the Buffalo site, representative of background noise levels at the nearest identified noise affected premises. Measurements were conducted between Friday 15 July 2016 and Thursday 21 July 2016, using a 01dB Duo Environmental Noise Logger (serial number 10299), calibrated before and after the measurement set using a Brüel & Kjær Type 4230 sound level calibrator (serial number 831147).

The lowest measured background noise levels during the proposed hours of operations, detailed in Table 1, have been used to derive relevant music noise criteria, detailed in Table 2.

Table 1: Measured background noise levels, dB L_{OCT90}

Background noise level	dBA	Octave Band Centre Frequency (Hz)						
		63	125	250	500	1k	2k	4k
Sun-Thu, to midnight	41	48	46	39	37	37	32	21
Sun-Thu, midnight to 2am	38	46	41	37	36	33	28	19
Fri-Sat, to midnight	49	52	47	44	44	46	40	24
Fri-Sat, midnight to 2am	44	47	44	41	41	40	34	20

Table 2: Derived music noise criteria, dB L_{OCT10}

Music noise criteria	dBA	Octave Band Centre Frequency (Hz)						
		63	125	250	500	1k	2k	4k
Sun-Thu, to midnight	46	56	54	47	45	45	40	29
Sun-Thu, midnight to 2am	43	54	49	45	44	41	36	27
Fri-Sat, to midnight	54	60	55	52	52	54	48	32
Fri-Sat, midnight to 2am	49	55	52	49	49	48	42	28

3.2 Patron Noise

Noise from patrons outdoors is not covered under any Environment Protection Policy or South Australian guideline. In the absence of an established policy, reference has been made to design criteria that have been adopted for a number of similar projects.

The assessment criteria have been developed with consideration of the *Environment Protection (Noise) Policy 2007* (EPP) as well as the existing background noise level in the area.

The EPP is a broad document designed to protect the amenity of residential or business areas from noise sources including mechanical plant, industry, construction activity etc, however it specifically excludes noise from licensed premises and patrons in general.

In June 2009, The SA EPA issued *Guidelines for the use of the Environment Protection (Noise) Policy 2007*, which states the following:

Noise from licensed premises is excluded from the objective assessment procedure of the Noise EPP to provide the OLGC with the flexibility to consider the range of factors under section 106 [of the Liquor Licensing Act 1997]. Notwithstanding this exclusion, comparison with the indicative noise levels and consideration of the factors of clause 19 of the Noise EPP may assist the Commissioner or Licensing Court in situations where an objective procedure is considered useful.

We therefore consider the above approach appropriate for the assessment of noise from the proposed outdoor patron areas.

The EPP separates the day into two different time periods – day (0700 – 2200 h) and night (2200 – 0700 h). Indicative noise levels are provided based on the land use category and zoning for the noise source and noise affected premise.

The relevant indicative noise level minus 5 dB for this site during the night period is 45 dB L_{Aeq}. This level has been adopted as the design criterion level when assessing patron noise at the nearest noise affected premises. The indicative noise level may however, be relaxed where it can be demonstrated that the background noise level is higher than the indicative noise levels. Accordingly, on Friday and Saturday nights until midnight, a criterion of 49 dB L_{Aeq} is deemed appropriate.

4.0 RECOMMENDATIONS

Preliminary noise predictions without acoustic treatment or other controls indicated exceedance of the recommended noise criteria. Accordingly, the music and patron noise assessment in Section 5.0 and Section 6.0 respectively have been based on the following recommendations (refer Appendix C for details).

- Background music and 2 piece live acoustic music, including guitar and a singer only, occurs at the site. No other live or DJ type music is played
- The outdoor alfresco dining area is 'sunk' into the ground approximately 500 mm. A minimum balustrade 1.8 m is to be provided to north, south and east ends.
- The western outdoor deck is shielded to residences on the western side of the Patawalonga River by the construction of an acoustically solid bar on west side of the deck, and with solid balustrade barrier to the north and south ends. The minimum height of the effective '3 sided barrier' required is 1.8 m above the outdoor deck. We note an access gate is required to the pontoon, this should be located at the northern or southern end of the western outdoor deck
- Provide a minimum 2.1 m high barrier and 2.4 m high barrier above the ship upper deck line to north and south ends respectively of the upper function space. The 2.1 m north end barrier to be acoustically lined with 50 mm thick, 32 kg/m² acoustic insulation lined with a fabric or perforated facing with a 25 % open area.

The above mentioned barriers, shall be constructed using material with a minimum surface density of 10 kg/m² -15 kg/m² and be free of gaps and penetrations. Suitable materials include 9 mm thick fibre cement sheet, 12 mm thick Perspex, 6 mm thick toughened glass or any other material that meets the minimum surface density requirement.

Recommendations for noise control were previously provided in MDA Rp 001 R01 2016306AL, issued 23 September 2016 and demonstrated compliance with the nominated criteria could be achieved. However, following review by the Development Assessment Commission (DAC), items were raised over visual amenity impacts. The above recommendations therefore incorporate DAC's review and feedback, noting a balance between visual and acoustic amenity is to be achieved.

5.0 MUSIC NOISE ASSESSMENT

5.1 Music Noise Levels

5.1.1 Background music

Background music only is proposed at the site. Background music noise can be defined as a level that enables patrons to conduct a normal conversation at a distance of 600 mm without having to raise their voices to a substantial degree. In MDA's opinion, this is equivalent to a music noise level of approximately 68 dB L_{Aeq} , with example spectra as shown in Table 3.

Table 3: Typical background music noise levels, dB

	dBA	Octave Band Centre Frequency (Hz)						
		63	125	250	500	1k	2k	4k
Background music level, L_{eq}	68	66	66	66	61	61	61	61
Background music level, L_{10} *	71	69	69	69	64	64	64	64

*Note: L_{10} based on $L_{10} = L_{eq} + 3$

The predicted noise levels indicate that background music will achieve the nominated criteria without additional treatment other than limiting music to background only or to a level of 71 dB L_{A10} when measured at 3 m from a speaker.

We also recommend the following with respect to outdoor speaker placement:

- Speakers in the outdoor alfresco dining area be placed on the eastern wall facing away from the Adelphi Terrace apartments
- Speakers on the outdoor deck be placed on the western wall facing away from residents located on the opposite side of the Patawalonga River
- Function space to be limited to a single speaker located at the north or south end of the area

5.1.2 Acoustic music

The proposal redevelopment seeks live music in the form of a duo, including an acoustic guitar and a singer in the upper deck function space or outdoor alfresco dining area. Previous noise level measurements by MDA of similar music indicate a noise level of 71 dB L_{Aeq} at 5 m. Based on this level, and limiting live music to acoustic guitar and a singer only, compliance with the music noise criteria can be readily achieved.

6.0 PATRON NOISE ASSESSMENT

6.1 Patron Noise Data

Marshall Day Acoustics and other acoustic consultants in Melbourne have measured patron noise from several different venues. These measurements indicate a large variation in the noise levels of crowds, due to a number of factors including the situational context of the crowd, and the intensity and character depending upon the number of patrons and their behaviour.

For the purpose of predicting noise levels from the Buffalo redevelopment, external patron areas have been categorised according to the envisaged use, as outlined in Table 4. Reference sound power data for one person is detailed in the 2011 Hayne paper¹.

Table 4: Patron area use categories

Area use category	Reference sound power data per one person		Area use definition
	Equivalent Level	Maximum Level	
Restaurant dining	78 dB L _{AW}	98 dB L _{AW}	Outdoor alfresco dining area and upper deck function space
Taverns with food offerings	83 dB L _{AW}	104 dB L _{AW}	Outdoor deck

Based on the above reference sound power data and measurements by Marshall Day Acoustics, a simplified empirical relationship to represent the total sound power level for which crowd numbers and character were varied has been derived for determining design equivalent and maximum sound power level as follows:

- Design equivalent sound power level - derived by assuming one third of the total crowd speaks continuously over the duration of the assessment period, and each of these speakers emit a constant total sound power level over the duration of the assessment period. In practice, the actual number of individuals speaking, the sound power emitted by each individual, and the temporal characteristics of each speaker will vary considerably over the assessment period. The derived values therefore do not represent the actual percentage of patrons speaking, or the emission of each patron, but simply represent the total sound power level for the number of patrons
- Design maximum sound power level - derived by assuming that the maximum noise level occurs as a result of two individuals simultaneously producing a maximum level.

The above empirical representation of typical patron noise levels are suitable for design purposes based on the measured data. This approach has been used for Victorian Civil and Administrative Tribunal (VCAT) hearings for the assessment of proposed outdoor venues, which have since been developed and operational.

¹ Hayne et al 2011, 'Prediction of noise from small to medium sized crowds', in *Acoustics 2011: Breaking New Ground, Proceedings of the Annual Conference of the Australian Acoustical Society*, AAS Queensland Division 2011, Gold Coast, paper number 133.

The sound power level used in this assessment is provided in Table 5.

Table 5: Patron noise sound power level data

Area	Semi-steady state	Maximum
Outdoor alfresco dining (100 patrons)	93 dB L _{AW}	101 dB L _{AW}
Outdoor deck (70 patrons)	97 dB L _{AW}	107 dB L _{AW}
Function space (80 patrons)	92 dB L _{AW}	101 dB L _{AW}

6.2 Predicted Patron Noise Levels

A 3-dimensional digital noise model of the site and surrounding built environment has been prepared using SoundPLAN v7.4 proprietary noise mapping software, in order to predict patron noise levels to the nearest noise affected premises.

The noise model has been used to calculate noise levels in accordance with ISO-9613-2:1996 *Acoustics – Attenuation of sound during propagation outdoors – Part 2: General method of calculation* (ISO 9613).

The ISO 9613 standard specifies an engineering method for calculating noise at a known distance from a variety of sources under meteorological conditions that are favourable to sound propagation. The standard defines favourable conditions as downwind propagation where the source blows from the source to the receiver within an angle of +/-45 degrees from a line connecting the source to the receiver, at wind speeds between approximately 1 m/s and 5 m/s, measured at a height of 3 m to 11 m above the ground. Equivalently, the method accounts for average propagation under a well-developed moderate ground based thermal inversion.

Accordingly, predictions on the basis of ISO 9613 account for instances when local atmospheric conditions at the site favour the propagation of sound to surrounding receptor locations. Under alternative atmospheric conditions, such as when the wind is blowing from a receiver location to the site, the noise levels would be lower than calculated.

To calculate noise levels according to the ISO 9613, the noise emissions of each source are firstly characterised in the form of octave band frequency levels. A series of octave band attenuation factors are then calculated for a range of effects including:

- Geometric divergence;
- Air absorption;
- Reflecting obstacles;
- Screening; and
- Ground reflections.

The highest predicted patron noise levels based on the proposed operations and recommendations in Section 4.0 are provided in Table 6. The predictions, to the nearest noise sensitive receivers as shown on Figure 1, are conservative with the assumption that all patrons are located in external areas to the main ship areas.

Table 6: Predicted patron noise levels, dB L_{Aeq}

Receiver	Sun - Thurs, to midnight Outdoor alfresco dining area (100), use of deck (70) and external function space (80)	Fri - Sat, to midnight Outdoor alfresco dining area (100), use of deck (70) and external function space (80)	Sun - Sat, midnight to 2am Outdoor alfresco dining area (100)
Aquarius Apartments			
Ground	41	41	39
Level 1	42	42	41
Level 2	44	44	43
Level 3	46	46	45
Level 4	47	47	47
Level 5 and above	48	48	47
New apartment			
Ground	41	41	39
Level 1	43	43	41
Level 2	44	44	42
Level 3	44	44	43
Level 4	45	45	44
Level 5	46	46	45
Level 6	47	47	46
Level 7 and above	48	48	46
Other			
R1 (west)	44	44	24
R2 (west)	44	44	24
Criteria	45	49	45

The predicted noise levels generally comply with the nominated criteria with the exception of the following minor exceedances:

- Exceedance of 1-2 dB at the upper floor levels of the apartment buildings to the east, between midnight and 2 am; and
- Exceedance of 1-3 dB at the upper floor levels of the apartment buildings to the east, on Sunday – Thursday nights, until midnight.

7.0 SUMMARY

The DeYoung Group now propose to redevelop the Buffalo ship which previously operated for many years as a restaurant.

Marshall Day Acoustics Pty Ltd (MDA) has undertaken a noise assessment of the proposed redevelopment which includes the following:

- An outdoor alfresco dining area located on south east side of the Buffalo Ship, proposed to operate until 2 am, and cater for up to 100 patrons.
- An outdoor deck located on west side of the Buffalo Ship, proposed to operate until midnight, and cater for up to 70 patrons.
- An outdoor function space located on the upper deck of the Buffalo Ship proposed to operate until midnight for corporate functions and weddings. The current license limits upper deck use for 150 patrons (internal and external locations); of which up to 80 patrons can be located external.

The assessment has considered the Development Assessment Commission's review of the proposal, where items had been raised over visual amenity impacts associated with recommended acoustic treatments provided in MDA report R01 issued 23 September 2016.

This revised assessment therefore incorporates DAC's review and feedback, noting a balance between visual and acoustic amenity is to be achieved. The assessment presented has been based on the following:

- Music to be limited to background music only and live acoustic music (e.g. 2 piece acoustic guitar and singer)
- Background music to be played at a level of 71 dB L_{A10} when measured at 3 m from the speaker
 - Speakers in the outdoor alfresco dining area be placed on the eastern wall facing away from the Adelphi Terrace apartments
 - Speakers on the outdoor deck be placed on the western wall facing away from residents located on the opposite side of the Patowalunga River
 - Speaker in outdoor function space to be limited to a single speaker located at the north or south end of the area
- Background music and 2 piece live acoustic music, including guitar and a singer only, occurs at the site. No other live or DJ type music is played
- The outdoor alfresco dining area is 'sunk' into the ground approximately 500 mm. A minimum balustrade 1.8 m is to be provided to north, south and east ends.
- The outdoor deck is shielded to residences on the western side of the Patowalunga River by an acoustically solid bar on west side of the deck, with solid balustrade barrier to the north and south ends. The minimum height of the effective '3 sided barrier' required is 1.8 m
- Provide a minimum 2.1 m high barrier and 2.4 m high above the deck line to north and south ends respectively of the upper function space. The 2.1 m north end barrier to be acoustically lined with 50 mm thick, 32 kg/m² acoustic insulation lined with a fabric or perforated facing with a 25 % open area

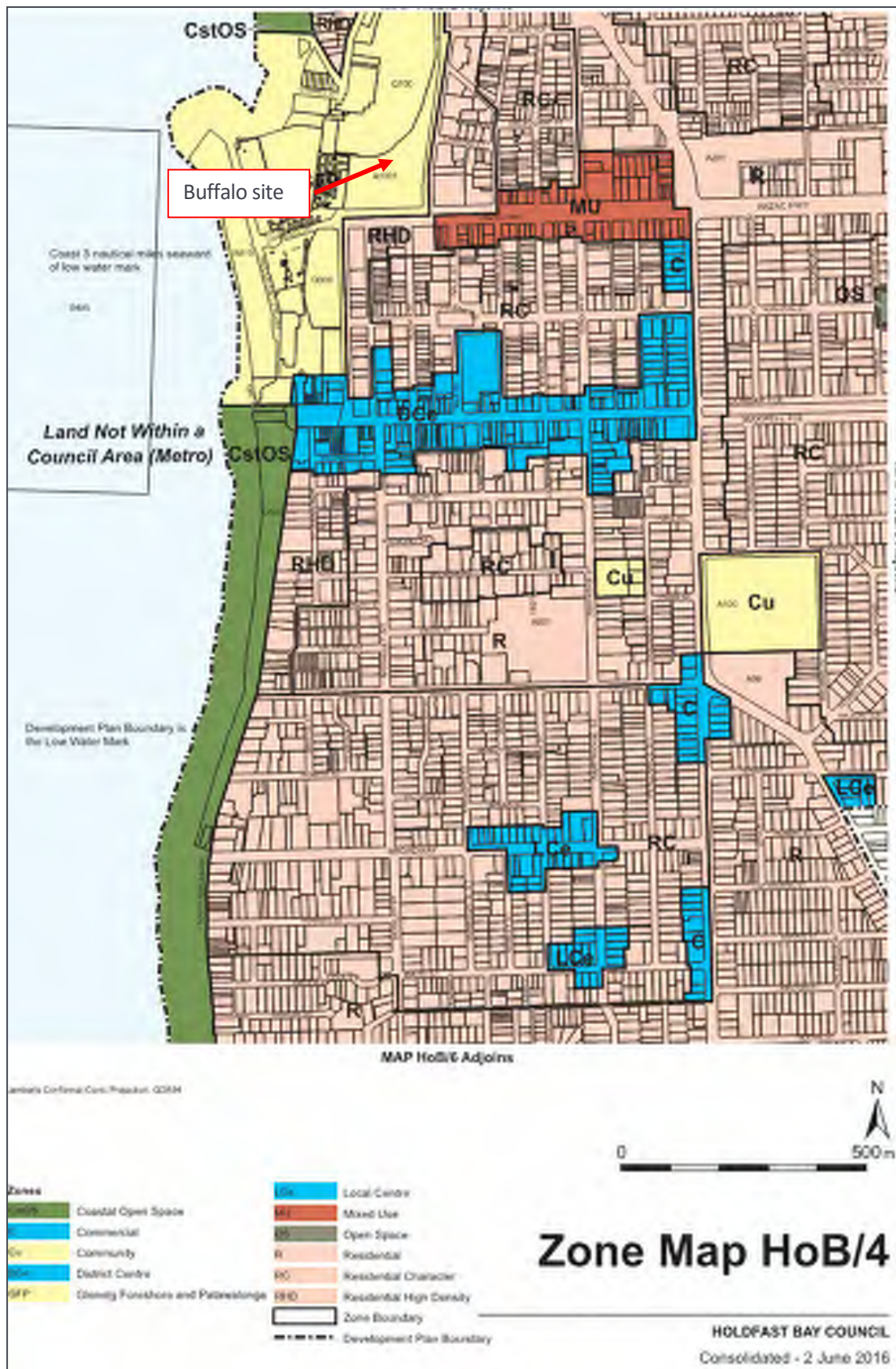
The barriers shall be constructed using material with a minimum surface density of 10 kg/m² - 15 kg/m² and be free of gaps and penetrations. Suitable materials include 9 mm thick fibre cement sheet, 12 mm thick Perspex, 6 mm thick toughened glass or any other material that meets the minimum surface density requirement.

APPENDIX A GLOSSARY OF TERMINOLOGY

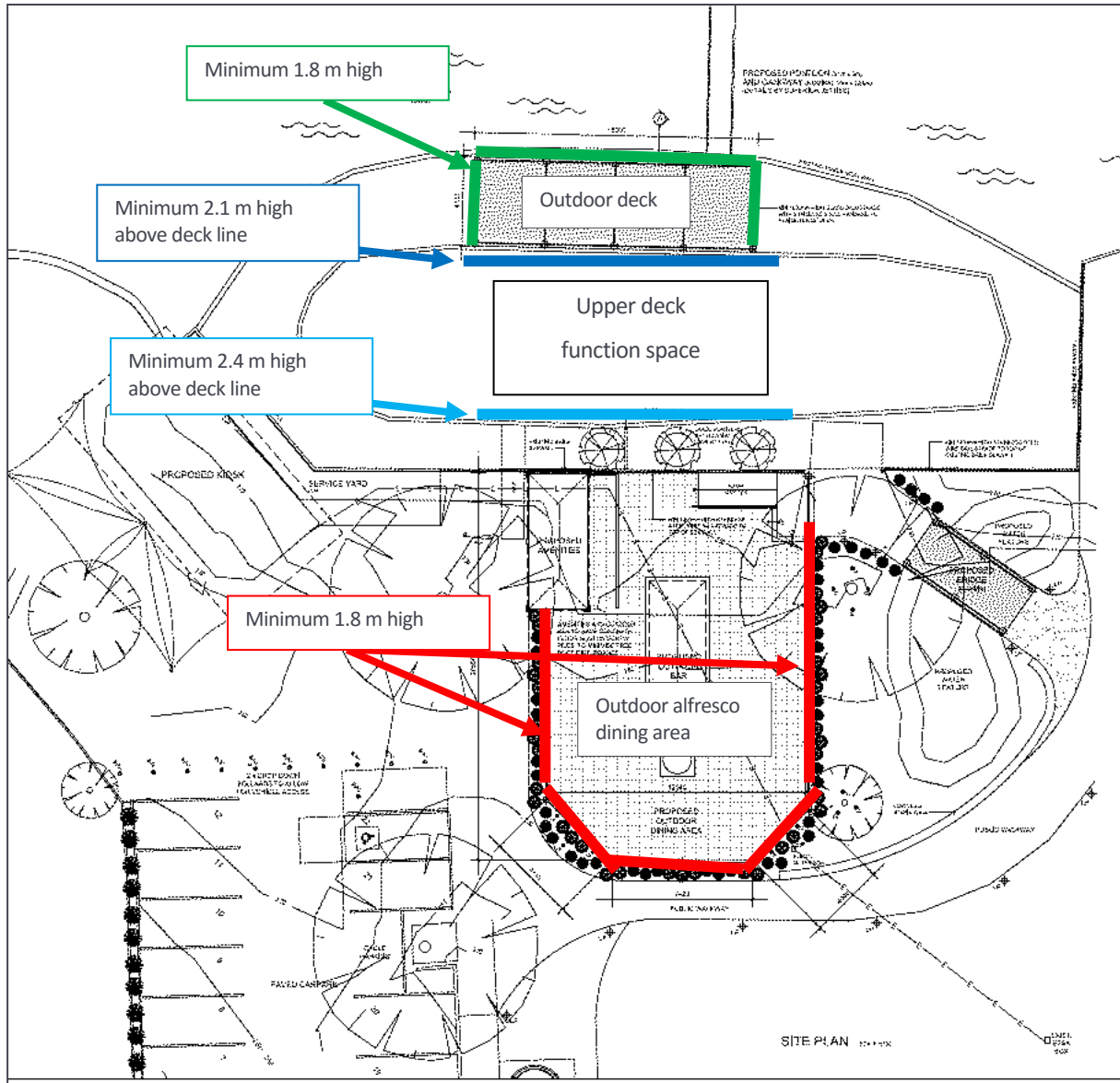
Ambient	The ambient noise level is the noise level measured in the absence of the intrusive noise or the noise requiring control. Ambient noise levels are frequently measured to determine the situation prior to the addition of a new noise source.
A-weighting	The process by which noise levels are corrected to account for the non-linear frequency response of the human ear.
dB	Decibel. The unit of sound level.
Frequency	The number of pressure fluctuation cycles per second of a sound wave. Measured in units of Hertz (Hz).
Hertz (Hz)	Hertz is the unit of frequency. One hertz is one cycle per second. One thousand hertz is a kilohertz (kHz).
L_{A10}	The noise level exceeded for 10% of the measurement period, measured in dBA. This is commonly referred to as the average maximum noise level.
L_{A90}	The noise level exceeded for 90% of the measurement period, measured in dBA. This is commonly referred to as the background noise level.
L_{Aeq}	The A-weighted equivalent continuous sound level. This is commonly referred to as the average noise level and is measured in dB.
L_{OCT10}	The noise level exceeded for 10% of the measurement period in the octave bands 63Hz-4kHz. Commonly referred to as the average maximum noise level.
L_{OCT90}	The noise level exceeded for 90% of the measurement period in the octave bands 63Hz-4kHz. Commonly referred to as the background noise level.
L_{WA}	The A-weighted Sound Power Level. The level of total sound power radiated by a sound source.
Octave Band	A range of frequencies where the highest frequency included is twice the lowest frequency. Octave bands are referred to by their logarithmic centre frequencies, these being 31.5 Hz, 63 Hz, 125 Hz, 250 Hz, 500 Hz, 1 kHz, 2 kHz, 4 kHz, 8 kHz, and 16 kHz for the audible range of sound.

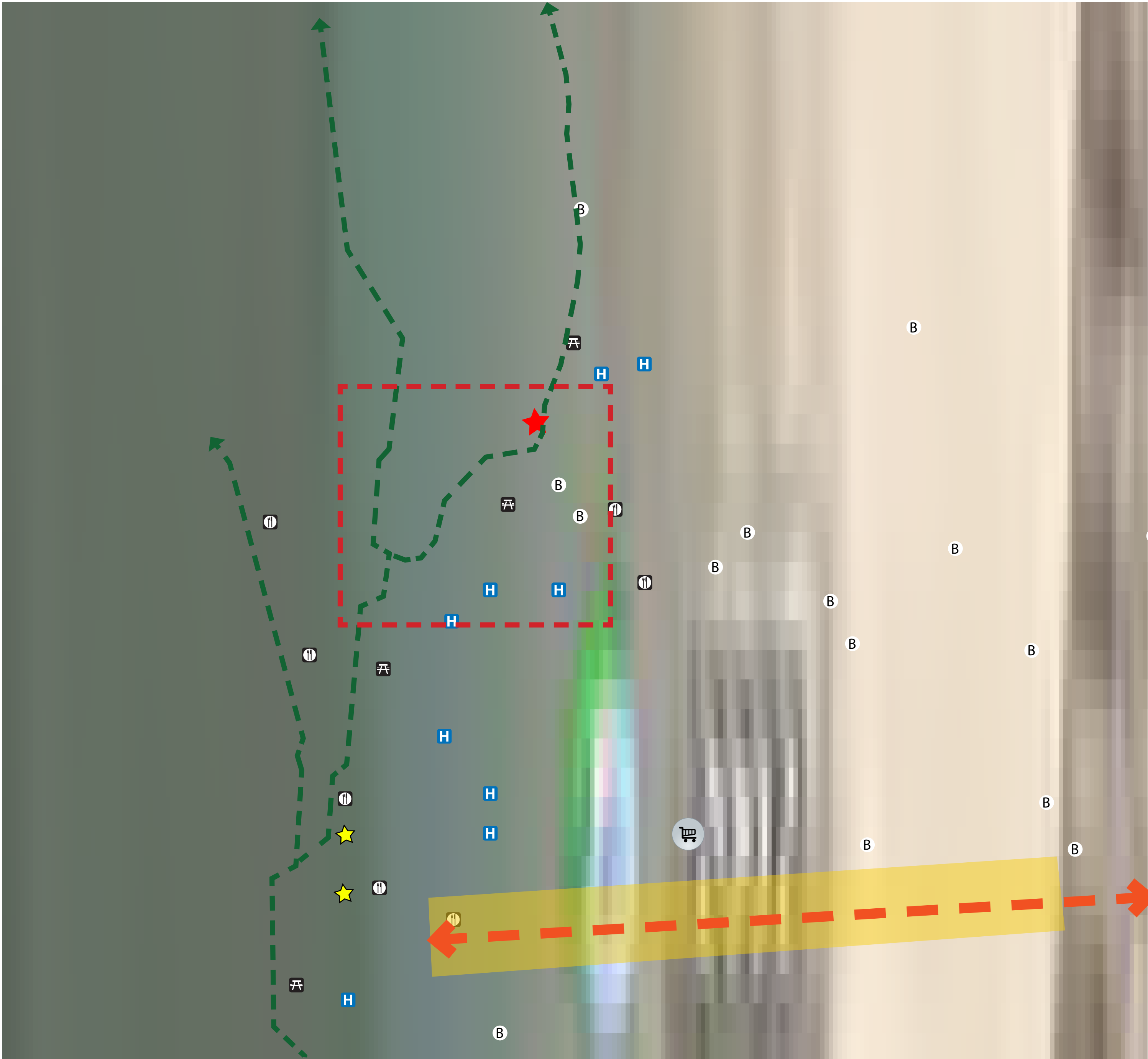
APPENDIX B CITY OF HOLDFAST BAY DEVELOPMENT PLAN

B1 Zone Map



APPENDIX C ACOUSTIC TREATMENT DETAILS





LOCATION PLAN & THE SURROUND DISTRICT
SCALE: NTS

DESIGN INTENT

The redeveloped Buffalo Restaurant has been designed with the intention of the following:

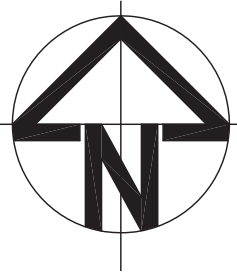
- Key destination for both locals and overseas visitors.
- The proposed water feature, acts as the gateway into the Buffalo precinct.
- Sand and native planting surrounding the water feature, which reflects the surrounding coastal environment.
- Exotic planting placed at certain locations to create burst of colour and features in the landscape.
- The outdoor spaces are surrounded by lush feature plants placed in carefully chosen pots, to create enclosure and soften the area.
- Tree planting will wrap around the Buffalo to soften the ship, but also to frame it. The planting gives rise to a 'Forest' feel around the boat.
- The dry gravel creek & boulders wrap around the ship to reflect the rocks found along wave breakers and connects with the entrance.

LEGEND

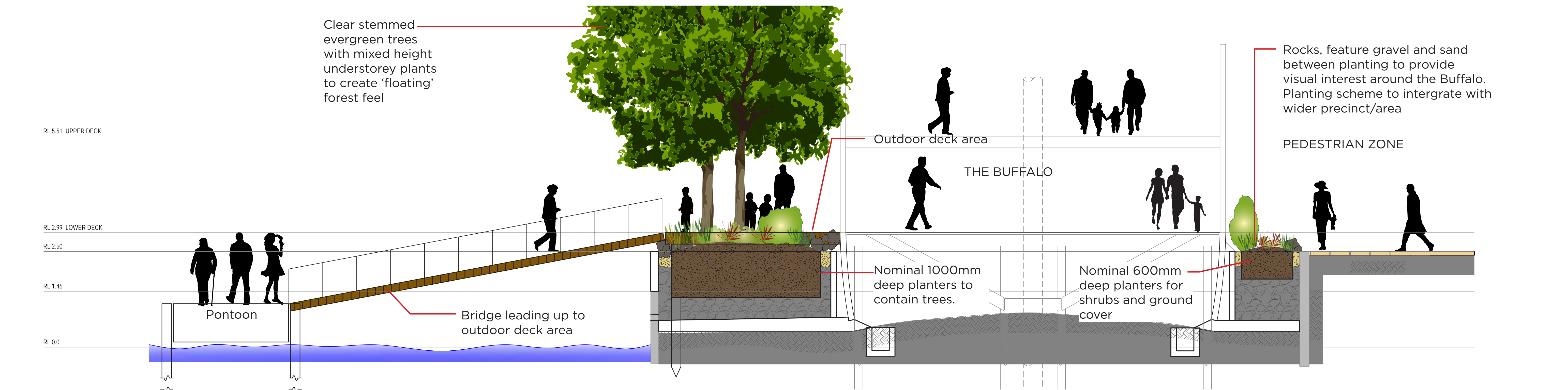
	RESTAURANT		RESERVE/OPEN SPACE
	PARKS/OPEN SPACES		VIEW POINTS LOOKING TOWARDS THE BUFFALO
	HOTEL/MOTEL ACCOMMODATION		HOLDING POINTS
	BUS STOP		PREMISES/LICENCE AREA
	LOCATION OF LOCAL ATTRACTIONS		
	LOCATION OF THE BUFFALO		
	GLENELG MAIN SHOPPING PRECINCT		
	TRAM LINE		
	FORESHORE/PEDESTRIAN PATH & CONNECTIVITY		



THE BUFFALO AND THE IMMEDIATE SURROUNDINGS
SCALE: NTS







CROSS SECTION
SCALE 1:50@ A1

FEATURE PLANTS

TREES



TUCKEROO 8m (H) x 5-8m (W) <i>Cupaniopsis anacardioides</i>	WHITE CEDAR (STERILE) 10m (H) x 8m (W) <i>Melia azedarach</i>	EUCKY DWARF 6m (H) x 3-5m (W) <i>Eucalyptus leucoxylon Dwarf</i>	POHUTUKAWA 6m (H) x 3-5m (W) <i>Metrosideros excelsa</i> <i>Father Xmas Tree</i>	DATE PALM (STERILE) 12-20m (H) x 6-12m (W) <i>Phoenix canariensis</i>	COTTON FAN PALM 15-20m (H) x 6-12m (W) <i>Washingtonia filifera</i>	PINK INDIAN HAWTHORN 1-2m (H) x 1-2m (W) <i>Raphiolepis delacourii</i>	WESTRINGIA 2m (H) x 2m (W) <i>Westringia 'Wynyabbie Gem'</i>	ROSEMARY 1m (H) x 1m (W) <i>Rosmarinus officinalis</i>	MIRROR PLANT 1.5m (H) x 1m (W) <i>Coprosma Evening Glow</i>	BOUGAINVILLEA 1m (H) x 1.5m (W) <i>Bougainvillea 'Bambinos shaba'</i>
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SHRUBS

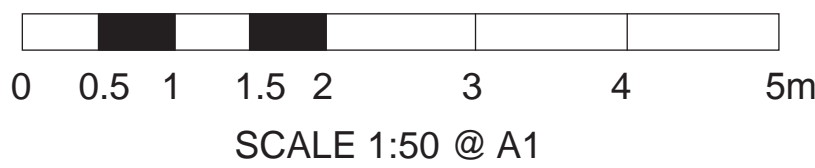


NZ CABBAGE TREE 5-6m (H) x 2m (W) <i>Cordyline australis</i>	KING SAGO PALM 5m (H) x 4m (W) <i>Cycas revoluta</i>	PURPLE NZ FLEX 1.2m (H) x 1m (W) <i>Phormium tenax Purpureum</i>	LILY TURF 0.6m (H) x 0.45m (W) <i>Liriope muscari</i>	MOTHER-IN-LAW'S TONGUE 1.2m (H) <i>Sansevieria Trifasciata</i>	MIRROR PLANT 1.2m (H) x 1m (W) <i>Coprosma 'Karo Red'</i>	PAROO LILY 0.2-1m (H) x 0.3-2.5m (W) <i>Dianella caerulea</i>	WILD IRIS 1.2m (H) x 1.2m (W) <i>Diets grandiflora</i>	BLUE ROSE ECHEVERIA 0.1m (H) x 0.3m (W) <i>Echeveria imbricata</i>	BLUE CHALK STICKS 0.1m (H) x 0.6m (W) <i>Senecio serpens</i>
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PROJECT
CLIENT
DRAWING

THE BUFFALO, GLENELG
DE YOUNG JAMESTOWN PTY LTD
LANDSCAPE SECTION A-A & FEATURE PLANTS



DATE: 24/07/2017
DWG NO. OS582_CP03
BY: AC/RO
APPROVED BY: PG



**CUTER
SPACE**

PROJECT
CLIENT
DRAWING

THE BUFFALO, GLENELG
DE YOUNG JAMESTOWN PTY LTD
DESIGN PERSPECTIVE (1)

DATE: 27/06/2017
DWG NO. OS582_CP04

BY: RO
APPROVED BY: PG



**OUTER
SPACE**

PROJECT
CLIENT
DRAWING

THE BUFFALO, GLENELG
DE YOUNG JAMESTOWN PTY LTD
DESIGN PERSPECTIVE (2)

DATE: 27/06/2017
DWG NO. OS582_CP05

BY: RO
APPROVED BY: PG

6 July 2017

Tom Gregory
Ben Green & Associates
PO Box 392
BRIGHTON SA 5048

Dear Tom,

RE: Buffalo Restaurant GLENELG

As requested I have perused the plans of Stuart J Patrick, sheets 1 to 12 and a report by D Nash of Tonkin Consulting dated 19 November 2013.

There doesn't appear to be any building work within the building which requires any upgrading.

Any application for Building Rules Consent should be lodged as soon as possible as the Government has released a draft Ministers Code for Upgrading Health and Safety in existing buildings. I am not sure the status of this documents however if it was to be adopted in its current form it will have a significant detrimental and costly effect on the proposal.

The application for Building Rules Consent can be lodged now and additional information can follow. The rules to apply are those in force on the date the application is lodged.

I cannot envisage any reason why Building Rules Consent cannot be granted however;

- The egress from the main level should be improved to meet the provisions of the Performance Requirements DP4 and DP5,
- To comply with these performance requirements a second exit should be provided. The second exit should be placed on the south-eastern wall of the front dining room and /or from the main dining area to the new western outdoor dining deck. A suitable gate from that outdoor dining deck should also be installed to afford egress to open space and public place
- With regard to the upper deck two exits are available and are satisfactory. The existing stair toward the rear of the load leads in to the internal part of the main dining area. This is on 4 metres to the main exit.

In summary, I believe there are no major issues which would prevent Building Rules Consent being granted.

Should you have any queries regarding this matter please do not hesitate calling me.

Yours faithfully,



Grant K Riches
Building Surveyor / Private Certifier

20131056FL2A

19 November 2013

Mr Hilton Solyom
Technical Engineer
City of Holdfast Bay
24 Jetty Road
Brighton SA 5048

Dear Hilton,

BUFFALO RESTAURANT

As requested please find answers to the questions raised by the Council at the last Council meeting.

What is the life span of the timber piles?

It is now understood that the treated pine piles that are driven about 7 to 8m into the soil are Hazard Level H6. The hazard level relates to the level of exposure the timber will have in its service life to insects or decay. The higher the hazard level the greater the threat to the timber from insects and decay. Hazard Level H6 is the worst level of exposure to insects and decay expected hence the treated timber used in such situations has the highest possible level of treatment so it can last in these environments.

Typically H6 environments are those where the timber is exposed to salt water.

The treated timber used for the timber piles for the Buffalo is therefore appropriate for the location and provides the best chance that they will last a long time.

Typically a life span of between 40 to 50 years could be expected. The piles are currently about 31 years old, with the Buffalo built in 1982.

The piles above the soil in the original report were rated as being in good condition which would have given them a life expectancy of another 7 to 12 years. Although it is not possible to view the piles within the soil they are expected to be either in a similar condition to the piles above the soil or at worst, in a slightly poorer condition.

Now that it is fairly certain that they are H6 treated pine it is believed reasonable to expect a life expectancy of another 15 to perhaps as much as 20 to 25 years.

What needs to be done to increase the piles life expectancy?

Unfortunately nothing can be done to increase their life expectancy.

The only option would be to coat the piles to provide further protection, a sacrificial layer, to slow down the affects of insects and the salt water.

It is however most likely that the piles will first fail in the soil. It is not possible to provide a coating to the piles where they are encased in the soil, hence there is nothing that can be done to increase their life expectancy.

With the piles most likely to fail in the soil first there would be no benefit in coating the piles above the soil.

What would the cost be to extend the life of the piles?

This question has no answer as it is believed that nothing can be done to increase the life expectancy of the timber piles.

What will it take and cost to make the Buffalo compliant with the Building Code of Australia (BCA)?

It is reiterated that an existing building/structure need only comply with the building legislation applicable at the time the application for the building/structure was lodged for approval.

As the ship was constructed in 1982 the emergency egress and fire fighting facilities in the building legislation that the ship would have had to comply with, were fairly similar to today's requirements. It is understood that access for disabled requirements first came in 1980 but only applied to access to the ground floor. Toilets for employees and hence for the disabled were not included in the building regulations back in 1982.

Accepting that the building/structure is still structurally sound, there are three possible exceptions to the above.

1. The Council's Building Fire Safety Committee can require that an existing building/structure be made safer in regard to fire. This can involve upgrading emergency egress, exit and emergency lighting, fire compartmentation and fire fighting equipment.
2. If a complaint regarding access for the disabled is made by a member of the public under the Disability Discrimination Act (DDA) to the Australian Human Rights Commission then the Commission can require the building to be upgraded for disability access.
3. If building works are carried out on the ship that require a Building Rules Consent then these building works will need to fully comply with the current BCA and other areas of the ship not being altered can be required to be upgraded for fire safety, structural safety or if they are in an unhealthy condition.

For the purposes of complying with the BCA it is assumed that works will be done to the main restaurant level only. Alterations to this floor must be upgraded to comply with the current BCA but for the upper level only those issues that relate to fire safety, structural safety or unhealthy conditions are included in the estimated costs below.

It is also assumed that the uses stay as they are now.

It is also assumed that the ceilings, internal linings of the external walls and windows are not being altered hence the requirements for energy efficiency contained in Section J of the BCA will not be applicable. It has however been decided to include below the cost of installing insulation to the external walls when the external cladding is replaced.

The upgrading of the existing fire detection system has been included for fire safety even though the BCA does not require a fire detection system in such a building. It is already installed so it is sensible to still utilise its early warning capability to evacuate occupants and alert the fire service.

Items to comply with BCA

Refurbishment of toilets to comply with access for disabled requirements excluding new exhaust fans, included below, but including new tempering valves and the replacement of sanitary and tap ware that were previously included under hydraulic services	\$47,000
New balustrade to external staircase	\$1,500
Raise the height of handrails to the two internal stairs	\$1,500
Provide new doors to comply with access for the disabled requirements to the main restaurant level excluding those doors serving the kitchen as the kitchen is considered an exempt area for access for the disabled	\$1,500
Changes to door hardware to exit doors to comply with Part D2.21 of the BCA	\$1,000
Upgrade to electrical and lighting, including upgrading the distribution board, changes to exit and emergency lighting, new light globes to the main restaurant level only, all to comply with the BCA	\$20,000
Upgrade of fire hose reels and fire extinguishers	\$3,000
Upgrading of fire detection system	\$20,000
Fit backflow prevention valve at water metre	\$2,000
Upgrade of air conditioning and exhaust fans to Comply with BCA	\$97,000
New external wall insulation	\$7,000
TOTAL	\$201,500

This is the total for BCA compliance based on the assumptions above. Please remember there are also those items of maintenance such as new timber cladding, carpets, etc., that would be required to be done. These items add up to about an extra \$130,000.

The air conditioning figure included before at \$115,000 was incorrect. It should have read \$97,000.

If you have any queries regarding the report please contact me on 8110 2220.

Yours faithfully
TONKIN CONSULTING

A handwritten signature in blue ink, appearing to read 'D. B. Nash', followed by a long horizontal flourish.

DAVID B NASH
Building Surveyor/Senior Engineer



16 02 2017



16.02.2017





16 02 2017



16.02.2017



16 02 2017



16 02 2017



16 02 2017



16 02 2017



16 02 2017



16 02 2017





16 02 2017



**Minutes of the 568th Meeting of the
Development Assessment Commission
held on Thursday, 19 January 2017 commencing at 9.45 AM
28 Leigh Street, Adelaide**

1. OPENING

1.1. PRESENT

Presiding Member	Simone Fogarty
Members	Helen Dyer (Deputy Presiding Member) Chris Branford David O'Loughlin Peter Dungey Dennis Mutton
Council Representation	Jenny Newman
Secretary	Cathryn Longdon
DPTI Staff	Dan Pluck (Agenda Item 2.2.1) Brett Miller (Agenda Item 2.2.1, 2.2.2) Yasmine Alliu (Agenda Item 2.2.3) Tom Victory (3.1.1) Jason Bailey (3.1.1) Simon Neldner (Agenda Item 2.2.1, 2.2.2, 2.2.3)

1.2. APOLOGIES – Sue Crafter

2. DAC APPLICATIONS

2.1. DEFERRED APPLICATIONS - Nil

2.2. NEW APPLICATIONS

2.2.1 De Young Jamestown

110/L002/15

Lot 1001 Adelphi Terrace, Glenelg North

City of Holdfast Bay

Proposal: Restaurant and kiosk

The Presiding member welcomed the following people to address the Commission:

Applicants(s)

- Troy De Young
- Stuart Patrick

Council

- Dean Spasic

Representor(s)

- Liam Hanna
- Loretta Hanna

The Commission discussed the application.

RESOLVED

1. To DEFER the application, for the proponent to supply the following information:

- Clearer plans to demonstrate the interrelationship between the public realm and the development.
- Further details on the western façade of the Buffalo structure and any other alterations that are required for the development.
- Details around the resolution of the moat - landscaping, water quality.
- Identify the space, facilities and utilities that are located in the Buffalo structure that are integral to the wider development.
- Provide a consistent package of plans and documentation eg. including resolution of the noise attenuation, elevations of all facades and consistent plans.

2.2.2 Coles Property Group (c/- Intro Design Pty Ltd)

453/E004/16

Lot 5 Adelaide Road, McCracken

City of Victor Harbor

Proposal: To construct a mixed use development comprising a group of shops, a bulky goods outlet and medical consulting rooms with ancillary car parking, loading areas, acoustic fencing and landscaping.

The Presiding member welcomed the following people to address the Commission:

Applicants(s)

- Chris Harman
- Lorne McClurg
- Damien Ellis
- Anthony Gatti
- Sam Adams

Council

- Ben Coventry

The Commission discussed the application.

RESOLVED

1. The proposed development is NOT seriously at variance with the policies in the Development Plan.
2. The Development Assessment Commission is satisfied that the proposal generally accords with the related Objectives and Principles of Development Control of the City of Victor Harbor Development Plan.
3. To grant Development Plan Consent to the proposal by Coles Property Group to construct a mixed use development comprising a group of shops, a bulky goods outlet and medical consulting rooms with ancillary car parking, loading areas, acoustic fencing and landscaping at allotment 5, Adelaide Road, McCracken subject to the following conditions of consent.

PLANNING CONDITIONS

1. Except where minor amendments may be required by other relevant Acts, or by conditions imposed by this application, the development shall be established in strict accordance with the details and following plans submitted in Development Application No 453/E004/16.

Plans – Hames Sharley

Drawing No	Drawing Title	Date	Rev
DA002	Location Plan	29.06.16	3
DA003	Demolition Plan	29.06.16	4
DA004	Site Plan	15.07.16	17
DA005	Roof Plan	15.07.16	5
DA006	Movement Plan	15.07.16	6
DA008	Lighting & Surveillance Plan	15.07.16	6
DA009	Landscape Plan	29.06.16	3
DA010	Street Perspective	20.07.16	7
DA011	Site Elevations	08.07.16	6
DA012	Elevations – Bunnings Sheet 1	08.07.16	6
DA013	Elevations – Bunnings Sheet 2	08.07.16	9
DA014	Elevations - Coles	20.07.16	8
DA015	Elevations – Allied Health	07.07.06	5
DA016	Materials Schedule Plan	29.06.16	3

Reports / Correspondence

- Traffic Impact Assessment, prepared by GTA Consultants, dated 19/07/16
- Environmental Noise Impact Assessment, prepared by Resonate dated 5 July 2016
- Stormwater Management Plan, prepared by Combe Pearson Reynolds (CPR), dated 5 July 2016
- Services Utilities Infrastructure Report, prepared by Lucid, dated 16 June 2016

2. Prior to the commencement of construction, a Construction Management Plan ("CEMP") shall be prepared to the satisfaction of the Development Assessment Commission and implemented during all site works upon the land. The CEMP shall include details of proposed actions and practices to address the following:
 - a) air quality, including odour and dust;
 - b) surface water including erosion and sediment control;
 - c) soil management including fill importation, stockpile management and prevention of soil contamination;
 - d) prevention of groundwater contamination; and
 - e) minimisation of noise
3. Prior to development approval being granted, the applicant shall provide amended plans showing bicycle parking in accordance with Table ViH/7 of the Victor Harbor (City) Development Plan.
4. Prior to Development Approval being granted the applicant shall submit a detailed Landscaping Plan for the site, such a plan shall be developed in conjunction with the City of Victor Harbor and be to the reasonable satisfaction of the Development Assessment Commission.
5. Landscaping shall be established prior to the operation of the development and shall be maintained and nurtured at all times with any diseased or dying plants being replaced.
6. A final Stormwater Plan based the Plan prepared by CPR 5 July 2016, in consultation with Council, to the satisfaction of the Development Assessment Commission.
7. All vehicle car parks, driveways, aisles, vehicle entry and manoeuvring areas shall be designed and constructed in accordance with the relevant Australian Standards and be constructed, drained and paved with bitumen, concrete or paving bricks in accordance with sound engineering practice and appropriately line marked to the reasonable satisfaction of the Development Assessment Commission prior to the occupation or use of the development.
8. Driveway, manoeuvring areas, car parking spaces, and landscape areas shall not be used for the storage or display of any materials or goods.
9. All external lighting of the site, including car parking areas, access points and buildings, shall be designed and constructed to conform with Australian Standards and must be located, directed and shielded and of such limited intensity that no demonstrable nuisance or loss of amenity is caused to any person beyond the site.
10. Waste collection vehicles shall only service the development between the hours of 7am and 7pm Monday to Saturday inclusive and 9am to 7pm on Sunday and Public Holidays.
11. The development and the site shall be maintained in a serviceable condition and operated in an orderly and tidy manner at all times.
12. The acoustic measures recommended on page 11 of the Resonate Acoustics Environmental Noise Assessment (A16176RP1 Revision A) shall be implemented (where applicable).

Commissioner of Highways Conditions

13. All road works required to facilitate safe access to/from the development shall be undertaken to DPTI and Council's satisfaction at the applicant's cost. All costs (including design, construction, project management and any changes to road drainage, lighting etc. required as well as any community consultation required) shall be borne by the applicant. Prior to undertaking detailed design, the applicant shall contact DPTI's Technical Officer, Ms Alison Allen on telephone (08) 7223 6022 or via email Alison.Allen@sa.gov.au to progress this. All road works associated with the development shall be completed prior to the commencement of operation of the development
14. Signage and line marking shall be utilised to reinforce the desired flow of traffic to, from and through the site.
15. The largest vehicle permitted on site shall be a 19 metre semi-trailer. This vehicle shall enter the site via the Adelaide Road / Hindmarsh Road / Port Elliot Road roundabout access and exit the site via the western McCracken Drive access point.
16. All service vehicle movements associated with the development shall be undertaken outside of peak traffic hours on the adjacent roads and peak times of site operation.
17. All redundant crossovers to/from the site shall be reinstated to Council standard kerb and gutter at the applicant's expense prior to the operation of the development.
18. All materials and finishes shall not be permitted to result in glare or other effects that will result in the discomfort or impairment of road users.
19. All Illuminated signage on this site shall be limited to a low level of illumination (≤ 200 cd/m²) so as to minimise distraction and discomfort to motorists.
20. All signage on this site shall be finished in a material of low reflectivity to minimise the risk of sun/headlamp glare that may dazzle or distract motorists.
21. The utilisation of Trailer Mounted Variable Message Displays for advertising purposes shall not occur on the subject land.
22. Stormwater run-off shall be collected on-site and discharged without jeopardising the safety and integrity of the adjacent roads. Any alterations to the road drainage infrastructure required to facilitate this shall be at the applicant's expense.

ADVISORY NOTES

- a. The new Shared Urban Paths and footpaths should be designed and positioned to provide appropriate pedestrian and cycling safety when traversing the proposed external entry and exit points to/from the site and across Down Street and McCracken Drive.
- b. Detailed plans should be provided to Council for approval prior to construction showing the cutting/battering of the nature strip in order to achieve required sight distance, including pedestrian management, stormwater management and landscaping.

- c. The applicant should construct a pedestrian refuge and SUP's in accordance with the GTA Transport Impact Assessment.
- d. The applicant should liaise with Council regarding the McCracken Drive pavement and seal being upgraded between Adelaide Road and the western egress point on McCracken Drive to accommodate the additional truck and traffic movements.
- e. A linemarking and signage plan should be provided for the portions of Adelaide Road, Hindmarsh Road, Port Elliot Road, McCracken Drive and Down Street that are adjacent to or affected by the development.
- f. Vehicular access to serve the development shall be as follows:
 - a) A new access point forming a fourth leg of the Adelaide Road / Hindmarsh Road / Port Elliot Road roundabout, serving passenger vehicles (all movements) and heavy vehicles (ingress only);
 - b) A new access point to/from Adelaide Road located approximately 150 metres to the north of the Adelaide Road / Hindmarsh Road / Port Elliot Road roundabout, which shall be in conjunction with a back-to-back channelized right turn treatment facilitating all movements at the access point and Adamson Street;
 - c) A passenger vehicle access to/from McCracken Drive accommodating all movements, and;
 - d) A heavy vehicle access to McCracken Drive, located adjacent the western boundary, accommodating egress movements only.
- g. The detailed design of road works to facilitate the development shall result in:
 - a) A road cross section between the Adelaide Road / Hindmarsh Road / Port Elliot Road roundabout and McCracken Drive of 3.5 metre through lanes (one in each direction) and 1.5 metre sealed shoulders, together with 3.0 metre left turn lanes, 3.5 metre right turn lanes and a 3.5 metre painted median extending from the roundabout to the back-to-back channelised turn treatment at the site access / Adamson Street and between Adamson Street and McCracken Drive;
 - b) A separation between the through lane and channelised left turn lane at both the site access and McCracken Drive junctions that overcomes dynamic visual obstruction;
 - c) The consideration and installation (where required) of pedestrian facilities to facilitate movement of pedestrians across Adelaide Road.
- h. The applicant is required by DPTI to enter into a Developer Agreement to undertake and complete the required road works.
- i. The development must be substantially commenced within 12 months of the date of this Notification, unless this period has been extended by the Development Assessment Commission.
- j. The authorisation will lapse if not commenced within 12 months of the date of this Notification.

- k. The applicant is also advised that any act or work authorised or required by this Notification must be completed within 3 years of the date of the Notification unless this period is extended by the Commission.
- l. The applicant has a right of appeal against the conditions which have been imposed on this Development Plan Consent or Development Approval.
- m. Such an appeal must be lodged at the Environment, Resources and Development Court within two months from the day of receiving this notice or such longer time as the Court may allow.
- n. The applicant is asked to contact the Court if wishing to appeal. The Court is located in the Sir Samuel Way Building, Victoria Square, Adelaide, (telephone number 8204 0289).

2.2.3 **Shahin Enterprises Pty Ltd**

180/E017/16

285-287 Kensington Road, Kensington Park

City of Burnside

Proposal: Demolition of existing improvements and construction of petrol filling station comprising a new control building, fuel canopy, underground fuel tanks, 2 x dog wash facilities, free standing signage and associated car parking and landscaping

Simone Fogarty declared a conflict of interest and left the meeting.

The Presiding member welcomed the following people to address the Commission:

Applicants(s)

- Thuy Luu-Nguyen
- Robert King
- Adrian Sasu (GHD)
- Jason Turner (Sonus)

Council

- Magnus Heinrich

Agency

- Matthew Henderson (DPTI)

Representor(s)

- Geoffrey Wingett
- Anthony Kelly (Mellor Olsen lawyers representing Council and Defence Housing Authority)
- Barry Cant (Council)
- Paul Deb (Council)
- Jill Properjohn (representing herself and the occupier of her property)
- Melissa Mellen (representing Stephanie and John Hamra)
- Chris Shakes (for Mario Giordano)

The Commission discussed the application.

RESOLVED

1. The proposed development is NOT seriously at variance with the policies in the Development Plan.
2. To DEFER the application to enable the applicant to further address the following, potentially through re-configuration and design of the site:

- To address the offsite impacts on the locality as per Zone objective 1, PDC 1 (parts c and d), in particularly:
 - i. Traffic movements and vehicle queuing on May Terrace and flow on affects to Kensington Road.
 - ii. The impact of the traffic median on Kensington Road as it effects surrounding properties.
 - iii. Loss of on-street parking on Kensington Road and May Terrace.
- Additional on-site car parking
- Landscaping as per Zone PDC 5

3. ADJOURN TO INNER METROPOLITAN DEVELOPMENT ASSESSMENT COMMITTEE

3.1. DEFERRED APPLICATIONS

3.1.1 Hames Sharley (SA) Pty LTd

1558/M007/16

30-36 The Parade West, Kent Town

City of Norwood Payneham and St Peters

Proposal: Prince Alfred College Boarding House

The Presiding member welcomed the following people to address the Commission:

Applicants(s)

- Sarahlee Aufderheide
- Liam Murphy
- Chris Vounasis

Agency

- Kirsteen Mackay
- Ellen Liebelt

The Commission discussed the application.

RESOLVED

1. The proposed development is NOT seriously at variance with the policies in the Development Plan.
2. The Development Assessment Commission is satisfied that the proposal meets the key objectives of the Urban Corridor Zone.
3. To grant Development Plan Consent to the proposal by Prince Alfred College/Hames Sharley for the construction of a student boarding house, pedestrian bridge, associated car parking and demolition of existing buildings on the subject land at 32A to 36 The Parade West subject to the following reserved matters and conditions of consent.

PLANNING CONDITIONS

1. Except where minor amendments may be required by other relevant Acts, or by conditions imposed by this application, the development shall be established in strict accordance with the details and plans, including the amended plans as submitted in development application number 155/M007/16:

Plans by Hames Sharley, Project Number 31505:

Sheet	Drawing Name	Revision	Date
DA001	Title Sheet	2	13/01/2017
DA002	Locality Plan	2	13/01/2017
DA003	Site Plan	3	13/01/2017
DA004	Ground Floor Plan	4	13/01/2017
DA005	First Floor Plan	3	13/01/2017
DA005a	Second Floor Plan	2	13/01/2017
DA005b	Third Floor Plan	2	13/01/2017
DA005c	Bridge Access Diagram	2	13/01/2017
DA005e	External Perspective 01	2	13/01/2017
DA005f	External Perspective 02	2	13/01/2017
DA005g	External Perspective 03	1	13/01/2017
DA006	Fourth Floor Plan	3	13/01/2017
DA006a	Roof Plan	3	13/01/2017
DA007	Landscaping Plan	2	13/01/2017
DA008	Streetscape Elevation	2	13/01/2017
DA009	Elevations	3	13/01/2017
DA0010	Sections	2	13/01/2017
DA0011	Perspectives	2	13/01/2017
DA0012	Shadow Diagrams	2	13/01/2017
DA0013	Bridge Plans	4	13/01/2017
DA0014	Bridge Perspectives and Building Signage	2	13/01/2017
DA0015	Bridge Detail	2	13/01/2017
DA0016	Seating and Planter Box Details	1	13/01/2017
DA0017	Cad-Survey	2	13/01/2017

Other Plans:

- Landscape and Public Realm Concept Design prepared by Oxygen
 - Stormwater Drainage Layout Plan prepared by PT Design, Drawing Number 18148-C01, Issue B dated February, 2016
2. Prior to the issue of Development Approval for superstructure works, a definitive statement from a suitably qualified site contamination or environmental professional that the land is suitable for its intended use (or can reasonably be made suitable for its intended use) shall be provided to the satisfaction of the Development Assessment Commission.
 3. Prior to the issue of Development Approval for superstructure works, the following shall be provided in consultation with the Government Architect to the satisfaction of the Development Assessment Commission:
 - A detailed materials schedule and samples board for the boarding house building, footbridge, staircase, lift shaft and all fencing.
 - Final samples of the bridge fritted glass signage demonstrating a discrete appearance for the signage.
 - An appropriate stormwater management plan for the bridge and staircase roofing which ties in with the design intent.
 - Details of the design approach to the junction between the bridge and the wall of the boarding house building.
 - Additional information that demonstrates that CPTED issues arising from the space underneath the staircase are addressed.
 - Details of the proposed new bronze perforated mesh fencing which is to run between the end of the bridged staircase to the existing stone

wall on the corner of The Parade West, to ensure its suitability where interfacing the public realm and its junction with the existing wall.

- A detailed landscape plan that includes the interface with the private gardens, where the building meets the perimeter fencing, The Parade West raised planters and garden beds and any streetscape treatments to either side of The Parade West.
4. Prior to the issue of Development Approval for any superstructure works, the applicant shall demonstrate, to the reasonable satisfaction of the Commission, that the preliminary acoustic design recommendations of the Bestec Acoustic Services report dated 16 March, 2016 have been incorporated into the detailed design of the proposal or that the appropriate noise levels, as identified within the report, have otherwise been achieved.
 5. The pedestrian bridge shall:
 - comply with DPTI Design Standard: Structural;
 - be constructed to ensure that earthing and bonding requirements are addressed to the satisfaction of the Commission; and
 - be designed for collision load and incorporate suitable crash protection.
 6. The proposed car parking layout, vehicular entry points and manoeuvring areas shall be designed and constructed to conform to the Australian Standard 2890.1:2004 (including clearance to columns and space requirements at the end of blind aisles) for Off-Street Parking Facilities; Australian Standard 2890.6-2009 Parking facilities – Off street commercial vehicle facilities and designed to conform with Australian Standard 2890.6:2009 for Off Street Parking for people with disabilities.
 7. Mechanical plant or equipment shall be designed, sited and screened to minimise noise impact on adjacent premises or properties. The noise level associated with the combined operation of plant and equipment such as air conditioning, ventilation and refrigeration systems when assessed at the nearest existing or envisaged noise sensitive location in or adjacent to the site shall not exceed:

50 dB(A) during daytime (7.00am to 10.00pm) and 40 dB(A) during night time (10.00pm to 7.00am) at the most affected residence when measured and adjusted in accordance with the relevant environmental noise legislation except where it can be demonstrated that a high background noise exists.

8. A Construction Environment Management Plan (CEMP) shall be prepared and implemented in accordance with current industry standards – including the EPA publication “Environmental Management of On-site Remediation” - to minimise environmental harm and disturbance during construction.

The CEMP must incorporate, without being limited to, the following matters:

- air quality, including odour and dust
- surface water including erosion and sediment control
- soils, including fill importation, stockpile management and prevention of soil contamination
- groundwater, including prevention of groundwater contamination
- noise
- occupational health and safety

For further information relating to what Site Contamination is, refer to the EPA Guideline: 'Site Contamination – what is site contamination?':

9. All stormwater design and construction shall be in accordance with Australian/New Zealand Standards, AS/NZS 3500-2003 and recognised engineering best practices to ensure that stormwater does not adversely affect any adjoining property.
10. The connection of any storm water discharge from the Land to any part of the Council's underground drainage system shall be undertaken in accordance with the relevant Council standards.

ADVISORY NOTES

- a. Separate Council approval is required for the encroachment of the footbridge over The Parade West Road reserve.
- b. The development must be substantially commenced within 12 months of the date of this Notification, unless this period has been extended by the Development Assessment Commission. The authorisation will lapse if not commenced within 12 months of the date of this Notification.
- c. The applicant is also advised that any act or work authorised or required by this Notification must be completed within 3 years of the date of the Notification unless this period is extended by the Commission.
- d. The applicant has a right of appeal against the conditions which have been imposed on this Development Plan Consent or Development Approval. Such an appeal must be lodged at the Environment, Resources and Development Court within two months from the day of receiving this notice or such longer time as the Court may allow. The applicant is asked to contact the Court if wishing to appeal. The Court is located in the Sir Samuel Way Building, Victoria Square, Adelaide, (telephone number 8204 0289).
- e. It is recommended that the applicant contact DPTI's Asset Management section in relation to the relevant DPTI design standards and requirements of the footbridge.

3.2. NEW APPLICATIONS – Nil

4. PORT ADELAIDE DEVELOPMENT ASSESSMENT COMMITTEE

4.1. DEFERRED APPLICATIONS – Nil

4.2. NEW APPLICATIONS – Nil

5. MAJOR DEVELOPMENTS

5.1. DEFERRED APPLICATIONS – Nil

5.2. NEW APPLICATIONS - Nil

6. ANY OTHER BUSINESS

7. NEXT MEETING – TIME/DATE

- 7.1. Thursday, 9 February 2017 in Leigh Street, Adelaide SA

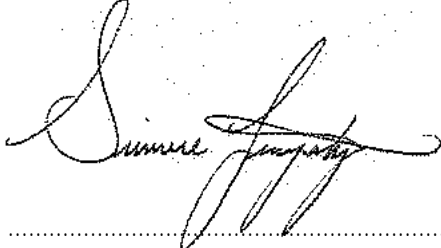
8. CONFIRMATION OF THE MINUTES OF THE MEETING

8.1. **RESOLVED** that the Minutes of this meeting held today be confirmed.

9. MEETING CLOSE

The Presiding Member thanked all in attendance and closed the meeting at 5.00 PM

Confirmed 20/01/2017

A handwritten signature in black ink, appearing to read 'Simone Fogarty', is written over a dotted line. The signature is stylized with a large initial 'S' and a long, sweeping flourish.

Simone Fogarty
PRESIDING MEMBER



Name of Applicant

De Young Jamestown Pty Ltd

Proposal

Proposed kiosk, alfresco bar and dining area, toilet facilities, outdoor dining timber deck, boat pontoon and gangway and associated landscaping and temporary construction fence.

Address

Adelphi Terrace, Glenelg North

DA Number

110/L002/15

TABLE OF CONTENTS

	PAGE NO
AGENDA REPORT	2-18
Development Plan Provisions	19-33
ATTACHMENTS	
1: MAPS & PHOTOS	34-76
2: ZONE MAP	77-78
3: APPLICATION & PLANS	79-113
a. Marshall Day – Acoustic Reports	114-148
4: COUNCIL COMMENTS	149-151
5: AGENCY COMMENTS	152-156
6: REPRESENTATIONS	157-178
7: RESPONSE TO REPRESENTATIONS	179-192





OVERVIEW

Application No	110/L002/15
Unique ID/KNET ID	2015/03970/01
Applicant	De Young Jamestown Pty Ltd
Proposal	Alterations and additions to the Buffalo Restaurant – comprising a new kiosk, alfresco bar and dining area, toilet facilities, outdoor dining timber deck, boat pontoon and gangway and associated landscaping and temporary construction fence.
Subject Land	Adelphi Terrace, Glenelg North The proposed works include or are immediately adjacent to the existing Buffalo Restaurant and Function Centre.
Zone/Policy Area	Glenelg Foreshore and Patawalonga Zone Map HoB/4
Relevant Authority	Development Assessment Commission – Schedule 10(2) – Council development – (b) development that involves the construction of or change in use to a shop or office.
Lodgement Date	10 March 2015
Council	Holdfast Bay Council
Development Plan	Holdfast Bay Development Plan Consolidated 18 December 2014
Type of Development	Merit
Public Notification	Category 3
Representations	11 received (2 wish to be heard)
Referral Agencies	Transport – Safety and Service Division, Minister for Historic Shipwrecks
Report Author	Daniel Pluck – Planning Officer
RECOMMENDATION	Development Plan Consent subject to conditions

EXECUTIVE SUMMARY

The proposal is for alterations and additions to the Buffalo Restaurant – comprising a new kiosk, alfresco bar and dining area, toilet facilities, outdoor dining timber deck, boat pontoon and gangway and associated landscaping and temporary construction fence.

The proposal is within the Glenelg Foreshore and Patawalonga Zone where this type of development being retail and food outlets is envisaged within the zone.

The planning matters assessed include land use and character, design and appearance, landscaping, local heritage, traffic access and parking, flood risk, noise emissions, interface conflicts, waste management, lighting and signage.

The application was referred to the local Council and relevant State Government agencies. The proposal was also notified to adjacent owners and occupiers of land (and to the community more generally) from which a number of representations were received. A range of issues and concerns – such as the potential for amenity impacts – were taken into account in the assessment.

In considering the context of the site, relevant plans and reports, policies in the Development Plan, responses from government agencies, Council and the community, the development displays sufficient merit to warrant consent, subject to one reserved matter and a number of conditions.



ASSESSMENT REPORT

1. BACKGROUND

The HMS Buffalo ship (the Buffalo) is a 'replica' of the original vessel that brought the first free settlers to South Australia in 1836. Located in the iconic tourist area of the Patawalonga foreshore area of Glenelg, the replica vessel has been used a tourist facility and more recently as a licensed restaurant and function area and forms a part of the popular tourist area of Glenelg.

The Buffalo no longer operates as a restaurant and has recently become disused resulting in a facility that does not positively contribute to the character and function of the active foreshore area. Despite the Buffalo car parking area still being used for vehicles visiting the area and passing pedestrian foot traffic there is little activity on the subject land.

Its former owner sold the replica ship to Mr Troy DeYoung of the DeYoung Group who is redeveloping the site. Because the City of Holdfast Bay owns the site where the replica ship is located, Council referred Mr DeYoung's development plans to the State Government's Development Assessment Commission for assessment.

2. DESCRIPTION OF PROPOSAL

Application details are contained in the ATTACHMENTS.

The Development

Alterations and additions to the Buffalo Restaurant – comprising a new kiosk (70m²), alfresco bar (35.62m²) and dining area (270m² approx.), toilet facilities (23.47m²), outdoor dining timber deck (75m²), boat pontoon and gangway, Service Yard (36.18m²) and associated landscaping and temporary construction fence. Specifics are as follows:

Colours and Finishes:

The Buffalo Structure

Painted "BLACK" with "WHITE" details (remains unchanged, as per existing)

Alfresco Dining Area

Perimeter fence - timber posts and beams (natural clear finish to all timber), clear and obscure glazed panels

Proposed Alfresco Bar Structure

Roof - True Oak Corrugated steel roof cladding in "STEALTH" finish (matte black)

Recycled timber posts and beams, timber shutters (natural clear finish to all timber)

Walls - Natural locally sourced stone

Proposed Amenities Building

Roof - True Oak Corrugated steel roof cladding in "STEALTH" finish (matte black)

Recycled timber posts and beams (natural clear finish to all timber)

Walls - Natural locally sourced stone

Proposed Kiosk

Roof - True Oak Corrugated steel roof cladding in Colorbond "BASALT"

Walls - Corrugated steel wall cladding in Colorbond "WINDSPRAY"

Windows - Colorbond "WINDSPRAY"

Posts - Colorbond "BASALT"



Shade Sails - "WHITE"

Signage Detail

1 x Buffalo Entrance Sign comprising a solid sandstone wall and laser cut metal signage panel with led backlighting
3 x signs incorporated into the structure being laser cut metal indicating the buffalo restaurant and kiosk

Hours of Operation

The kiosk portion of the development would be open from 6:00am and 9:00pm 7 days per week.

The alfresco bar and dining area are to be licenced and subject to the existing licensing arrangement for the Buffalo Restaurant. The licensed hours are:

- 5am to 12:00pm midnight Monday to Saturday; and
- 11:00am and 12:00 midnight on Sunday.

3. SITE AND LOCALITY

3.1 Site Description

The site comprises two allotments, described as follows:

Lot No	Street	Suburb	Hundred	Title Reference
1001 in DP49600	Adelphi Tce	Glenelg North	Adelaide and Noarlunga	CT 5935/965
700 in DP55022	Adelphi Tce	Glenelg North	Adelaide and Noarlunga	CT 6154/277

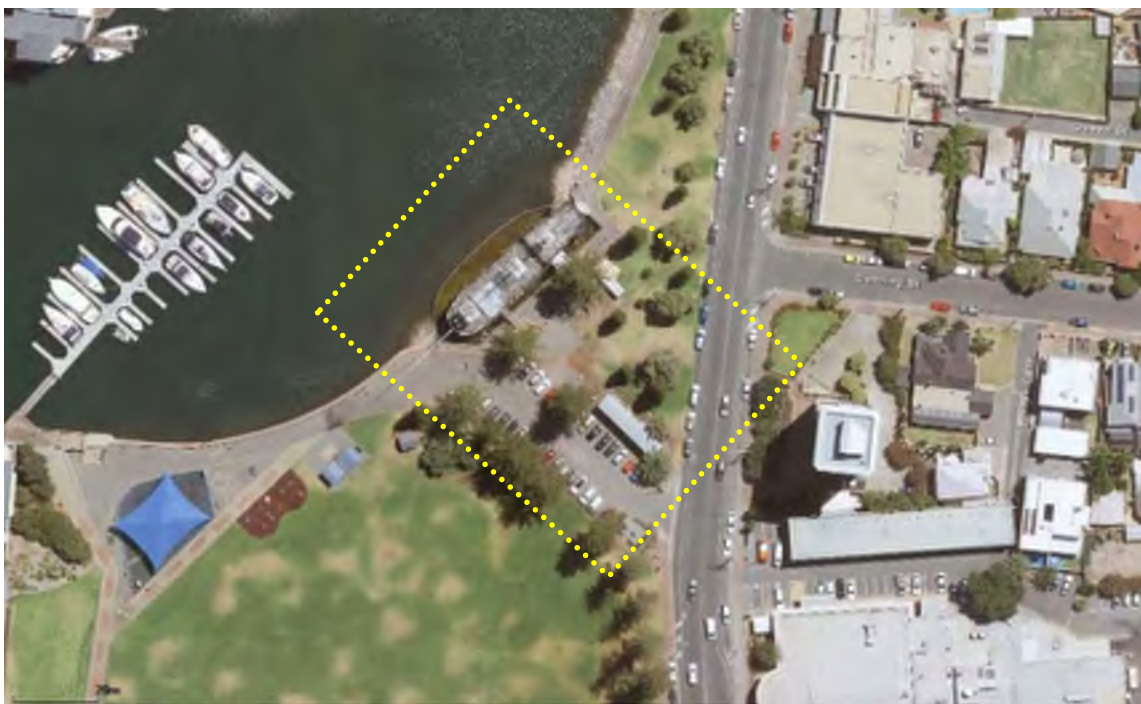
The subject land forms a part of the southern and central portion of the Patawalonga Basin and contains the disused Buffalo replica and restaurant and associated car parking area with a permanently fixed tourist heritage tram, the eastern portion of Wigley Reserve, the foreshore area of the Patawalonga Boat Haven and a portion of the Patawalonga foreshore reserve.

The locality comprises a range of land uses that form the foreshore area and Boat Haven Reserve of Glenelg North. This comprises the Buffalo and its car parking area, the remaining area of Wigley Reserve, the marina development of Holdfast shores, residential properties and motel developments to the east of Adelphi Terrace and the retail and office buildings located to the south of Anzac Highway.

3.2 Locality

The locality is the recreational focal point of Glenelg outside Moseley Square, being the Glenelg beach (between the Glenelg Jetty and breakwater) the entertainment facilities adjacent to the beach and Colley and Wigley Reserves at the Holdfast Shores development, and associated entertainment venues and kiosks

Figure 1 – Location Map



4. COUNCIL COMMENTS

4.1 Holdfast Bay Council

The Holdfast Bay Council provided comment on the proposal. Council are supportive of the proposal however within their response requested that further information be provided in relation to:

- Noise



- Outdoor lighting and safety
- Outdoor advertising
- Coastal risk
- Bicycle parking
- Colours and materials

The applicant has provided a response in relation to this request discussed further in the report.

5. NON STATUTORY AGENCY REPORTS

5.1 DEWNR – Crown Lands

A report was received from Crown Lands on 16 December 2015 stating that the applicant would be required to seek the consent of the Minister for Sustainability, Environment and Conservation prior to any works commencing.

Upon investigation this approval was not required and Crown Lands sent a follow up letter on 15 March 2016 advising that the Ministerial approval was not required – see Attachment 5 for detail.

5.2 Coast Protection Board

A referral to the Coast Protection Board was **not required** as the proposal is adjacent Patawalonga waterway which is not subject to the ebb and flow of the tide due to the controlled entrance gates and therefore not coast.

5.3 Safety and Services Division, DPTI

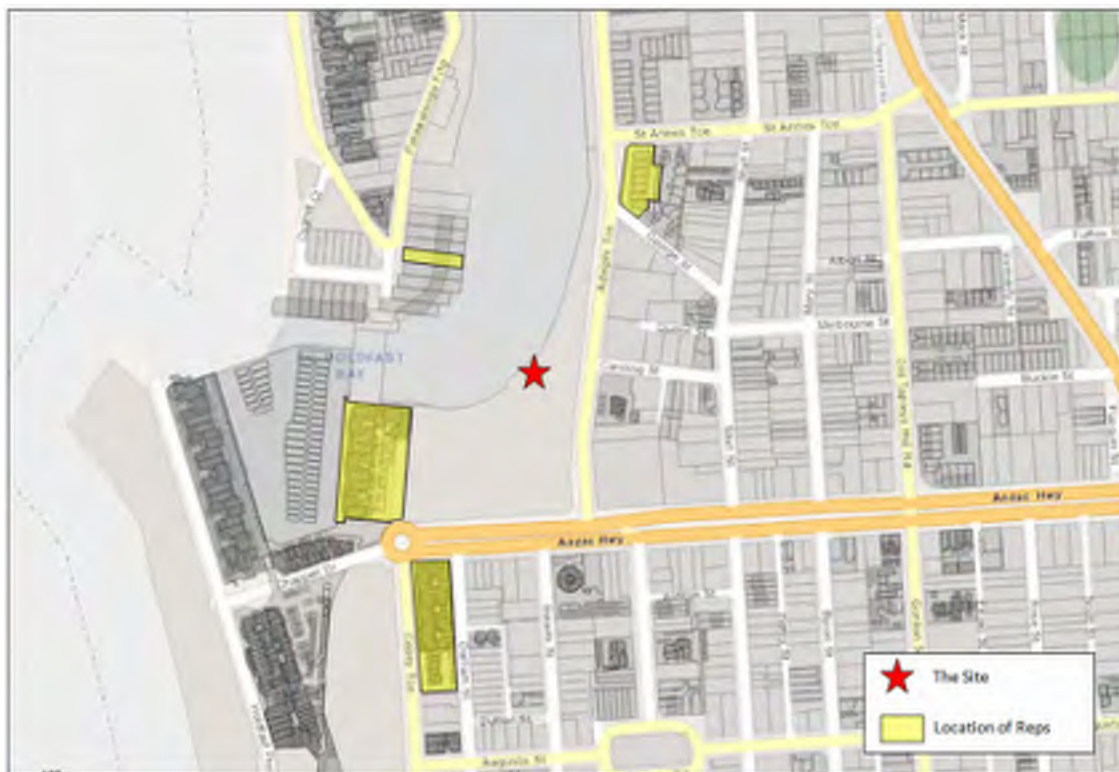
Informal comment was sought from Safety and Service Division in relation to the gangway and pontoon on the Patawalonga and whether this raised concern from a Marine Transport perspective.

In response no objection was raised with a recommended condition ensuring the pontoon is designed in accordance with the provisions of AS 3962-2001 Guidelines for the design of marinas, for the maximum size vessel that will be berthed on the pontoon.

6. PUBLIC NOTIFICATION

The application was notified as a Category 3 development (pursuant to Section 38(2)(c) of the *Development Act 1993* – i.e. neither Category 1 or 2). Public notification was undertaken by directly contacting adjoining owners and occupiers of land, those potentially affected to a significant degree and via public notice in a newspaper with local distribution and 11 representations were received. The following figure illustrates the location of the general location of the representations.

Figure 2 – Representation Received Map



Generally the planning matters of concern were:

- Built Form
- Service of alcohol
- Visual amenity
- Alienation of community reserve land
- Buffalo structure upgrade
- Noise
- Waste management
- Traffic and Access
- Hours of operation
- Car parking
- Charter wharf – noise
- Vandalism

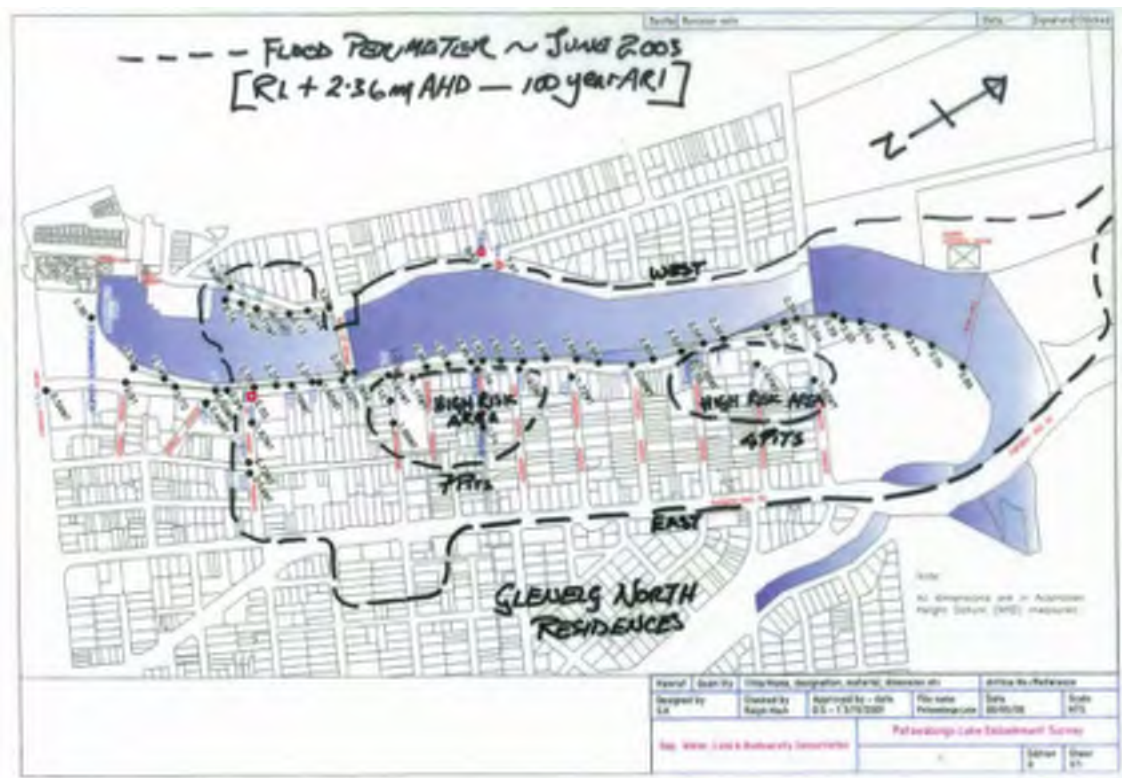
Two representors wish to be heard by the Commission.

The applicant provided advice in response:

Coastal risk

The applicant consulted with Mr Thomas Balacco from O'Donnell Griffin, who provided a copy of the Patawalonga Lake Embankment Survey and made the following comment:

I have attached a copy of the survey that was carried out a few years back. I have also marked the flood areas from the 2003 king tide. According to my records the king tide reached 2.36m AHD which is only supposed to occur once in a hundred years. A significant amount of flood damage to residential areas was due to the Patawalonga Gates not opening to let water from the lake out to sea which was due to a PLC programming parameter which has since been changed.



Noise

Security measures, including the presence of on-site security staff, are to be implemented, professionally monitored and controlled in order to mitigate anti-social and disruptive behaviour on the premises.

No amplified music will be allowed on the new northern deck, excepting soft ambient or background music. This deck will not be accessible to patrons after midnight any day of the week.

Similarly, the bar area to the south of the restaurant will be professionally monitored and controlled. It is envisaged that there will generally be only soft ambient music to this area.

There may be potential also for occasional low volume performances by solo or duo acts, but not for loud amplified full bands.

Service of Alcohol / Hours of Operation

Alcohol will only be able to be purchased or consumed within these two areas prior to midnight any day.

Alcohol will be available only with the provision of food within both the Buffalo internal restaurant area, and within the outdoor dining area, until 2.00am as per the pre-existing liquor licensing conditions.

Visual Amenity



Careful consideration has been given to the design and siting of the proposed additions to the existing development, and the solution has sought to ensure that the height of all new buildings and additions will be well below the level of the existing Buffalo restaurant, and so views from adjoining residential properties will not be unduly compromised.

Further, the proposed additions to the development i.e. kiosk, amenities building, outdoor deck, alfresco dining area and bar structure have been designed, modelled and articulated in order to minimise any perceived bulk, and take on the form of a minor 'human' scale addition relative to the existing Buffalo structure.

Built form

It is not the intention of the proposal to replicate the style, design or colours of the existing structure, but rather implement design features and colours that complement, rather than mimic, those of the existing. To this end natural, earthy, muted and non-reflective materials have been selected. Natural timber and stonework have been used extensively on structures around the immediate area. These materials dull off to a produce a natural, low impact effect.

A grey palette colour has been chosen for the kiosk in order to minimal visual impact, and to provide a visually pleasing transition between the contrasting darker hues of the boat structure and the blue of the adjoining playground and picnic shelters.

Waste Management

Rubbish bins will be visually obscured from public areas in a screened bin enclosure. Waste will be managed and collected on a regular basis by a private waste collection company as required to mitigate odours and pollution.

Buffalo Structure

The interior and exterior of existing Buffalo structure is to be refurbished and reinstated to its former condition. Structural engineering assessment confirms that the structure is generally sound, and any deficiencies are to be brought into compliance to meet current engineering and safety standards.

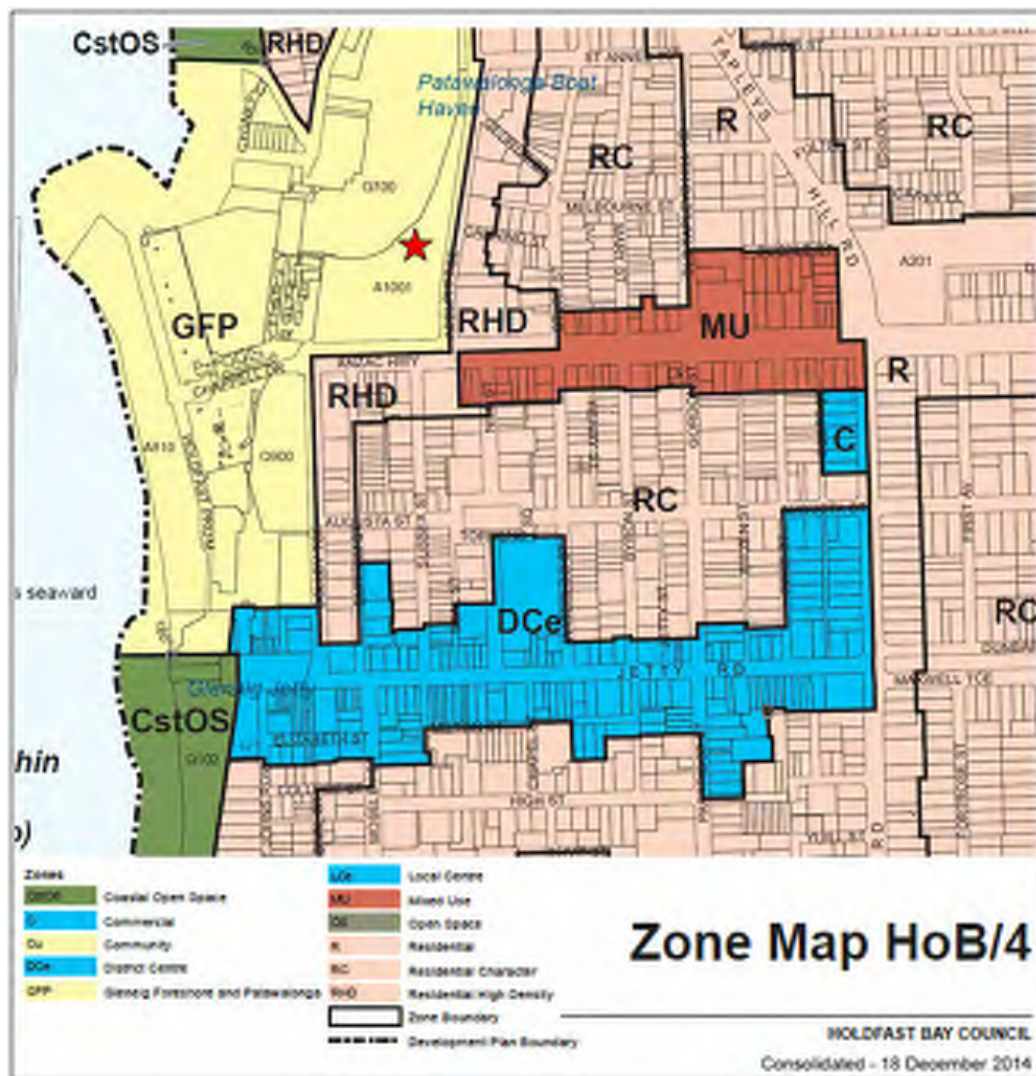
A copy of each representation and the applicant's response is contained in the ATTACHMENTS

7. POLICY OVERVIEW

The subject site is within the Glenelg Foreshore and Patawalonga Zone as described within the Holdfast Bay Development Plan (Consolidated 18 December 2014).

Relevant planning policies are contained in Appendix One and summarised below.

Figure 3 – Zoning Map



7.1 Zone

This zone covers an area along the Glenelg foreshore extending from Moseley Square in the south to the Michael Herbert (Wardlipulthu Waadlakatha) Bridge over the Patawalonga in the north and east to Colley Terrace and Adelphi Terrace. The zone has three main functions which are separated spatially including passive recreational uses, high density residential development, and visitor retail and entertainment uses.

The zone is a major seaside tourist and recreational focus for the City of Holdfast Bay and the State. It contains a variety of land uses including recreational, residential, a variety of tourist accommodation, retail, commercial, family entertainment, community, boating, marina and limited service industrial marine related land uses.

The zone contains the recreational focal point of Glenelg outside Moseley Square, being the Glenelg beach (between the Glenelg Jetty and breakwater) the entertainment facilities adjacent to the beach and Colley Reserve at the Holdfast Shores development, and associated entertainment venues and kiosks. The zone also contains community facilities such as the surf lifesaving club, the Glenelg Community Centre and public car parking underground and integrated with the Marina Pier complex.



Colley and Wigley Reserves are important open space areas within the zone and buildings and structures will enhance the appearance of these reserves (and the Patawalonga) and encourage their use for recreation.

Buildings and structures will be of a high standard of building material, considering the coastal environment and full lifecycle costs for maintenance and replacement, and designed with regard to external appearance, colours, siting and landscaping.

Development will integrate with adjacent public spaces to produce high quality public environments conducive to public activity, including walking and cycling, and having particular regard to personal safety, sunlight access and visual interest.

Retail, restaurant and entertainment uses will be integrated at ground level with public open spaces and pedestrian routes. Public amenities will be incorporated into the design of commercial and community development.

Public spaces will provide visual and tactile interest, the ability to sit or to participate, a sense of vitality and elements of public art.

A network of linked public pathways will provide public pedestrian and bicycle accessibility to and along the coast, reserves, and to key recreation and other facilities, and will be of a practical and usable width and level.

Concept Plan Map HoB/3 Foreshore and Patawalonga designate this site as part of the Patawalonga Basin for marina berths, integrated mixed use developments. Focal point for tourists, water activities, ferry landing and charter craft.

8. PLANNING ASSESSMENT

The application has been assessed against the relevant provisions of the Holdfast Bay Development Plan, which are contained in Appendix One.

8.1 Land Use and Character

The Development Plan outlines that the Glenelg Foreshore and Patawalonga Zone should be developed as a major tourist and recreational focus for Glenelg and the State. Such development includes family entertainment facilities, amusements, a range of tourist accommodation facilities (including a hotel and apartments), residential housing, marinas, ferry terminal and associated facilities, public plazas, and complementary retail and food outlets.

Colley and Wigley Reserves are important open space areas within the zone and buildings and structures should enhance the appearance of these reserves (and the Patawalonga) and encourage their use for recreation.

A retail and food outlet on the southern side of the Patawalonga Basin is envisaged.

This proposal would provide complementary retail and food outlets that would enhance and activate the Buffalo structure and surrounding recreational areas. Providing retail facilities in this location would directly serve the general public and tourists and is seen as a desirable activity.

I therefore consider that the proposed development of an associated retail and food outlet in this location to be consistent with the intent of the development plan and its desired future character.



8.2 Design and Appearance

In regard to the design and appearance of new development, planning policies seeks buildings and structures of a high standard (both in terms of materiality and durability), considering the coastal environment and full lifecycle costs for maintenance and replacement, and sympathetically designed with regard to external appearance, colours, siting and landscaping.

Furthermore, retail, restaurant and entertainment uses will be integrated at ground level with public open spaces and pedestrian routes. Public amenities will be incorporated into the design of commercial and community development.

Information has been provided on the colours and materiality of the proposal. The application documentation states:

It is not the intention to replicate the style, design or colours of the existing structure, but rather implement design features and colours that complement, rather than mimic, those of the existing. To this end natural, earthy, muted and non-reflective materials have been implemented. Natural timber and stonework have been used extensively on structures around the immediate area. These materials dull off to produce a natural, low impact effect.

A grey palette of colours has been chosen for the kiosk in order to ensure minimal visual impact, and to provide a visually pleasing transition between the black of the boat structure and the blue of the adjoining playground and picnic shelters.

Revolution Roofing 'Stealth' has been chosen for the roof colour of the amenities building and bar, which is a non-reflective matt black colour, which will complement the 'black' colour of the existing boat structure.

I consider the selected colours and materials are suitable and would integrate suitably with the surrounding environment and when set against the back drop of the Buffalo structure. The proposed structures are low scale and of a suitable colour to suitably integrate into the surrounding area with public amenities are provided as part of the proposal.

The proposal accords with the requirements of the Development Plan in regard to design and appearance.

Landscaping

Landscaping detail has been provided for the outdoor bar area which includes blue flax lily (Australian Native) and graceful bamboo. Both of these have been selected to provide screening at both a low and higher level to screen the outdoor bar.

In addition, detail has been provided for the 'moat' area surrounding the Buffalo structure. The landscaping indicates a moss rock style garden to include selected coastal and native plants and grasses yet to be determined. In addition large palm trees and water features are proposed.

For both landscaping concept plans, it is recommended that a landscape architect be engaged to provide a final landscaping plan, incorporating suitable native species tolerant of a coastal environment. A condition has been recommended.

8.3 Heritage



The Development Plan designates Wigley Reserve as a Local Heritage Item. The Council advice to the Commission does not raise any issues in relation to heritage adjacency or impact to the listed values of the reserve area.

8.4 Traffic Impact, Access and Parking

An existing 22-space carpark already services the Buffalo restaurant. Currently the parking is marked as "Buffalo Restaurant Carpark Parking Area" in anticipation of the development progressing.

Table HoB/1 – Off Street Vehicle Parking Requirements (other than where located within the District Centre Zone Glenelg Policy Area 2) seeks the provision of car parking at the following rates for a restaurant:

1 per 10 square metres of total floor area, or 1 for every 3 seats provided or able to be provided, whichever provides the greater number, together with additional car parking spaces if food is able to be taken away from the premises, to ensure that all car parking occurs on the site of the development.

Based on its existing provision and the reservation of adjoining land for recreational purposes, it is unlikely that additional off-street parking that could exclusively service the development could be provided at the level stipulated in the Development Plan which would equate to 35 car parks based on the total floor area of restaurant space (22 dedicated parks provided). However, the Liquor licence already provides for an overall licence capacity for the site to be 250 persons. This would not change as a result of this development. Therefore the operational impact of the development is already established. So whilst the parking may not be provided at the level anticipated by the Plan (as if it were a new development), the existing arrangements (including off-street, on-street and public carparking) do provide a sufficient level of carparking for the intended uses.

It is also noted that given the close physical proximity of residential premises and tourist accommodation, and the availability of shared use paths, patrons of the establishment will have a readily available option of walking and/or riding.

In relation to access to the development from the water, Transport SA has provided guidance stating that the design and installation of the gangway and pontoon will need to meet relevant design standards and regulations – including the maximum size of vessel that can be berthed on the pontoon (refer *AS 3962-2001 Guidelines for the design of marina*).

PDC2 (CW: Marinas and Maritime Structures) states that -

The design of marinas, berths, channels, fairways, gangways and floating structures should comply with:

- (a) Australian Standard AS 3962: Guidelines for Design of Marinas*
- (b) Australian Standard AS 4997: Guidelines for the Design of Maritime Structures.*

Further information in relation to the gangway and pontoon have been provided to indicate that the structures can meet these requirements. However, this could only be confirmed with the preparation and submission of more detailed design specifications, and which can account for low-water levels in the Patawalonga (i.e. where water levels do not meet the development site). A reserve matter has been recommended to ensure the pontoon and gangway design is compliant.

8.5 Environmental Factors



8.5.1 Flood Risk

The Development Plan states that development should not occur on land where the risk of flooding is likely to be harmful to personal safety or damage property. Where flood protection measures are not provided for development situated on low-lying or flood affected land, development should be at least 0.25 metres above the minimum 1-in-100 year average return interval flood.

In June 2003 Glenelg was subject to a king tide, in addition to this the Patawalonga Gates failed to open causing a 1-100 year flood. According to details provided by O'Donnell Griffin, an engineering firm, the flood level reached 2.36m AHD. The proposed development was not within this flood affected area, and site levels have been provided showing that the site of the development is above the 2.36m AHD. Based on the engineering advice and site level survey provided, I am satisfied that the site is above the 1-100 year flood level and therefore accords with the policy

8.5.2 Noise Emissions/Interface

The control of external noise impacts and the protection of residential amenity is a key outcome sought by planning policies. The potential for such impacts are considered in the general section of the Development Plan. Development that emits noise (other than music noise) should include noise attenuation measures that achieve the relevant Environment Protection (Noise) Policy criteria when assessed at the nearest existing noise sensitive premises.

Marshall Day Acoustics has provided a noise assessment report outlining the acoustic treatment to be applied to the development to ensure it meets with the relevant Environmental Protection (Noise) Policy criteria.

The original noise report (dated 23 September 2016) recommended acoustic treatments which sought the installation of a 4 metre canopy in addition to a 3.2 metre high acoustic wall on the upper deck function space. No materials were identified, whilst the structural additions could have negatively impacted on the overall development.

These concerns were relayed to the applicant who re-engaged Marshall Day to provide more detail on both the acoustic treatments and the materiality of the treatments. An amended report was provided on 14 October 2016.

The report provides more context on the materiality of the acoustic treatment being either 9mm thick fibre cement sheeting, 12mm thick Perspex, 6mm thick toughened glass. I would recommend either Perspex or toughened glass be used to form the barriers for this proposal.

Amended treatments involve generally reducing the height and scale of the treatments whilst still achieving (for the majority) the relevant Environment Protection (Noise) Policy criteria. The updated report has removed the 4 metre canopy and reduced the height of the 3.2 metre high acoustic wall to 2.4 metres on the upper deck space.

The proposed noise attenuation measures provide a suitable balance between the provision of adequate and suitable noise attenuation whilst not eroding the established character and amenity of the locality.

8.5.3 Waste Management



Development should include appropriately sized areas to facilitate the storage of bins that enable the efficient recycling of waste.

The development would include a service yard in which rubbish bins would be stored behind a screened bin enclosure. Waste would be managed and collected on a regular basis by a private waste collection company as required to mitigate odours and pollution.

Given the screening and management of waste proposed, it is unlikely the proposal would impact upon the surrounding amenity from a waste perspective.

8.5.4 Lighting

Development, including car park facilities should incorporate signage and lighting that indicate the entrances and pathways to, from and within sites.

The proposal includes the installation of light poles which would be located along the edge of the walkway adjacent to the southern outdoor area. There is also a light pole adjacent to the parking station, and floodlighting on the existing tram shelter to illuminate the car park. It is unlikely this lighting would impact residential development across the Patawalonga as it is shielded by the Buffalo structure.

The provision of public lighting for the development is considered to be appropriate and necessary from an access and crime prevention perspective. I consider the lighting accords with the provisions of the development plan.

8.6 Signage

The signage proposed includes a main entrance sign made of a solid sandstone wall with an attached laser cut metal panel displaying "the buffalo". The entire sign is 5.2 metres long and up to 1.6m high. Three other signs are identified on the concept plan, two for the buffalo restaurant and one for the kiosk. All of the signage proposed are made from the laser cut metal as outlined in the concept plans.

The development plan states that outdoor advertising displays should be designed to provide clearly visible property and business identification without dominating the appearance of the site upon which it is located. They should also be specifically designed to have an overall co-ordinated appearance with all other advertisements associated with the building or site.

I consider that the signage proposed is unlikely to dominate the streetscape as it achieve a proportionate scale with the Buffalo development and adjacent structures, such as the decommissioned tram carriage. All of the signage proposed is of similar materiality and therefore displays a consistent approach throughout the development. I consider the signage to be suitable when assessed against the development plan provisions for advertising.

9. CONCLUSION

Overall the development displays significant merit when assessed against the relevant provisions of the development plan. The following is appropriate:

- Land use and character – the development provides or expands upon uses envisaged for the zone and further activates this space;



- Design and Appearance – the selected colours and materials are consistent with the coastal location and would complement the existing Buffalo structure;
- Heritage – No objections were raised from a local heritage perspective;
- Traffic impact, access and parking – with the availability of off-street, on-street and council parking areas will allow the provision of sufficient carparking (noting that many patrons will choose to walk or ride to the development).
- Flood Risk – flood mapping and site levels indicate the risk is minimal.
- Waste Management – storage bins would be obscured from view and rubbish collected on a regular basis;
- Signage – size, location, materiality and number is appropriate;
- Lighting – public and background lighting ensures appropriate safety and surveillance.
- Noise – noise mitigation measures will minimise external impacts whilst not significantly and detrimentally impacting upon the amenity of the surrounding area.

Portions of the development, whilst suitable need further refinement:

- Landscaping – a final landscaping plan be prepared by a landscape architect. The planting plan should include suitable native species tolerant to the environment in which they would be planted.
- Gangway and Pontoon – provision of a final design to ensure the pontoon and gangway meet relevant standards. A reserve matter is recommended.

Overall, and in considering the context of the site, relevant plans and reports, policies in the Development Plan, responses from government agencies, Council and the community, the development displays sufficient merit to warrant consent, subject to one reserved matter and a number of conditions.

The proposal is not seriously at variance with the Development Plan and I recommend conditional Development Plan Consent.

10. RECOMMENDATION

It is recommended that the Development Assessment Commission:

- 1) RESOLVE that the proposed development is NOT seriously at variance with the policies in the Development Plan.
- 2) RESOLVE that the Development Assessment Commission is satisfied that the proposal meets the key objectives of the Glenelg Foreshore and Patawalonga Zone.
- 3) RESOLVE to grant Development Plan Consent to the proposal by De Young Jamestown Pty Ltd for Proposed kiosk, alfresco bar and dining area, toilet facilities, outdoor dining timber deck, boat pontoon and gangway and associated landscaping and temporary construction fence at Adelphi Terrace, Glenelg North (being Allotment 1001, DP49600: Certificate of Title: Volume 5935 Folio 965; and part Piece 700, DP55022: Certificate of Title: Volume 6154 Folio 277) subject to the following reserved matters and conditions of consent.

RESERVED MATTER

Pursuant to Section 33 (3) of the *Development Act 1993*, the following matters shall be reserved for further assessment, to the satisfaction of the Development Assessment Commission, prior to the granting of Development Approval:



1. A final design and specification be provided to ensure the pontoon and gangway are consistent with relevant guidelines in the Development Plan being:

- (a) Australian Standard AS 3962: Guidelines for Design of Marinas
- (b) Australian Standard AS 4997: Guidelines for the Design of Maritime Structures.

to the satisfaction of the Development Assessment Commission.

PLANNING CONDITIONS

1. Except where minor amendments may be required by other relevant Acts, or by conditions imposed by this application, the development shall be established in strict accordance with the details and following plans submitted in Development Application No 110/L002/15.

Stuart J. Patrick Plans

- Plan Set Reference - ADI/GLG/1001 – Site Plans - Sheets 1 to 9 – October 2015
- Plan - ADI/GLG – Carpark refurbishment - Sheets 1 of 1 – May 2015
- Plan Set Reference - ADI/GLG – Concept Plans - Sheets 1 to 6 – January 2015
- Plan Set Reference– ADI/GLG/1001 Signage/landscaping – Sheets 1 & 2 – June 2015

Other

- Marshal Day Acoustics – Buffalo Redevelopment Noise Assessment - Rp 001 R02 2016306AL - 14 October 2016
2. The acoustic measures recommended on pages 9 and 17 of the 'Marshal Day Acoustics – Buffalo Redevelopment Noise Assessment - Rp 001 R02 2016306AL - 14 October 2016' shall be implemented prior to commencement of the use herein approved on the site.
 3. That all vehicle car parks, driveways and vehicle entry and manoeuvring areas shall be designed and constructed in accordance with the relevant Australian Standards and be constructed, drained and paved with bitumen, concrete or paving bricks in accordance with sound engineering practice and appropriately line marked to the reasonable satisfaction of the Development Assessment Commission prior to the occupation or use of the development.
 4. That the applicant shall submit a detailed landscaping plan for approval by the Development Assessment Commission prior to the commencement of site works. The planting plan should include suitable native species tolerant to the environment in which they would be planted. The landscaping shown on that approved plan shall be established prior to the operation of the development and shall be maintained and nurtured at all times with any diseased or dying plants being replaced.
 5. That the development and the site shall be maintained in a serviceable condition and operated in an orderly and tidy manner at all times.
 6. That all trade waste and other rubbish shall be stored in covered containers prior to removal and shall be kept screened from public view.
 7. Graffiti shall be removed within five (5) business days of the graffiti becoming known or visible with the timely removal of graffiti being the responsibility of the operators of the development.



8. That all external lighting of the site, including car parking areas and buildings, shall be designed and constructed to conform with Australian Standards and must be located, directed and shielded and of such limited intensity that no nuisance or loss of amenity is caused to any person beyond the site.
9. That all stormwater design and construction shall be in accordance with Australian Standards and recognised engineering best practices to ensure that stormwater does not adversely affect any adjoining property or public road.

ADVISORY NOTES

- a. The development must be substantially commenced within 12 months of the date of this Notification, unless this period has been extended by the Development Assessment Commission.
- b. The authorisation will lapse if not commenced within 12 months of the date of this Notification.
- c. The applicant is also advised that any act or work authorised or required by this Notification must be completed within 3 years of the date of the Notification unless this period is extended by the Commission.
- d. The applicant has a right of appeal against the conditions which have been imposed on this Development Plan Consent or Development Approval.
- e. Such an appeal must be lodged at the Environment, Resources and Development Court within two months from the day of receiving this notice or such longer time as the Court may allow.
- f. The applicant is asked to contact the Court if wishing to appeal. The Court is located in the Sir Samuel Way Building, Victoria Square, Adelaide, (telephone number 8204 0289).

A handwritten signature in blue ink, appearing to read 'Daniel Pluck', is positioned above the printed name.

**DANIEL PLUCK
PLANNING OFFICER
DEVELOPMENT DIVISION
DEPARTMENT OF PLANNING, TRANSPORT and INFRASTRUCTURE**



Development Plan Provisions

Glenelg Foreshore and Patawalonga Zone

OBJECTIVES

- 1** A major tourist and recreational focus for Glenelg and the State including family entertainment facilities, amusements, a range of tourist accommodation facilities including a hotel and apartments, residential housing, marinas, ferry terminal and associated facilities, public plazas, and complementary retail and food outlets.
- 2** A range of tourism and accommodation facilities and activities associated with the Glenelg foreshore and environs.
- 3** Development that is integrated in function and complements the character of adjacent zones.
- 4** Retention of Colley and Wigley Reserves as focal public open space areas, maintained and landscaped for general public recreation.
- 5** Retention of Colley and Wigley Reserves and Patawalonga Recreation and Boat Haven Reserve as the focus of recreational activities.
- 6** Development of the Patawalonga Recreation and Boat Haven Reserve as an area for mooring, incorporating marinas, and use of boats with limited areas for the servicing and repairing of boats.
- 7** Conservation and improvement of the landscape features of the Patawalonga and the coast.
- 8** Development that contributes to the desired character of the zone.
- 9** Maintain the Patawalonga Recreation and Boat Haven Reserve as an area for mooring (incorporating marinas) and use of boats with limited areas for servicing and repairing of boats.

DESIRED CHARACTER

This zone covers an area along the Glenelg foreshore extending from Moseley Square in the south to the Michael Herbert (Wardlipulthu Waadlakatha) Bridge over the Patawalonga in the north and east to Colley Terrace and Adelphi Terrace. The zone has three main functions which are separated spatially including passive recreational uses, high density residential development, and visitor retail and entertainment uses.

The zone is a major seaside tourist and recreational focus for the City of Holdfast Bay and the State. It contains a variety of land uses including recreational, residential, a variety of tourist accommodation, retail, commercial, family entertainment, community, boating, marina and limited service industrial marine related land uses.

The zone contains the recreational focal point of Glenelg outside Moseley Square, being the Glenelg beach (between the Glenelg Jetty and breakwater) the entertainment facilities adjacent to the beach and Colley Reserve at the Holdfast Shores development, and associated entertainment venues and kiosks. The zone also contains community facilities such as the surf lifesaving club, the Glenelg Community Centre and public car parking underground and integrated with the Marina Pier complex.

New development should also have regard to the Glenelg Foreshore and Patawalonga Zone provisions as detailed in the Land Not Within a Council Area (Metropolitan) Development Plan and shown on Map LNWCA (MA)/4 and on Concept Plan Map HoB/5 Extent - Foreshore and Patawalonga Zone.

Colley and Wigley Reserves are important open space areas within the zone and buildings and structures will enhance the appearance of these reserves (and the Patawalonga) and encourage their use for recreation.



Buildings and structures will be of a high standard of building material, considering the coastal environment and full lifecycle costs for maintenance and replacement, and designed with regard to external appearance, colours, siting and landscaping.

Development will integrate with adjacent public spaces to produce high quality public environments conducive to public activity, including walking and cycling, and having particular regard to personal safety, sunlight access and visual interest.

Retail, restaurant and entertainment uses will be integrated at ground level with public open spaces and pedestrian routes. Public amenities will be incorporated into the design of commercial and community development.

Public spaces will provide visual and tactile interest, the ability to sit or to participate, a sense of vitality and elements of public art.

A network of linked public pathways will provide public pedestrian and bicycle accessibility to and along the coast, reserves, and to key recreation and other facilities, and will be of a practical and usable width and level.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

1 The following forms of development are envisaged in the zone:

- family entertainment facilities and amusements
- ferry terminal and associated facilities
- marina
- public plaza
- residential development
- retail and food outlets on the southern side of the Patawalonga Basin
- tourist accommodation, such as hotel and apartment.

Form and Character

4 Development should not be undertaken unless it is consistent with the desired character for the zone.

5 Development should be undertaken in accordance with Concept Plan Map HoB/3 - Foreshore and Patawalonga and Concept Plan Map HoB/5 - Extent - Foreshore and Patawalonga Zone.

9 Development should not restrict public access to or along the beach, or reserve area frontages.

12 Shopping, commercial, entertainment, marine related and transport uses should have a floor space commensurate with catering for the reasonable needs of tourists, visitors, the boating community, and residents within the zone, and should complement the services and facilities along Jetty Road and Anzac Highway.

13 Retail, restaurant and entertainment uses should be integrated at ground level with public open spaces and pedestrian routes, and may extend in to these areas.

15 The appearance of land, buildings, fencing and other structures should be of contemporary design, but not excessively ornate, with empathy for the traditional design elements of the locality and a style complementary to the coastal/seaside environment.

General Section

OBJECTIVES

1 Urban landscapes that are not disfigured by advertisements and/or advertising hoardings.

2 Advertisements and/or advertising hoardings that do not create a hazard.

3 Advertisements and/or advertising hoardings designed to enhance the appearance of the building and locality.



4 Outdoor advertising displays that are designed to provide clearly visible property and business identification without dominating the appearance of the site upon which it is located or the streetscape.

5 Outdoor advertising displays that are specifically designed to have an overall co-ordinated appearance with all other advertisements associated with the building or site.

PRINCIPLES OF DEVELOPMENT CONTROL

1 The location, siting, design, materials, size, and shape of advertisements and/or advertising hoardings should be:

- (a) consistent with the predominant character of the urban or rural landscape
- (b) in harmony with any buildings or sites of historic significance or heritage value in the area
- (c) coordinated with and complement the architectural form and design of the building they are to be located on.

2 The number of advertisements and/or advertising hoardings associated with a development should be minimised to avoid:

- (a) clutter
- (b) disorder
- (c) untidiness of buildings and their surrounds
- (d) driver distraction.

4 The content of advertisements should be limited to information relating to the legitimate use of the associated land.

5 Advertisements and/or advertising hoardings should:

- (a) be completely contained within the boundaries of the subject allotment
- (b) be sited to avoid damage to, or pruning or lopping of, on-site landscaping or street trees
- (c) not obscure views to vistas or objects of high amenity value.

6 Advertisements and/or advertising hoardings should not be erected on:

- (a) a public footpath or veranda post
- (b) a road, median strip or traffic island
- (c) a vehicle adapted and exhibited primarily as an advertisement
- (d) residential land.

7 Advertisements and/or advertising hoardings attached to buildings should not be sited on the roof or higher than the walls of a building, the advertisement or advertising hoarding is appropriately designed to form an integrated and complementary extension of the existing building.

10 Advertisements should convey the owner/occupier and/or generic type of business, merchandise or services using simple, clear and concise language, symbols, print style and layout and a small number of colours.

11 Advertisements which perform a secondary role in identifying the business, goods or services should only be readable in the immediate vicinity of the site.

Safety

12 Advertisements and/or advertising hoardings should not create a hazard by:

- (a) being so highly illuminated as to cause discomfort to an approaching driver, or to create difficulty in the driver's perception of the road or persons or objects on the road
- (b) being liable to interpretation by drivers as an official traffic sign, or convey to drivers information that might be confused with instructions given by traffic signals or other control devices, or impair the conspicuous nature of traffic signs or signals
- (c) distracting drivers from the primary driving task at a location especially where the demands on driver concentration are high
- (d) obscuring a driver's view of other road or rail vehicles at/or approaching level crossings, or of pedestrians or of features of the road that are potentially hazardous (eg junctions, bends, changes in width, traffic control devices).

13 Any internally illuminated advertising signs and/or advertising hoardings which utilise LED, LCD or other similar technologies should be located a minimum of 80 metres from traffic signals, level crossings and other important traffic control devices.



Freestanding Advertisements

14 Freestanding advertisements and/or advertising hoardings should be:

- (a) limited to only one primary advertisement per site or complex, except where a site has multiple road frontages, in which case, a maximum of one freestanding advertising hoarding per road frontage
- (b) of a scale and size in keeping with the desired character of the locality and compatible with the development on the site.

Crime Prevention

OBJECTIVES

1 A safe, secure, crime resistant environment where land uses are integrated and designed to facilitate community surveillance.

PRINCIPLES OF DEVELOPMENT CONTROL

1 Development should be designed to maximise surveillance of public spaces through the incorporation of clear lines of sight, appropriate lighting and the use of visible permeable barriers wherever practicable.

2 Buildings should be designed to overlook public and communal streets and public open space to allow casual surveillance.

3 Development should provide a robust environment that is resistant to vandalism and graffiti.

4 Development should provide lighting in frequently used public spaces including those:

- (a) along dedicated cyclist and pedestrian pathways, laneways and access routes
- (b) around public facilities such as toilets, telephones, bus stops, seating, litter bins, automatic teller machines, taxi ranks and car parks.

5 Development, including car park facilities should incorporate signage and lighting that indicate the entrances and pathways to, from and within sites.

6 Landscaping should be used to assist in discouraging crime by:

- (a) screen planting areas susceptible to vandalism
- (b) planting trees or ground covers, rather than shrubs, alongside footpaths
- (c) planting vegetation other than ground covers a minimum distance of two metres from footpaths to reduce concealment opportunities.

7 Site planning, buildings, fences, landscaping and other features should clearly differentiate public, communal and private areas.

8 Buildings should be designed to minimise and discourage access between roofs, balconies and windows of adjoining dwellings.

9 Public toilets should be located, sited and designed:

- (a) to promote the visibility of people entering and exiting the facility (eg by avoiding recessed entrances and dense shrubbery that obstructs passive surveillance)
- (b) near public and community transport links and pedestrian and cyclist networks to maximise visibility.

10 Development should avoid pedestrian entrapment spots and movement predictors (eg routes or paths that are predictable or unchangeable and offer no choice to pedestrians).

Design and Appearance

OBJECTIVES

1 Development of a high architectural standard that responds to and reinforces positive aspects of the local environment and built form.

2 Roads, open spaces, buildings and land uses laid out and linked so that they are easy to understand and navigate.

PRINCIPLES OF DEVELOPMENT CONTROL

1 The design of a building may be of a contemporary nature and exhibit an innovative style provided the overall form is sympathetic to the scale of development in the locality and with the context of its setting with regard to shape, size, materials and colour.

2 Buildings should be designed and sited to avoid creating extensive areas of uninterrupted walling facing areas exposed to public view.



3 Buildings should be designed to reduce their visual bulk and provide visual interest through design elements such as:

- (a) articulation
- (b) colour and detailing
- (c) small vertical and horizontal components
- (d) design and placing of windows
- (e) variations to facades.

5 Building form should not unreasonably restrict existing views available from neighbouring properties and public spaces.

7 The external walls and roofs of buildings should not incorporate highly reflective materials which will result in glare to neighbouring properties or drivers.

8 Structures located on the roofs of buildings to house plant and equipment should form an integral part of the building design in relation to external finishes, shaping and colours.

9 Building design should emphasise pedestrian entry points to provide perceptible and direct access from public street frontages and vehicle parking areas.

10 Development should provide clearly recognisable links to adjoining areas and facilities.

11 Buildings, landscaping, paving and signage should have a coordinated appearance that maintains and enhances the visual attractiveness of the locality.

12 Buildings (other than ancillary buildings or group dwellings) should be designed so that their main façade faces the primary street frontage of the land on which they are situated.

14 Development should be designed and sited so that outdoor storage, loading and service areas are screened from public view by an appropriate combination of built form, solid fencing and/or landscaping.

15 Outdoor lighting should not result in light spillage on adjacent land.

Building Setbacks from Road Boundaries

17 The setback of development from public roads should:

- (a) be similar to, or compatible with, setbacks of buildings on adjoining land and other buildings in the locality
- (b) contribute positively to the streetscape character of the locality
- (c) not result in or contribute to a detrimental impact upon the function, appearance or character of the locality.

Hazards

OBJECTIVES

1 Maintenance of the natural environment and systems by limiting development in areas susceptible to natural hazard risk.

4 Development located and designed to minimise the risks to safety and property from flooding.

PRINCIPLES OF DEVELOPMENT CONTROL

Flooding

4 Development should not occur on land where the risk of flooding is likely to be harmful to safety or damage property.

5 Development should not be undertaken in areas liable to inundation by tidal, drainage or flood waters unless the development can achieve all of the following:

- (a) it is developed with a public stormwater system capable of catering for a 1-in-100 year average return interval flood event
- (b) buildings are designed and constructed to prevent the entry of floodwaters in a 1-in-100 year average return interval flood event.

Interface between Land Uses

OBJECTIVES



- 1 Development located and designed to prevent adverse impact and conflict between land uses.
- 2 Protect community health and amenity and support the operation of all desired land uses.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:
 - (a) the emission of effluent, odour, smoke, fumes, dust or other airborne pollutants
 - (b) noise
 - (c) vibration
 - (d) electrical interference
 - (e) light spill
 - (f) glare
 - (g) hours of operation
 - (h) traffic impacts.
- 2 Development should be sited and designed to minimise negative impact on existing and potential future land uses considered appropriate in the locality.
- 3 Development adjacent to a Residential Zone should be designed to minimise overlooking and overshadowing of nearby residential properties.
- 6 Non-residential development on land abutting a residential zone should be designed to minimise noise impacts to achieve adequate levels of compatibility between existing and proposed uses.

Noise Generating Activities

- 7 Development that emits noise (other than music noise) should include noise attenuation measures that achieve the relevant Environment Protection (Noise) Policy criteria when assessed at the nearest existing noise sensitive premises.
- 9 Outdoor areas (such as beer gardens or dining areas) associated with licensed premises should be designed or sited to minimise adverse noise impacts on adjacent existing or future noise sensitive development.
- 10 Development proposing music should include noise attenuation measures that achieve the following desired noise levels:

Noise level assessment location	Desired noise level
Adjacent existing noise sensitive development property boundary	<p>Less than 8 dB above the level of background noise ($L_{Aeq,15min}$) in any octave band of the sound spectrum</p> <p>and</p> <p>Less than 5 dB(A) above the level of background noise ($L_{Aeq,15min}$) for the overall (sum of all octave bands) A-weighted level</p>
Adjacent land property boundary	<p>Less than 65dB(Lin) at 63Hz and 70dB(Lin) in all other octave bands of the sound spectrum</p> <p>or</p> <p>Less than 8 dB above the level of background noise ($L_{Aeq,15min}$) in any octave band of the sound spectrum and 5 dB(A) overall (sum of all octave bands) A-weighted level</p>

Air Quality

- 11 Development with the potential to emit harmful or nuisance-generating air pollution should incorporate air pollution control measures to prevent harm to human health or unreasonable interference with the amenity of sensitive uses within the locality.
- 12 Chimneys or exhaust flues associated with commercial development (including cafes, restaurants and fast food outlets) should be designed to ensure they do not cause a nuisance or health concerns to nearby sensitive receivers by:



- (a) incorporating appropriate treatment technology before exhaust emissions are released to the atmosphere
- (b) ensuring that the location and design of chimneys or exhaust flues maximises dispersion and takes into account the location of nearby sensitive uses.

Landscaping, Fences and Walls

OBJECTIVES

- 1** The amenity of land and development enhanced with appropriate planting and other landscaping works, using locally indigenous plant species where possible.
- 2** Functional fences and walls that enhance the attractiveness of development.

PRINCIPLES OF DEVELOPMENT CONTROL

1 Development should incorporate open space and landscaping and minimise hard paved surfaces in order to:

- (a) complement built form and reduce the visual impact of larger buildings (eg taller and broader plantings against taller and bulkier building components)
- (b) enhance the appearance of road frontages
- (c) screen service yards, loading areas and outdoor storage areas
- (d) minimise maintenance and watering requirements
- (e) enhance and define outdoor spaces, including car parking areas
- (f) maximise shade and shelter
- (g) assist in climate control within and around buildings
- (h) minimise heat absorption and reflection
- (i) maintain privacy
- (j) maximise stormwater re-use
- (k) complement existing vegetation, including native vegetation
- (l) contribute to the viability of ecosystems and species
- (m) promote water and biodiversity conservation
- (n) establish buffers to adjacent development and areas.

2 Landscaping should:

- (a) include mature vegetation, the planting of locally indigenous species where appropriate and species tolerant of salt-laden winds near the coast
- (b) be oriented towards the street frontage
- (c) result in the appropriate clearance from powerlines and other infrastructure being maintained.

3 Landscaping should not:

- (a) unreasonably restrict solar access to adjoining development
- (b) cause damage to buildings, paths and other landscaping from root invasion, soil disturbance or plant overcrowding
- (c) introduce pest plants
- (d) increase the risk of bushfire
- (e) remove opportunities for passive surveillance
- (f) increase leaf fall in watercourses
- (g) increase the risk of weed invasion
- (h) obscure driver sight lines
- (i) create a hazard for train or tram drivers by obscuring sight lines at crossovers.

4 Existing substantial vegetation should be retained and incorporated within landscaping of new development where practicable.

5 Fences and walls, including retaining walls, should:

- (a) not result in damage to neighbouring trees
- (b) be compatible with the associated development and with existing predominant, attractive fences and walls in the locality
- (c) enable some visibility of buildings from and to the street to enhance safety and allow casual surveillance
- (d) incorporate articulation or other detailing where there is a large expanse of wall facing the street
- (e) assist in highlighting building entrances



- (f) be sited and limited in height, to ensure adequate sight lines for motorists and pedestrians especially on corner sites
- (g) in the case of side and rear boundaries, be of sufficient height to maintain privacy and/or security without adversely affecting the visual amenity or access to sunlight of adjoining land
- (h) be constructed of non-flammable materials.

Marinas and Maritime Structures

OBJECTIVE

- 1** The provision, in appropriate locations, of marinas, pontoons, jetties, piers, wharves and boat moorings that cater for vessels and:
- (a) maintain public access to the waterfront
 - (b) do not compromise public safety
 - (c) preserve the structural integrity of the marine infrastructure
 - (d) minimise adverse impacts on the natural environment.

PRINCIPLES OF DEVELOPMENT CONTROL

- 2** The design of marinas, berths, channels, fairways, gangways and floating structures should comply with:
- (a) Australian Standard AS 3962: Guidelines for Design of Marinas
 - (b) Australian Standard AS 4997: Guidelines for the Design of Maritime Structures.

Natural Resources

OBJECTIVES

- 1** Retention, protection and restoration of the natural resources and environment.
- 2** Protection of the quality and quantity of South Australia's surface waters, including inland, marine and estuarine and underground waters.
- 3** The ecologically sustainable use of natural resources including water resources, including marine waters, ground water, surface water and watercourses.
- 4** Natural hydrological systems and environmental flows reinstated, and maintained and enhanced.
- 5** Development consistent with the principles of water sensitive design.
- 6** Development sited and designed to:
 - (a) protect natural ecological systems
 - (b) achieve the sustainable use of water
 - (c) protect water quality, including receiving waters
 - (d) reduce runoff and peak flows and prevent the risk of downstream flooding
 - (e) minimise demand on reticulated water supplies
 - (f) maximise the harvest and use of stormwater
 - (g) protect stormwater from pollution sources.
- 7** Storage and use of stormwater which avoids adverse impact on public health and safety.
- 8** Native flora, fauna and ecosystems protected, retained, conserved and restored.
- 9** Restoration, expansion and linking of existing native vegetation to facilitate habitat corridors for ease of movement of fauna.
- 10** Minimal disturbance and modification of the natural landform.
- 11** Protection of the physical, chemical and biological quality of soil resources.
- 12** Protection of areas prone to erosion or other land degradation processes from inappropriate development.
- 13** Protection of the scenic qualities of natural and rural landscapes.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1** Development should be undertaken with minimum impact on the natural environment, including air and water quality, land, soil, biodiversity, and scenically attractive areas.
- 2** Development should ensure that South Australia's natural assets, such as biodiversity, water and soil, are protected and enhanced.



Water Sensitive Design

5 Development should be designed to maximise conservation, minimise consumption and encourage re-use of water resources.

6 Development should not take place if it results in unsustainable use of surface or underground water resources.

7 Development should be sited and designed to:

- (a) capture and re-use stormwater, where practical
- (b) minimise surface water runoff
- (c) prevent soil erosion and water pollution
- (d) protect and enhance natural water flows
- (e) protect water quality by providing adequate separation distances from watercourses and other water bodies
- (f) not contribute to an increase in salinity levels
- (g) avoid the water logging of soil or the release of toxic elements
- (h) maintain natural hydrological systems and not adversely affect:
 - (i) the quantity and quality of groundwater
 - (ii) the depth and directional flow of groundwater
 - (iii) the quality and function of natural springs.

9 Development should include stormwater management systems to protect it from damage during a minimum of a 1-in-100 year average return interval flood.

10 Development should have adequate provision to control any stormwater over-flow runoff from the site and should be sited and designed to improve the quality of stormwater and minimise pollutant transfer to receiving waters.

11 Development should include stormwater management systems to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure the carrying capacities of downstream systems are not overloaded.

12 Development should include stormwater management systems to minimise the discharge of sediment, suspended solids, organic matter, nutrients, bacteria, litter and other contaminants to the stormwater system.

13 Stormwater management systems should preserve natural drainage systems, including the associated environmental flows.

14 Stormwater management systems should:

- (a) maximise the potential for stormwater harvesting and re-use, either on-site or as close as practicable to the source
- (b) utilise, but not be limited to, one or more of the following harvesting methods:
 - (i) the collection of roof water in tanks
 - (ii) the discharge to open space, landscaping or garden areas, including strips adjacent to car parks
 - (iii) the incorporation of detention and retention facilities
 - (iv) aquifer recharge.

15 Where it is not practicable to detain or dispose of stormwater on site, only clean stormwater runoff should enter the public stormwater drainage system.

Orderly and Sustainable Development

OBJECTIVES

1 Orderly and economical development that creates a safe, convenient and pleasant environment in which to live.

2 Development occurring in an orderly sequence and in a compact form to enable the efficient provision of public services and facilities.

3 Development that does not jeopardise the continuance of adjoining authorised land uses.

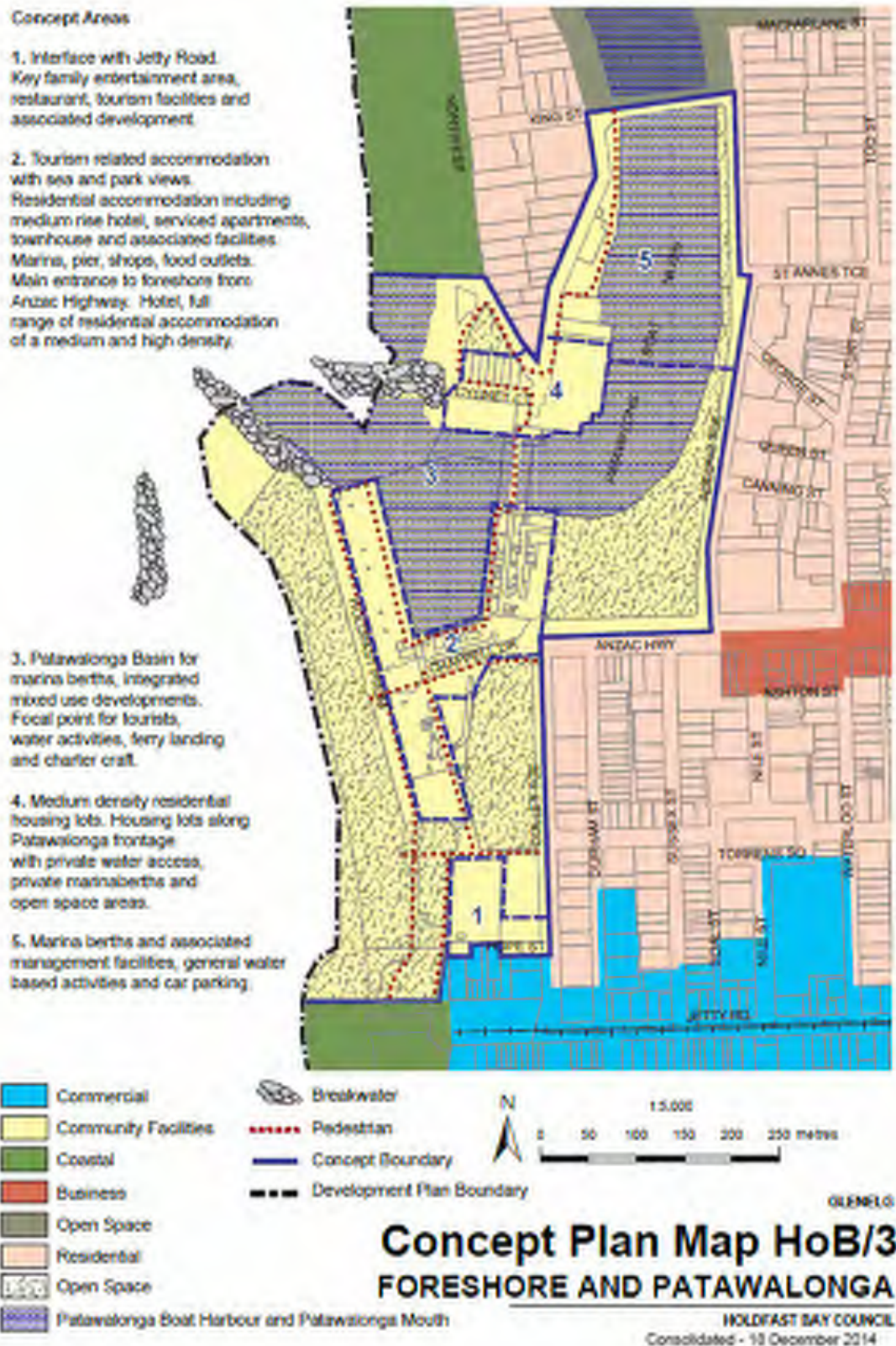
4 Development that does not prejudice the achievement of the provisions of the Development Plan.

5 Development abutting adjoining Council areas having regard to the policies of that Council's Development Plan.

PRINCIPLES OF DEVELOPMENT CONTROL



- 1** Development should not prejudice the development of a zone for its intended purpose.
- 2** The economic base of the region should be expanded in a sustainable manner.
- 3** Urban development should form a compact extension to an existing built-up area.
- 6** Where development is expected to impact upon the existing infrastructure network (including the transport network), development should demonstrate how the undue effect will be addressed.
- 7** Vacant or underutilised land should be developed in an efficient and co-ordinated manner to not prejudice the orderly development of adjacent land.
- 8** Development in localities having an inappropriate or unsatisfactory layout, or unhealthy or obsolete development should improve or rectify those conditions.
- 9** Development should be undertaken in accordance with the following Structure Plan Map and Concept Plan Maps:





Siting and Visibility

OBJECTIVES

1 Protection of scenically attractive areas, particularly natural, rural and coastal landscapes.

PRINCIPLES OF DEVELOPMENT CONTROL

1 Development should be sited and designed to minimise its visual impact on:

- (a) the natural, rural or heritage character of the area
- (b) areas of high visual or scenic value, particularly rural and coastal areas
- (c) views from the coast, near-shore waters, public reserves, tourist routes and walking trails
- (d) the amenity of public beaches.

2 Buildings should be sited in unobtrusive locations and, in particular, should:

- (a) be grouped together
- (b) where possible be located in such a way as to be screened by existing vegetation when viewed from public roads as shown on Overlay Maps - Transport.

4 Buildings and structures should be designed to minimise their visual impact in the landscape, in particular:

- (a) the profile of buildings should be low and the rooflines should complement the natural form of the land
- (b) the mass of buildings should be minimised by variations in wall and roof lines and by floor plans which complement the contours of the land
- (c) large eaves, verandas and pergolas should be incorporated into designs so as to create shadowed areas that reduce the bulky appearance of buildings.

5 The nature of external surface materials of buildings should not detract from the visual character and amenity of the landscape.

Tourism Development

OBJECTIVES

1 Environmentally sustainable and innovative tourism development.

2 Tourism development that assists in the conservation, interpretation and public appreciation of significant natural and cultural features including State or local heritage places.

3 Tourism development that sustains or enhances the local character, visual amenity and appeal of the area.

4 Tourism development that protects areas of exceptional natural value, allows for appropriate levels of visitation, and demonstrates an environmental analysis and design response which enhances environmental values.

5 Tourism development that contributes to local communities by adding vitality to neighbouring townships, regions and settlements.

PRINCIPLES OF DEVELOPMENT CONTROL

1 Tourism development should have a functional or locational link with its natural, cultural or historical setting.

2 Tourism development and any associated activities should not damage or degrade any significant natural and cultural features.

3 Tourism development should ensure that its scale, form and location will not overwhelm, over commercialise or detract from the intrinsic natural values of the land on which it is sited or the character of its locality.

4 Tourism development should, where appropriate, add to the range of services and accommodation types available in an area.

5 Any upgrading of infrastructure to serve tourism development should be consistent with the landscape and the intrinsic natural values of the land and the basis of its appeal.

6 Major tourism developments should generally be located within designated areas and existing townships, towns or cities.

Transportation and Access



OBJECTIVES

2 Development that:

- (a) provides safe and efficient movement for all motorised and non-motorised transport modes
- (b) ensures access for vehicles including emergency services, public infrastructure maintenance and commercial vehicles
- (c) provides off street parking
- (d) is appropriately located so that it supports and makes best use of existing transport facilities and networks.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

1 Land uses arranged to support the efficient provision of sustainable transport networks and encourage their use.

Cycling and Walking

14 Development should ensure that a permeable street and path network is established that encourages walking and cycling through the provision of safe, convenient and attractive routes with connections to adjoining streets, paths, open spaces, schools, public and community transport stops and activity centres.

Access

22 Development should have direct access from an all weather public road.

23 Development should be provided with safe and convenient access which:

- (a) avoids unreasonable interference with the flow of traffic on adjoining roads
- (b) provides appropriate separation distances from existing roads or level crossings
- (c) accommodates the type and volume of traffic likely to be generated by the development or land use and minimises induced traffic through over-provision
- (d) is sited and designed to minimise any adverse impacts on the occupants of and visitors to neighbouring properties.

24 Development should not restrict access to publicly owned land such as recreation areas.

Access for People with Disabilities

30 Development should be sited and designed to provide convenient access for people with a disability.

Vehicle Parking

33 Development should provide off-street vehicle parking and specifically marked disabled car parking places to meet anticipated demand in accordance with zone requirements or, if not specified by the zone, Table HoB/1 - Off Street Vehicle Parking Requirements (with resultant numerical figure rounded to the nearest whole number) unless all the following conditions are met *(the following does not apply to this development)*:

- (a) the site is located within the Glenelg Policy Area 2
- (b) an agreement is reached between the Council and the applicant for a reduced number of parking spaces
- (c) a financial contribution is paid into the Council Car Parking Fund specified by the Council, in accordance with the gazetted rate per car park.

34 Development should be consistent with Australian Standard AS 2890 Parking facilities.

35 Vehicle parking areas should be sited and designed in a manner that will:



- (a) facilitate safe and convenient pedestrian linkages to the development and areas of significant activity or interest in the vicinity of the development
- (b) include safe pedestrian and bicycle linkages that complement the overall pedestrian and cycling network
- (c) not inhibit safe and convenient traffic circulation
- (d) result in minimal conflict between customer and service vehicles
- (e) avoid the necessity to use public roads when moving from one part of a parking area to another
- (f) minimise the number of vehicle access points to public roads
- (g) avoid the necessity for backing onto public roads
- (h) where reasonably possible, provide the opportunity for shared use of car parking and integration of car parking areas with adjoining development to reduce the total extent of vehicle parking areas and the requirement for access points
- (i) not dominate the character and appearance of a site when viewed from public roads and spaces
- (j) provide landscaping that will shade and enhance the appearance of the vehicle parking areas.

36 Vehicle parking areas should be designed to reduce opportunities for crime by:

- (a) maximising the potential for passive surveillance by ensuring they can be overlooked from nearby buildings and roads
- (b) incorporating walls and landscaping that do not obscure vehicles or provide potential hiding places
- (c) being appropriately lit
- (d) having clearly visible walkways.

38 Parking areas that are likely to be used during non daylight hours should provide illuminated entrance and exit points and site lighting directed and shaded in a manner that will not cause nuisance to adjacent properties or users of the car park.

39 Parking areas should be sealed or paved in order to minimise dust and mud nuisance.

40 To assist with stormwater detention and reduce heat loads in summer, vehicle parking areas should include soft (living) landscaping.

41 Parking areas should be line-marked to indicate parking bays, movement aisles and direction of traffic flow.

Waste

OBJECTIVES

2 Development that includes the treatment and management of solid and liquid waste to prevent undesired impacts on the environment including, soil, plant and animal biodiversity, human health and the amenity of the locality.

PRINCIPLES OF DEVELOPMENT CONTROL

1 Development should be sited and designed to prevent or minimise the generation of waste (including wastewater) by applying the following waste management hierarchy in the order of priority as shown below:

- (a) avoiding the production of waste
- (b) minimising waste production
- (c) reusing waste
- (d) recycling waste
- (e) recovering part of the waste for re-use
- (f) treating waste to reduce the potentially degrading impacts
- (g) disposing of waste in an environmentally sound manner.

2 The storage, treatment and disposal of waste materials from any development should be achieved without risk to health or impairment of the environment.

5 Development should include appropriately sized area to facilitate the storage of receptacles that will enable the efficient recycling of waste.



- 6** Development that involves the production and/or collection of waste and/or recyclable material should include designated collection and storage area(s) that are:
- (a) screened and separated from adjoining areas
 - (b) located to avoid impacting on adjoining sensitive environments or land uses
 - (c) designed to ensure that wastes do not contaminate stormwater or enter the stormwater collection system
 - (d) located on an impervious sealed area graded to a collection point in order to minimise the movement of any solids or contamination of water
 - (e) protected from wind and stormwater and sealed to prevent leakage and minimise the emission of odours
 - (f) stored in such a manner that ensures that all waste is contained within the boundaries of the site until disposed of in an appropriate manner.

ATTACHMENT 1

LOCALITY MAP AND SITE PHOTOS

Locality Map





Welcome Aboard
MAIN ENTRANCE



Welcome Aboard
MAIN ENTRANCE



























BUFFALO
RESTAURANT
CARPARK
PARKING AREA
24P
90° ANGLE
TICKET
PARKING





ARARAT
ARNDHILL
BALLARAT
BATHURST
BENDIGO
BENDIGO
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COSTAMUNDRA
COYNE
DELRANE
DUNEDIN
EDMONT
FREMANTLE
GAWLER
GEEBONG
GEOFFCOTE
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TICKETS

BUFFALO
RESTAURANT
CARPARK
PARKING AREA

24P
90° ANGLE
TICKET
PARKING













































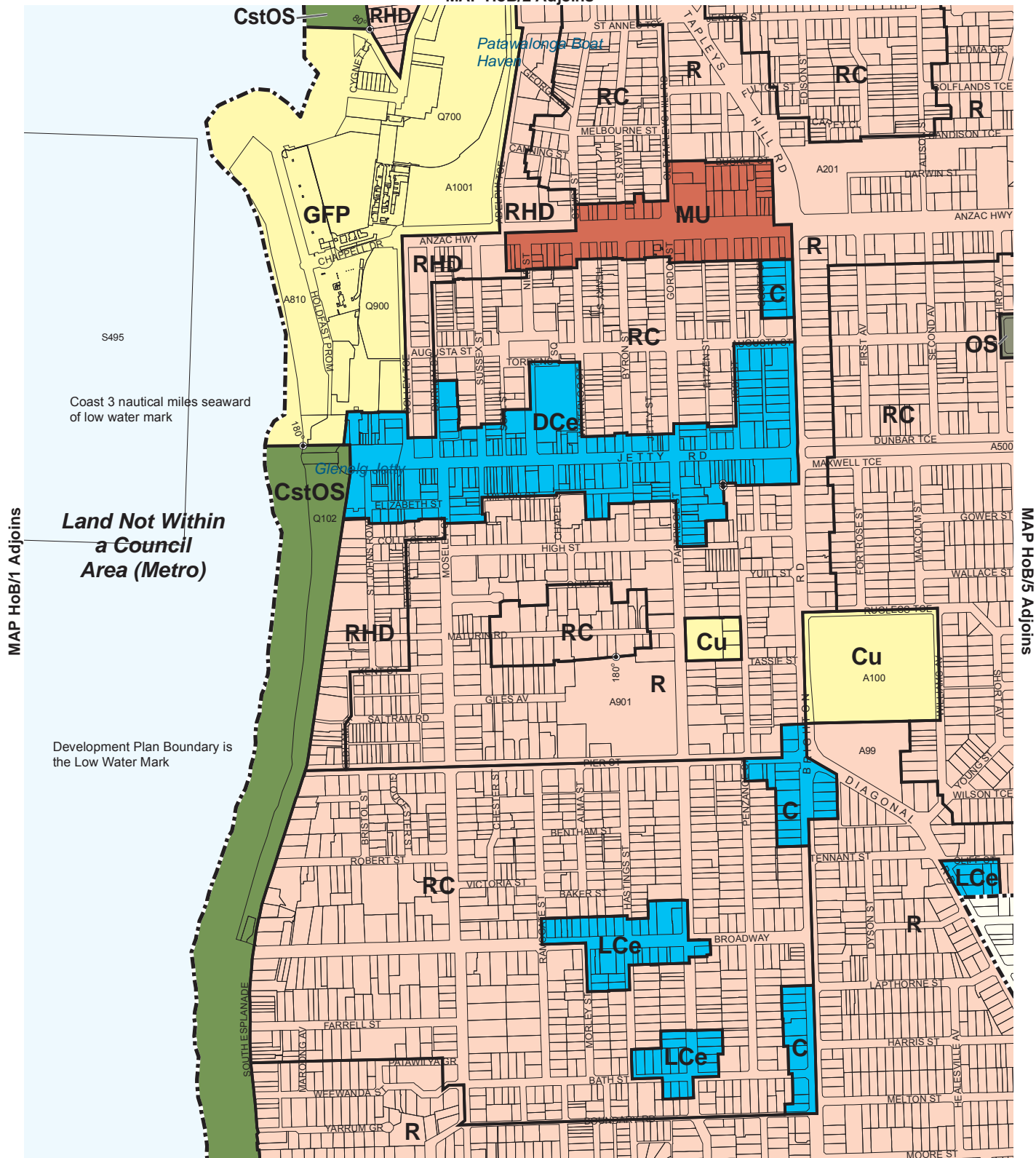




ATTACHMENT 2

ZONE MAPS

MAP HoB/2 Adjoins



Lamberts Conformal Conic Projection, GDA94



Zones

CstOS	Coastal Open Space	LCe	Local Centre
C	Commercial	MU	Mixed Use
Cu	Community	OS	Open Space
DCE	District Centre	R	Residential
GFP	Glenelg Foreshore and Patawalonga	RC	Residential Character
		RHD	Residential High Density
			Zone Boundary
			Development Plan Boundary

Zone Map HoB/4

HOLDFAST BAY COUNCIL

Consolidated - 18 December 2014

ATTACHMENT 3

APPLICATION DOCUMENTS

20 February 2015

The Hon John Rau MP
Minister for Planning
GPO Box 1815
ADELAIDE SA 5001



Attn: Mark Adcock

Dear Minister Rau

Development Application	110/00146/15
Location of Proposed Development	Allot 1001 Sec 1023 Adelphi Terrace Glenelg North - DP 49600 Vol 5935 Fol 965
Nature of Proposed Development	Proposed kiosk, alfresco bar and dining area, toilet facilities, outdoor dining timber deck, boat pontoon and gangway and associated landscaping and temporary construction fence
Applicant	De Young Jamestown Pty Ltd

Council requests that the Development Assessment Commission act as the relevant authority for the abovementioned development application.

Council is the owner of the land the beneficiary from future leasing agreements.

The perception of bias in this case therefore warrants the Development Assessment Commission being the relevant authority. Pursuant to Section 34(1)(b)(iii) of the Development Act 1993, it would be greatly appreciated if the Honourable Minister would direct the Development Assessment Commission to act as the relevant authority for the development application.

Should further require be information, please do not hesitate to contact me on 8229 9954.

Yours sincerely



Anthony Marroncelli
Manager Development Services
encl.



DEVELOPMENT APPLICATION FORM

APPLICANT: De Young Jamestown Pty Ltd

Postal Address: P.O. Box 2136
McLaren Vale SA 5171

OWNER: As above

Postal Address: _____

BUILDER: To be advised

Postal Address: _____

Licence No: _____

Telephone: _____

ARCHITECT/DRAFTSPERSON: Stuart J Patrick & Assoc. Pty Ltd Telephone: 08 8323 0080

Postal Address: P.O. Box 2136, McLaren Vale SA 5171

CONTACT PERSON FOR FURTHER INFORMATION

Name: Stuart Patrick

Telephone: 08 8323 0080

(work)

0414 442 910

(Ah)

Fax _____

EXISTING USE: Restaurant

DESCRIPTION OF PROPOSED DEVELOPMENT: Proposed kiosk, alfresco bar & dining area, toilet facilities, outdoor dining timber deck, boat pontoon & gangway + assoc. landscaping
+ temporary construction fencing

LOCATION OF PROPOSED DEVELOPMENT:

House No: _____ Street: Adelphi Terrace

Town/Suburb: Glenelg North

Lot No: _____

Volume: _____

Folio: _____

NATURE OF ASSESSMENT SOUGHT (tick only one):



PLANNING CONSENT ONLY



BUILDING CONSENT ONLY



FULL DEVELOPMENT APPROVAL

BUILDING RULES CLASSIFICATION SOUGHT: 6

Present classification: 6

HAS THE CONSTRUCTION INDUSTRY TRAINING FUND ACT 1993 LEVY BEEN PAID?

YES ☐

NO ☒

DOES THIS APPLICATION INVOLVE TREE DAMAGING ACTIVITY TO A SIGNIFICANT TREE?

YES ☐

NO ☒

APPROX YEAR DWELLING CONSTRUCTED: Early 80s

ROOF TYPE: TILE

YES ☐

NO ☒

TRUSS

YES ☐

NO ☒

DEVELOPMENT COST (do not include any fit - out costs for residential development): \$ 500,000

I acknowledge that copies of this application and supporting documentation may be provided to interested persons in accordance with the Development Regulations 1993

SIGNATURE: _____

Dated: 09/01 /2015

NB: The Quick Reference symbol shown allows Smart Phone users to apply a Quick Reference Application (App), scan the reference bar code, linking you to Council's website for further information associated with lodging a Development Application.



CITY OF
HOLDFAST BAY



Product	Register Search
Date/Time	14/10/2015 11:11AM
Customer Reference	
Order ID	20151014003552
Cost	\$27.25

The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



Registrar-General

Certificate of Title - Volume 5935 Folio 965

Parent Title(s)	CR 5645/338
Dealing(s) Creating Title	RLG 10140840
Title Issued	21/02/2005
Edition	1
Edition Issued	21/02/2005

REAL PROPERTY ACT, 1986



Estate Type

FEE SIMPLE

Registered Proprietor

CITY OF HOLDFAST BAY
OF PO BOX 19 BRIGHTON SA 5048

Description of Land

ALLOTMENT 1001 DEPOSITED PLAN 49600
IN THE AREA NAMED GLENELG NORTH
HUNDREDS OF ADELAIDE AND NOARLUNGA

Conditions

IN TRUST TO PERMIT SUFFER AND TO BE USED AT ALL TIMES AS A RESERVE FOR RECREATION PURPOSES

Easements

NIL

Schedule of Dealings

NIL

Notations

Dealings Affecting Title

NIL

Priority Notices



NIL

Notations on Plan

NIL

Registrar-General's Notes

NIL

Administrative Interests

NIL

* Denotes the dealing has been re-lodged.

Marinas

Site specific marinas built to suit your requirements.

Berthing site conditions can differ enormously so we provide a range of construction options to suit your specific needs.

Elite Marina System | Super Elite Marina System | Capri Marina System | Ultimate Modular Dock System

Please call (07) 5594 8200 to arrange a Free on-site consultation



We have a marina to suit you.

Superior can provide full "turnkey" marina construction projects as well as "kit" options for local, national and overseas projects. With four unique marina systems to choose from, Superior has the capacity and capabilities to deliver your 4 berth to 1,000 berth marina project on time and on budget.

By being able to provide a full turnkey package, which includes design, engineering, manufacture, project management and installation, Superior can efficiently facilitate any project from small boutique marinas to large scale berthing requirements, both domestically and internationally.

Features Include

- Lighting / Power
- Fire Services
- Sewage Pump Out
- Fuel Services
- Custom Gangways
- Engineering



Elite Marina System

Reinforced Concrete Deck Marina

- > Heavy duty concrete pontoon arrangement constructed using a solid monolithic flotation system
- > Protected by a polyethylene skin that is tough and flexible
- > Independent expansion and contraction from the flotation and concrete deck



Super Elite Marina System

Reinforced Concrete Deck Marina plus Concrete Walls & Water Boards

- > Heavy duty concrete system with concrete pontoon walls and water boards added as an option to the Elite System
- > Suitable for large scale commercial marinas and for Super Yacht applications



Capri Marina System

Polyethylene Floats Bolted to Aluminium Structure

- > Strong aluminium framed structure with rotomoulded polyethylene flotation
- > Tough, non deteriorating materials for harsh marine environments
- > Adjustable cleats and easy access to dual service ducts along each side
- > Easy transport, simple fast assembly and more open floats to allow for debris removal
- > Less draft in the water than concrete and numerous decking options

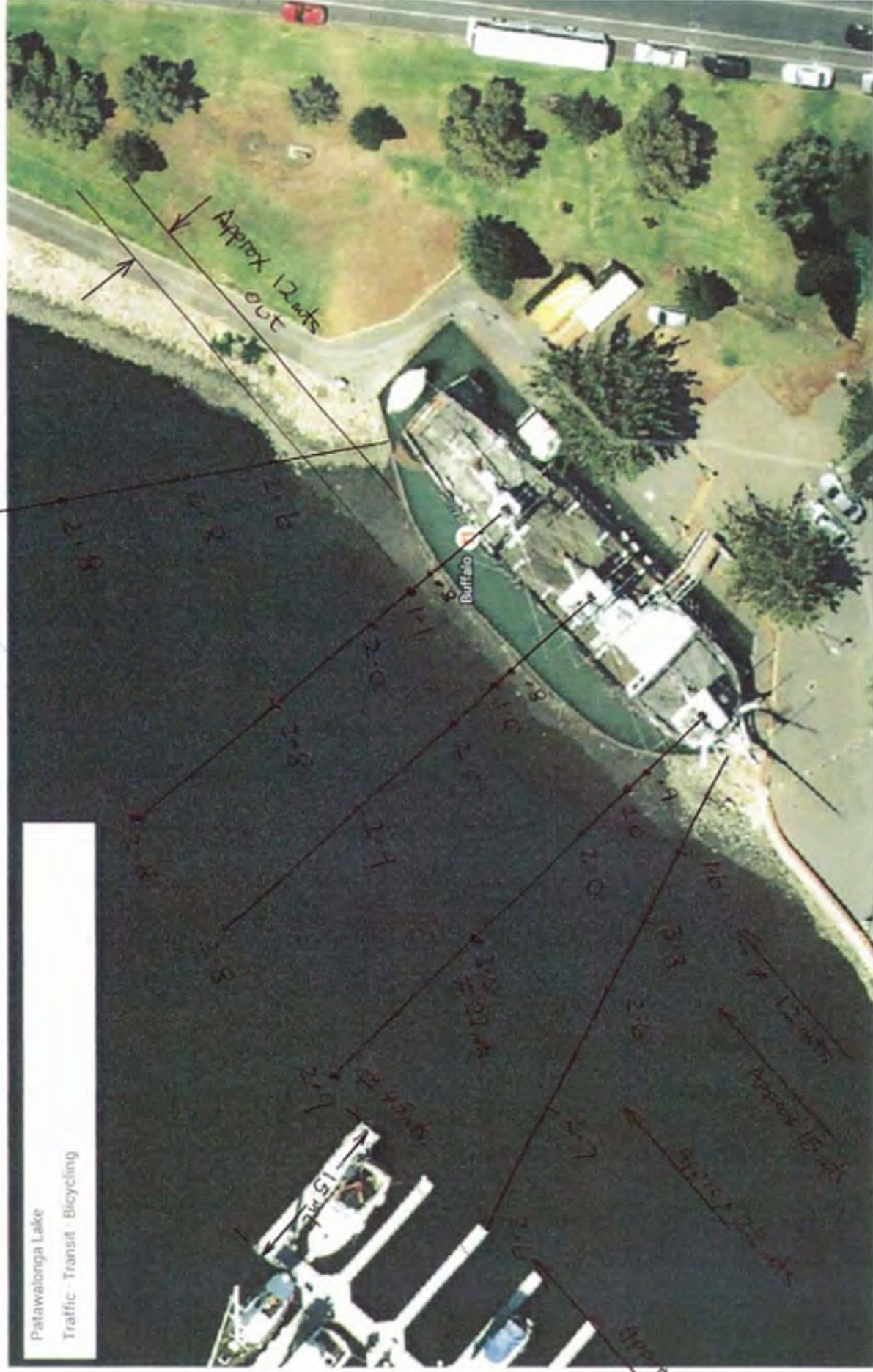


Ultimate Modular Dock System

Linkable Polyethylene Module System

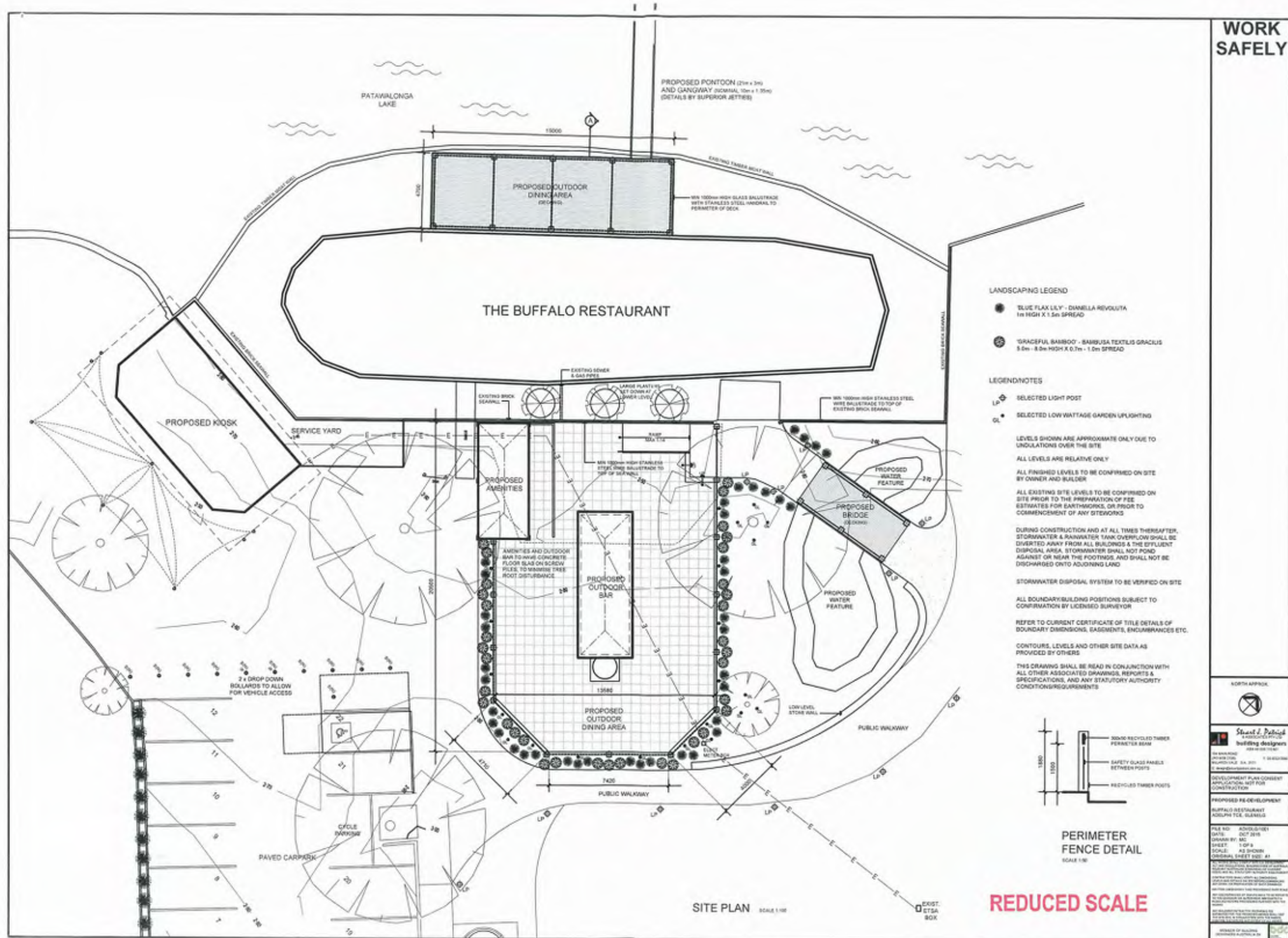
- > Tough polyethylene modules that are very tough yet flexible
- > Designed for non deterioration in marine environments and deflects U.V. Rays
- > Very Versatile – Can be arranged in many configurations

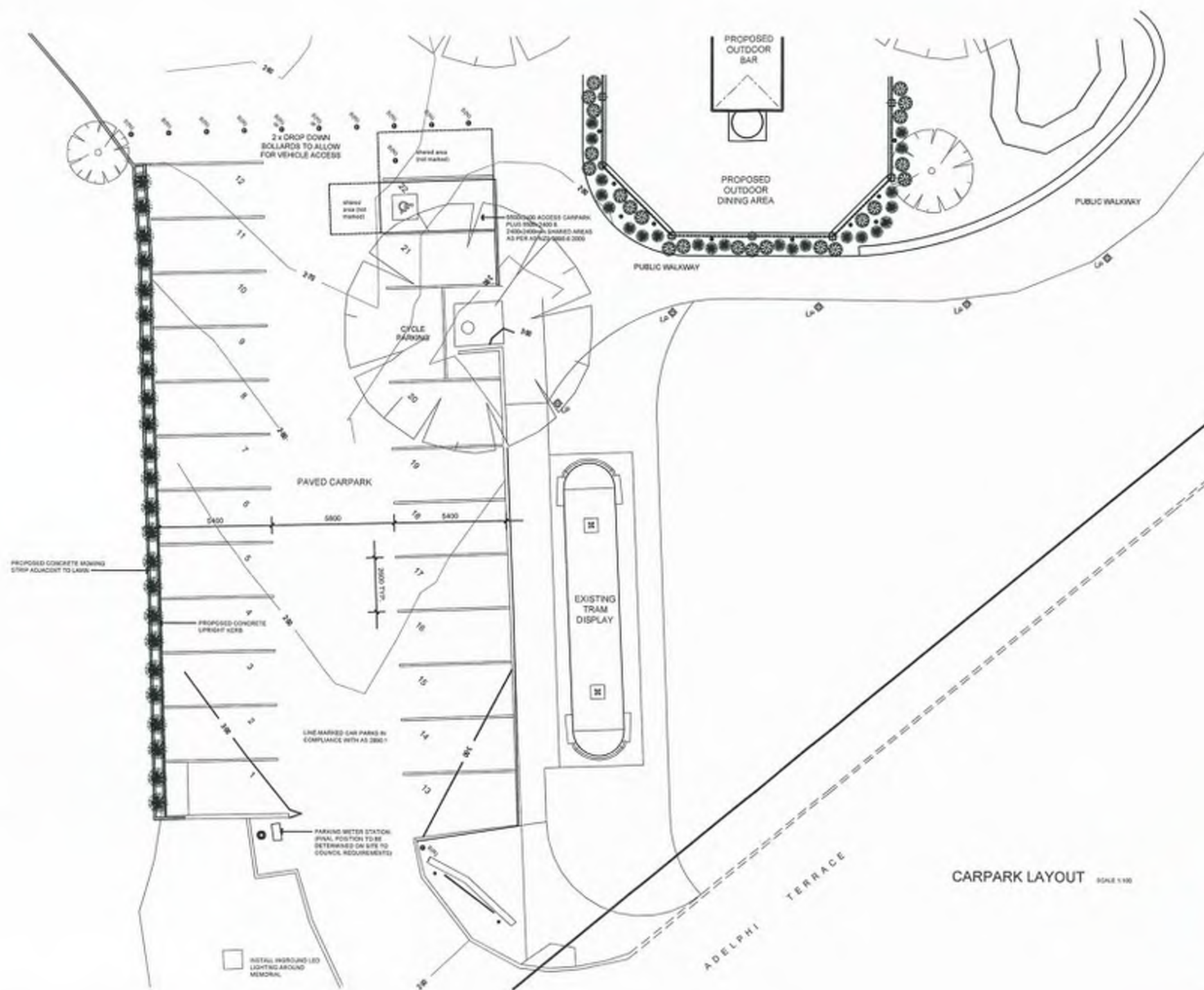




AT High Tide, water-level approx 1mt
down from top of water retaining wall,
of Buffalo.

Imagery ©2014, Data ©2014, Map data ©2014, Google





CARPARK LAYOUT SCALE 1:100

REDUCED SCALE

- EXISTING BOLLARD TO BE REMOVED
- NEW BOLLARD
- EXISTING BOLLARD
- NEW BOLLARD DROP DOWN BOLLARD



Shant & Patrick
building designers
10/100 ROAD
MURDOCH, WA 6150
T: 9447 1000
E: info@shantpatrick.com.au

DEVELOPMENT PLAN CONSENT
APPLICATION - NOT FOR
CONSTRUCTION

PROPOSED CARPARK
BUNRAUD RESTAURANT
KOLUPH TCE, GLENELG

FILE NO: ADVISORY
DATE: OCT 2018
DRAWN BY: MJ
SHEET: 2 OF 8
SCALE: AS SHOWN
ORIGINAL SHEET SIZE: A3

NOT TO BE USED FOR CONSTRUCTION
This plan is a preliminary design and is not to be used for construction. It is subject to change without notice. The client is responsible for ensuring that the plan is used in accordance with the relevant legislation and standards. The client is also responsible for ensuring that the plan is used in accordance with the relevant legislation and standards.

SHANT & PATRICK
BUILDING DESIGNERS
10/100 ROAD
MURDOCH, WA 6150
T: 9447 1000
E: info@shantpatrick.com.au

WORK SAFELY

NORTH APPROX.



**Stuart J. Patrick
& ASSOCIATES PTY LTD**
building designers
ABN 49 008 110 951

164 MAIN ROAD
(PO BOX 2136)
MOLAREN VALE S.A. 5171
E: design@stuartpatrick.com.au
T: 08 8323 0080

DEVELOPMENT PLAN CONSENT
APPLICATION - NOT FOR
CONSTRUCTION

PROPOSED KIOSK

BUFFALO RESTAURANT
ADELPHI TCE, GLENELG

AREAS (m²)

PROPOSED KIOSK	70m ²
SERVICE YARD	36.18m ²

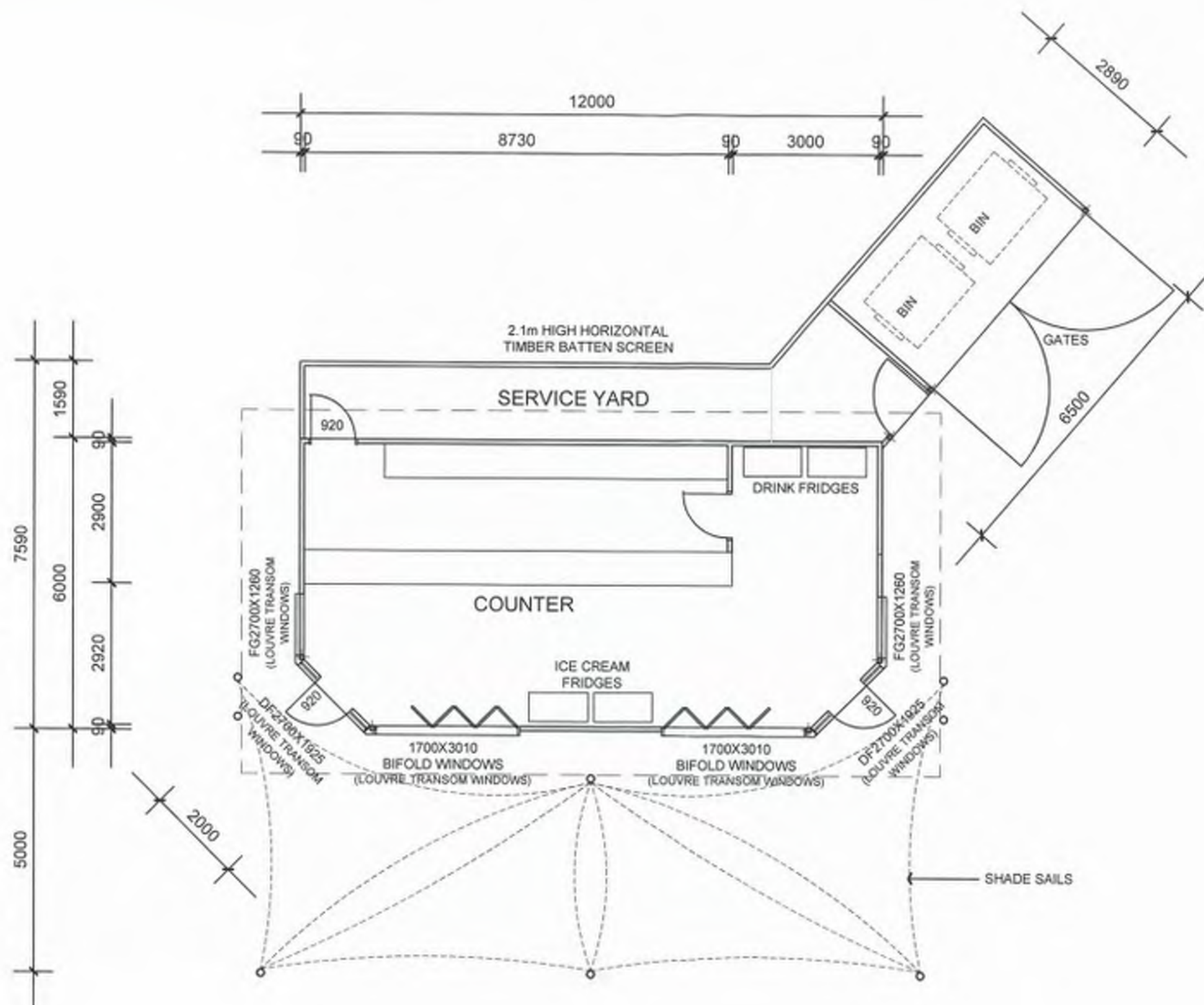
FILE NO: AD/MLG/1001
DATE: JULY 2015
DRAWN BY: MC
SHEET: 3 OF 9
SCALE: AS SHOWN
ORIGINAL SHEET SIZE: A3

ALL WORKS SHALL COMPLY WITH S.A. DEVELOPMENT
ACT AND REGULATIONS, BUILDING CODE OF AUSTRALIA,
RELEVANT AUSTRALIAN STANDARDS (OF CURRENT
ISSUE) AND ALL STATUTORY AUTHORITY REQUIREMENTS

CONTRACTORS SHALL VERIFY ALL DIMENSIONS,
LEVELS AND DETAILS ON SITE BEFORE COMMENCING
ANY WORK, OR PREPARATION OF SHOP DRAWINGS
WRITTEN DIMENSIONS TAKE PRECEDENCE OVER SCALE
ANY DISCREPANCIES OF SIGNIFICANCE TO BE REPORTED
TO THE DESIGNER OR SUPERVISOR IMMEDIATELY A
RESOLVED BEFORE PROCEEDING FURTHER WITH THE
WORKS

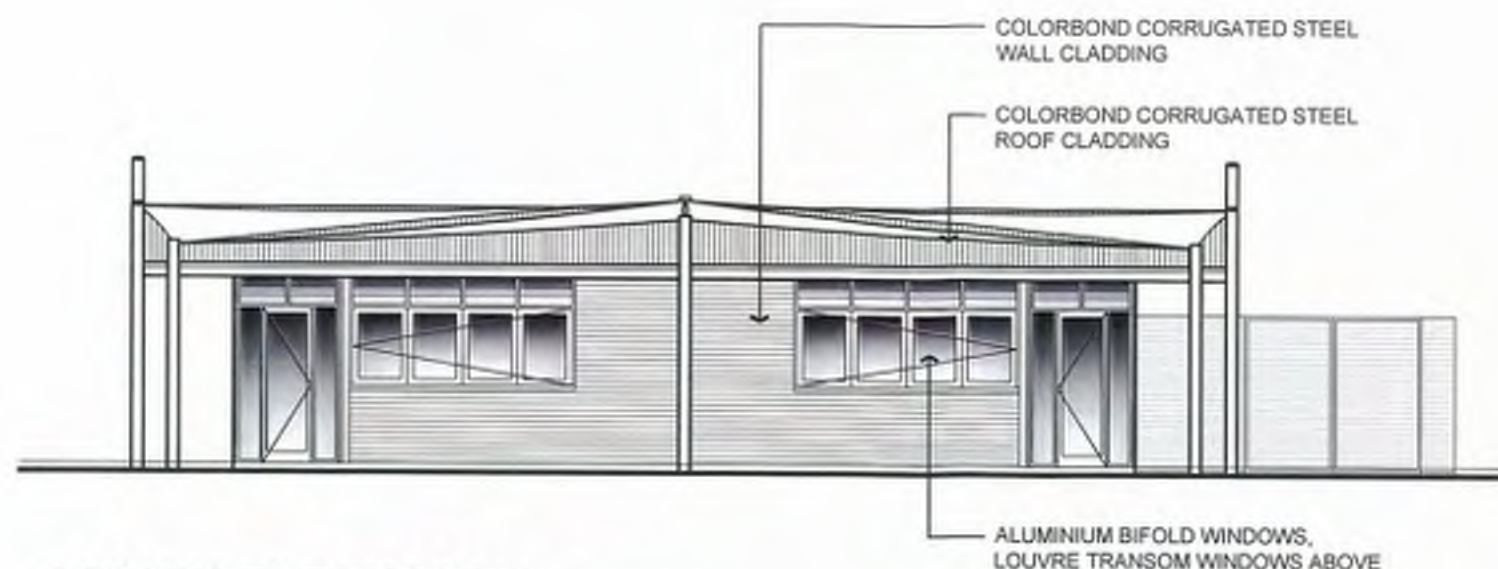
ANY BUILDER/CONTRACTOR PREPARING FEE
ESTIMATES FOR THE PROPOSED WORKS SHALL VISIT
THE SITE AND, IN CONJUNCTION WITH THE OWNER,
CONFIRM THE NATURE AND EXTENT OF ALL WORKS

MEMBER OF BUILDING
DESIGNERS AUSTRALIA SA

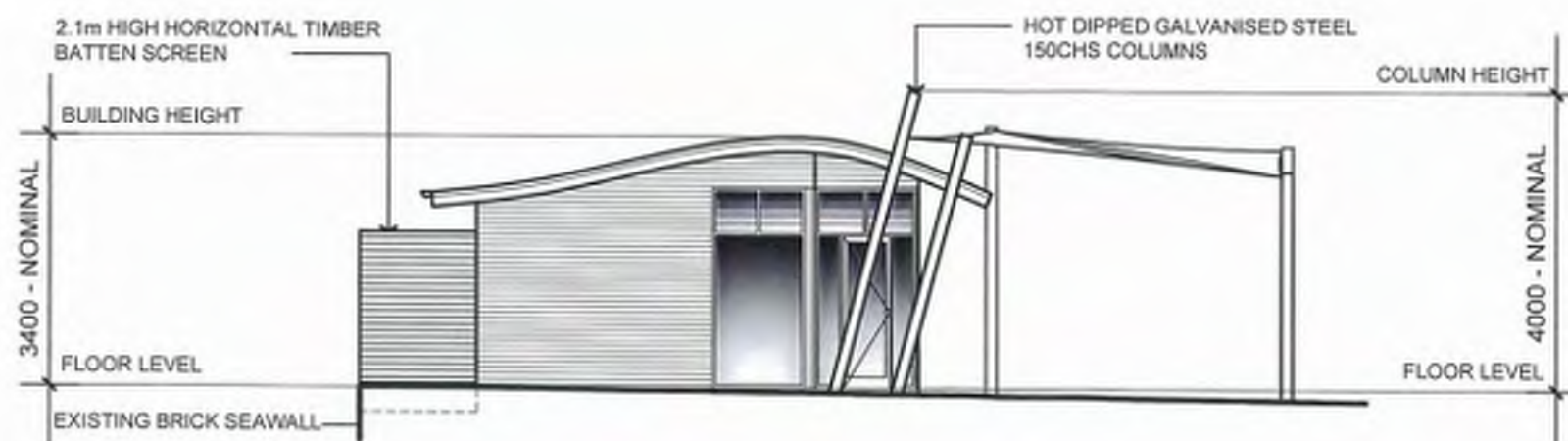


FLOOR PLAN SCALE 1:100

WORK SAFELY



SOUTH ELEVATION SCALE 1:100



WEST ELEVATION SCALE 1:100

NOTE:
ALL EXTERNAL COLOURS TO MATCH AND COMPLEMENT
EXISTING ADJACENT COUNCIL BBQ SHELTER AND TOILET BLOCK

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& ASSOCIATES PTY LTD
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ABN 49 008 110 851

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MELBURN VIC 3171
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DEVELOPMENT PLAN CONSENT
APPLICATION- NOT FOR
CONSTRUCTION

PROPOSED KIOSK

BUFFALO RESTAURANT
ADELPHI TCE, GLENELG

FILE NO: AD/1001
DATE: JULY 2015
DRAWN BY: MC
SHEET: 4 OF 9
SCALE: AS SHOWN
ORIGINAL SHEET SIZE: A3

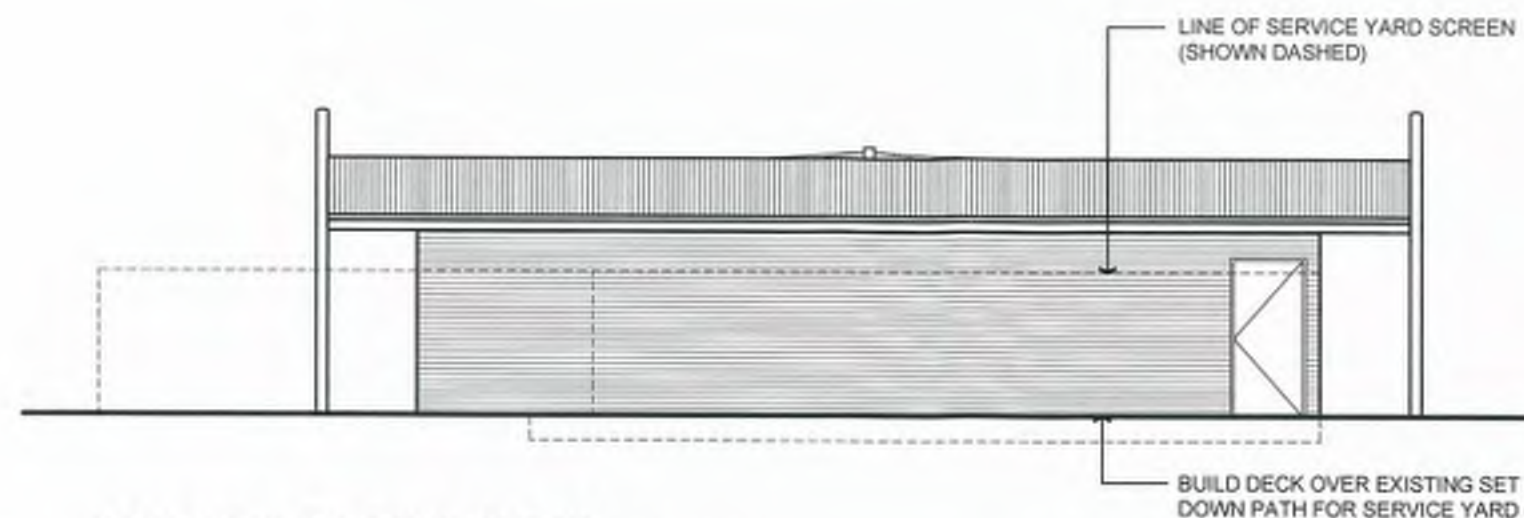
ALL WORKS SHALL COMPLY WITH S.A. DEVELOPMENT
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ANY DISCREPANCIES OF SIGNIFICANCE TO BE REPORTED
TO THE DESIGNER OR SUPERVISOR IMMEDIATELY
RESOLVED BEFORE PROCEEDING FURTHER WITH THE
WORKS

ANY BUILDER/CONTRACTOR PREPARING FEE
ESTIMATES FOR THE PROPOSED WORKS SHALL VISIT
THE SITE AND, IN CONJUNCTION WITH THE OWNER,
CONFIRM THE NATURE AND EXTENT OF ALL WORKS

MEMBER OF BUILDING
DESIGNERS AUSTRALIA SA



WORK SAFELY



NORTH ELEVATION SCALE 1:100



EAST ELEVATION SCALE 1:100

Stuart J. Patrick
& ASSOCIATES PTY LTD
building designers
ABN 49 008 110 861
164 MAIN ROAD
(PO BOX 2136)
MCLAREN VALE S.A. 5171
T: 08 8323 0080
E: design@stuartpatrick.com.au

DEVELOPMENT PLAN CONSENT
APPLICATION- NOT FOR
CONSTRUCTION

PROPOSED KIOSK

BUFFALO RESTAURANT
ADELPHI TCE, GLENELG

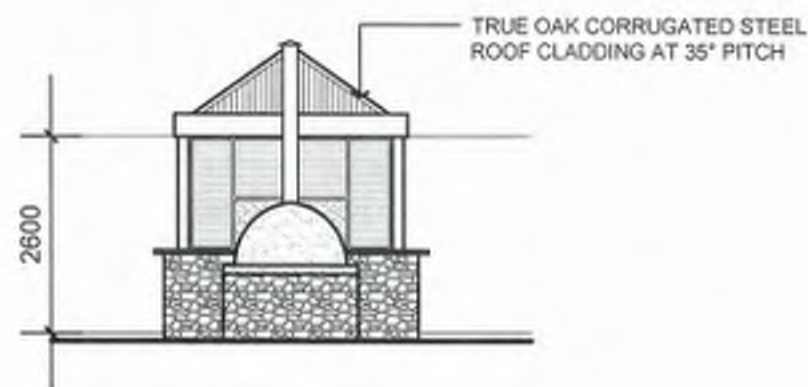
FILE NO: ADM/CLG/1001
DATE: JULY 2015
DRAWN BY: MC
SHEET: 5 OF 9
SCALE: AS SHOWN
ORIGINAL SHEET SIZE: A3

ALL WORKS SHALL COMPLY WITH S.A. DEVELOPMENT
ACT AND REGULATIONS, BUILDING CODE OF AUSTRALIA,
RELEVANT AUSTRALIAN STANDARDS (OF CURRENT
ISSUE) AND ALL STATUTORY AUTHORITY REQUIREMENTS
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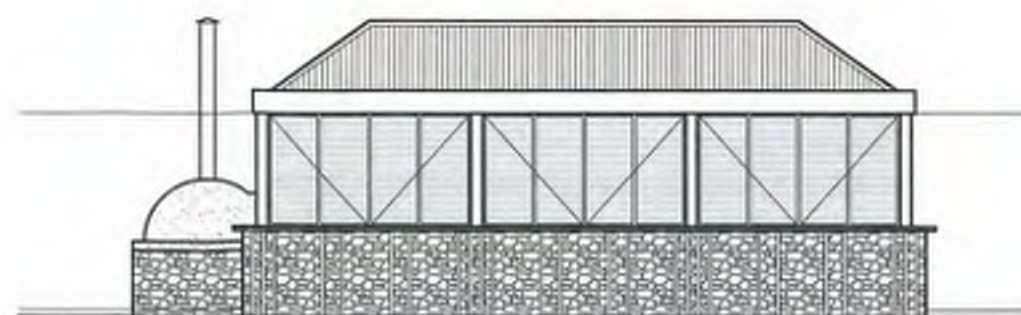


WORK SAFELY



ELEVATION 1

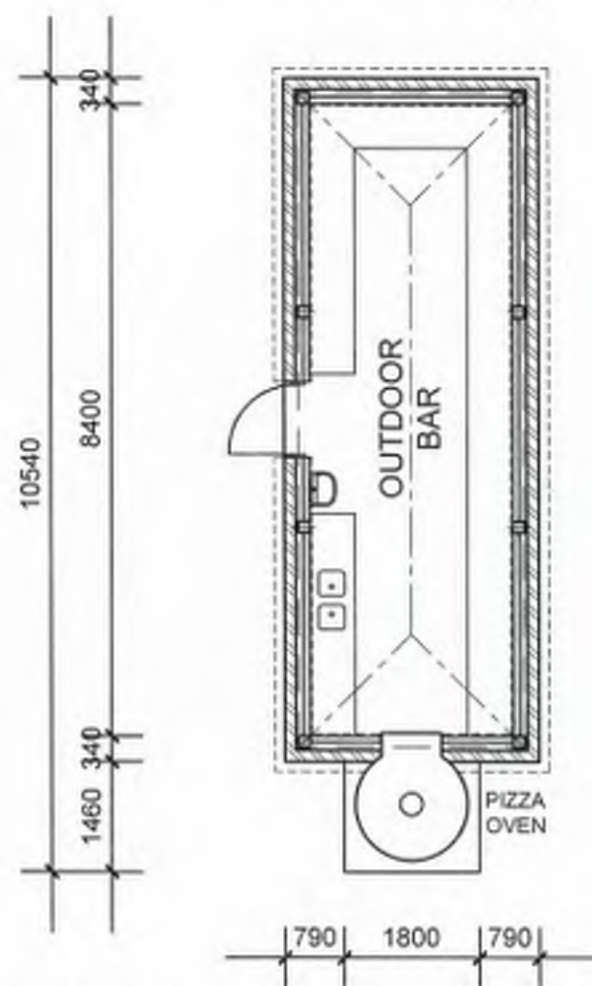
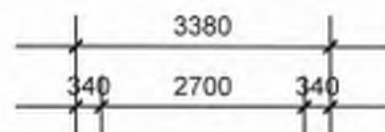
SCALE 1:100



ELEVATION 2

SCALE 1:100

FEATURE TIMBER UPRIGHTS FIXED
TO STONEWORK



FLOOR PLAN

SCALE 1:100



ELEVATION 3

SCALE 1:100

LIFT UP SHUTTERS



ELEVATION 4

SCALE 1:100

SELECTED STONEWORK

NORTH APPROX.



**Stuart J. Patrick
& ASSOCIATES PTY LTD**
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(PO BOX 2136)
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T: 08 8323 0080

DEVELOPMENT PLAN CONSENT
APPLICATION- NOT FOR
CONSTRUCTION

PROPOSED BAR / SERVICE AREA

BUFFALO RESTAURANT
ADELPHI TCE, GLENELG

AREAS (m²)

BAR / SERVICE AREA - 35.62m²

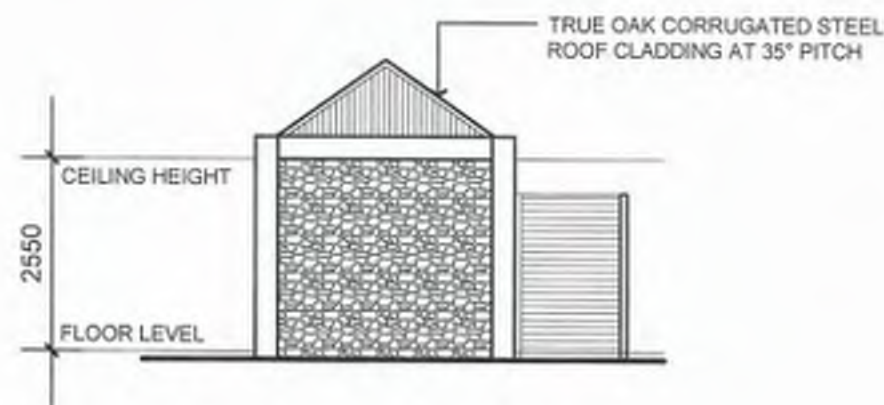
FILE NO: ADI/GLG/1001
DATE: OCT 2015
DRAWN BY: MC
SHEET: 7 OF 9
SCALE: AS SHOWN
ORIGINAL SHEET SIZE: A3

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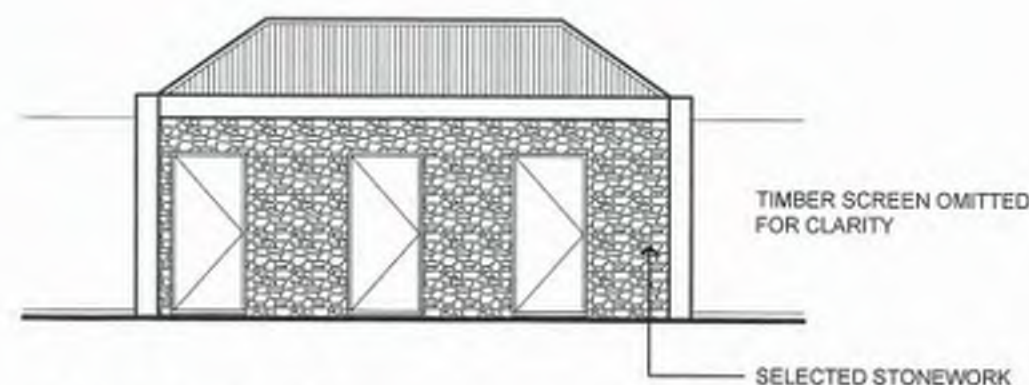
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ELEVATION 1

SCALE 1:100



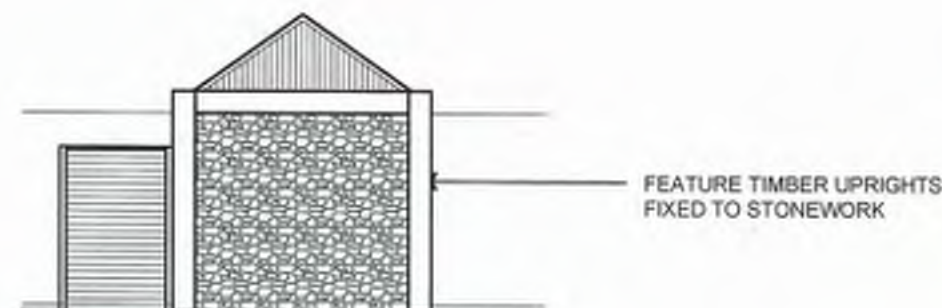
ELEVATION 2

SCALE 1:100



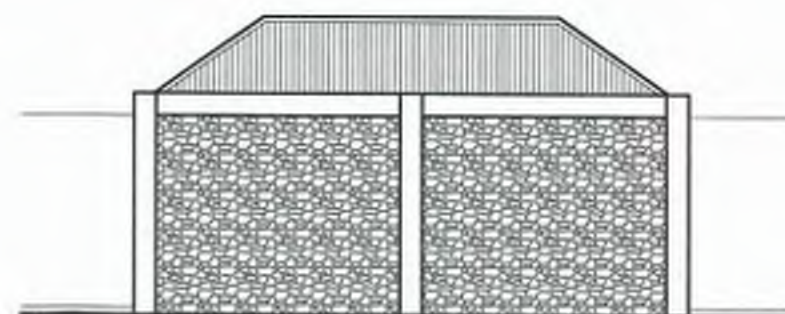
FLOOR PLAN

SCALE 1:100



ELEVATION 3

SCALE 1:100



ELEVATION 4

SCALE 1:100

WORK SAFELY

NORTH APPROX.



**Stuart J. Patrick
& ASSOCIATES PTY LTD**
building designers

164 MAIN ROAD
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ABN 49 006 110 861

DEVELOPMENT PLAN CONSENT
APPLICATION- NOT FOR
CONSTRUCTION

PROPOSED AMENITIES

BUFFALO RESTAURANT
ADELPHI TCE, GLENELG

AREAS (m²)

AMENITIES - 23.47m²

FILE NO: ADI/GLG/1001
DATE: OCT 2015
DRAWN BY: MC
SHEET: 8 OF 9
SCALE: AS SHOWN
ORIGINAL SHEET SIZE: A3

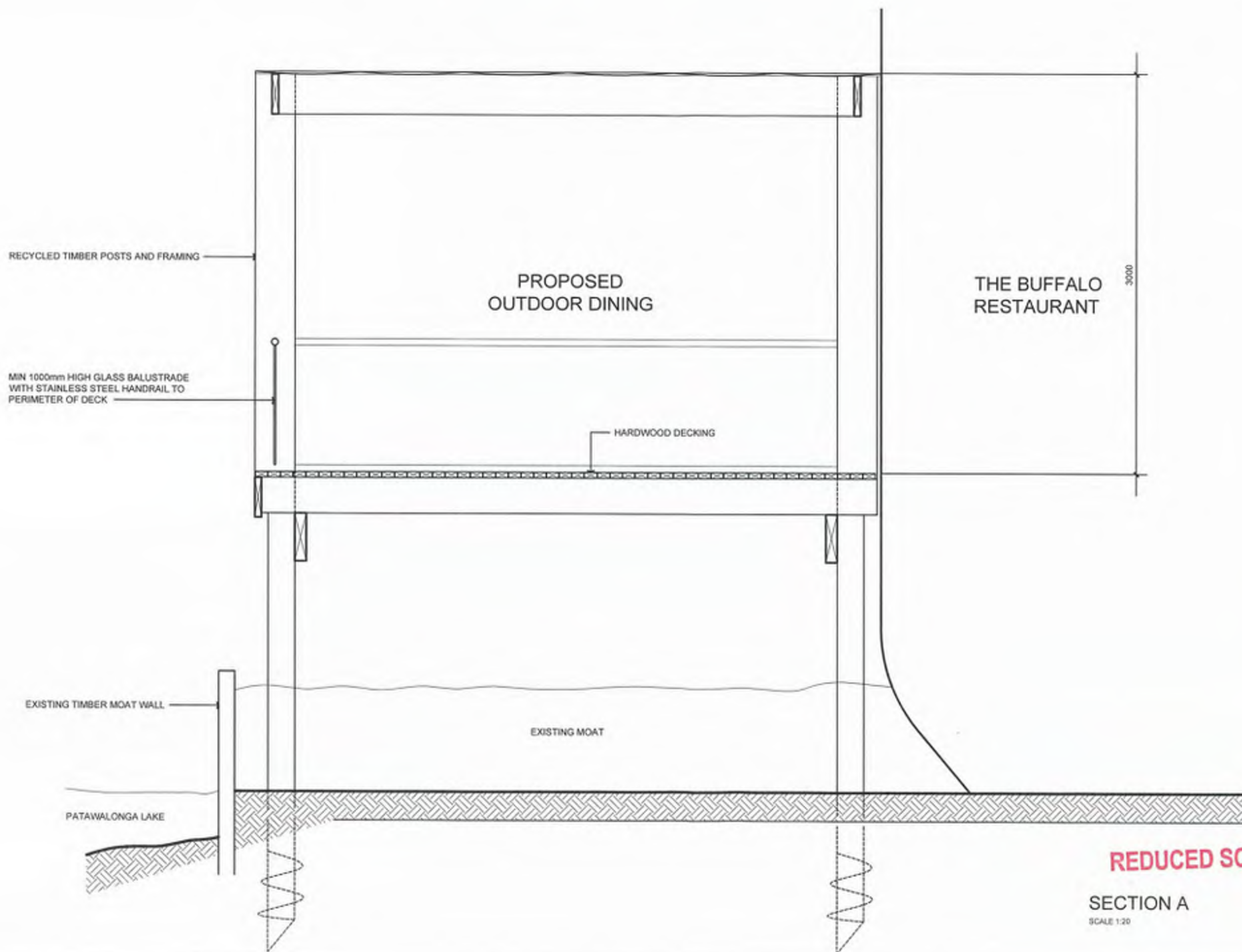
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CONFIRM THE NATURE AND EXTENT OF ALL WORKS

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DESIGNERS AUSTRALIA SA



**WORK
SAFELY**



REDUCED SCALE

SECTION A
SCALE 1:20

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building designers
184 MAIN ROAD
(PO BOX 2138)
MILLERS VILLAGE, S.A. 5171
E: design@stuartpatrick.com.au
T: 08 8329 0000

DEVELOPMENT PLAN CONSENT
APPLICATION - NOT FOR
CONSTRUCTION

**PROPOSED OUTDOOR
DINING DECK**
BUFFALO RESTAURANT
ADELPHI TCE, GLENELG

FILE NO: ADUGLG1001
DATE: OCT 2015
DRAWN BY: MC
SHEET: 9 OF 9
SCALE: AS SHOWN
ORIGINAL SHEET SIZE: A2

ALL WORKS SHALL BE DONE IN ACCORDANCE WITH THE BUILDING ACT 2010 AND REGULATIONS BUILDING CODE OF AUSTRALIA, RELEVANT AUSTRALIAN STANDARDS (IF CURRENT) AND ALL STATUTORY AUTHORITY REQUIREMENTS. CONTRACTORS SHALL VERIFY ALL DIMENSIONS, LEVELS AND DETAILS ON SITE BEFORE COMMENCING ANY WORK. ON PREPARATION OF SHOP DRAWINGS WRITTEN DIMENSIONS TAKE PRECEDENCE OVER SCALE. ANY DISCREPANCIES OF DIMENSIONS TO BE REPORTED TO THE DESIGNER OR SUPERVISOR IMMEDIATELY. A PERMITS BEFORE PROCEEDING WITH ANY WORK.

ANY BUILDING CONTRACTOR PREPARING FOR ESTIMATES FOR THE PROPOSED WORKS SHALL VISIT THE SITE AND AS-CONSTRUCTED WITH THE OWNER, COMPLYING THE NATURE AND EXTENT OF ALL WORKS.

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bda
BUILDING DESIGNERS AUSTRALIA

WORK
SAFELY

CONSISTENCY & CURRENCY OF DOCUMENTATION

THE BALDER IS TO ENSURE THAT ALL TRADES HAVE THE LATEST DOCUMENTATION PROVIDED BY THE DESIGNER AND CONSULTANTS AND THAT THE DOCUMENTATION IS CONSISTENT THROUGHOUT

ARE NOW IN IT

NOELPHI
TERRACE

CAR PARK LAYOUT PLAN SCALE 1:1M

- ☐ (DN) EXISTING BOLLARD TO BE REMOVED
- ☒ (DN) NEW BOLLARD
- ☐ (RE) EXISTING BOLLARD

NORMAL, 36" WIDE MATCH
CARPET SET WITH PLATING
TO MATCH EXIST.

PROPOSED CONCRETE SLOWING


PROPOSED CONCRETE

LINE-UP/ICE-CAR PARKING

- REFURBISH EXISTING LIGHT & POLE

INSTALL IN-GROUND LED LIGHTING AROUND PERIMETER.

SCIENCE APPENDIX



Stewart J. Patrick
 & ASSOCIATES, P.C.
 building designers

THE UNIVERSITY OF CHICAGO

PROPOSED BUFFALO RESTAURANT CASHIER REQUISITION

AREAS (sq ft)

FILE NO. 100-100000

DATE: MAY 2018

SHEET: 1 OF 1

SCALE: AS SHOWN
CONTINUED ON NEXT PAGE

1-800-451-7243

1998年，中国科学院南京地质古生物研究所的科学家们，在安徽繁昌繁港村发现了距今约2.5亿年的三叶虫化石。这是我国首次发现三叶虫化石，也是世界上首次发现三叶虫化石。这一发现对于研究三叶虫的演化具有重要意义。

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SITE LOCATION PLAN
1 : 200



WORK SAFELY

CONSISTENCY & CURRENCY
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PROVIDED BY THE DESIGNER
AND CONSULTANTS AND THAT
THE DOCUMENTATION IS
CONSISTENT THROUGHOUT

REVISIONS:

REDUCED SCALE

DEVELOPMENT PLAN CONSENT APPLICATION

Stuart J. Patrick
A PROFESSIONAL DESIGNER
building designers

1000 10th Street, Suite 100
St. John's, NL A1B 1X1
Tel: (709) 576-1111 Fax: (709) 576-1112

PROPOSED DEVELOPMENT
AT 1000 10th Street, Suite 100
ADELPHI TERRACE
ST. JOHN'S, NL A1B 1X1
FOR DE YOUNG JAMESON INC.

FILED	DATE
2014/01/14	2014/01/14
2014/01/14	2014/01/14
2014/01/14	2014/01/14

ALL INFORMATION CONTAINED HEREIN IS UNCLASSIFIED
DATE 2014/01/14 BY 1000 10th Street, Suite 100
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ST. JOHN'S, NL A1B 1X1

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DESIGNED BY: STUART J. PATRICK
DRAWN BY: STUART J. PATRICK
CHECKED BY: STUART J. PATRICK
APPROVED BY: STUART J. PATRICK

DATE: 2014/01/14
SCALE: 1:200
SHEET: 1 OF 1

CONSISTENCY & CURRENCY OF DOCUMENTATION
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APPENDIX 1

REDUCED SCALE

DEVELOPMENT PLAN
CONSENT APPLICATION

Stuart J. Patrick
A. Richard Hill, AIA, LEED
building designers
4000 40th St. S.W.
Burien, WA 98148
206/835-1100
www.patrickhill.com

PROPOSED DEVELOPMENT
AT BUFFALO RESTAURANT
ADJACENT TERRACE
GLADYS NORTH-LE NOB
FOR DE YOUNG JARISTOWN

FILE NO:	4375-2
DATE:	2/6/85
CRASH:	84
CRASH:	2-17-85
STATE:	42-8-000
ORIGINAL SHEET NO:	21

the authors acknowledge with an enthusiastic welcome the publication of this book. Although the authors themselves are young, the book is a very useful reference work.

The authors are a team of scientists and engineers who have been working on the development of a new type of computer system for the design of integrated circuits.

The authors are a team of scientists and engineers who have been working on the development of a new type of computer system for the design of integrated circuits.

The authors are a team of scientists and engineers who have been working on the development of a new type of computer system for the design of integrated circuits.

6. 在 4.2.1 节中, 我们假设了 \mathcal{H} 是有限维的。然而, 在更一般的情况下, \mathcal{H} 可能是无限维的。在这种情况下, 我们需要考虑 \mathcal{H} 的基的完备性。假设 $\{e_i\}_{i=1}^{\infty}$ 是 \mathcal{H} 的一组正交基, 那么对于任意 $f \in \mathcal{H}$, 我们有 $f = \sum_{i=1}^{\infty} \langle f, e_i \rangle e_i$ 。这个展开式在 L^2 范数下收敛。因此, 我们可以将 f 表示为基向量的线性组合。这为我们在无限维空间中处理函数提供了便利。

SITE PLAN
1:75





WORK SAFELY

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AMENDMENTS

REDUCED SCALE

DEVELOPMENT PLAN CONSENT APPLICATION

Shane J. Patrick
Building Designer

PROPOSED DEVELOPMENT
AT BUFFALO RESTAURANT
ADELPHI TERRACE
ADELPHI NORTH 33 300
FOR DE YOUNG JEWELLERY

PLAN NO. 2020/0001
DATE: 10/10/2020
SHEET: 1 OF 1
TOTAL: 10 SHEETS
DRAWN BY: SHANE J. PATRICK

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REVISIONS
REVISION NO. 1
REVISION DESCRIPTION



AERIAL PERSPECTIVE VIEW OF PONTOON & DECK STRUCTURE



PROPOSED PONTOON & DECK PERSPECTIVE VIEW



PROPOSED WALK PERSPECTIVE VIEW



ALFREDO AREA AERIAL PERSPECTIVE VIEW



BUFFALO MAIN ENTRANCE PERSPECTIVE VIEW



ALFREDO AREA PERSPECTIVE VIEW FROM ENTRANCE



ALFREDO AREA INTERNAL PERSPECTIVE VIEW

WORK SAFELY

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AREAS/NOTES:

REDUCED SCALE

DEVELOPMENT PLAN CONSENT APPLICATION

Simon J. Petrovic
Building Designer
10/10/2010

PROPOSED DEVELOPMENT
AT BUFFALO RESTAURANT
ADELPHI TERRACE
CLYDE NORTH SA 5085
FOR DE YOUNG JAMESON PT

FILE NO: 10/10/2010
DATE: 10/10/2010
LAYER: 10/10/2010
SHEET: 10/10/2010
SHEET: 10/10/2010

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240

REDUCED SCALE

DEVELOPMENT PLAN
CONSENT APPLICATION

Stuart J. Patrick
A COMMITMENT TO
building designers

PROPOSED DEVELOPMENT
AT SUFFOLD RESTAURANT
ADELPHI TERRACE
GLENEIL NORTH SA 505
FOR DE YOUNG JAMESON INC.

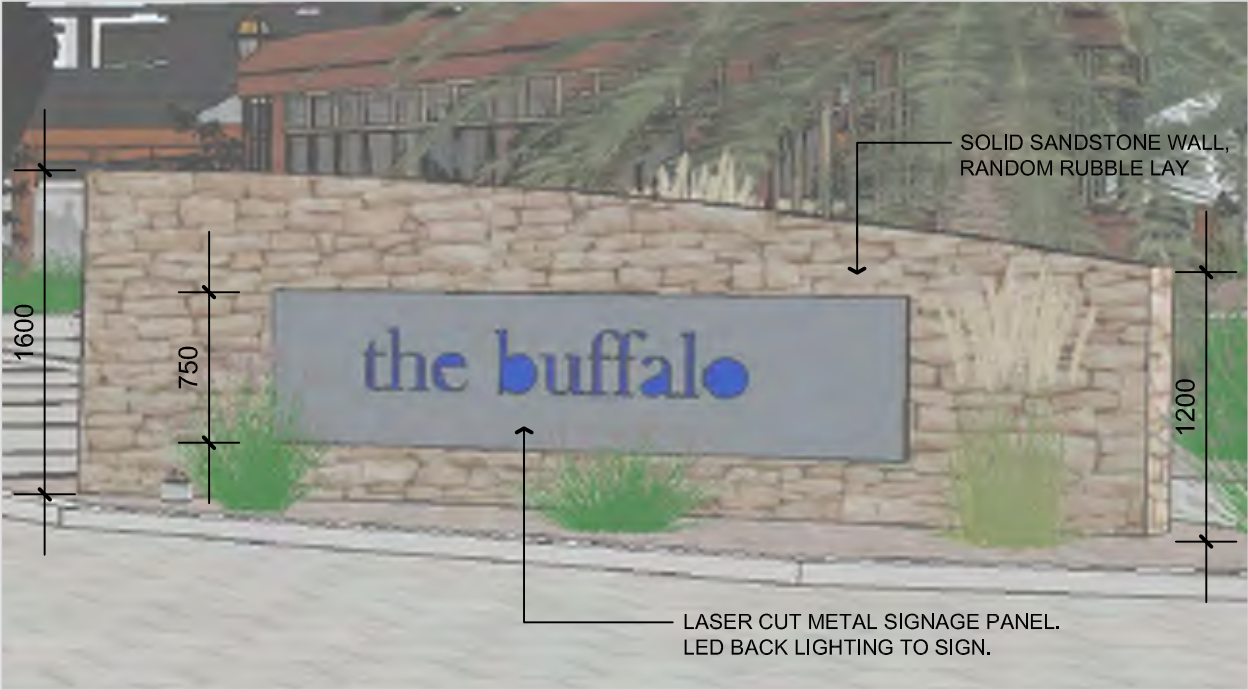
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DUE	10/26/10
DATE	10/26/10
TIME	10/26/10
BY	10/26/10
REMARKS	10/26/10

10. <http://www.oxfordjournals.org/abstract/doi/10.1093/oxfordjournals.oxfam.a0000000>

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Journal of Internal Medicine 247: 355–362

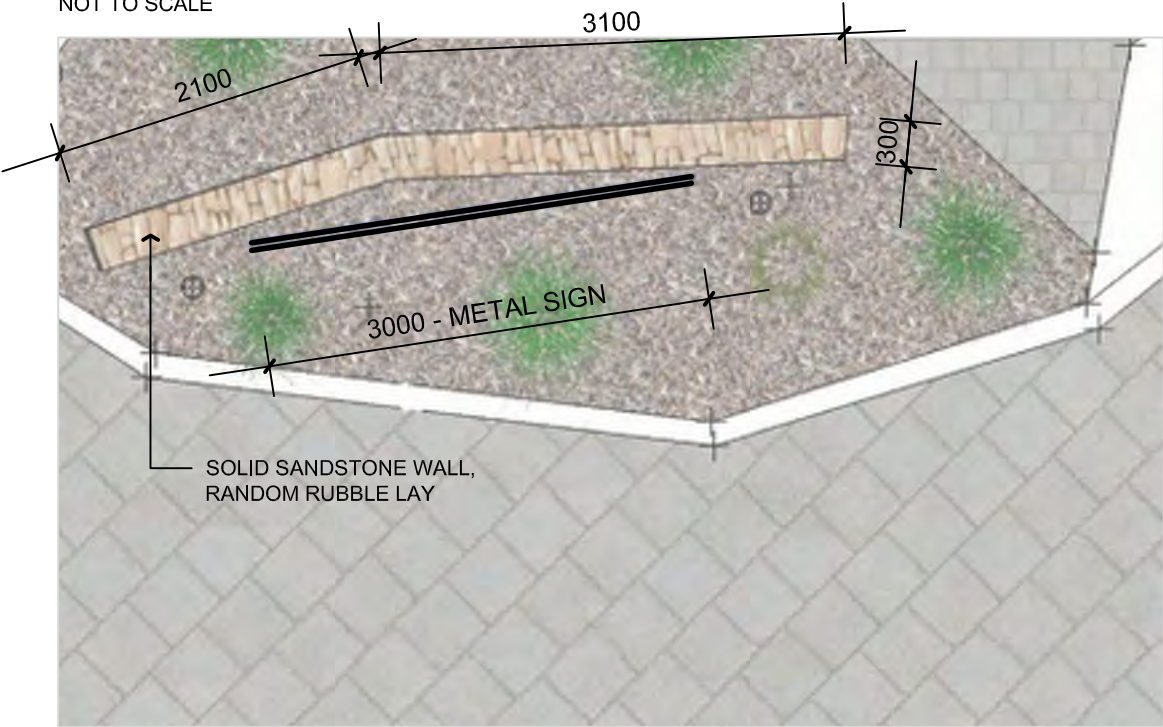
ELEVATION

NOT TO SCALE



PLAN

NOT TO SCALE



SIGNAGE DETAIL

CARPARK ENTRANCE

NOT TO SCALE



ANCHOR DETAIL (EXISTING ANCHOR)

NOT TO SCALE

WORK SAFELY

AMENDMENTS:



Stuart J. Patrick
& ASSOCIATES PTY LTD
building designers
ABN 49 008 110 861

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WILLUNGA S.A. 5172
T: 08 8323 0080
E: design@stuartjpatrick.com.au

DEVELOPMENT PLAN CONSENT
APPLICATION- NOT FOR
CONSTRUCTION

PROPOSED RE-DEVELOPMENT

BUFFALO RESTAURANT
ADELPHI TCE, GLENELG

FILE NO: ADI/GLG/1001
DATE: JUNE 2015
DRAWN BY: MC
SHEET: 1 OF 2
SCALE: AS SHOWN
ORIGINAL SHEET SIZE: A3

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ASSOCIATION OF SA INC.



WORK SAFELY

AMENDMENTS:

Stuart J. Patrick
& ASSOCIATES PTY LTD
building designers
ABN 49 008 110 861

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DEVELOPMENT PLAN CONSENT
APPLICATION- NOT FOR
CONSTRUCTION

PROPOSED RE-DEVELOPMENT

**BUFFALO RESTAURANT
ADELPHI TCE, GLENELG**

FILE NO: ADI/GLG/1001
DATE: JUNE 2015
DRAWN BY: MC
SHEET: 2 OF 2
SCALE: AS SHOWN
ORIGINAL SHEET SIZE: A3

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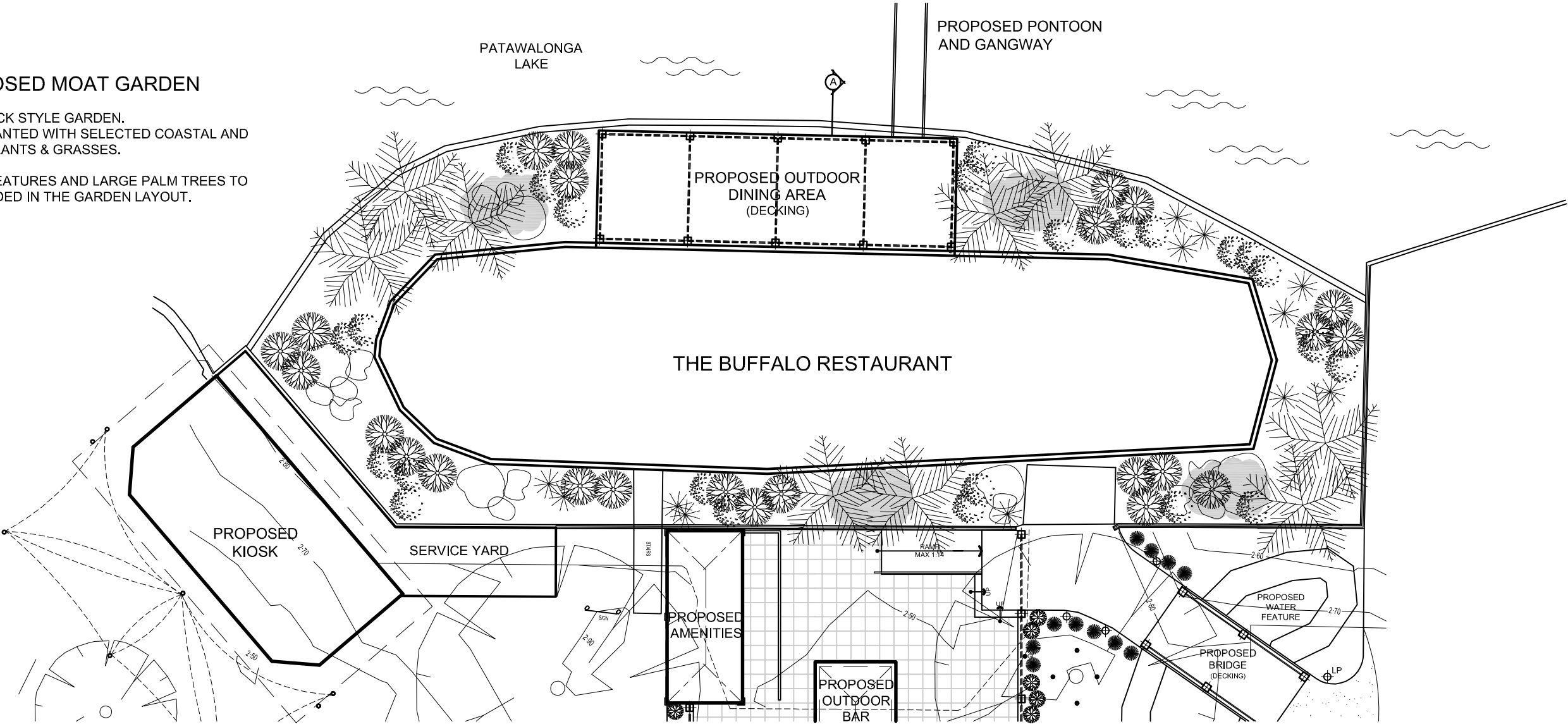
MEMBER OF BUILDING DESIGNERS ASSOCIATION OF SA INC.



PROPOSED MOAT GARDEN

MOSS ROCK STYLE GARDEN.
TO BE PLANTED WITH SELECTED COASTAL AND NATIVE PLANTS & GRASSES.

WATER FEATURES AND LARGE PALM TREES TO BE INCLUDED IN THE GARDEN LAYOUT.



MOAT DETAIL

REFURBISHMENT OF EXISTING MOAT
(ROCK GARDEN, TREES AND WATER FEATURES)

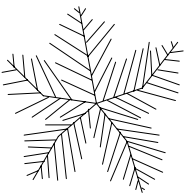
SCALE 1:200



EXAMPLES OF PROPOSED
MOAT GARDEN



LEGEND



- PROPOSED PALM TREES



- PROPOSED WATER FEATURES



EXAMPLE OF PROPOSED MOAT GARDEN

City of Holdfast Bay

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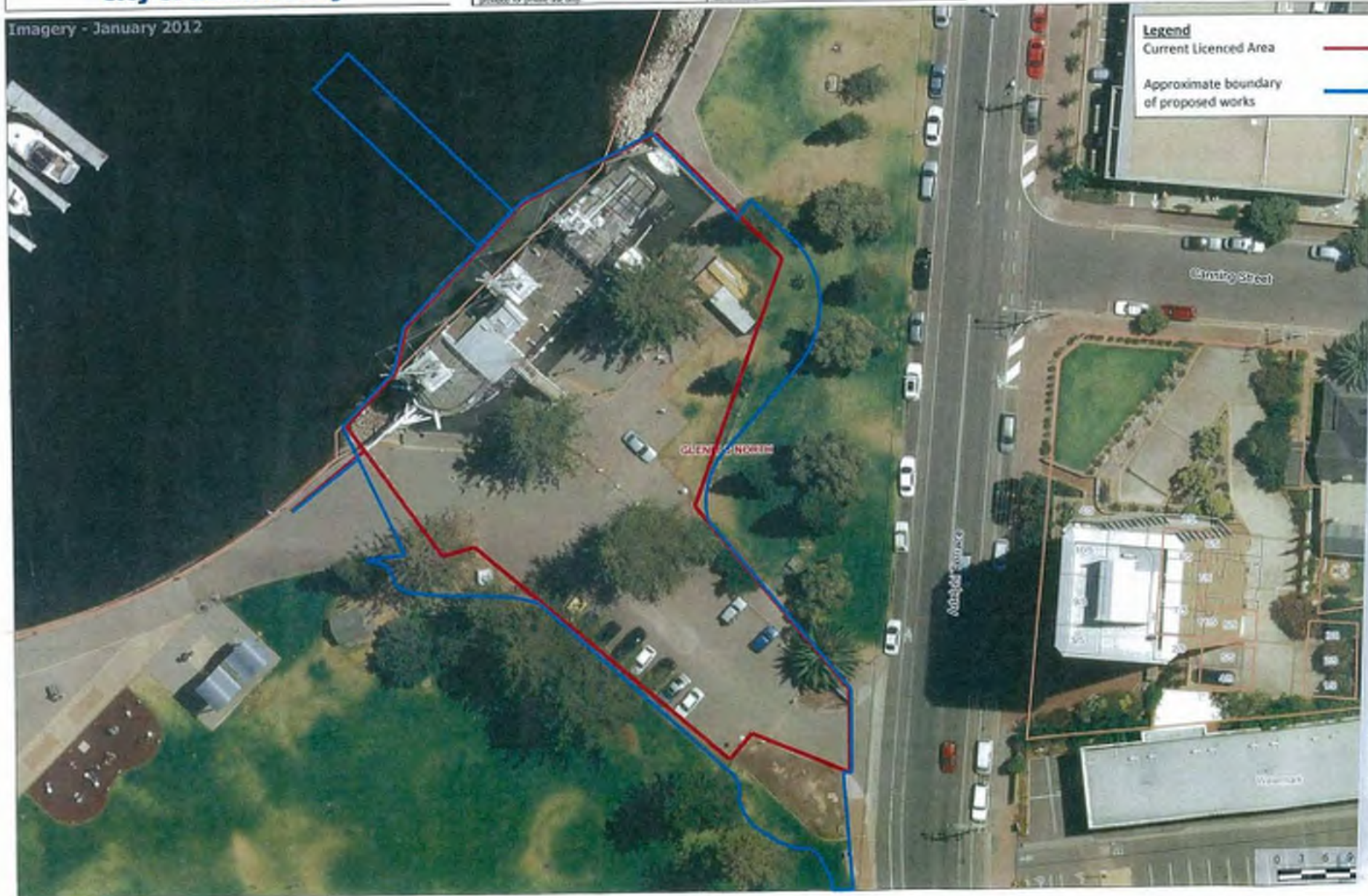
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The Buffalo Licence Area

Map Scale: 1:400.7
Created by user
Thursday, 20 February

Imagery - January 2012





14th October 2015

Development Assessment Commission
GPO Box 1815
Adelaide SA 5001

Good Morning

Re: Application Number 110/L002/15 (Council: 110/00146/15)
Lot 1001, Section 1023 Adelphi Terrace, Glenelg North

In response to your RFI letter dated 13th March 2015, I submit the following for your consideration, in support of the application.

1. Scaled elevation drawings of the kiosk, dining and bar area, and toilets are provided. There will be some storage incorporated within the amenities building as shown on the floor plan, with additional existing storage area inside the ship. No other plant is proposed, other than standard bar equipment, which will be contained within the bar building itself.
2. A drawing of the proposed typical timber deck is provided. The pontoon and gangway is to be a proprietary product (refer attached information from Superior Jetties).

The pontoons are held in position by a pylon system which accommodates vertical tidal movement, in a similar fashion to other pontoon systems in the adjacent marina.

Water depth measurements adjacent to The Buffalo are provided. The depths are adequate for mooring of the charter boat, and no dredging is required.

All stone walling is existing, and is not subject to any alteration.

3. A copy of the Certificate of Title is provided.
4. The intended use of The Buffalo building is to remain unchanged from its previous use as a restaurant.
5. Allowable trading hours for the kiosk are to be between 6.00 am and 9.00 pm 7 days per week, allowing the proprietor discretion to trade in order to take advantage of maximum number of patrons in the area during various times of the year.

Allowable trading hours for the alfresco bar and dining area are to be as per the existing Restaurant Liquor Licence 50615910 (copy attached). Opening hours and activities would be subject to Liquor Licensing Act requirements and conditions. Please refer also to letter of support from Mr Scott Reardon, Liquor Licensing and Community Safety Officer, City of Holdfast Bay (attached).

6. Maximum number of patrons for the site is to be as per current Liquor Licence capacity. An increase in total number of patrons is not sought, rather patron numbers may be re-allocated from existing licensed areas to the alfresco bar and dining area. It is proposed to cater for up to 100 persons in the alfresco bar and dining area, and up to 70 persons on the outdoor deck. An equivalent reduction to the other existing licensed areas, subject to Liquor Licensing Act requirements and approval is proposed. Please refer to letter of support from Mr Scott Reardon for additional information.

7. A maximum number of 3 staff will be employed for the kiosk, and up to a maximum number of 8 staff will be required to service the alfresco bar and dining area.
8. The pontoon is to be used for the mooring of a charter boat. It is envisaged that a third party operator would conduct booked tours for restaurant patrons. The charter boat would not be permanently moored at this site, but rather called in from a remote site as required. No other boats will be permanently moored at this facility.
9. The existing shed/shipping containers have been removed from the site.
10. The existing car park has been refurbished, with new bitumen hot-mix surface, concrete edging and landscaping. Council are to install a parking meter system. 21 carparks in compliance with AS 2890.1 plus an access car park in compliance with AS 2890.6 have been provided (plan attached). In addition, on-street parking is available on Adelphi Terrace and Jetty Road.
11. It is proposed to connect into, the existing services (i.e. sewer, gas, electricity and communications. and infrastructure within the ship in order to mitigate root disturbance to the existing trees. The proposed electrical cable location has been indicated on the site plan, and is to be routed to minimise any root disturbance
12. Position of lighting has been indicated on the site/car parking plan. Glass fencing details have been shown on the drawings. Extent of landscaping has been indicated on the drawings, species to match and or complement existing plantings adjacent to the tram and within the car park area.
13. Loading area for commercial vehicles adjacent to the kiosk has been indicated on the site/car parking plan. Drop-down bollards will provide for access for commercial vehicles to this area. New signage to replace existing sign is to be provided at driveway entrance to the carpark and will be subject to separate application prior to commencement of operation of facility.

I trust the above is satisfactory for your current requirements, however, please don't hesitate to contact me should you require any further clarification or additional information.

Sincerely



Stuart Patrick



The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



Registrar-General

Certificate of Title - Volume 5935 Folio 965

Parent Title(s)	CR 5645/338
Dealing(s) Creating Title	RLG 10140840
Title Issued	21/02/2005
Edition	1
Edition Issued	21/02/2005



Estate Type

FEE SIMPLE

Registered Proprietor

CITY OF HOLDFAST BAY
OF PO BOX 19 BRIGHTON SA 5048

Description of Land

ALLOTMENT 1001 DEPOSITED PLAN 49600
IN THE AREA NAMED GLENELG NORTH
HUNDREDS OF ADELAIDE AND NOARLUNGA

Conditions

IN TRUST TO PERMIT SUFFER AND TO BE USED AT ALL TIMES AS A RESERVE FOR RECREATION PURPOSES

Easements

NIL

Schedule of Dealings

NIL

Notations

Dealings Affecting Title

NIL

Priority Notices



NIL

Notations on Plan

NIL

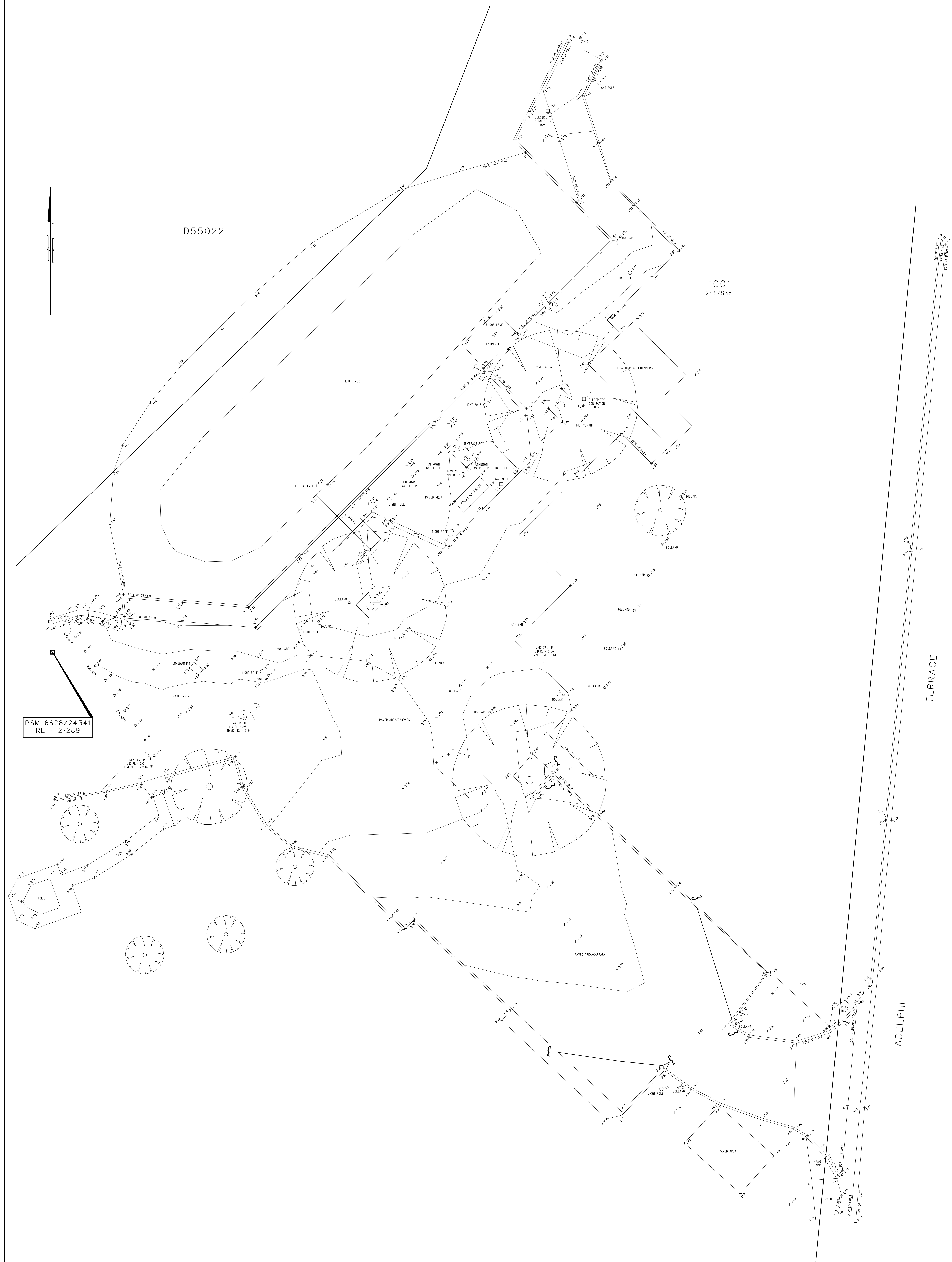
Registrar-General's Notes

NIL

Administrative Interests

NIL

* Denotes the dealing has been re-lodged.



<div>WEBER FRANKW AND ASSOCIATES PTY.LTD.</div> <div>Licensed Surveyors and Land Division Consultants A.C.N. 008 175 957</div> <div>178 Main Road Millicers Vale South Australia 5011 Telephone (08) 8323 8991 Facsimile (08) 8323 9886 Email enquiries@wfa.com.au ACH 008 173 957</div>			<div>NOTES</div> <div>CONTOUR INTERVAL OF 0-100m</div> <div>HEIGHT DATUM IS AHD VIDE PSM 6628/24341 RL = -2.289</div> <div>BOUNDARIES ARE SHOWN FOR PLOT PURPOSES ONLY</div>		<div>DETAIL AND CONTOUR SURVEY</div> <div>PORTION OF ALLOTMENT 1001 IN DP49600 HUNDRED OF NOARLUNGA GLENELG NORTH</div> <div><div><div>0246810</div><div>SCALE METRES</div></div></div>	<div>JOB NO. 6736</div> <div>CAD REF. 6736dt1.cld</div> <div><div>SHEET 1</div><div>REV A</div></div>
	REV	DATE	APP	<div>SURVEY AC DATE 26/03/2014</div> <div>APPROVED SV DATE 31/03/2014</div>		

--- FLOOD PERIMETER ~ JUNE 2003
[RL + 2.36m AHD — 100 year ARI]



Itemref	Quantity	Title/Name, designation, material, dimension etc			Article No./Reference	
Designed by S.K	Checked by Ralph Hack	Approved by - date D.S - 13/11/2009	File name Patawalonga Lake	Date 00/00/00	Scale NTS	
Dep. Water, Land & Biodiversity Conservation			Patawalonga Lake Embankment Survey			
			1	Edition 0	Sheet 1/1	

ATTACHMENT 3A

MARSHALL DAY - ACOUSTIC REPORTS

- ORIGINAL DATED 23 SEPTEMBER 2016
- MOST RECENT (ADOPTED) 14 OCTOBER 2016



MARSHALL DAY
Acoustics 

BUFFALO REDEVELOPMENT NOISE ASSESSMENT

Rp 001 R01 2016306AL | 23 September 2016

Project: **BUFFALO RESTAURANT REDEVELOPMENT**

Prepared for: **DeYoungs**
PO Box 104
O'Sullivan's Beach SA 5166
Australia

Attention: **Mr Troy DeYoung**

Report No.: **Rp 001 R01 2016306AL**

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Document Control

Status:	Rev:	Comments	Date:	Author:	Reviewer:
final	-	Issued	5/8/2016	A. Morabito	P. Heinze
final	R01	Minor amendments	23/9/2016	A. Morabito	P. Heinze

TABLE OF CONTENTS

1.0	INTRODUCTION.....	1
2.0	SITE DESCRIPTION	1
2.1	Location	1
2.2	Proposed Operations	2
2.2.1	Outdoor alfresco dining.....	2
2.2.2	Outdoor deck	2
2.2.3	Outdoor function space.....	2
2.3	Planning Considerations	3
3.0	ASSESSMENT CRITERIA	4
3.1	Music Noise	4
3.1.1	Derived music noise criteria	4
3.2	Patron Noise.....	5
4.0	RECOMMENDATIONS	6
5.0	MUSIC NOISE ASSESSMENT	7
5.1	Music Noise Levels	7
5.1.1	Background music.....	7
5.1.2	Acoustic music	7
6.0	PATRON NOISE ASSESSMENT	8
6.1	Patron Noise Data	8
6.2	Predicted Patron Noise Levels	9
7.0	SUMMARY	11

APPENDIX A GLOSSARY OF TERMINOLOGY

APPENDIX B CITY OF HOLDFAST BAY DEVELOPMENT PLAN

APPENDIX C ACOUSTIC TREATMENT DETAILS

1.0 INTRODUCTION

The Buffalo ship, moored in the Patawalonga River in Glenelg's Wigley Reserve previously operated for many years as a restaurant. In 2015, the former owner sold the replica ship to the DeYoung Group.

The DeYoung Group now propose to redevelop the Buffalo, and include an outdoor alfresco dining area, bar/deck, kiosk, and upper deck function space.

Marshall Day Acoustics Pty Ltd (MDA) has been commissioned by The DeYoung Group to undertake a noise assessment of the proposed redevelopment.

The following report details the proposal, locality, relevant noise criteria and an assessment of noise to the nearest residences.

Acoustic terminology used throughout the report is provided in Appendix A.

2.0 SITE DESCRIPTION

2.1 Location

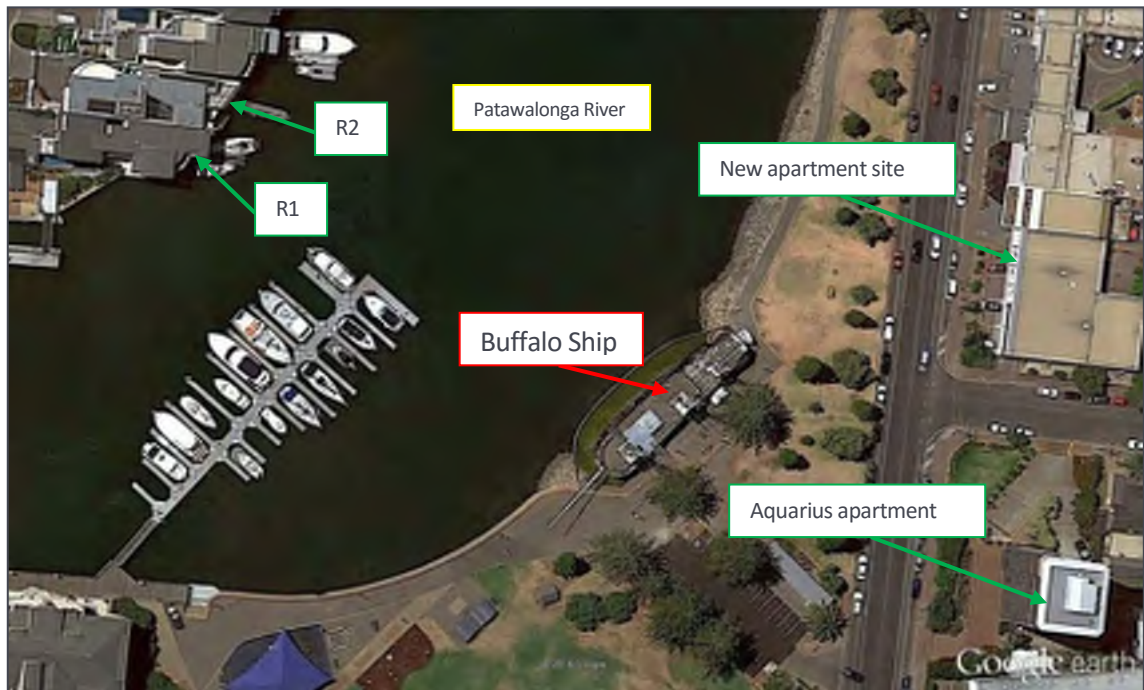
The Buffalo ship is located adjacent Glenelg's Wigley Reserve, at 1 Adelphi Terrace, Glenelg, shown in Figure 1. The site is bounded by the following:

- Wigley Reserve to the east and south. Adelphi Terrace is on the east side of Wigley Reserve, with existing apartments and a new approved hotel development on the east side of Adelphi Terrace
- Patawalonga River to the west and north. Existing residential properties are located on the west side of Patawalonga River

The nearest noise affected premises to the Buffalo ship are identified as follows:

- Existing Aquarius apartments on the eastern side of Adelphi Terrace, approximately 80 m east south east of the Buffalo
- Approved 12 storey apartment complex to be built on the eastern side of Adelphi Terrace, approximately 70 m east north east of the Buffalo
- Existing 3 storey residential properties on the western side of the Patawalonga River, approximately 110 m north west of the Buffalo

Figure 1: Buffalo Ship location (Google Earth)



2.2 Proposed Operations

The Buffalo redevelopment site plan is provided in Figure 2. The current License for the premise, limits the overall capacity to not exceed 250 persons at any one time.

In terms of external late night operations, with the potential to cause noise, the following areas and operations are proposed.

2.2.1 Outdoor alfresco dining

The outdoor alfresco dining area located on south east side of the Buffalo Ship, is proposed to operate until 2am, 7 days a week, and cater for up to 100 patrons.

Music in this dining area will be limited to background music only and 2 piece live acoustic music, including guitar and a singer.

2.2.2 Outdoor deck

The outdoor deck located on west side of the Buffalo Ship, is proposed to operate until midnight, and cater for up to 70 patrons, 7 days a week.

Music on the deck will be limited to background music only.

2.2.3 Outdoor function space

The outdoor function space is located on the upper deck of the Buffalo Ship is proposed to operate until midnight, 7 days a week for events such as corporate functions and weddings. The current license for the upper deck including the internal and outdoor areas is for 150 people. The outdoor function space can cater for up to 80 patrons.

Music in this outdoor space will be limited to background music only and 2 piece live acoustic music, including guitar and a singer.

3.0 ASSESSMENT CRITERIA

3.1 Music Noise

The City of Holdfast Bay Development Plan details applicable noise criteria in the form of the following principle of development control for venues proposing music noise:

10 Development proposing music should include noise attenuation measures that achieve the following desired noise levels:

<i>Noise level assessment location</i>	<i>Desired noise level</i>
<i>Adjacent existing noise sensitive development property boundary</i>	<i>Less than 8 dB above the level of background noise ($L_{90,15min}$) in any octave band of the sound spectrum</i> <i>and</i> <i>Less than 5 dB(A) above the level of background noise ($L_{A90,15min}$) for the overall (sum of all octave bands) A-weighted level</i>
<i>Adjacent land property boundary</i>	<i>Less than 65dB(Lin) at 63Hz and 70dB(Lin) in all other octave bands of the sound spectrum</i> <i>or</i> <i>Less than 8 dB above the level of background noise ($L_{90,15min}$) in any octave band of the sound spectrum and 5 dB(A) overall (sum of all octave bands) A-weighted level</i>

The “desired” music noise level is not explicitly defined in the Development Plan. The above principle of development control is however consistent with music noise criteria generally used in South Australia. Music noise in South Australia is typically assessed with reference to the L_{10} level, the noise level exceeded for 10% of the measurement period. Accordingly, we expect the “desired noise level” referred in the above principle of development control relates to the L_{10} noise metric.

3.1.1 Derived music noise criteria

In order to derive applicable music noise criteria, in accordance with the City of Holdfast Bay principles of development control, the background noise at the noise affected premise must be established.

Long term noise measurements of the background noise were undertaken at the north end of the Buffalo site, representative of background noise levels at the nearest identified noise affected premises. Measurements were conducted between Friday 15 July 2016 and Thursday 21 July 2016, using a 01dB Duo Environmental Noise Logger (serial number 10299), calibrated before and after the measurement set using a Brüel & Kjær Type 4230 sound level calibrator (serial number 831147).

The lowest measured background noise levels during the proposed hours of operations, detailed in Table 1, have been used to derive relevant music noise criteria, detailed in Table 2.

Table 1: Measured background noise levels, dB L_{OCT90}

Background noise level	dBA	Octave Band Centre Frequency (Hz)						
		63	125	250	500	1k	2k	4k
Sun-Thu, to midnight	41	48	46	39	37	37	32	21
Sun-Thu, midnight to 2am	38	46	41	37	36	33	28	19
Fri-Sat, to midnight	49	52	47	44	44	46	40	24
Fri-Sat, midnight to 2am	44	47	44	41	41	40	34	20

Table 2: Derived music noise criteria, dB L_{OCT10}

Music noise criteria	dBA	Octave Band Centre Frequency (Hz)						
		63	125	250	500	1k	2k	4k
Sun-Thu, to midnight	46	56	54	47	45	45	40	29
Sun-Thu, midnight to 2am	43	54	49	45	44	41	36	27
Fri-Sat, to midnight	54	60	55	52	52	54	48	32
Fri-Sat, midnight to 2am	49	55	52	49	49	48	42	28

3.2 Patron Noise

Noise from patrons outdoors is not covered under any Environment Protection Policy or South Australian guideline. In the absence of an established policy, reference has been made to design criteria that have been adopted for a number of similar projects.

The assessment criteria have been developed with consideration of the *Environment Protection (Noise) Policy 2007* (EPP) as well as the existing background noise level in the area.

The EPP is a broad document designed to protect the amenity of residential or business areas from noise sources including mechanical plant, industry, construction activity etc, however it specifically excludes noise from licensed premises and patrons in general.

In June 2009, The SA EPA issued *Guidelines for the use of the Environment Protection (Noise) Policy 2007*, which states the following:

Noise from licensed premises is excluded from the objective assessment procedure of the Noise EPP to provide the OLGC with the flexibility to consider the range of factors under section 106 [of the Liquor Licensing Act 1997]. Notwithstanding this exclusion, comparison with the indicative noise levels and consideration of the factors of clause 19 of the Noise EPP may assist the Commissioner or Licensing Court in situations where an objective procedure is considered useful.

We therefore consider the above approach appropriate for the assessment of noise from the proposed outdoor patron areas.

The EPP separates the day into two different time periods – day (0700 – 2200 h) and night (2200 – 0700 h). Indicative noise levels are provided based on the land use category and zoning for the noise source and noise affected premise.

The relevant indicative noise level minus 5 dB for this site during the night period is 45 dB L_{Aeq}. This level has been adopted as the design criterion level when assessing patron noise at the nearest noise affected premises. The indicative noise level may however, be relaxed where it can be demonstrated that the background noise level is higher than the indicative noise levels. Accordingly, on Friday and Saturday nights until midnight, a criterion of 49 dB L_{Aeq} is deemed appropriate.

4.0 RECOMMENDATIONS

Preliminary noise predictions without acoustic treatment or other controls indicated exceedance of the recommended noise criteria. Accordingly, the music and patron noise assessment in Section 5.0 and Section 6.0 respectively have been based on the following recommendations (refer Appendix C):

- Background music and 2 piece live acoustic music, including guitar and a singer only, occurs at the site. No other live or DJ type music is played
- The outdoor alfresco dining area is 'sunk' into the ground approximately 500 mm. A minimum balustrade 1.8 m is to be provided to north, south and east ends. A cranked barrier at approximately 45 degrees, forming a 4 m partial canopy is also recommended on the east end, acoustically lined with 50mm thick, 32 kg/m² acoustic insulation faced with a fabric or perforated facing with 25% open area
- The outdoor deck is shielded to residences on the western side of the Patawalonga River by the construction of an acoustically solid bar on west side of the deck, and with solid balustrade barrier to the north and south ends. The minimum height of the effective '3 sided barrier' required is 1.8 m above the outdoor deck
- Provide a minimum 2.1 m high barrier and 3.2 m high barrier above the ship upper deck line to north and south ends respectively of the upper function space. The 2.1 m north end barrier to be acoustically lined with 50 mm thick, 32 kg/m² acoustic insulation lined with a fabric or perforated facing with a 25 % open area.
- Limiting the number of patrons on the upper deck external space (i.e. Outdoor Function Space) to 40 patrons, Sunday to Thursday nights.

5.0 MUSIC NOISE ASSESSMENT

5.1 Music Noise Levels

5.1.1 Background music

Background music only is proposed at the site. Background music noise can be defined as a level that enables patrons to conduct a normal conversation at a distance of 600 mm without having to raise their voices to a substantial degree. In MDA's opinion, this is equivalent to a music noise level of approximately 68 dB L_{Aeq} , with example spectra as shown in Table 3.

Table 3: Typical background music noise levels, dB

	dBA	Octave Band Centre Frequency (Hz)						
		63	125	250	500	1k	2k	4k
Background music level, L_{eq}	68	66	66	66	61	61	61	61
Background music level, L_{10} *	71	69	69	69	64	64	64	64

*Note: L_{10} based on $L_{10} = L_{eq} + 3$

The predicted noise levels indicate that background music will achieve the nominated criteria without additional treatment other than limiting music to background only or to a level of 71 dB L_{A10} when measured at 3 m from a speaker.

We also recommend the following with respect to outdoor speaker placement:

- Speakers in the outdoor alfresco dining area be placed on the eastern wall facing away from the Adelphi Terrace apartments
- Speakers on the outdoor deck be placed on the western wall facing away from residents located on the opposite side of the Patawalonga River
- Function space to be limited to a single speaker located at the north or south end of the area

5.1.2 Acoustic music

The proposal redevelopment seeks live music in the form of a duo, including an acoustic guitar and a singer in the upper deck function space or outdoor alfresco dining area. Previous noise level measurements by MDA of similar music indicate a noise level of 71 dB L_{Aeq} at 5 m. Based on this level, and limiting live music to acoustic guitar and a singer only, compliance with the music noise criteria can be readily achieved.

6.0 PATRON NOISE ASSESSMENT

6.1 Patron Noise Data

Marshall Day Acoustics and other acoustic consultants in Melbourne have measured patron noise from several different venues. These measurements indicate a large variation in the noise levels of crowds, due to a number of factors including the situational context of the crowd, and the intensity and character depending upon the number of patrons and their behaviour.

For the purpose of predicting noise levels from the Buffalo redevelopment, external patron areas have been categorised according to the envisaged use, as outlined in Table 4. Reference sound power data for one person is detailed in the 2011 Hayne paper¹.

Table 4: Patron area use categories

Area use category	Reference sound power data per one person		Area use definition
	Equivalent Level	Maximum Level	
Restaurant dining	78 dB L _{AW}	98 dB L _{AW}	Outdoor alfresco dining area and upper deck function space
Taverns with food offerings	83 dB L _{AW}	104 dB L _{AW}	Outdoor deck

Based on the above reference sound power data and measurements by Marshall Day Acoustics, a simplified empirical relationship to represent the total sound power level for which crowd numbers and character were varied has been derived for determining design equivalent and maximum sound power level as follows:

- Design equivalent sound power level - derived by assuming one third of the total crowd speaks continuously over the duration of the assessment period, and each of these speakers emit a constant total sound power level over the duration of the assessment period. In practice, the actual number of individuals speaking, the sound power emitted by each individual, and the temporal characteristics of each speaker will vary considerably over the assessment period. The derived values therefore do not represent the actual percentage of patrons speaking, or the emission of each patron, but simply represent the total sound power level for the number of patrons
- Design maximum sound power level - derived by assuming that the maximum noise level occurs as a result of two individuals simultaneously producing a maximum level.

The above empirical representation of typical patron noise levels are suitable for design purposes based on the measured data. This approach has been used for Victorian Civil and Administrative Tribunal (VCAT) hearings for the assessment of proposed outdoor venues, which have since been developed and operational.

¹ Hayne et al 2011, 'Prediction of noise from small to medium sized crowds', in *Acoustics 2011: Breaking New Ground, Proceedings of the Annual Conference of the Australian Acoustical Society*, AAS Queensland Division 2011, Gold Coast, paper number 133.

The sound power level used in this assessment is provided in Table 5.

Table 5: Patron noise sound power level data

Area	Semi-steady state	Maximum
Outdoor alfresco dining (100 patrons)	93 dB L _{AW}	101 dB L _{AW}
Outdoor deck (70 patrons)	97 dB L _{AW}	107 dB L _{AW}
Function space (80 patrons)	92 dB L _{AW}	101 dB L _{AW}

6.2 Predicted Patron Noise Levels

A 3-dimensional digital noise model of the site and surrounding built environment has been prepared using SoundPLAN v7.4 proprietary noise mapping software, in order to predict patron noise levels to the nearest noise affected premises.

The noise model has been used to calculate noise levels in accordance with ISO-9613-2:1996 *Acoustics – Attenuation of sound during propagation outdoors – Part 2: General method of calculation* (ISO 9613).

The ISO 9613 standard specifies an engineering method for calculating noise at a known distance from a variety of sources under meteorological conditions that are favourable to sound propagation. The standard defines favourable conditions as downwind propagation where the source blows from the source to the receiver within an angle of +/-45 degrees from a line connecting the source to the receiver, at wind speeds between approximately 1 m/s and 5 m/s, measured at a height of 3 m to 11 m above the ground. Equivalently, the method accounts for average propagation under a well-developed moderate ground based thermal inversion.

Accordingly, predictions on the basis of ISO 9613 account for instances when local atmospheric conditions at the site favour the propagation of sound to surrounding receptor locations. Under alternative atmospheric conditions, such as when the wind is blowing from a receiver location to the site, the noise levels would be lower than calculated.

To calculate noise levels according to the ISO 9613, the noise emissions of each source are firstly characterised in the form of octave band frequency levels. A series of octave band attenuation factors are then calculated for a range of effects including:

- Geometric divergence;
- Air absorption;
- Reflecting obstacles;
- Screening; and
- Ground reflections.

The highest predicted patron noise levels based on the proposed operations and recommendations in Section 4.0 are provided in Table 6. The predictions, to the nearest noise sensitive receivers as shown on Figure 1, are conservative with the assumption that all patrons are located in external areas to the main ship areas.

Table 6: Predicted patron noise levels, dB L_{Aeq}

Receiver	Sun - Thurs, to midnight Outdoor alfresco dining area (100), use of deck (70) and external function space (40)	Fri - Sat, to midnight Outdoor alfresco dining area (100), use of deck (70) and external function space (80)	Sun - Sat, midnight to 2am Outdoor alfresco dining area (100)
Aquarius Apartments			
Ground	38	38	37
Level 1	39	40	38
Level 2	41	42	41
Level 3	43	43	43
Level 4	44	44	44
Level 5	45	45	44
Level 6	45	45	45
Level 7	45	45	45
Level 8 and above	45	46	45
New apartment			
Ground	40	41	40
Level 1	42	42	41
Level 2	43	44	43
Level 3	45	45	44
Level 4	45	45	45
Level 5	46	46	45
Level 6	46	47	45
Level 7 and above	47	48	45
Other			
R1 (west)	44	44	24
R2 (west)	44	44	24
Criteria	45	49	45

The predicted noise levels comply with the nominated criteria with the exception of a minor exceedance (1-2 dB) at the upper floor levels of the new apartment, on Sunday – Thursday nights, until midnight.

7.0 SUMMARY

The DeYoung Group now propose to redevelop the Buffalo ship which previously operated for many years as a restaurant.

Marshall Day Acoustics Pty Ltd (MDA) has undertaken a noise assessment of the proposed redevelopment which includes the following:

- An outdoor alfresco dining area located on south east side of the Buffalo Ship, proposed to operate until 2 am, and cater for up to 100 patrons.
- An outdoor deck located on west side of the Buffalo Ship, proposed to operate until midnight, and cater for up to 70 patrons.
- An outdoor function space located on the upper deck of the Buffalo Ship proposed to operate until midnight for corporate functions and weddings. The current license limits upper deck use for 150 patrons (internal and external locations); of which up to 80 patrons can be located external.

The assessment has demonstrated the proposed redevelopment can achieve the recommended noise objectives, based on the following recommendations:

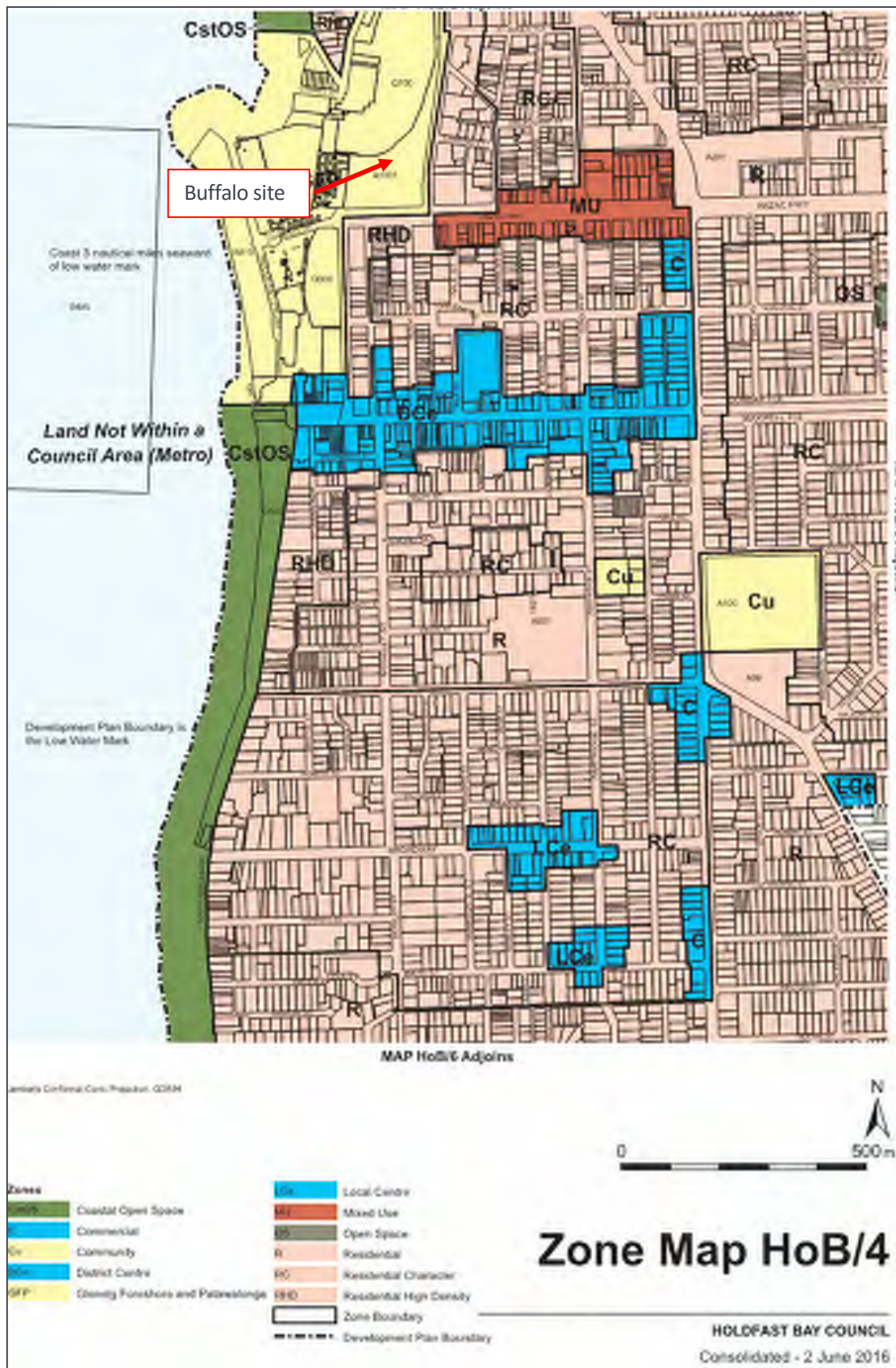
- Music to be limited to background music only and live acoustic music (e.g. 2 piece acoustic guitar and singer)
- Background music to be played at a level of 71 dB L_{A10} when measured at 3 m from the speaker
 - Speakers in the outdoor alfresco dining area be placed on the eastern wall facing away from the Adelphi Terrace apartments
 - Speakers on the outdoor deck be placed on the western wall facing away from residents located on the opposite side of the Patawalonga River
 - Speaker in outdoor function space to be limited to a single speaker located at the north or south end of the area
- Background music and 2 piece live acoustic music, including guitar and a singer only, occurs at the site. No other live or DJ type music is played
- The outdoor alfresco dining area is 'sunk' into the ground approximately 500 mm. A minimum balustrade 1.8 m is to be provided to north, south and east ends. A cranked barrier at approximately 45 degrees, forming a 4 m - partial canopy is also recommended on the east end, acoustically lined with 50mm thick, 32 kg/m² acoustic insulation faced with a fabric or perforated facing with 25% open area
- The outdoor deck is shielded to residences on the western side of the Patawalonga Rive by an acoustically solid bar on west side of the deck, with solid balustrade barrier to the north and south ends. The minimum height of the effective '3 sided barrier' required is 1.8 m
- Provide a minimum 2.1 m high barrier and 3.2 m high above the deck line to north and south ends respectively of the upper function space. The 2.1 m north end barrier to be acoustically lined with 50 mm thick, 32 kg/m² acoustic insulation lined with a fabric or perforated facing with a 25 % open area
- Limiting the number of patrons on the upper deck external space to 40 patrons, Sunday to Thursday nights.

APPENDIX A GLOSSARY OF TERMINOLOGY

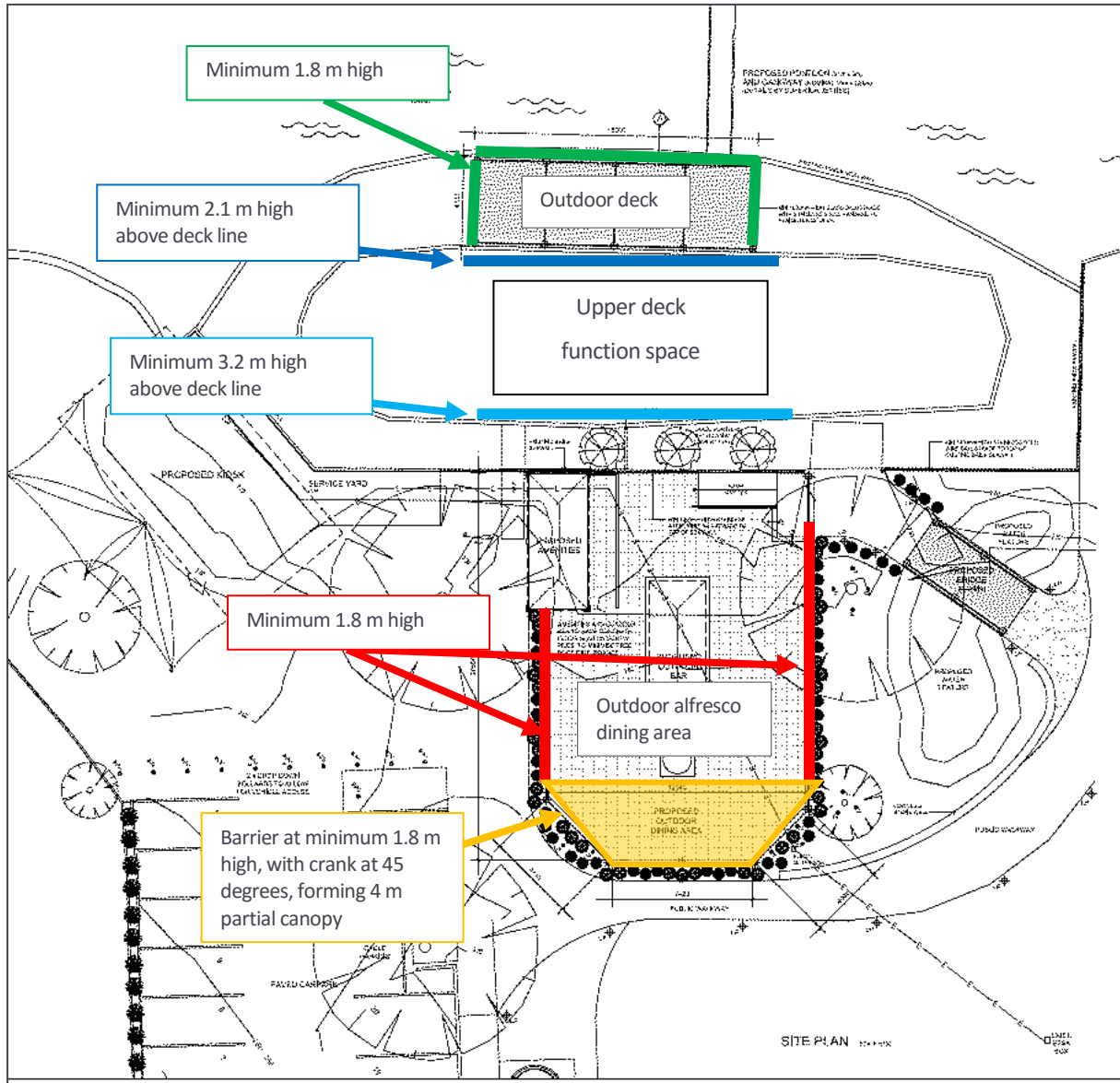
Ambient	The ambient noise level is the noise level measured in the absence of the intrusive noise or the noise requiring control. Ambient noise levels are frequently measured to determine the situation prior to the addition of a new noise source.
A-weighting	The process by which noise levels are corrected to account for the non-linear frequency response of the human ear.
dB	Decibel. The unit of sound level.
Frequency	The number of pressure fluctuation cycles per second of a sound wave. Measured in units of Hertz (Hz).
Hertz (Hz)	Hertz is the unit of frequency. One hertz is one cycle per second. One thousand hertz is a kilohertz (kHz).
L_{A10}	The noise level exceeded for 10% of the measurement period, measured in dBA. This is commonly referred to as the average maximum noise level.
L_{A90}	The noise level exceeded for 90% of the measurement period, measured in dBA. This is commonly referred to as the background noise level.
L_{Aeq}	The A-weighted equivalent continuous sound level. This is commonly referred to as the average noise level and is measured in dB.
L_{OCT10}	The noise level exceeded for 10% of the measurement period in the octave bands 63Hz-4kHz. Commonly referred to as the average maximum noise level.
L_{OCT90}	The noise level exceeded for 90% of the measurement period in the octave bands 63Hz-4kHz. Commonly referred to as the background noise level.
L_{WA}	The A-weighted Sound Power Level. The level of total sound power radiated by a sound source.
Octave Band	A range of frequencies where the highest frequency included is twice the lowest frequency. Octave bands are referred to by their logarithmic centre frequencies, these being 31.5 Hz, 63 Hz, 125 Hz, 250 Hz, 500 Hz, 1 kHz, 2 kHz, 4 kHz, 8 kHz, and 16 kHz for the audible range of sound.

APPENDIX B CITY OF HOLDFAST BAY DEVELOPMENT PLAN

B1 Zone Map



APPENDIX C ACOUSTIC TREATMENT DETAILS





MARSHALL DAY
Acoustics 

BUFFALO REDEVELOPMENT NOISE ASSESSMENT

Rp 001 R02 2016306AL | 14 October 2016

Project: **BUFFALO RESTAURANT REDEVELOPMENT**

Prepared for: **DeYoungs**
PO Box 104
O'Sullivans Beach SA 5166
Australia

Attention: **Mr Troy DeYoung**

Report No.: **Rp 001 R02 2016306AL**

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Document Control

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final	-	Issued	5/8/2016	A. Morabito	P. Heinze
final	R01	Minor amendments	23/9/2016	A. Morabito	P. Heinze
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TABLE OF CONTENTS

1.0	INTRODUCTION	1
2.0	SITE DESCRIPTION	1
2.1	Location	1
2.2	Proposed Operations	2
2.2.1	Outdoor alfresco dining.....	2
2.2.2	Outdoor deck	2
2.2.3	Outdoor function space.....	2
2.3	Planning Considerations	3
3.0	ASSESSMENT CRITERIA	4
3.1	Music Noise	4
3.1.1	Derived music noise criteria	4
3.2	Patron Noise	5
4.0	RECOMMENDATIONS	6
5.0	MUSIC NOISE ASSESSMENT	7
5.1	Music Noise Levels	7
5.1.1	Background music.....	7
5.1.2	Acoustic music	7
6.0	PATRON NOISE ASSESSMENT	8
6.1	Patron Noise Data	8
6.2	Predicted Patron Noise Levels	9
7.0	SUMMARY	11

APPENDIX A GLOSSARY OF TERMINOLOGY

APPENDIX B CITY OF HOLDFAST BAY DEVELOPMENT PLAN

APPENDIX C ACOUSTIC TREATMENT DETAILS

1.0 INTRODUCTION

The Buffalo ship, moored in the Patawalonga River in Glenelg's Wigley Reserve previously operated for many years as a restaurant. In 2015, the former owner sold the replica ship to the DeYoung Group.

The DeYoung Group now propose to redevelop the Buffalo, and include an outdoor alfresco dining area, bar/deck, kiosk, and upper deck function space.

Marshall Day Acoustics Pty Ltd (MDA) has been commissioned by The DeYoung Group to undertake a noise assessment of the proposed redevelopment.

The following report details the proposal, locality, relevant noise criteria and an assessment of noise to the nearest residences.

Acoustic terminology used throughout the report is provided in 0.

2.0 SITE DESCRIPTION

2.1 Location

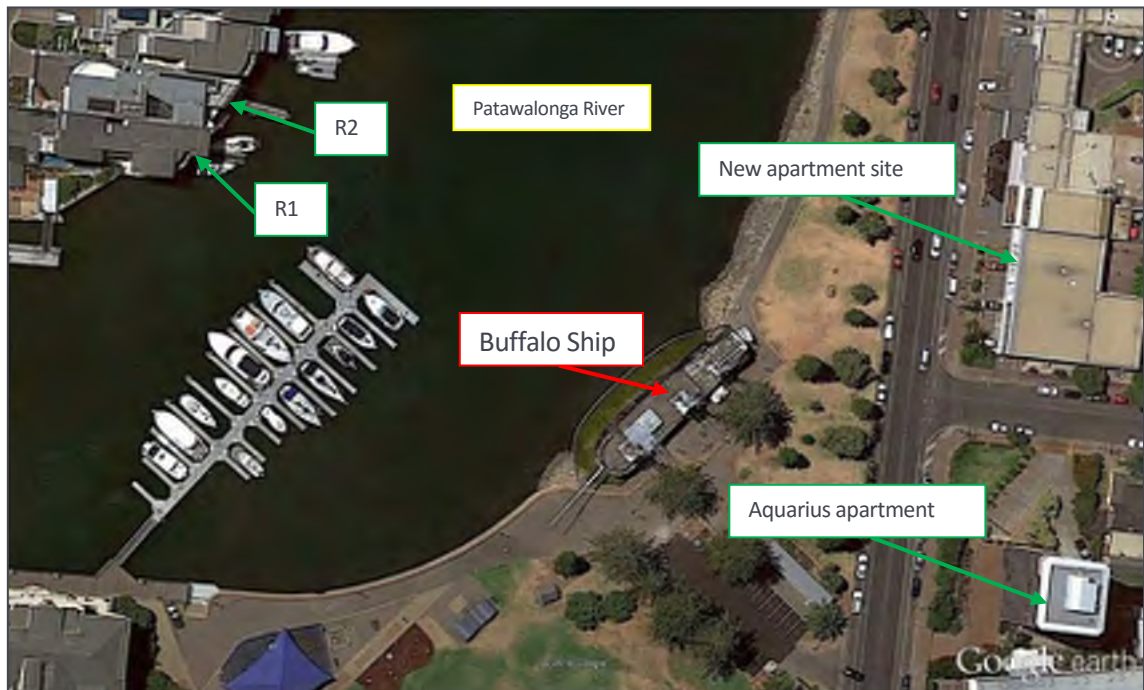
The Buffalo ship is located adjacent Glenelg's Wigley Reserve, at 1 Adelphi Terrace, Glenelg, shown in Figure 1. The site is bounded by the following:

- Wigley Reserve to the east and south. Adelphi Terrace is on the east side of Wigley Reserve, with existing apartments and a new approved hotel development on the east side of Adelphi Terrace
- Patawalonga River to the west and north. Existing residential properties are located on the west side of Patawalonga River

The nearest noise affected premises to the Buffalo ship are identified as follows:

- Existing Aquarius apartments on the eastern side of Adelphi Terrace, approximately 80 m east south east of the Buffalo
- Approved 12 storey apartment complex to be built on the eastern side of Adelphi Terrace, approximately 70 m east north east of the Buffalo
- Existing 3 storey residential properties on the western side of the Patawalonga River, approximately 110 m north west of the Buffalo

Figure 1: Buffalo Ship location (Google Earth)



2.2 Proposed Operations

The Buffalo redevelopment site plan is provided in Figure 2. The current License for the premise, limits the overall capacity to not exceed 250 persons at any one time.

In terms of external late night operations, with the potential to cause noise, the following areas and operations are proposed.

2.2.1 Outdoor alfresco dining

The outdoor alfresco dining area located on south east side of the Buffalo Ship, is proposed to operate until 2am, 7 days a week, and cater for up to 100 patrons.

Music in this dining area will be limited to background music only and 2 piece live acoustic music, including guitar and a singer.

2.2.2 Outdoor deck

The outdoor deck located on west side of the Buffalo Ship, is proposed to operate until midnight, and cater for up to 70 patrons, 7 days a week.

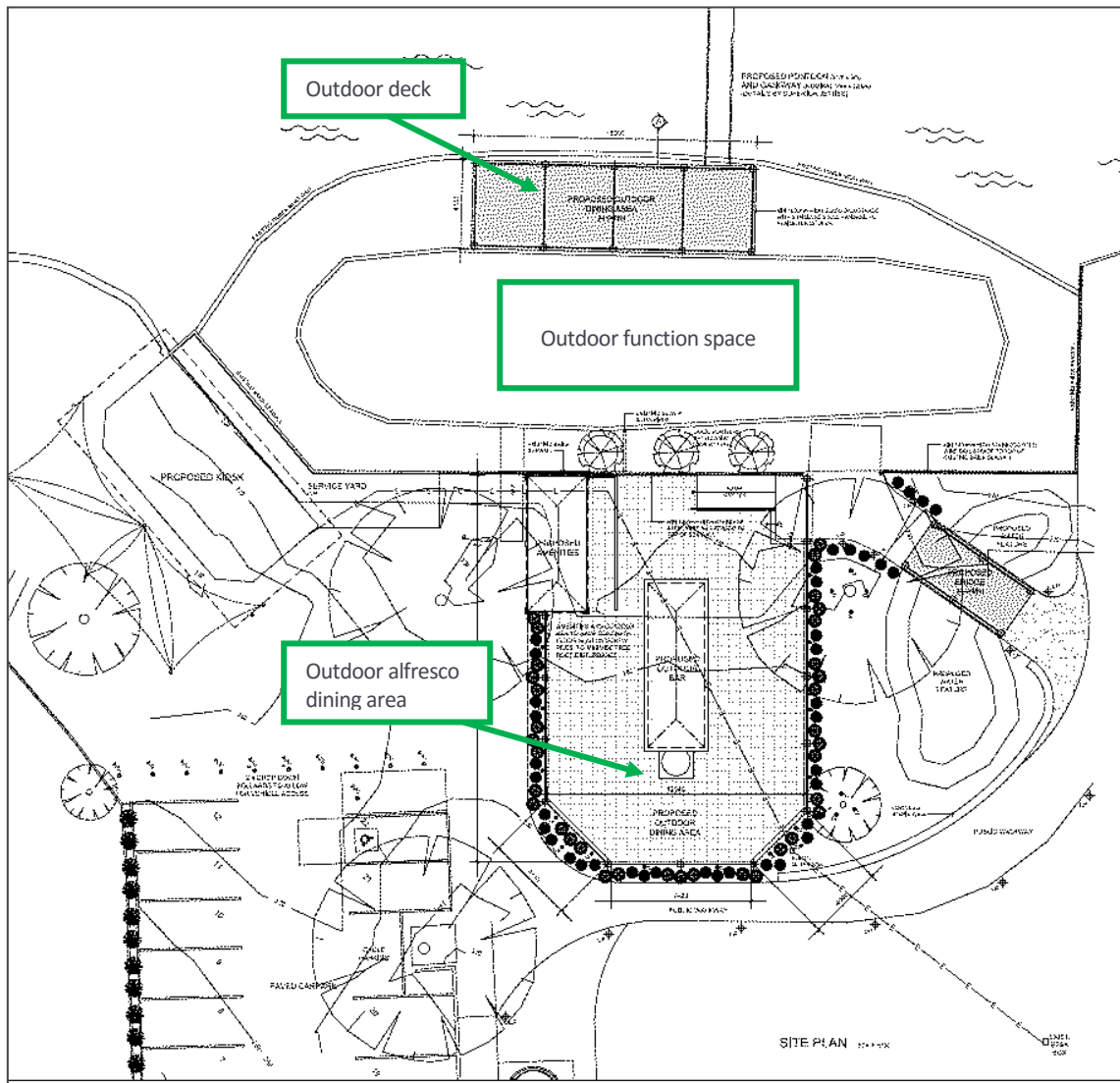
Music on the deck will be limited to background music only.

2.2.3 Outdoor function space

The outdoor function space is located on the upper deck of the Buffalo Ship is proposed to operate until midnight, 7 days a week for events such as corporate functions and weddings. The current license for the upper deck including the internal and outdoor areas is for 150 people. The outdoor function space can cater for up to 80 patrons.

Music in this outdoor space will be limited to background music only and 2 piece live acoustic music, including guitar and a singer.

Figure 2: Buffalo redevelopment site plan



2.3 Planning Considerations

The Buffalo Ship and surrounds are located within the City of Holdfast Bay.

With reference to the City of Holdfast Bay Development Plan consolidated 2 June 2016, the Buffalo site is within the Glenelg Foreshore and Patawalonga (GFP) Zone.

Residents on the east side of Adelphi Terrace are within the Residential Character (RC) Zone, and residents on the west side of the Patawalonga River are within the Residential High Density (RHD) Zone, both specifically within 'Policy Area 15 Urban Glenelg'.

The land use zoning map applicable to the site is provided in Appendix B.

The site land use category is for the purpose of this assessment is 'commercial' while the land use category for the nearest noise-affected premises to the east and west of the site is of a 'residential' nature.

3.0 ASSESSMENT CRITERIA

3.1 Music Noise

The City of Holdfast Bay Development Plan details applicable noise criteria in the form of the following principle of development control for venues proposing music noise:

10 Development proposing music should include noise attenuation measures that achieve the following desired noise levels:

Noise level assessment location	Desired noise level
Adjacent existing noise sensitive development property boundary	Less than 8 dB above the level of background noise ($L_{90,15min}$) in any octave band of the sound spectrum and Less than 5 dB(A) above the level of background noise ($L_{A90,15min}$) for the overall (sum of all octave bands) A-weighted level
Adjacent land property boundary	Less than 65dB(Lin) at 63Hz and 70dB(Lin) in all other octave bands of the sound spectrum or Less than 8 dB above the level of background noise ($L_{90,15min}$) in any octave band of the sound spectrum and 5 dB(A) overall (sum of all octave bands) A-weighted level

The “desired” music noise level is not explicitly defined in the Development Plan. The above principle of development control is however consistent with music noise criteria generally used in South Australia. Music noise in South Australia is typically assessed with reference to the L_{10} level, the noise level exceeded for 10% of the measurement period. Accordingly, we expect the “desired noise level” referred in the above principle of development control relates to the L_{10} noise metric.

3.1.1 Derived music noise criteria

In order to derive applicable music noise criteria, in accordance with the City of Holdfast Bay principles of development control, the background noise at the noise affected premise must be established.

Long term noise measurements of the background noise were undertaken at the north end of the Buffalo site, representative of background noise levels at the nearest identified noise affected premises. Measurements were conducted between Friday 15 July 2016 and Thursday 21 July 2016, using a 01dB Duo Environmental Noise Logger (serial number 10299), calibrated before and after the measurement set using a Brüel & Kjær Type 4230 sound level calibrator (serial number 831147).

The lowest measured background noise levels during the proposed hours of operations, detailed in Table 1, have been used to derive relevant music noise criteria, detailed in Table 2.

Table 1: Measured background noise levels, dB L_{OCT90}

Background noise level	dBA	Octave Band Centre Frequency (Hz)						
		63	125	250	500	1k	2k	4k
Sun-Thu, to midnight	41	48	46	39	37	37	32	21
Sun-Thu, midnight to 2am	38	46	41	37	36	33	28	19
Fri-Sat, to midnight	49	52	47	44	44	46	40	24
Fri-Sat, midnight to 2am	44	47	44	41	41	40	34	20

Table 2: Derived music noise criteria, dB L_{OCT10}

Music noise criteria	dBA	Octave Band Centre Frequency (Hz)						
		63	125	250	500	1k	2k	4k
Sun-Thu, to midnight	46	56	54	47	45	45	40	29
Sun-Thu, midnight to 2am	43	54	49	45	44	41	36	27
Fri-Sat, to midnight	54	60	55	52	52	54	48	32
Fri-Sat, midnight to 2am	49	55	52	49	49	48	42	28

3.2 Patron Noise

Noise from patrons outdoors is not covered under any Environment Protection Policy or South Australian guideline. In the absence of an established policy, reference has been made to design criteria that have been adopted for a number of similar projects.

The assessment criteria have been developed with consideration of the *Environment Protection (Noise) Policy 2007* (EPP) as well as the existing background noise level in the area.

The EPP is a broad document designed to protect the amenity of residential or business areas from noise sources including mechanical plant, industry, construction activity etc, however it specifically excludes noise from licensed premises and patrons in general.

In June 2009, The SA EPA issued *Guidelines for the use of the Environment Protection (Noise) Policy 2007*, which states the following:

Noise from licensed premises is excluded from the objective assessment procedure of the Noise EPP to provide the OLGC with the flexibility to consider the range of factors under section 106 [of the Liquor Licensing Act 1997]. Notwithstanding this exclusion, comparison with the indicative noise levels and consideration of the factors of clause 19 of the Noise EPP may assist the Commissioner or Licensing Court in situations where an objective procedure is considered useful.

We therefore consider the above approach appropriate for the assessment of noise from the proposed outdoor patron areas.

The EPP separates the day into two different time periods – day (0700 – 2200 h) and night (2200 – 0700 h). Indicative noise levels are provided based on the land use category and zoning for the noise source and noise affected premise.

The relevant indicative noise level minus 5 dB for this site during the night period is 45 dB L_{Aeq}. This level has been adopted as the design criterion level when assessing patron noise at the nearest noise affected premises. The indicative noise level may however, be relaxed where it can be demonstrated that the background noise level is higher than the indicative noise levels. Accordingly, on Friday and Saturday nights until midnight, a criterion of 49 dB L_{Aeq} is deemed appropriate.

4.0 RECOMMENDATIONS

Preliminary noise predictions without acoustic treatment or other controls indicated exceedance of the recommended noise criteria. Accordingly, the music and patron noise assessment in Section 5.0 and Section 6.0 respectively have been based on the following recommendations (refer Appendix C for details).

- Background music and 2 piece live acoustic music, including guitar and a singer only, occurs at the site. No other live or DJ type music is played
- The outdoor alfresco dining area is 'sunk' into the ground approximately 500 mm. A minimum balustrade 1.8 m is to be provided to north, south and east ends.
- The western outdoor deck is shielded to residences on the western side of the Patawalonga River by the construction of an acoustically solid bar on west side of the deck, and with solid balustrade barrier to the north and south ends. The minimum height of the effective '3 sided barrier' required is 1.8 m above the outdoor deck. We note an access gate is required to the pontoon, this should be located at the northern or southern end of the western outdoor deck
- Provide a minimum 2.1 m high barrier and 2.4 m high barrier above the ship upper deck line to north and south ends respectively of the upper function space. The 2.1 m north end barrier to be acoustically lined with 50 mm thick, 32 kg/m² acoustic insulation lined with a fabric or perforated facing with a 25 % open area.

The above mentioned barriers, shall be constructed using material with a minimum surface density of 10 kg/m² -15 kg/m² and be free of gaps and penetrations. Suitable materials include 9 mm thick fibre cement sheet, 12 mm thick Perspex, 6 mm thick toughened glass or any other material that meets the minimum surface density requirement.

Recommendations for noise control were previously provided in MDA Rp 001 R01 2016306AL, issued 23 September 2016 and demonstrated compliance with the nominated criteria could be achieved. However, following review by the Development Assessment Commission (DAC), items were raised over visual amenity impacts. The above recommendations therefore incorporate DAC's review and feedback, noting a balance between visual and acoustic amenity is to be achieved.

5.0 MUSIC NOISE ASSESSMENT

5.1 Music Noise Levels

5.1.1 Background music

Background music only is proposed at the site. Background music noise can be defined as a level that enables patrons to conduct a normal conversation at a distance of 600 mm without having to raise their voices to a substantial degree. In MDA's opinion, this is equivalent to a music noise level of approximately 68 dB L_{Aeq} , with example spectra as shown in Table 3.

Table 3: Typical background music noise levels, dB

	dBA	Octave Band Centre Frequency (Hz)						
		63	125	250	500	1k	2k	4k
Background music level, L_{eq}	68	66	66	66	61	61	61	61
Background music level, L_{10} *	71	69	69	69	64	64	64	64

*Note: L_{10} based on $L_{10} = L_{eq} + 3$

The predicted noise levels indicate that background music will achieve the nominated criteria without additional treatment other than limiting music to background only or to a level of 71 dB L_{A10} when measured at 3 m from a speaker.

We also recommend the following with respect to outdoor speaker placement:

- Speakers in the outdoor alfresco dining area be placed on the eastern wall facing away from the Adelphi Terrace apartments
- Speakers on the outdoor deck be placed on the western wall facing away from residents located on the opposite side of the Patawalonga River
- Function space to be limited to a single speaker located at the north or south end of the area

5.1.2 Acoustic music

The proposal redevelopment seeks live music in the form of a duo, including an acoustic guitar and a singer in the upper deck function space or outdoor alfresco dining area. Previous noise level measurements by MDA of similar music indicate a noise level of 71 dB L_{Aeq} at 5 m. Based on this level, and limiting live music to acoustic guitar and a singer only, compliance with the music noise criteria can be readily achieved.

6.0 PATRON NOISE ASSESSMENT

6.1 Patron Noise Data

Marshall Day Acoustics and other acoustic consultants in Melbourne have measured patron noise from several different venues. These measurements indicate a large variation in the noise levels of crowds, due to a number of factors including the situational context of the crowd, and the intensity and character depending upon the number of patrons and their behaviour.

For the purpose of predicting noise levels from the Buffalo redevelopment, external patron areas have been categorised according to the envisaged use, as outlined in Table 4. Reference sound power data for one person is detailed in the 2011 Hayne paper¹.

Table 4: Patron area use categories

Area use category	Reference sound power data per one person		Area use definition
	Equivalent Level	Maximum Level	
Restaurant dining	78 dB L _{AW}	98 dB L _{AW}	Outdoor alfresco dining area and upper deck function space
Taverns with food offerings	83 dB L _{AW}	104 dB L _{AW}	Outdoor deck

Based on the above reference sound power data and measurements by Marshall Day Acoustics, a simplified empirical relationship to represent the total sound power level for which crowd numbers and character were varied has been derived for determining design equivalent and maximum sound power level as follows:

- Design equivalent sound power level - derived by assuming one third of the total crowd speaks continuously over the duration of the assessment period, and each of these speakers emit a constant total sound power level over the duration of the assessment period. In practice, the actual number of individuals speaking, the sound power emitted by each individual, and the temporal characteristics of each speaker will vary considerably over the assessment period. The derived values therefore do not represent the actual percentage of patrons speaking, or the emission of each patron, but simply represent the total sound power level for the number of patrons
- Design maximum sound power level - derived by assuming that the maximum noise level occurs as a result of two individuals simultaneously producing a maximum level.

The above empirical representation of typical patron noise levels are suitable for design purposes based on the measured data. This approach has been used for Victorian Civil and Administrative Tribunal (VCAT) hearings for the assessment of proposed outdoor venues, which have since been developed and operational.

¹ Hayne et al 2011, 'Prediction of noise from small to medium sized crowds', in *Acoustics 2011: Breaking New Ground, Proceedings of the Annual Conference of the Australian Acoustical Society*, AAS Queensland Division 2011, Gold Coast, paper number 133.

The sound power level used in this assessment is provided in Table 5.

Table 5: Patron noise sound power level data

Area	Semi-steady state	Maximum
Outdoor alfresco dining (100 patrons)	93 dB L _{AW}	101 dB L _{AW}
Outdoor deck (70 patrons)	97 dB L _{AW}	107 dB L _{AW}
Function space (80 patrons)	92 dB L _{AW}	101 dB L _{AW}

6.2 Predicted Patron Noise Levels

A 3-dimensional digital noise model of the site and surrounding built environment has been prepared using SoundPLAN v7.4 proprietary noise mapping software, in order to predict patron noise levels to the nearest noise affected premises.

The noise model has been used to calculate noise levels in accordance with ISO-9613-2:1996 *Acoustics – Attenuation of sound during propagation outdoors – Part 2: General method of calculation* (ISO 9613).

The ISO 9613 standard specifies an engineering method for calculating noise at a known distance from a variety of sources under meteorological conditions that are favourable to sound propagation. The standard defines favourable conditions as downwind propagation where the source blows from the source to the receiver within an angle of +/-45 degrees from a line connecting the source to the receiver, at wind speeds between approximately 1 m/s and 5 m/s, measured at a height of 3 m to 11 m above the ground. Equivalently, the method accounts for average propagation under a well-developed moderate ground based thermal inversion.

Accordingly, predictions on the basis of ISO 9613 account for instances when local atmospheric conditions at the site favour the propagation of sound to surrounding receptor locations. Under alternative atmospheric conditions, such as when the wind is blowing from a receiver location to the site, the noise levels would be lower than calculated.

To calculate noise levels according to the ISO 9613, the noise emissions of each source are firstly characterised in the form of octave band frequency levels. A series of octave band attenuation factors are then calculated for a range of effects including:

- Geometric divergence;
- Air absorption;
- Reflecting obstacles;
- Screening; and
- Ground reflections.

The highest predicted patron noise levels based on the proposed operations and recommendations in Section 4.0 are provided in Table 6. The predictions, to the nearest noise sensitive receivers as shown on Figure 1, are conservative with the assumption that all patrons are located in external areas to the main ship areas.

Table 6: Predicted patron noise levels, dB L_{Aeq}

Receiver	Sun - Thurs, to midnight Outdoor alfresco dining area (100), use of deck (70) and external function space (80)	Fri - Sat, to midnight Outdoor alfresco dining area (100), use of deck (70) and external function space (80)	Sun - Sat, midnight to 2am Outdoor alfresco dining area (100)
Aquarius Apartments			
Ground	41	41	39
Level 1	42	42	41
Level 2	44	44	43
Level 3	46	46	45
Level 4	47	47	47
Level 5 and above	48	48	47
New apartment			
Ground	41	41	39
Level 1	43	43	41
Level 2	44	44	42
Level 3	44	44	43
Level 4	45	45	44
Level 5	46	46	45
Level 6	47	47	46
Level 7 and above	48	48	46
Other			
R1 (west)	44	44	24
R2 (west)	44	44	24
Criteria	45	49	45

The predicted noise levels generally comply with the nominated criteria with the exception of the following minor exceedances:

- Exceedance of 1-2 dB at the upper floor levels of the apartment buildings to the east, between midnight and 2 am; and
- Exceedance of 1-3 dB at the upper floor levels of the apartment buildings to the east, on Sunday – Thursday nights, until midnight.

7.0 SUMMARY

The DeYoung Group now propose to redevelop the Buffalo ship which previously operated for many years as a restaurant.

Marshall Day Acoustics Pty Ltd (MDA) has undertaken a noise assessment of the proposed redevelopment which includes the following:

- An outdoor alfresco dining area located on south east side of the Buffalo Ship, proposed to operate until 2 am, and cater for up to 100 patrons.
- An outdoor deck located on west side of the Buffalo Ship, proposed to operate until midnight, and cater for up to 70 patrons.
- An outdoor function space located on the upper deck of the Buffalo Ship proposed to operate until midnight for corporate functions and weddings. The current license limits upper deck use for 150 patrons (internal and external locations); of which up to 80 patrons can be located external.

The assessment has considered the Development Assessment Commission's review of the proposal, where items had been raised over visual amenity impacts associated with recommended acoustic treatments provided in MDA report R01 issued 23 September 2016.

This revised assessment therefore incorporates DAC's review and feedback, noting a balance between visual and acoustic amenity is to be achieved. The assessment presented has been based on the following:

- Music to be limited to background music only and live acoustic music (e.g. 2 piece acoustic guitar and singer)
- Background music to be played at a level of 71 dB L_{A10} when measured at 3 m from the speaker
- Speakers in the outdoor alfresco dining area be placed on the eastern wall facing away from the Adelphi Terrace apartments
- Speakers on the outdoor deck be placed on the western wall facing away from residents located on the opposite side of the Patawalonga River
- Speaker in outdoor function space to be limited to a single speaker located at the north or south end of the area
- Background music and 2 piece live acoustic music, including guitar and a singer only, occurs at the site. No other live or DJ type music is played
- The outdoor alfresco dining area is 'sunk' into the ground approximately 500 mm. A minimum balustrade 1.8 m is to be provided to north, south and east ends.
- The outdoor deck is shielded to residences on the western side of the Patawalonga River by an acoustically solid bar on west side of the deck, with solid balustrade barrier to the north and south ends. The minimum height of the effective '3 sided barrier' required is 1.8 m
- Provide a minimum 2.1 m high barrier and 2.4 m high above the deck line to north and south ends respectively of the upper function space. The 2.1 m north end barrier to be acoustically lined with 50 mm thick, 32 kg/m² acoustic insulation lined with a fabric or perforated facing with a 25 % open area

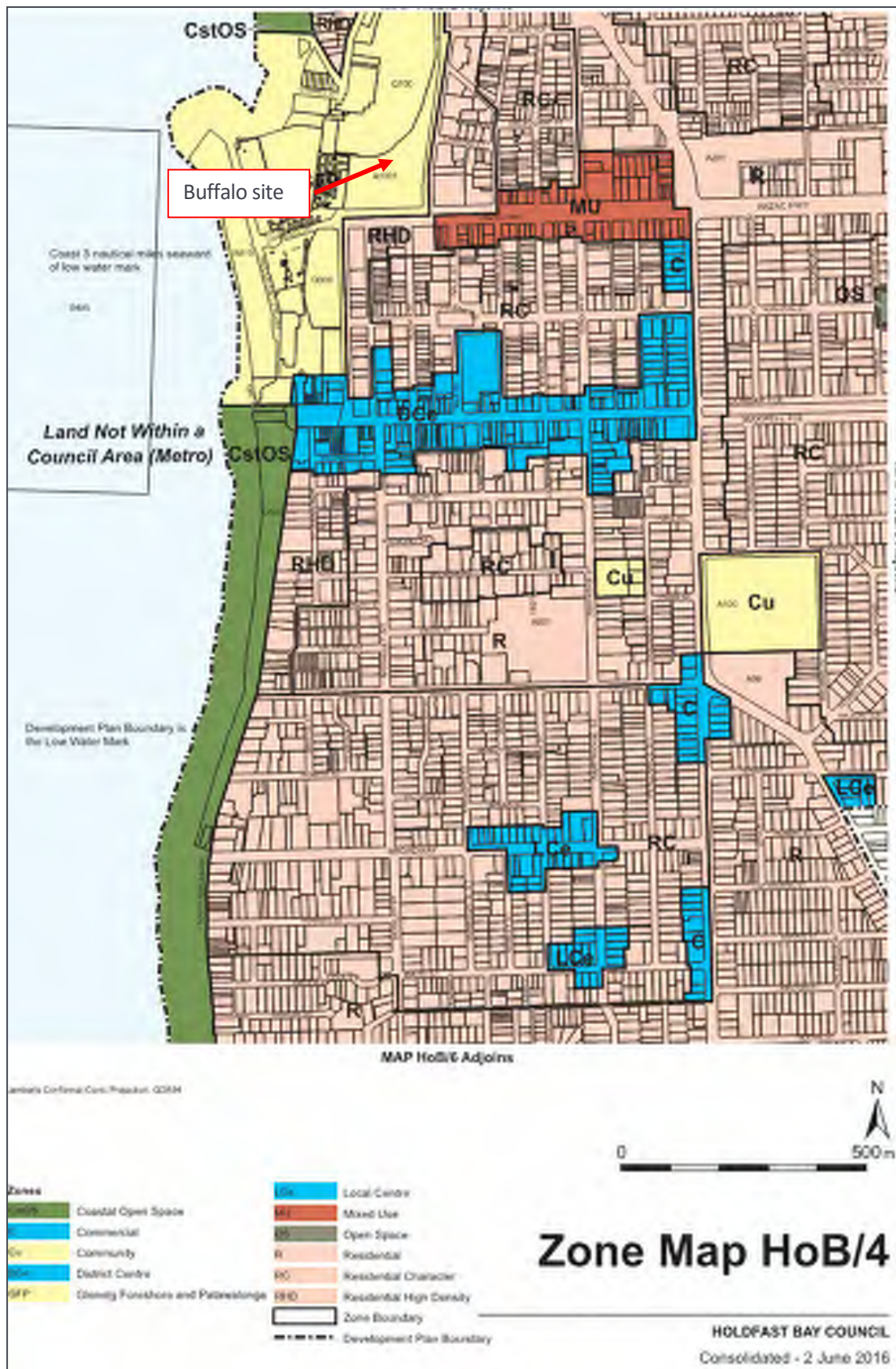
The barriers shall be constructed using material with a minimum surface density of 10 kg/m² - 15 kg/m² and be free of gaps and penetrations. Suitable materials include 9 mm thick fibre cement sheet, 12 mm thick Perspex, 6 mm thick toughened glass or any other material that meets the minimum surface density requirement.

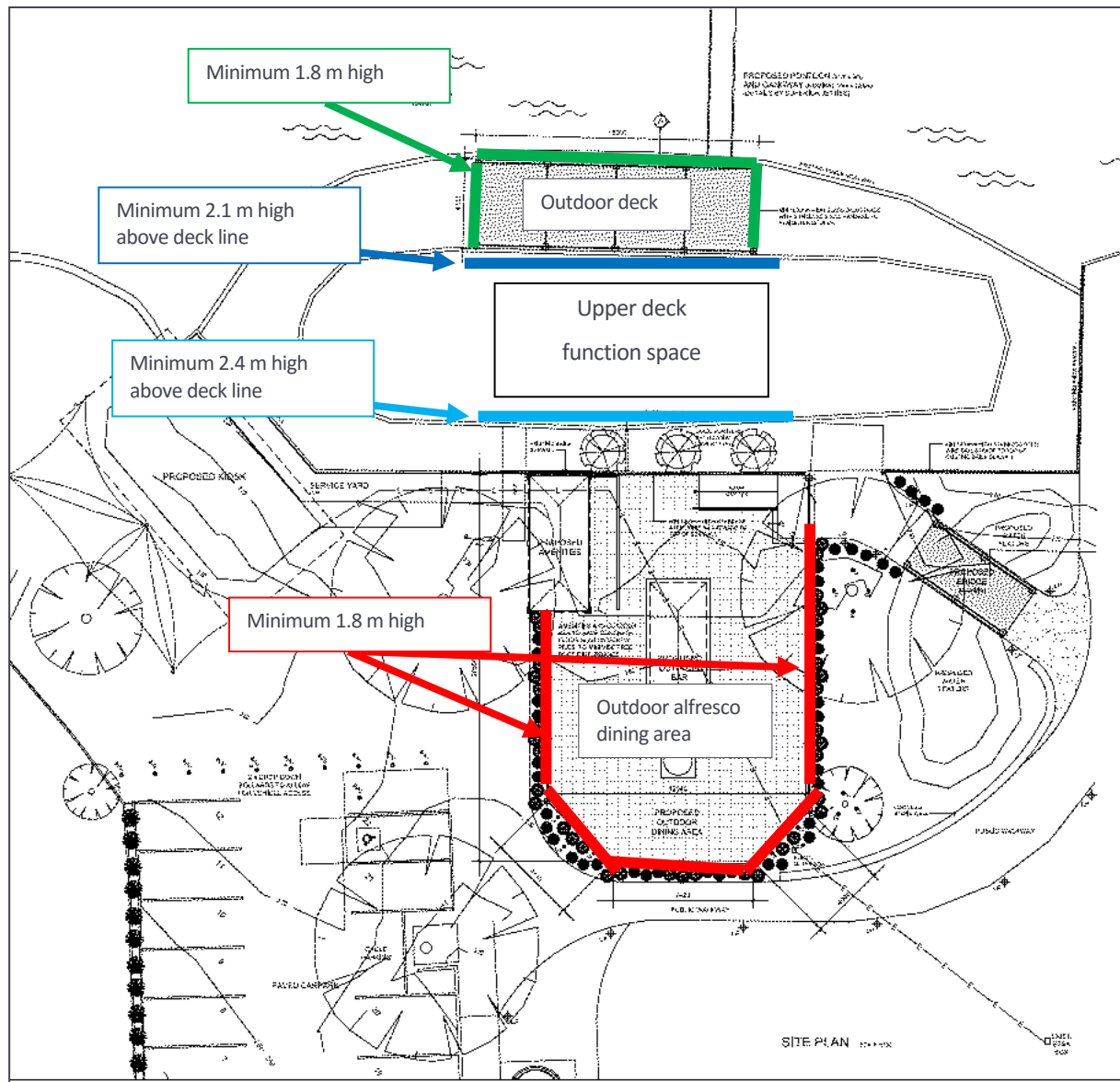
APPENDIX A GLOSSARY OF TERMINOLOGY

Ambient	The ambient noise level is the noise level measured in the absence of the intrusive noise or the noise requiring control. Ambient noise levels are frequently measured to determine the situation prior to the addition of a new noise source.
A-weighting	The process by which noise levels are corrected to account for the non-linear frequency response of the human ear.
dB	Decibel. The unit of sound level.
Frequency	The number of pressure fluctuation cycles per second of a sound wave. Measured in units of Hertz (Hz).
Hertz (Hz)	Hertz is the unit of frequency. One hertz is one cycle per second. One thousand hertz is a kilohertz (kHz).
L_{A10}	The noise level exceeded for 10% of the measurement period, measured in dBA. This is commonly referred to as the average maximum noise level.
L_{A90}	The noise level exceeded for 90% of the measurement period, measured in dBA. This is commonly referred to as the background noise level.
L_{Aeq}	The A-weighted equivalent continuous sound level. This is commonly referred to as the average noise level and is measured in dB.
L_{OCT10}	The noise level exceeded for 10% of the measurement period in the octave bands 63Hz-4kHz. Commonly referred to as the average maximum noise level.
L_{OCT90}	The noise level exceeded for 90% of the measurement period in the octave bands 63Hz-4kHz. Commonly referred to as the background noise level.
L_{WA}	The A-weighted Sound Power Level. The level of total sound power radiated by a sound source.
Octave Band	A range of frequencies where the highest frequency included is twice the lowest frequency. Octave bands are referred to by their logarithmic centre frequencies, these being 31.5 Hz, 63 Hz, 125 Hz, 250 Hz, 500 Hz, 1 kHz, 2 kHz, 4 kHz, 8 kHz, and 16 kHz for the audible range of sound.

APPENDIX B CITY OF HOLDFAST BAY DEVELOPMENT PLAN

B1 Zone Map





ATTACHMENT 4

COUNCIL COMMENTS

RECEIVED

15 DEC 2015

DAC

Contact Officer: Callum Little (Direct No. 8229 9854)

8 December 2015

Mr Daniel Pluck
Development Assessment Commission
GPO Box 1815
ADELAIDE SA 5001

DEVELOPMENT NO: 110/00146/15

APPLICANT: De Young Jamestown Pty Ltd

NATURE OF THE DEVELOPMENT: Proposed kiosk, alfresco bar and dining area, toilet facilities, outdoor dining timber deck, boat pontoon and gangway and associated landscaping and temporary construction fence

SUBJECT LAND: Buffalo Restaurant/Ship Adelphi Terrace GLENELG NORTH SA 5045

Thank you for the opportunity to comment on the proposed development. Overall Council are supportive of the proposal. We would however like to take this opportunity to request that some additional information is provided in relation to the following issues:

Noise

The applicant proposes the construction of a new deck along the northern edge of the Buffalo restaurant and a new bar area within the reserve to the south. In order to ensure noise associated with the operation of these two areas does not adversely impact any residential properties within the locality we believe that an assessment of potential noise impacts should be undertaken.

This may require the provision of an acoustic report.

Outdoor Lighting and Safety

Given the potential for the proposed use to increase activity within the locality at night, we request that the DAC consider the following condition of approval:

Pedestrian walkways on the subject site shall be adequately lit in accordance with Australian / New Zealand Standard AS/NZS 1158.3.1:1999 "Road Lighting Part 3.1: Pedestrian area (Category P) lighting - Performance and installation design guidelines". Such lighting shall be maintained at all times, to the reasonable satisfaction of the Council.

Outdoor Advertising

The application appears to include limited information regarding advertising. We request that the DAP clarify whether any advertising is proposed.

Coastal Risk

We request that the DAC confirm that the proposed development (particularly the proposed outdoor dining area) is consistent with Coastal Areas Principle of Development Control 22:

Buildings to be sited over tidal water or which are not capable of being raised or protected by flood protection measures in future, should have a floor level of at least 1.25 metres above the standard sea-flood risk level.

And Marinas and Maritime Structures Principle of Development Control 2:

The design of marinas, berths, channels, fairways, gangways and floating structures should comply with:

(a) Australian Standard AS 3962: Guidelines for Design of Marinas

(b) Australian Standard AS 4997: Guidelines for the Design of Maritime Structures.

Bicycle Parking

We request that the DAC consider the provision of bicycle parking as part of the proposed development.

Colours and Materials

The applicant does not appear to have supplied a schedule of colours. Council's preference is for muted colours that blend with the environment.

Conclusion

Should you have any questions regarding any of the above please contact the officer listed above.

Yours faithfully



**Anthony Marroncelli
MANAGER DEVELOPMENT SERVICES**

ATTACHMENT 5

AGENCY REPORTS

In reply please quote 2015/00166/01, Process ID: 332087
Enquiries to Reece Loughron
Telephone (08) 8226 8386
Facsimile (08) 8226 8330
E-mail dpti.luc@sa.gov.au



Government of South Australia

Department of Planning,
Transport and Infrastructure

SAFETY AND SERVICE
DIVISION

GPO Box 1533
Adelaide SA 5001

Telephone: 61 8 8226 8222
Facsimile: 61 8 8226 8330

10/04/2015



Mr David Storey
Development Assessment Commission
GPO Box 1815,
ADELAIDE SA 5001

Dear David,

CONSULTATION ADVICE

Development No.	110/L002/15
Applicant	De Young Jamestown Pty Ltd
Location	Lot 1001 in DP 49600, (CT 5935/965) Adelphi Terrace, Glenelg North
Proposal	Proposed kiosk, alfresco bar and dining area, toilet facilities, outdoor dining timber deck, boat pontoon and gangway and associated landscaping and temporary construction fence.

I refer to the above development application forwarded to the Safety and Service Division of the Department of Planning, Transport and Infrastructure (DPTI) requesting informal comment. DPTI provides the following report to assist the planning authority with its decision.

THE PROPOSAL

The proposed development involves the construction of an outdoor bar and kiosk adjacent the existing Buffalo ship. A new timber deck, gangway and pontoon will be constructed extending into the water.

CONSIDERATION

A review of the application has been undertaken. It is noted that the access point to the development is located on a local road under the care and control of Council. Subsequently, DPTI has no comment to make in relation to the access.

DPTI also notes that components of the development being constructed on/over water are also located on land owned by Council and as such there are no property related comments that need to be considered on behalf of the Minister for Transport & Infrastructure.

In regards to the design of the gangway and pontoon the applicant should ensure that:

- All relevant standards and regulations of the state are adhered to during the design and construction of the land and water facilities.

- The pontoon is designed in accordance with the provisions of AS 3962-2001 Guidelines for the design of marinas, for the maximum size vessel that will be berthed on the pontoon (note that maximum size vessel expected to be moored is not mentioned with the application).

Pedestrian and Bicycle Facilities

The development is located along the foreshore of the Patawalonga Lake and surrounding parklands. All existing pedestrian and cycling networks should be considered within the final design of footpaths/shared paths and connections to existing facilities to ensure the suitability and safety is maintained. All new facilities should be designed in accordance with the relevant Austroads and Australian Standards.

CONCLUSION

In-principle, no objections are raised to the proposed development subject to the following conditions.

ADVICE

The planning authority is advised to attach the following conditions to any approval:

1. The pontoon shall be designed in accordance with the provisions of AS 3962-2001 Guidelines for the design of marinas, for the maximum size vessel that will be berthed on the pontoon.

Yours sincerely,



A/GENERAL MANAGER, OPERATIONAL SERVICES

A copy of the decision notification form should be forwarded to dpti.developmentapplications@sa.gov.au



10/1011

Date: 16 December 2015

Level 9 Chesser House
91-97 Grenfell Street
Adelaide SA 5000

GPO Box 1047
Adelaide SA 5001
Australia

Ph: +61 8 8204 9323
Fax: +61 8 8204 9321

www.environment.sa.gov.au

Mr Daniel Pluck
Planning Officer
Development Assessment Commission
GPO Box 1815
ADELAIDE SA 5001

Dear Mr Pluck,

I write regarding development application **110/L002/15**, a proposed kiosk, alfresco bar and dining area, toilet facilities, outdoor dining timber deck, boat pontoon and gangway and associated landscaping and temporary construction fence.

The *Crown Land Management Act 2009* (the Act) requires work occurring on Crown land (including dedicated land) to be permitted by lawful authority. The Act also requires the consent of the Minister for Sustainability, Environment and Conservation for persons other than the Minister to grant leases over dedicated land.

The proposed development occurs on dedicated land held in trust to be used at all times as a reserve for recreation purposes by the City of Holdfast Bay. As such, the applicant will be required to seek the consent of the Minister for the proposed works on Crown land. This consent will be required prior to any works being commenced.

Any proposed leases to be issued by the City of Holdfast Bay will also require Ministerial consent.

To further discuss these requirements, the applicant should contact Tony Halls, Senior Property Officer on 8463 3848 or Tony.Halls@sa.gov.au.

Yours Sincerely



Tony Halls

**SENIOR PROPERTY OFFICER
DEPARTMENT OF ENVIRONMENT, WATER AND NATURAL RESOURCES**



Government of South Australia
Department of Environment,
Water and Natural Resources

10/1011

Date: 15 March 2016

Level 3
25 Grenfell Street
Adelaide SA 5000
GPO Box 1047
Adelaide SA 5001
Australia
Ph: +61 8 8204 1218

www.environment.sa.gov.au

Mr Daniel Pluck
Planning Officer
Development Assessment Commission
GPO Box 1815
ADELAIDE SA 5001

Dear Mr Pluck,

I write regarding development application **110/L002/15**, a proposed kiosk, alfresco bar and dining area, toilet facilities, outdoor dining timber deck, boat pontoon and gangway and associated landscaping and temporary construction fence.

In December last year I submitted advice that the City of Holdfast Bay would be required to seek the consent of the Minister for Sustainability, Environment and Conservation for the proposed development and any lease between Council and a third party over the land.

This previous advice was incorrect. As the land is held as a freehold title under trust, no consent from the Minister for Sustainability, Environment and Conservation is required. The only requirement is that Council continue to use the land in accordance with the trust.

I apologise for any inconvenience caused. To further discuss the matter please contact Tony Halls, Senior Property Officer on 8463 3848 or Tony.Halls@sa.gov.au.

Yours Sincerely

Tony Halls
SENIOR PROPERTY OFFICER
DEPARTMENT OF ENVIRONMENT, WATER AND NATURAL RESOURCES

ATTACHMENT 6

REPRESENTATIONS

South Australian
DEVELOPMENT ACT, 1993

REPRESENTATION ON APPLICATION - CATEGORY 3

Applicant:	De Young Jamestown Pty Ltd
Development Number:	110/L002/15
Nature of Development:	Proposed kiosk, alfresco bar and dining area, toilet facilities, outdoor dining timber deck, boat pontoon and gangway and associated landscaping and temporary construction fence
Type of development:	Merit
Zone / Policy Area:	Glenelg Foreshore and Patawalonga Zone Map HoB/4
Subject Land:	Adelphi Terrace, Glenelg North (being Allotment 1001, DP49600: Certificate of Title: Volume 5935 Folio 965; and part Piece 700, DP55022: Certificate of Title: Volume 6154 Folio 277) The proposed works include or are immediately adjacent to the existing Buffalo Restaurant and Function Centre.
Contact Officer:	Daniel Pluck
Phone Number:	7109 7196
Close Date:	16 December 2015

My name: KEVIN TIMES
My phone number: 08 8294 5858

PRIMARY METHOD(S) OF CONTACT:

Email address: _____

Postal address: 39 MAYWELL TCE
GLEN GOWRIE

Postcode 5044

RECEIVED

16 DEC 2015

DAC

You will be contacted via your nominated PRIMARY METHOD(S) OF CONTACT if you indicate below that you wish to be heard in support of your submission.

My interests are:

- ☒ owner of local property
☐ occupier of local property
☐ a representative of a company/other organisation affected by the proposal
☐ a private citizen

The address of the property affected is 72/33 COLLYER TCE GLENELG Postcode 5045

The specific aspects of the application to which I make comment on are:

I WOULD LIKE THE TRADING OF ALCOHOL
TO BE RESTRICTED TO WITHIN THE BOAT
ONLY, AFTER 11:00 PM (23:00 HRS). IF ALFRESCO
BAR & TIMBER DECK TO CLOSE AT 23:00 HRS.
THIS IS TO LIMIT DISTURBANCE FROM NOISE
AND ALCOHOL RELATED ACTIVITIES FOR
LOCAL RESIDENTIAL TO SLEEP.

- I ☐ wish to be heard in support of my submission
☒ do not wish to be heard in support of my submission
(Please tick one)

- By ☐ appearing personally
☐ being represented by the following person:
(Please tick one)

Date 14-12-2015

Signature [Signature]

★ ATT: DANIEL PLUCK - see dept. below ↓

South Australian
DEVELOPMENT ACT, 1993

REPRESENTATION ON APPLICATION - CATEGORY 3

Applicant:	De Young Jamestown Pty Ltd
Development Number:	110/L002/15
Nature of Development:	Proposed kiosk, alfresco bar and dining area, toilet facilities, outdoor dining timber deck, boat pontoon and gangway and associated landscaping and temporary construction fence
Type of development:	Merit
Zone / Policy Area:	Glenelg Foreshore and Patawalonga Zone Map HoB/4
Subject Land:	Adelphi Terrace, Glenelg North (being Allotment 1001, DP49600: Certificate of Title: Volume 5935 Folio 965; and part Piece 700, DP55022: Certificate of Title: Volume 6154 Folio 277) The proposed works include or are immediately adjacent to the existing Buffalo Restaurant and Function Centre.
Contact Officer:	Daniel Pluck
Phone Number:	7109 7196
Close Date:	16 December 2015

My name: Helen Merrett

My phone number: 043124745

PRIMARY METHOD(s) OF CONTACT:

Email address:

Postal address:

901/25 Colley Tce Glenelg
Postcode 5045

You will be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard in support of your submission.

My interests are:

- ☒ owner of local property
☒ occupier of local property
☐ a representative of a company/other organisation affected by the proposal
☒ a private citizen

The address of the property affected is

901/25 Colley Tce Glenelg Postcode 5045

The specific aspects of the application to which I make comment on are:

- Congestion: both foot traffic, cars and people in that short strip between development to set of lights at Anzac Hwy. Already very busy + dangerous in summer months.
- Retail shops all struggle now on Jetty Rd in off season, this will be the same!
- My view of the airport - which is why I purchased this particular property, will be non-existent and I am NOT happy about that at all. Also, annoyed by references to "catering for the Chinese market" in proposal - why should I suffer for the Chinese!

- I ☐ wish to be heard in support of my submission
☒ do not wish to be heard in support of my submission
(Please tick one)

By

- ☐ appearing personally
☐ being represented by the following person:
(Please tick one)

Date 13.12.15

Signature

H Merrett

Return Address: The Secretary, Development Assessment Commission, GPO Box 1815, Adelaide SA 5001.

*Fax failed!

South Australian
DEVELOPMENT ACT, 1993

REPRESENTATION ON APPLICATION – CATEGORY 3

Applicant:	De Young Jamestown Pty Ltd
Development Number:	110/L002/15
Nature of Development:	Proposed kiosk, alfresco bar and dining area, toilet facilities, outdoor dining timber deck, boat pontoon and gangway and associated landscaping and temporary construction fence
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Zone / Policy Area:	Glenelg Foreshore and Patawalonga Zone Map HoB/4
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Contact Officer:	Daniel Pluck
Phone Number:	7109 7196
Close Date:	16 December 2015

RECEIVED

15 DEC 2015

DAC

My name: M. CORCORAN

My phone number: 0417710123

PRIMARY METHOD(s) OF CONTACT: Email address: mvc.eternity@gmail.com.au

Postal address: _____
Postcode _____

You will be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard in support of your submission.

- My interests are:
- ☐ owner of local property
 - ☐ occupier of local property
 - ☐ a representative of a company/other organisation affected by the proposal
 - ☒ a private citizen

The address of the property affected is _____ Postcode _____

The specific aspects of the application to which I make comment on are:

The development is very large & extends parallel to the southern public waterway. We oppose alcohol being served. A bar is not necessary with a childrens playground adjacent & the Watermark hotel adjacent. Over the 100 Mannum Pier restaurants, many have closed down & people cannot afford restaurant meals. Brown timber clowns used extensively are out of character with the heritage colours of black, grey & white of the Buffalo. Bar is large & unattractive. A kiosk w/ no alcohol is needed.

- ☒ wish to be heard in support of my submission
☐ do not wish to be heard in support of my submission

(Please tick one)

- By ☒ appearing personally
☐ being represented by the following person:

(Please tick one)

Date 14 Dec 2015 Signature [Signature]

Return Address: The Secretary, Development Assessment Commission, GPO Box 1815, Adelaide SA 5001.

South Australian
DEVELOPMENT ACT, 1993
REPRESENTATION ON APPLICATION – CATEGORY 3

Applicant:	De Young Jamestown Pty Ltd
Development Number:	110/L002/15
Nature of Development:	Proposed kiosk, alfresco bar and dining area, toilet facilities, outdoor dining timber deck, boat pontoon and gangway and associated landscaping and temporary construction fence
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Contact Officer:	Daniel Pluck
Phone Number:	7109 7196
Close Date:	16 December 2015

My name: TREVOR & ANNA ARS

My phone number: 83765252 041 8825862

PRIMARY METHOD(S) OF CONTACT: Email address: TREVARNA @ INTERNODE.ON.NET (lower case)
Postal address: 75/33 CONVEY TERR
GLENELG Postcode 5045

You will be contacted via your nominated PRIMARY METHOD(S) OF CONTACT if you indicate below that you wish to be heard in support of your submission.

My interests are: ☒ owner of local property
☐ occupier of local property
☐ a representative of a company/other organisation affected by the proposal
☐ a private citizen

The address of the property affected is 75/33 CONVEY TERR GLENELG (MAGINN BRST) Postcode 5045

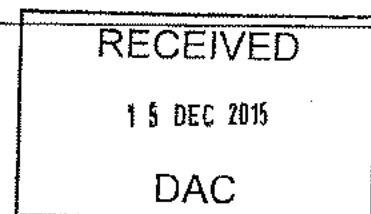
The specific aspects of the application to which I make comment on are:

WE ARE CONCERNED WITH INCREASE NOISE FROM OUTSIDE DINING
& BAR AREAS OVER PREVIOUS DEVELOPMENT USE
(VOICE & MUSIC)

I ☐ wish to be heard in support of my submission
☒ do not wish to be heard in support of my submission
(Please tick one)

By ☐ appearing personally
☐ being represented by the following person:
(Please tick one)

Date 14-12-2015 Signature [Signature]



South Australian
DEVELOPMENT ACT, 1993
REPRESENTATION ON APPLICATION – CATEGORY 3

Applicant:	De Young Jamestown Pty Ltd
Development Number:	110/L002/15
Nature of Development:	Proposed kiosk, alfresco bar and dining area, toilet facilities, outdoor dining timber deck, boat pontoon and gangway and associated landscaping and temporary construction fence
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Zone / Policy Area:	Glenelg Foreshore and Patawalonga Zone Map HoB/4
Subject Land:	Adelphi Terrace, Glenelg North (being Allotment 1001, DP49600: Certificate of Title: Volume 5935 Folio 965; and part Piece 700, DP55022: Certificate of Title: Volume 6154 Folio 277) The proposed works include or are immediately adjacent to the existing Buffalo Restaurant and Function Centre.
Contact Officer:	Daniel Pluck
Phone Number:	7109 7195
Close Date:	16 December 2015

My name: LINDA CUNNINGHAM
My phone number: 0427 762 043
PRIMARY METHOD(S) OF CONTACT: Email address: _____
Postal address: P.O. 3260 Pt Lincoln Postcode 5606

You will be contacted via your nominated PRIMARY METHOD(S) OF CONTACT if you indicate below that you wish to be heard in support of your submission.

My interests are: ☒ owner of local property
☐ occupier of local property
☐ a representative of a company/other organisation affected by the proposal
☐ a private citizen



The address of the property affected is _____ Postcode _____

The specific aspects of the application to which I make comment on are:

I support development
THERE DOESN'T SEEM TO BE ANY FORM OF
AGREEMENT OF APPLICATION GIVEN THEREFORE
I HAVE USED THIS FORM FOR SUCH

I ☐ wish to be heard in support of my submission
☒ do not wish to be heard in support of my submission
(Please tick one)

By ☐ appearing personally
☐ being represented by the following person:
(Please tick one)

Date 8.12.15 Signature [Signature]

South Australian
DEVELOPMENT ACT, 1993
REPRESENTATION ON APPLICATION - CATEGORY 3

Applicant:	De Young Jamestown Pty Ltd
Development Number:	110/L002/15
Nature of Development:	Proposed kiosk, alfresco bar and dining area, toilet facilities, outdoor dining timber deck, boat pontoon and gangway and associated landscaping and temporary construction fence
Type of development:	Merit
Zone / Policy Area:	Glenelg Foreshore and Patawalonga Zone Map Ho8/4
Subject Land:	Adelphi Terrace, Glenelg North (being Allotment 1001, DP49600: Certificate of Title: Volume 5935 Folio 965; and part Piece 700, DP55022: Certificate of Title: Volume 6154 Folio 277) The proposed works include or are immediately adjacent to the existing Buffalo Restaurant and Function Centre
Contact Officer:	Daniel Pluck
Phone Number:	7109 7196
Close Date:	16 December 2015

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11 DEC 2015

DAC

My name: Dianne Bignell
My phone number: 0418802072

PRIMARY METHOD(S) OF CONTACT:

Email address: _____

Postal address: _____

11 Cygnet Ct
Glenelg Nth Postcode 5045

You will be contacted via your nominated PRIMARY METHOD(S) OF CONTACT if you indicate below that you wish to be heard in support of your submission.

My interests are:

- ☒ owner of local property
☐ occupier of local property
☐ a representative of a company/other organisation affected by the proposal
☐ a private citizen

The address of the property affected is 11 Cygnet Ct Glenelg Nth Postcode 5045

The specific aspects of the application to which I make comment on are:

The Buffalo is just across the water from my home.
Congestion and noise. I'm willing to sign any petition.
There needs to be a Curfew if it did go ahead which I'm sure it want.

- ☐ wish to be heard in support of my submission
☐ do not wish to be heard in support of my submission
(Please tick one)

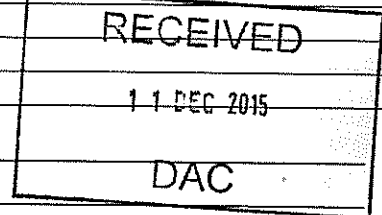
- By ☐ appearing personally
☐ being represented by the following person:
(Please tick one)

Date 9.12.15 Signature _____

Return Address: The Secretary, Development Assessment Commission, GPO Box 1815, Adelaide SA 5001.

South Australian
DEVELOPMENT ACT, 1993
REPRESENTATION ON APPLICATION – CATEGORY 3

Applicant:	De Young Jamestown Pty Ltd
Development Number:	110/L002/15
Nature of Development:	Proposed kiosk, alfresco bar and dining area, toilet facilities, outdoor dining timber deck, boat pontoon and gangway and associated landscaping and temporary construction fence
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Contact Officer:	Daniel Pluck
Phone Number:	7109 7196
Close Date:	16 December 2015



My name: Karen Waites

My phone number: 04035472101

PRIMARY METHOD(S) OF CONTACT:

Email address: Kwaites@bigpond.net.au

Postal address: 42/31 Esplanade

Henley Beach South Postcode 5022

You will be contacted via your nominated PRIMARY METHOD(S) OF CONTACT if you indicate below that you wish to be heard in support of your submission.

My interests are:

- ☒ owner of local property
☐ occupier of local property
☐ a representative of a company/other organisation affected by the proposal
☐ a private citizen

The address of the property affected is 46/11 Adelphi Tce, Glenelg SA Postcode 5045

The specific aspects of the application to which I make comment on are:

* Parking - this is already a major problem on the week ends for residents & friends of residents.

* Noise from alfresco bar and charter wharf.

* Apposed to charter wharf - there is a dedicated Charter wharf in Marina Pier - there is no need for another public charter wharf that would create too much noise and excess cars.

- I ☐ wish to be heard in support of my submission
☒ do not wish to be heard in support of my submission
(Please tick one)

- By ☐ appearing personally
☐ being represented by the following person:
(Please tick one)

Date 10/12/15 Signature K Waites

South Australian
DEVELOPMENT ACT, 1993
REPRESENTATION ON APPLICATION – CATEGORY 3

Applicant:	De Young Jamestown Pty Ltd
Development Number:	110/L002/15
Nature of Development:	Proposed kiosk, alfresco bar and dining area, toilet facilities, outdoor dining timber deck, boat pontoon and gangway and associated landscaping and temporary construction fence
Type of development:	Merit
Zone / Policy Area:	Glenelg Foreshore and Patawalonga Zone Map HoB/4
Subject Land:	Adelphi Terrace, Glenelg North (being Allotment 1001, DP49600: Certificate of Title: Volume 5935 Folio 965; and part Piece 700, DP55022: Certificate of Title: Volume 6154 Folio 277) The proposed works include or are immediately adjacent to the existing Buffalo Restaurant and Function Centre.
Contact Officer:	Daniel Pluck
Phone Number:	7109 7196
Close Date:	16 December 2015

My name: RANE NAISH
My phone number: 61-427123125

PRIMARY METHOD(S) OF CONTACT:

Email address: _____

Postal address: _____

PO BOX 198
GLENELG Postcode 5045

RECEIVED

11 DEC 2015

DAC

You will be contacted via your nominated PRIMARY METHOD(S) OF CONTACT if you indicate below that you wish to be heard in support of your submission.

My interests are:

- ☒ owner of local property
☒ occupier of local property
☐ a representative of a company/other organisation affected by the proposal
☐ a private citizen

The address of the property affected is 74/33 Colley Tce Postcode 5045

The specific aspects of the application to which I make comment on are:

1. Excessive noise & aggressive behaviour
 2. Excessive vandalism
 3. Too close to large residential complex
- Excessive noise, fighting, both young men, vandalism (of playground) also nearby, now a row with alcohol freely available so close to our residence the situation will get worse
- ☐ wish to be heard in support of my submission
☒ do not wish to be heard in support of my submission
(Please tick one)

By

- ☐ appearing personally
☐ being represented by the following person:

(Please tick one)

Date 8/12/2015 Signature R. Naish

South Australian
DEVELOPMENT ACT, 1993

REPRESENTATION ON APPLICATION - CATEGORY 3

Applicant:	De Young Jamestown Pty Ltd
Development Number:	110/L002/15
Nature of Development:	Proposed kiosk, alfresco bar and dining area, toilet facilities, outdoor dining timber deck, boat pontoon and gangway and associated landscaping and temporary construction fence
Type of development:	Merit
Zone / Policy Area:	Glenelg Foreshore and Patawalonga Zone Map Ho8/4
Subject Land:	Adelphi Terrace, Glenelg North (being Allotment 1001, DP49600: Certificate of Title: Volume 5935 Folio 965; and part Piece 700, DP55022: Certificate of Title: Volume 6154 Folio 277) The proposed works include or are immediately adjacent to the existing Buffalo Restaurant and Function Centre.
Contact Officer:	Daniel Pluck
Phone Number:	7109 7196
Close Date:	16 December 2015

My name: NORMAN AND MARGARET BEECHEY FOR GEBU Pty Ltd.

My phone number: 03 94396454

PRIMARY METHOD(S) OF CONTACT: Email address: MBEECHEY@TPG.COM.AU
Postal address: 97 BONDS ROAD
LOWER PLENTY Postcode 3093

You will be contacted via your nominated PRIMARY METHOD(S) OF CONTACT if you indicate below that you wish to be heard in support of your submission.

My interests are:
☒ owner of local property
☒ occupier of local property
☒ a representative of a company/other organisation affected by the proposal
☒ a private citizen

Properties 30/31 Colley Terrace Glenelg
The address of the property affected is 34/31 Colley Terrace Postcode 5045
Glenelg

The specific aspects of the application to which I make comment on are:

We are appalled on Reading this application. This area is already over supplied with restaurants and bars. The Patawalonga is serviced by a huge hotel complex (Watermark) and nearby Marina. Parking is already at a premium. The noise affecting local residents would be intolerable. We can see no benefit from this application being approved.

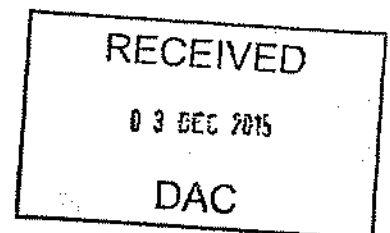
I ☐ wish to be heard in support of my submission
☒ do not wish to be heard in support of my submission
(Please tick one)

By ☐ appearing personally
☐ being represented by the following person:
(Please tick one)

Date 1 DEC 2015

Signature

[Signature]



**South Australian
DEVELOPMENT ACT, 1993**

REPRESENTATION ON APPLICATION – CATEGORY 3

Applicant:	De Young Jamestown Pty Ltd	
Development Number:	110/L002/15	
Nature of Development:	Proposed kiosk, alfresco bar and dining area, toilet facilities, outdoor dining timber deck, boat pontoon and gangway and associated landscaping and temporary construction fence	
Type of development:	Merit	
Zone / Policy Area:	Glenelg Foreshore and Patawalonga Zone Map HoB/4	
Subject Land:	Adelphi Terrace, Glenelg North (being Allotment 1001, DP49600: Certificate of Title: Volume 5935 Folio 965; and part Piece 700, DP55022: Certificate of Title: Volume 6154 Folio 277) The proposed works include or are immediately adjacent to the existing Buffalo Restaurant and Function Centre.	
Contact Officer:	Daniel Pluck	<div style="border: 1px solid black; padding: 5px; text-align: center;"> RECEIVED 02 DEC 2015 DAC </div>
Phone Number:	7109 7196	
Close Date:	16 December 2015	

My name: CHRISTOPHER J HAINES

My phone number: 0437288840

PRIMARY METHOD(s) OF CONTACT:

Email address: drchristhaines@gmail.com

Postal address: 428/29 Colley Tce
Glenelg

Postcode 5045

You will be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard in support of your submission.

My interests are:

- ☒ owner of local property
☒ occupier of local property
☐ a representative of a company/other organisation affected by the proposal
☐ a private citizen

The address of the property affected is 428/29 Colley Tce

Postcode 5045

The specific aspects of the application to which I make comment on are:

① Adelphi Tce is a small road already heavily utilised, especially on weekends. It is extremely popular with cyclists with bike lanes on each side and speed limit of 50km/hr already (which indicates a current concern of safety on Adelphi Tce). The new development will compromise safety and also reduce the quiet surrounding environment.

- ☐ wish to be heard in support of my submission
☒ do not wish to be heard in support of my submission

(Please tick one)

By

- ☐ appearing personally
☐ being represented by the following person:

(Please tick one)

Date

30/11/15

Signature

[Signature]

There will be more noise, many more people and much more transport - cars, delivery vehicles, maintenance vehicles.

Return Address: The Secretary, Development Assessment Commission, GPO Box 1815, Adelaide SA 5001.

Also car parking on the street is already heavily utilised and will be much worse

South Australian
DEVELOPMENT ACT, 1993

REPRESENTATION ON APPLICATION – CATEGORY 3

RECEIVED

16 DEC 2015

Applicant:	De Young Jamestown Pty Ltd
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Contact Officer:	Daniel Pluck
Phone Number:	7109 7196
Close Date:	16 December 2015

My name: LIAM JOSEPH HANNA
 My phone number: H: 8294 0342 M: 0431 715633
 PRIMARY METHOD(S) OF CONTACT: Email address: hanna.liam@inet.net.au
 Postal address: APT. 67 MARINA EAST
33 COLLEY TCE, GLENELG. Postcode 5045.

You will be contacted via your nominated PRIMARY METHOD(S) OF CONTACT if you indicate below that you wish to be heard in support of your submission.

My interests are: ☒ owner of local property
☒ occupier of local property
☒ a representative of a company/other organisation affected by the proposal *other residents,*
☐ a private citizen

The address of the property affected is MARINA EAST, 33 COLLEY TCE Postcode 5045.

The specific aspects of the application to which I make comment on are:

DETAILS ON ATTACHMENT.
- ALIENATION OF COMMUNITY RESERVE LAND
- NON-REFURBISHMENT OF BUFFALO.
- NON-COMPLIANCE OF ORIGINAL STRUCTURE (BUFFALO)
- OPEN NATURE OF OPERATIONS/NON-CONTAINMENT OF NOISE etc.
- QUESTIONABLE QUALITY/DURABILITY OF STRUCTURES.
- ACCESS AROUND MARINA
- VISUAL/SMELL POLLUTION FROM RUBBISH/WORK AREAS.
- COMMUNITY LAND BEING USED TO SUBSIDISE BUSINESS OPS.

I ☒ wish to be heard in support of my submission
☐ do not wish to be heard in support of my submission
 (Please tick one)

By ☒ appearing personally
☐ being represented by the following person:
 (Please tick one)

Date 15/12/15.

Signature

Liam Hanna

Attachment: Development Application No. 110/L002/15

I consider that the development application submitted may violate the provisions enacted by John Hill, Minister for Environment & Conservation in September 2004. At that time the Minister issued "..... a new Trust Grant to the City of Holdfast Bay for the land (parcel of land covered by CoT Vol 5935, Folio 965) to be used 'for the recreation of the inhabitants of the Corporation of the City of Holdfast Bay' – see **Attachment 1 Media Release**.

This provision was duly enacted in SA Govt Gazette 18 November 2004 – see **Attachment 2, Gazette Notice**. I understand that this action restored Wigley Reserve to its prior status as community recreation reserve – see **Attachment 3 background paper by Liam Hanna**.

The Mayor of City of Holdfast Bay on 24 March 2014 replied to my enquiries stating "As the area of the lease is excluded from Community Land classification there was no requirement to undertake community consultation in respect of the new lease." It seems that the Mayor/Council administration was unaware of the Minister's action which post-dated the action referred to in para 2 of the Mayor's letter – see **Attachment 4 Letter to Liam Hanna, 24 March 2015**.

The alienation of this parcel of land in favour of a business cannot reasonably be claimed as a "recreational" facility for the people of the City of Holdfast Bay.

Other issues of contention

1. Considerable publicity extolling the proposed project early in 2015 focussed on intent and opportunity to refurbish the Buffalo building, on which basis the developer was to take over the lease for \$1. The cynical among us are justified in anticipating a post hoc 'discovery' that the building is unsafe and non-complying. As a building it must comply with the Building Code of Australia. It seems it does not - see Mayor's letter at Attachment 4, second dot point. Therefore any extension to the building i.e. timber deck will be non-complying. It also appears that the Buffalo building is to be left in its present derelict state, perhaps to be later demolished thus voiding the original raison d'être for allowing this development in the first instance.
2. Because of the open nature of the dining and drinking locations and the light structure of the buildings, it can be anticipated that there will be considerable disruption to the peace and amenity for the nearby residents of Marina East (82 house-holds) plus those across the marina – all within 200m of the development: music and loud behaviour from open areas (not contained as in conventional venues); clatter of bulk bottle disposal after hours/early morning; noisy dispersal of customers at closing time 10.00 pm or later.
3. On a stated budget of \$500,000 it may be legitimate to question the standard and quality of the buildings and surrounds. It may be legitimate to conjecture that within 5 years we could be left with a legacy of a further disintegrating Buffalo along with dilapidated out-buildings.
4. The pontoon wharf will further restrict passage of vessels through an already narrow section of the marina. The marina has considerable traffic from sailing craft, charter/dive boats, canoes/kayaks, surf life-saving training boats.
5. It appears from the plans that work/storage areas and rubbish storage are open air spaces, most certainly giving rise to visual and smell pollution.
6. With the planned 'densification' of the inner urban areas including Glenelg, there is an increasing need for public open space. This proposal degrades declared open space to the detriment of the citizens of the City of Holdfast Bay

This development would appear to be another example of community land initially being alienated for a sympathetic "heritage" building (the Buffalo replica) only to be later replaced by a fully

commercial business. The original PR claim of refurbishing the heritage site appears to be lacking in good faith; are we to see the demolition of the Buffalo in a few years, leaving only another commercial venue for provision of food/liquor in an area already well-provided for by traders who have had to bear the full commercial cost of operating such a business. Community land must not be used to subsidise commercial businesses.

Further, residents (on both sides of the water) who chose to live in a location enjoying the pleasant buffer of a natural recreation reserve, and paid accordingly, now find they are to be encroached upon by noise and disturbance from an open-air venue from early morning (6.00 am) until late evening 10.00pm or later. We are all content to accept the hustle and bustle of modern life from developments on private land, but the alienation of public community land for these purposes should not be accepted.

We oppose the development.

ATTACHMENT 1.

Hon John Hill

Minister for Environment and Conservation

Minister for the Southern Suburbs

Minister Assisting the Premier in the Arts

8204

Tuesday 28 September 2004

WIGLEY RESERVE LAND WILL BE RESTORED TO GLENELG COMMUNITY

Land on the corner of Adelphi Terrace and Anzac Highway at Glenelg will be re-dedicated for public recreation following representations to the State Government on behalf of the local community.

Minister for Environment and Conservation John Hill said the land, which is part of Wigley Reserve, was transferred to allow for boundary changes during the development of Stage 1 of the Holdfast Shores development but was never returned to the community.

"The ALP candidate for Hindmarsh Steve Georganas and Liam Hanna from the *Enough is Enough* community group have lobbied the State Government about this oversight and asked for the land to be restored to the people of Glenelg for recreation," the Minister said.

"The Department for Environment and Heritage has looked into the matter and will issue a new Trust Grant to the City of Holdfast Bay for the land to be used 'for the recreation of the inhabitants of The Corporation of the City of Holdfast Bay'.

"I appreciate the actions of the ALP candidate for Hindmarsh, Steve Georganas and Liam Hanna from *Enough is Enough* in raising this matter on behalf of the Glenelg community and I am pleased that the land can be promptly restored to the community."

Wigley Reserve has always remained as Community Land under the Local Government Act 1999.

Steve Georganas

4 8463 3682

edit 8463 5680
Nick

Y. ... to Mr. Hunter @ Saigo
can

ATTACHMENT 2.

18 November 2004]

THE SOUTH AUSTRALIAN GOVERNMENT GAZETTE

4333

CROWN LANDS ACT 1929: SECTION 5

TAKE NOTICE that pursuant to the Crown Lands Act 1929, I, JOHN HILL, Minister for Environment and Conservation, Minister of the Crown to whom the administration of the Crown Lands Act 1929 is committed DO HEREBY dedicate the Crown Land defined in The Schedule as Recreation Reserves (two) and declare that such land shall be under the care, control and management of The Corporation of the City of Whyalla.

The Schedule

1. Allotment 101 of Deposited Plan 61444, Hundred of Randell, County of York, exclusive of all necessary roads, being the whole of the land comprised in Crown Record Volume 3917 Folio 31.
2. Allotment 105 of Deposited Plan 61444, Hundred of Randell, County of York, exclusive of all necessary roads, being the whole of the land comprised in Crown Record Volume 3917 Folio 35.

Dated 18 November 2004.

J. HILL, Minister for Environment and Conservation

DEHAA 13/0726

CROWN LANDS ACT 1929: SECTION 5

TAKE NOTICE that pursuant to the Crown Lands Act 1929, I, JOHN HILL, Minister for Environment and Conservation, Minister of the Crown to whom the administration of the Crown Lands Act 1929 is committed DO HEREBY:

1. Resume the land defined in The First Schedule.
2. Dedicate the Crown Land defined in The Second Schedule as a Recreation Reserve and declare that such land shall be under the care, control and management of the City of Holdfast Bay.

The First Schedule

Reserve for the Recreation and Amusement of the Inhabitants of the City of Holdfast Bay, Allotment 1001 of Deposited Plan 49600, Hundred of Noarlunga, County of Adelaide, the notice of which was published in the *Government Gazette* of 25 June 1998 at page 2632. The First Schedule, being the whole of the land comprised in Crown Record Volume 5643 Folio 338.

The Second Schedule

Allotment 1001 of Deposited Plan 49600, Hundred of Noarlunga, County of Adelaide, exclusive of all necessary roads, being the whole of the land comprised in Crown Record Volume 5643 Folio 338.

Dated 18 November 2004.

J. HILL, Minister for Environment and Conservation

DEH 17/1011

CROWN LANDS ACT 1929: SECTION 5

TAKE NOTICE that pursuant to the Crown Lands Act 1929, I, JOHN HILL, Minister for Environment and Conservation, Minister of the Crown to whom the administration of the Crown Lands Act 1929 is committed DO HEREBY dedicate the Crown Land defined in The Schedule as a Sewer Reserve and declare that such land shall be under the care, control and management of the South Australian Water Corporation.

The Schedule

Allotment 831, Town of Port Augusta West, Hundred of Copley, County of Manchester, exclusive of all necessary roads, being the whole of the land comprised in Crown Record Volume 5767 Folio 531.

Dated 18 November 2004.

J. HILL, Minister for Environment and Conservation

DEH 13/0931

DEVELOPMENT ACT 1993, SECTION 25 (17): ALEXANDRINA COUNCIL—RURAL LIVING AND AGISTMENT ZONE PLAN AMENDMENT

Preamble

1. The Development Plan amendment entitled 'Alexandrina Council—Rural Living and Agistment Zone Plan Amendment' (the Plan Amendment) has been finalised in accordance with the provisions of the Development Act 1993.

2. The Minister for Urban Development and Planning has decided to approve the Plan Amendment.

NOTICE

PURSUANT to section 25 of the Development Act 1993, I—

- (a) approve the Plan Amendment; and
- (b) fix the day on which this notice is published in the *Gazette* as the day on which the Plan Amendment will come into operation.

Dated 18 November 2004.

TRISH WHITE, Minister for Urban Development and Planning

PLN00/0557

DEVELOPMENT ACT 1993, SECTION 25 (17): CITY OF PROSPECT—RESIDENTIAL DESIGN PLAN AMENDMENT

Preamble

1. The Development Plan amendment entitled 'City of Prospect—Residential Design Plan Amendment' (the Plan Amendment) has been finalised in accordance with the provisions of the Development Act 1993.

2. The Minister for Urban Development and Planning has decided to approve the Plan Amendment.

NOTICE

PURSUANT to section 25 of the Development Act 1993, I—

- (a) approve the Plan Amendment; and
- (b) fix the day on which this notice is published in the *Gazette* as the day on which the Plan Amendment will come into operation.

Dated 18 November 2004.

TRISH WHITE, Minister for Urban Development and Planning

PLN02/0156

DEVELOPMENT ACT 1993, SECTION 48: DECISION BY THE DEVELOPMENT ASSESSMENT COMMISSION AS DELEGATE OF THE GOVERNOR

Preamble

1. The decision of the Governor under section 48 of the Development Act 1993, to approve the development of the Myponga/Sellicks Hill Wind Farm located on the Sellicks Hill Range near Myponga was published in the *Gazette* on 20 November 2003.

2. The development was the subject of a Public Environmental Report and an Assessment Report under sections 46 and 46C of the Development Act 1993.

3. Application has now been made to the Development Assessment Commission as delegate of the Governor under section 48 of the Development Act 1993, for the approval of a variation to the development.

4. The proposed variation is for the erection of two 50 m high anemometer (wind monitoring) towers for two years (temporary use).

5. The amendments to the development are contained in:

- (a) the letter from TrustPower Australia Pty Ltd to the Development Assessment Commission dated 6 October 2004 and attached tower specifications;

ATTACHMENT 3,

Heritage precinct at Glenelg – Liam Hanna – 10/9/04

City of Holdfast Bay *The Landing*

Proposal for return & integration of community land

This is an historic initiative. An initiative that, should it be realised, will deliver outcomes that will greatly enhance the civic stature and credibility of the Rann Government in the annals of community history.

The land in question, up to 1997, had been held in trust for use and recreation of the community for over 130 years. As the site of the birth place of South Australia, it was most appropriate that this land should have been so dedicated to community use and available for more visionary planning as the community and its heritage values matured. See Kelly documentation for details. e.g.:

In 1875 when the large allotment No 18, in the township of St Leonards, (consisting of 134 acres) owned by the Corporation of the Town of Glenelg was exchanged for the freehold of Colley and Wigley Reserves, **Council members and their successors were directed by the Governor To Hold In Trust these public reserves as places for the recreation and amusement of the inhabitants, and for no other purpose whatsoever.**

16 November 1875. **LAND GRANT** Vol 215 folio 236, for Public reserve. Know all Men by these Present, that I, Sir Anthony Musgrave, Governor in the Province of South Australia,Do Hereby,....Grant unto the Corporation of the Town of Glenelg all those pieces of land containing Fourteen acres (Colley and Wigley Reserves).... **To Hold unto the said Corporation and their successors In Trust** to permit and suffer the said pieces of land to be used at all times as places for the recreation and amusement of the inhabitants of the Corporation of the Town of Glenelg, and for no other purpose whatsoever. Witnessed and Signed and countersigned on the back with the same words as above by the Commissioner of Crown Lands, Jas. P. Boucaut. (Later a Supreme Court Judge and Premier of South Australia.)

These present negotiations will redress the serious wrong that was done to the community of the City of Holdfast Bay and the heritage values of SA.

Issues to be clarified:

- Agreement to restore land to community ownership vested in a Trust so iron clad that it will not be broken for next 200 years.
- Extent of land to be restored to community ownership e.g. two reserves, Magic Mountain site and surrounding public land.
- Make-up of Trustee body – not solely the Council (once bitten...) – e.g. nominations from resident associations, Mayor plus two Councillors, State and Federal Members, nomination from service club sector (in rotation).

Heritage precinct at Glenelg – Liam Hanna – 10/9/04

- Conditions of Trust Deed to be drawn up.
- Creation of an integrated precinct to strengthen cohesion, heritage values and strength of identity of area as a protection against alienation of parts of a part etc.
- Creation of a heritage precinct by giving area an identity and heritage status, with prominent entrance marker, interpretive plaque etc. Retain individual names for reserves but create a cohesive heritage area under name e.g. The Landing, The Foundry, The Camp or some colloquial name that will develop stature over next 50, 80, 100 years. E.g. The Domain, The Rocks, Central Park.
- Planning for a significant handover event or announcement, in near future. Involve Premier, big turn out of local and metro. Communities

Liam Hanna
10 September 2004

ATTACHMENT 4.



holdfast.sa.gov.au

Brighton Civic Centre 24 Terry Road Brighton SA 5042
PO Box 17 Brighton SA 5042

Phone 08 829 0600 Fax 08 829 4160

Glenelg Customer Service Centre and Library
2 Colley Terrace Glenelg SA 5045

OFFICE OF THE MAYOR

24 March 2015

Liam J Hanna
Apartment 67 Marina East
33 Colley Terrace
GLENELG SA 5045

Dear Sir

Re: The Buffalo

attached

Thank you for your letter dated 13 March 2015 wherein you raise some concerns about the redevelopment of The Buffalo. I will endeavor to answer your questions in the order you have raised them.

As you are aware both Colley Reserve and portion of Wrigley Reserve were classified as community land following the Local Government Act, 1999 being proclaimed. At that time Councils were able to exclude land from Community Land Classification provided they followed the procedures detailed in the Local Government Act, 1999.

*John Hill
action
2004*

At that time Council excluded a number of properties from Community Land Classification including the area that was leased to the owner of The Buffalo.

As you would be aware the previous owner of The Buffalo chose to sell the business. At that time the former lease was surrendered and a new lease entered into with the new owner. As the area of the lease is excluded from Community Land classification there was no requirement to undertake community consultation in respect of the new lease. The new owner is looking to reinvigorate this area and is therefore seeking to undertake some development to this site.

Therefore to answer your 'dot' points in order;

- The owner has not revealed the costings for each part of the development to Council. The lease fee was set on the basis of the total cost for the redevelopment.

- Advice received is that the vessel does not meet current standards in respect to building and safety regulations. These issues will be addressed by the new owner as part of the approval process for the redevelopment.
- We understand there will be an outdoor dining area to which the owner will be applying for a liquor licence in due course.
- The plans provided are for the purpose of seeking Planning consent. These plans have been prepared following extensive discussions with Council staff in respect to negating the impact on adjoining community land. Council has approved the plans which are the same plans that have been submitted for planning consent. Should these plans change the Lessee will need to seek Council approval and resubmit the Planning consent application. There will be no increase in parking within the current community land boundaries.
- The lease provides strict provisions on the use of this land as will any planning consent or building rules consent. The Lessee is fully aware of the surrounds and is working to mitigate any possible concerns that the residents may have.

We hope that this letter alleviates your concerns.

Please do not hesitate to contact Mr Stewart Galliford, Council's Strategic Property Officer on 8229 9817 should you have any further queries relating to this matter.

Yours sincerely

A handwritten signature in black ink, appearing to read 'Stephen Patterson', with a long horizontal flourish extending to the right.

Stephen Patterson
Mayor

Liam J Hanna
Apartment 67 Marina East
33 Colley Terrace
Glenelg SA 5045

13 March 2015

Mayor Stephen Patterson
City of Holdfast Bay
Brighton SA

Dear Mayor

I write to raise some considerations for your urgent attention in relation to the proposal for the development of the Buffalo site.

While visiting Council offices yesterday, seeking documentation on the proposal, I became aware that the Development Assessment Commission meeting considering this proposal was in progress. I am concerned that such action is in train given that a number of issues remain unclear and that no community consultation has occurred.

Firstly, may I provide and draw your attention to some background. As you are aware, extensive lands around the Colley and Wigley Reserves were generously bequeathed to the community by William Colley in 1860s – under a Deed of Grant. These lands were resumed, in a most irregular fashion, in 1997 by the Council and State Government of the day. After community activism in resistance to over-development of the Glenelg foreshore, it is my understanding that, after community representations, the balance of the land was returned to community status in 2004. Consequently, if the development or the extension of the lease area impacts on this community land, it is required that there be a period of community consultation.

Furthermore, several issues remain unclear and /or ill-defined:

- How much of the proposed investment is being applied to the refurbishment of the Buffalo (the supposed basis for the minimal lease fee);
- What is the current state of the Buffalo in regards to building and safety regulations; are these to be addressed by the developer?
- There is a lack of clarity between talk of a kiosk and a bar; what licence provisions will the developer be seeking?
- The Buffalo concept animation is a concept only and developers have been known to move far beyond such idealistic “concepts”; what is the extent of infringement on community land; is there any increase in car-parking?
- What steps are being taken to address possible concerns of Holdfast Bay residents in the adjacent areas.

These and many other issues are of concern to residents adjacent to the proposed development. In view of this concern and issues yet to be clarified (e.g. status of community land and necessary *consultation*), *may I request that no further action be taken* (e.g. DAC deliberations/decisions, commitment/agreement to allow development work to commence) until such time as these and other issues have been clarified and required community consultation has occurred.

With thanks for your attention to this matter.

Yours sincerely

Liam J Hanna

ATTACHMENT 7

RESPONSE TO REPRESENTATIONS

Pluck, Daniel (DPTI)

From: John <john@deyoungs.com.au>
Sent: Friday, 6 May 2016 1:54 PM
To: Pluck, Daniel (DPTI)
Subject: FW: 110/L002/15 - Buffalo Redevelopment - Public Notification Outcome
Attachments: CL Response 2 - 110-L002-15.pdf

From: John [mailto:john@deyoungs.com.au]
Sent: Friday, 6 May 2016 11:04 AM
To: 'Daniel.Pluck@sa.gov.au' <Daniel.Pluck@sa.gov.au>
Cc: 'Troy DeYoung' <troy@deyoungs.com.au>
Subject: FW: 110/L002/15 - Buffalo Redevelopment - Public Notification Outcome

Hi Daniel,

Apologies for the delay on this response, as you are aware Stuey is off unwell. I will endeavour to answer your questions to the best of my ability, but will not be in the same format as a Stuey response. Please come back to me should you require further information.

At this point we are keen to get this across the line as council are starting to apply some pressure.

I will chat to you shortly in regards to staging this development if you feel this will be more beneficial in regards to kiosk stage 1, boat and surrounding structures stage 2, and pontoon stage 3. This said if we think we can achieve total approval without too many further hold ups, I believe this would be the public and councils preference. I will await your advice in this matter.

1. Outdoor signage & advertising: At this stage our proposed signage & design is as per concept plans provided. I am aware that any further signage added or required, an additional application will be needed.
2. Carpark stone wall entrance: This will be a solid sandstone wall as per concept drawing provided. This will be constructed in a tasteful recycled bull nose sandstone with low impact signage of protruding backlit cut out metal. This will be designed in a tasteful manner with low amperage subdued opal lighting. Additional to this, there will be ground lighting in gardens and around signage, more of a glimmer than a light which will have no glare or impact. Existing Buffalo carpark sign will be relocated to an appropriate position agreed with council. As you would have seen on your site inspection we have refurbished the existing carpark which has already enhanced the overall concept.
3. New Anchor: The existing anchor situated in front of the boat will be relocated to the front door entrance, and positioned amongst the garden, once again, all lighting will be opal low impact design.
4. Moat water and deck: As discussed the other day on the phone, we are both in agreeance that the moat will require attention to overcome the ongoing problem current and past. Originally it had a pump and ball float system maintaining the water level and pumping clean water into the existing moat. Additional to this the stormwater from the top of the vessel is also going into the moat. The pump is now non-existent, as the previous owner has taken it. Further to this, the debris blowing form the parklands create substantial paper waste in the moat. We have been doing our best to maintain it and clean the waste from the moat, pumping water in and out on a semi regular basis. This is purely a stop gap measure. The proposed solution, all water and waste will be completely pumped from the moat and all debris will be sucked out and removed from site. As discussed on the phone, as refurbishment of the boat takes place, we will install rocks around the base of the boat to reduce further decay to the boat. My general thoughts are to reduce the total water volume of the moat with more a rockeries-creek effect, with water plants and a pump system to keep water flowing and clean. I feel this will enhance the vessel and surrounds. Additional to this, once there is an

operating business present, it will be a criteria for the operator to maintain the surrounding moat and gardens to a high standard. I feel we can make this problem quite an enhancing asset. I am aware that the existing plans don not show any concept in regards to the moat re instatement. Please advise if this information will be sufficient in the short term, and if we can get an approval based on reinstatement of the moat being a condition.

I strongly believe that this development will be an asset to Holdfast Bay and the tourism industry, our vision and creativity will only be to the highest standards. As you can appreciate this has been a challenging project to rebirth. But our company and our team are very excited to get started.

Please advise at your earliest convenience if you need any further information, I look forward to working with you through this.

Regards,

Troy DeYoung

per

John Lawton

DeYoungs

www.deyoungs.com.au

P: 8186 3093

F: 8186 2932

M: 0438 821 010

From: Stuart Patrick [<mailto:design@stuartpatrick.com.au>]

Sent: Thursday, 31 March 2016 4:42 PM

To: troy@deyoungs.com.au

Subject: FW: 110/L002/15 - Buffalo Redevelopment - Public Notification Outcome

Hi Troy

Just resending this in case you didn't receive it earlier.

Please don't hesitate to contact me if you require additional information.

Kind regards

Stuart

Please excuse the brevity of this email.

To save your time and mine, I'm restricting my responses to a minimum.

Stuart J Patrick

& Associates Pty Ltd

Building Designers

164 Main Road (PO Box 2136) McLaren Vale SA 5171

T. 08 8323 0080

M. 0414 442 910

E. design@stuartpatrick.com.au

W. www.stuartpatrick.com.au

Member of Building Designers Association of SA Inc.

Winner BDASA 2009 Design Excellence awards -Industrial Buildings Category

Winner BDAA 2010 National Design Awards - Industrial Buildings Category



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From: Pluck, Daniel (DPTI) [<mailto:Daniel.Pluck@sa.gov.au>]

Sent: Tuesday, 15 March 2016 11:08 AM

To: 'Stuart Patrick'

Subject: RE: 110/L002/15 - Buffalo Redevelopment - Public Notification Outcome

Hi Stuart,

How are you?

I've been looking over your application and also visited the site to get an appreciation of the development. A few observations that I would like comment on please?

1. Outdoor advertising

Signage is as per existing, and no additional signage is proposed.

Whilst at the site I did not observe in particular the new stone 'The Buffalo' sign as outlined in plan 4 of 6 dated Jan 2015.

I consider this to be a new sign as opposed to the existing black on white parking sign shown. Can i please request some detail of this new sign and also the new anchor both with up lighting.

Any other details for new signage attached to the proposed structures will also need to be provided (plan 5 of 6).

Pic.



2. Moat Water and deck

Whilst I note you are going to be refurbishing the Buffalo itself, I would also like to understand what is proposed with the water inside the moat area. Upon inspection the water looked of extremely poor quality and would impact upon the amenity of the development as a whole. In addition you propose to build a deck over this area. The unsightly nature of the water would likely be unattractive to customers. I would like to see some type of management plan or operational arrangement to ensure the quality of the moat water was improved. Can you please provide a response on this issue?

Pic.



Lastly i had a chat with the DEWNR people who have retracted their comment in relation to gaining approval from their Minister for the lease site. I've attached the letter for your reference.

Thankyou kindly.

Regards,

Daniel Pluck

Planning Officer

Strategic Investment

Department of Planning, Transport and Infrastructure

T 7109 7196 • E daniel.pluck@sa.gov.au

Roma Mitchell House 136 North Terrace Adelaide SA 5000 • GPO Box 1815 Adelaide SA 5001 •

DX 56601 • www.dpti.sa.gov.au



collaboration . honesty . excellence . enjoyment . respect

We acknowledge and respect Aboriginal peoples as South Australia's first peoples and nations, we recognise Aboriginal peoples as traditional owners and occupants of land and waters in South Australia and that their spiritual, social, cultural and economic practices come from their traditional lands and waters; and they maintain their cultural and heritage beliefs, languages and laws which are of ongoing importance; We pay our respects to their ancestors and to their Elders.

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From: Stuart Patrick [<mailto:design@stuartjpatrick.com.au>]

Sent: Friday, 26 February 2016 3:04 PM

To: Pluck, Daniel (DPTI)

Subject: RE: 110/L002/15 - Buffalo Redevelopment - Public Notification Outcome

Many thanks for your help, Daniel...have a great weekend!

Cheers

Please don't hesitate to contact me if you require additional information.

Kind regards

Stuart

Please excuse the brevity of this email.

To save your time and mine, I'm restricting my responses to a minimum.

Stuart J Patrick

& Associates Pty Ltd

Building Designers

164 Main Road (PO Box 2136) McLaren Vale SA 5171

T. 08 8323 0080

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E. design@stuartjpatrick.com.au

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Winner BDAA 2010 National Design Awards - Industrial Buildings Category



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From: Pluck, Daniel (DPTI) [<mailto:Daniel.Pluck@sa.gov.au>]
Sent: Friday, 26 February 2016 2:57 PM
To: 'Stuart Patrick'
Cc: troy@deyoungs.com.au
Subject: RE: 110/L002/15 - Buffalo Redevelopment - Public Notification Outcome

Thanks Stuart,

I'll go through the details and see if I have any further questions of you. Barring this, and that lease plan, I will be looking to write a report for the development.

Will be in touch shortly.

Thankyou
Daniel Pluck

From: Stuart Patrick [<mailto:design@stuartjpatrick.com.au>]
Sent: Friday, 26 February 2016 1:17 PM
To: Pluck, Daniel (DPTI)
Cc: troy@deyoungs.com.au
Subject: RE: 110/L002/15 - Buffalo Redevelopment - Public Notification Outcome

Attention: Mr Daniel Pluck

To: Mr K. Tumes, Ms H Merrett, M Corcoran, T & A Angus, Ms L Cunningham, Ms D Bignell, Ms K Waites, R Naish, N & M Beechey for Gebu Pty Ltd, Mr C Haines, Mr LJ Hanna

Thank you for your interest in this exciting development. In response to your representations we submit the following for your consideration.

Noise

There will be no amplified music on the new northern deck, only soft ambient music. This deck will not be accessible to patrons after midnight any day of the week.

Similarly the upper outdoor deck on the boat will not be open to the public after midnight on any day.

It is envisaged that there will generally be only soft ambient music to the outdoor dining area to the south of the restaurant.

There may be potential also for occasional low volume performances by solo or duo acts, but not for loud amplified full bands.

Measures are to be implemented, including presence of on-site security staff to professionally monitor and control behaviour in order to mitigate intoxication, vandalism and disruptive behaviour on the premises.

This, along with the restriction of service of alcohol after midnight will ensure the minimisation of any disturbance to adjoining residential properties from alcohol related activities.

Noise generated from the charter wharf will be minimal, as the pontoon will be for immediate boarding and disembarking from the boat only. Patrons will be required to vacate the mooring pontoon in an orderly and timely manner. The charters will be conducted during daylight and twilight hours, via a booking/reservation system.

Restriction/curfew on service of alcohol

We propose to reduce the operating hours currently allowed by the Liquor Licence to the existing upper deck and the new northern deck.

To this end, alcohol will be able to be purchased or consumed within these two areas only prior to midnight any day.

Alcohol will be available only with the provision of food within both the Buffalo internal restaurant area, and within the outdoor dining area, until 2.00am as per the pre-existing liquor licensing conditions.

Service of alcohol adjacent to the playground

The kiosk building and the associated outdoor seating area adjacent to the existing council playground will not be licensed to serve alcohol, and so does not present any issues.

The outdoor dining licensed area for the service of alcohol is screened and fenced, with the only access to this area being via a single entry point on the eastern-most point of the development, adjacent to the main entrance foyer to the boat, facing away from the playground, and some 100 metres away via pedestrian walkway.

Pedestrian activity and safety

Pedestrian safety will be enhanced and improved.

The current light pole and fixture adjacent to the proposed parking meter station at the entrance to the site is to be upgraded/refurbished in order to bring illumination levels for the required purpose into compliance with current compliance and expectations.

Floodlighting exists on the tram shelter building which provides illumination to the car park area.

Light poles are also to be positioned along the edge of the proposed new walkway adjacent to the southern outdoor area.

All new lighting is to be designed and installed to Australian Standards to ensure no glare which would detrimentally affect nearby residential development, but still maximising pedestrian safety and security.

Outdoor advertising

Signage is as per existing, and no additional signage is proposed.

Ownership of development

The proposed development is proudly and exclusively South Australian owned and operated.

Local trades and suppliers only will be utilised during the construction and refurbishment of the development.

Many local employment opportunities will become available for the operation of the businesses after completion of the renovation works.

Disruption to views

Careful consideration has been given to the design and siting of the proposed additions to the development, and the solution has ensured that the height of all new buildings and additions will be well below the level of the existing Buffalo restaurant, and so views from adjoining residential properties will not be compromised.

Further, the proposed additions to the development i.e. kiosk, amenities building, outdoor deck, alfresco dining area and bar structure have been designed, modelled and articulated in order to minimise any bulk, and take on the form of a minor 'human' scale relative to the existing Buffalo structure.

Impact on nearby retail precinct

The Buffalo proposal intends to offer a unique dining experience and does not in any way intend to compete with nearby businesses and eateries, rather, work with, encourage and supplement existing local businesses for the benefit of all. We see our development as being a vibrant destination with flow-on effects to other businesses in the area.

Licensing of kiosk

It is agreed that a kiosk with alcohol is not needed. The kiosk will not be licensed to serve alcohol.

Affordability of meals

It is proposed that there will be a range of meals on offer, comprising bar and café style meals, pizza etc. in the alfresco dining area. Similar meals will be available inside the main indoor dining area, supplemented with a more 'formal' menu from which to choose. This will ensure a wide choice of meals and affordability for patrons.

Extent/magnitude of development

The proposed development is contained within and restricted to the existing currently approved lease boundary. The existing pedestrian walkway adjacent to the southern boundary of the proposed alfresco dining area is to be extended, levelled and widened which will render it much safer as a thoroughfare than the current pavement which has a very uneven surface.

The scale of the proposed buildings will provide for the enhancement of the existing large bulky structure by providing visual articulation.

Colours and materials of development

It is not the intention to replicate the style, design or colours of the existing structure, but rather implement design features and colours that complement, rather than mimic, those of the existing. To this end natural, earthy, muted

and non-reflective materials have been implemented. Natural timber and stonework have been used extensively on structures around the immediate area. These materials dull off to produce a natural, low impact effect. A grey palette of colours has been chosen for the kiosk in order to ensure minimal visual impact, and to provide a visually pleasing transition between the black of the boat structure and the blue of the adjoining playground and picnic shelters.

Revolution Roofing 'Stealth' has been chosen for the roof colour of the amenities building and bar, which is a non-reflective matt black colour, which will complement the 'black' colour of the existing boat structure.

The Buffalo Structure

Painted "BLACK" with "WHITE" details (remains unchanged, as per existing)

Alfresco Dining Area

Perimeter fence - timber posts and beams (natural clear finish to all timber), clear and obscure glazed panels

Proposed Alfresco Bar Structure

Roof - True Oak Corrugated steel roof cladding in "STEALTH" finish (matte black)

Recycled timber posts and beams, timber shutters (natural clear finish to all timber)

Walls - Natural locally sourced stone

Proposed Amenities Building

Roof - True Oak Corrugated steel roof cladding in "STEALTH" finish (matte black)

Recycled timber posts and beams (natural clear finish to all timber)

Walls - Natural locally sourced stone

Proposed Kiosk

Roof - True Oak Corrugated steel roof cladding in Colorbond "BASALT"

Walls - Corrugated steel wall cladding in Colorbond "WINDSPRAY"

Windows - Colorbond "WINDSPRAY"

Posts - Colorbond "BASALT"

Shade Sails - "WHITE"

Appearance of bar

The floor level of the bar/servery in the alfresco dining area is at nominally the current (lower level) paving level of the existing outdoor area. The proposed obscured glazed perimeter screening to the alfresco dining area, supplemented with landscaped garden beds, will ensure that the bar/servery will be largely obscured, and any visual protrusions above the line of the glazing is designed to be aesthetically pleasing.

Vehicular and pedestrian traffic congestion

We are not seeking to increase maximum number of patrons above the pre-existing approved use. The number of patrons frequenting the premises will not be of such a magnitude to impact detrimentally on either vehicular or pedestrian congestion.

The pedestrian walkway adjacent to the southern side of the development is to be upgraded and widened, with improved lighting and elimination of dangerous tripping hazards.

Similarly the existing carpark has already been refurbished and resurfaced to eliminate the previous uneven surface, along with improved lighting to ensure safety and security of patrons and passers-by. The 50 km/h speed limit along Adelphi terrace, along with traffic light control at the junction of Anzac Highway provides for safe access and egress to and from the site.

Charter boat facility

We have consulted with an existing local charter operator. Exclusive rights would be granted under contract to a single commercial operator only.

The charters will be conducted during daylight and twilight hours, via a booking/reservation system.

The charter boats would not be permanently moored at the site, and would be brought in only as required to service individual charters

Existing approved marina occupants would also be welcomed to make use of the mooring facility. This would encourage residents from nearby Holdfast Shores apartments to avail themselves of The Buffalo experience and facilities.

Waste collection

Rubbish bins will be visually obscured from public areas in a screened bin enclosure. Waste will be managed and collected on a regular basis by a private waste collection company as required to mitigate odours and pollution.

The Buffalo Structure

The interior and exterior of existing Buffalo structure is to be refurbished and reinstated to its former glory. Structural engineering assessment confirms that the structure is generally sound, and any deficiencies are to be brought into compliance to meet or exceed acceptable engineering and safety standards.

Response to DEWNR

The proposed Buffalo re-development is contained entirely within the current lease boundaries. Please refer to current lease plan drawing which will be forwarded in due course.

I trust that the above addresses questions and concerns to your satisfaction.

Please don't hesitate to contact me if you require additional information.

Kind regards

Stuart Patrick
On behalf of The De Young Group

Stuart J Patrick

& Associates Pty Ltd

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Winner BDAA 2010 National Design Awards - Industrial Buildings Category



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From: Pluck, Daniel (DPTI) [<mailto:Daniel.Pluck@sa.gov.au>]

Sent: Thursday, 11 February 2016 9:36 AM

To: 'Stuart Patrick'

Subject: RE: 110/L002/15 - Buffalo Redevelopment - Public Notification Outcome

Thankyou for the response Stuart.

I will also need a response directly in relation to:

- representations made by the general public, and
- Response provided by DEWNR

Once these have been received I will review and seek clarification where necessary whilst completing the report to the DAC Panel.

Feel free to contact me should you have any questions.

Thanks

Daniel Pluck

Planning Officer

Strategic Investment

Department of Planning, Transport and Infrastructure

T 7109 7196 • E daniel.pluck@sa.gov.au

Roma Mitchell House 136 North Terrace Adelaide SA 5000 • GPO Box 1815 Adelaide SA 5001 •

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We acknowledge and respect Aboriginal peoples as South Australia's first peoples and nations, we recognise Aboriginal peoples as traditional owners and occupants of land and waters in South Australia and that their spiritual, social, cultural and economic practices come from their traditional lands and waters; and they maintain their cultural and heritage beliefs, languages and laws which are of ongoing importance; We pay our respects to their ancestors and to their Elders.

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From: Stuart Patrick [<mailto:design@stuartjpatrick.com.au>]

Sent: Wednesday, 10 February 2016 9:10 PM

To: Pluck, Daniel (DPTI)

Cc: troy@deyoungs.com.au; 'Bob'

Subject: FW: 110/L002/15 - Buffalo Redevelopment - Public Notification Outcome

Hi Daniel

Many thanks for your email. In response to Anthony Marroncelli's comments, I submit the following for your consideration.

Noise

Security measures, including presence of on-site security staff, are to be implemented, professionally monitored and controlled in order to mitigate intoxication and disruptive behaviour on the premises.

There will be no amplified music on the new northern deck, only soft ambient music. This deck will not be accessible to patrons after midnight any day of the week.

The bar area to the south of the restaurant similarly will be professionally monitored and controlled. It is envisaged that there will generally be only soft ambient music to this area.

There may be potential also for occasional low volume performances by solo or duo acts, but not for loud amplified full bands.

Outdoor lighting and safety

Light poles are located along the edge of the walkway adjacent to the southern outdoor area. These are denoted as 'LP' on the previously submitted site plan. There is also a light pole adjacent to the parking station, and floodlighting on the existing tram shelter to illuminate the car park. The proposed lighting will not detrimentally affect nearby residential development.

Outdoor advertising

Signage is existing, and no additional signage is proposed.

Coastal risk

We consulted with Mr Thomas Balacco from O'Donnell Griffin, whom kindly provided a copy of the Patawolonga Lake Embankment Survey (attached). His accompanying email is reproduced below.

Hi Stuart / Bob,

As per our phone conversation earlier, I have attached a copy of the survey that was carried out a few years back. I have also marked the flood areas from the 2003 king tide. According to my records the king tide

reached 2.36m AHD which is only supposed to occur once in a hundred years. A significant amount of flood damage to residential areas was due to the Patawalonga Gates not opening to let water from the lake out to sea which was due to a PLC programming parameter which has since been changed.

Regards

Thomas Balacco | Service Key Account Manager
RCR Infrastructure | O'Donnell Griffin | HADEN

A: Building 20, Charles Sturt Industrial Estate Cheltenham Parade, Woodville, SA, 5011, Australia
P: +61 8 8406 2620 | F: 8346 5600
M: 0437426275
W: www.rcrtom.com.au

I have also attached a copy of a detail survey drawing for your reference. I believe this information demonstrates that there are no coastal risk issues present.

Bicycle parking

Bicycle parking is to be provided, and is indicated on the previously submitted site plan.

Colours and materials

The Buffalo

Existing Boat - Painted "BLACK" with "WHITE" details (as per existing)

Outdoor Dining Area

Perimeter fence - recycled timber posts and beams (natural clear finish to all timber), glazed panels

Proposed Outdoor Bar

Roof - True Oak Corrugated steel roof cladding in "STEALTH" finish (matte black)

Recycled timber posts and beams, timber shutters (natural clear finish to all timber)

Walls - Natural locally sourced stone

Proposed Amenities

Roof - True Oak Corrugated steel roof cladding in "STEALTH" finish (matte black)

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Walls - Corrugated steel wall cladding in Colorbond "WINDSPRAY"

Windows - Colorbond "WINDSPRAY"

Posts - Colorbond "BASALT"

Shade Sails - "WHITE"

I trust the above is to your complete satisfaction.

Please don't hesitate to contact me if you require additional information or clarification.

Kind regards

Stuart

Please excuse the brevity of this email.

To save your time and mine, I'm restricting my responses to a minimum.

Stuart J Patrick

& Associates Pty Ltd

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From: Pluck, Daniel (DPTI) [<mailto:Daniel.Pluck@sa.gov.au>]
Sent: Friday, 18 December 2015 9:29 AM
To: 'design@stuartjpatrick.com.au'
Subject: 110/L002/15 - Buffalo Redevelopment - Public Notification Outcome

Hi Stuart,

I have just completed Category 3 notification for the above mentioned proposal. For your information all of the adjoining owners were sent a letter inviting them for comment (close to 400), as well as an advert in the local Messenger newspaper.

Please find attached for your consideration copies of submissions received by the Commission following public notification/advice to adjacent owners of the above proposal. I have in this instance also attached comments from both the Holdfast Bay Council and the Department of Environment, Water and Natural Resources. Please respond in kind to the issues raised in both of these responses.

Officially your response to the submissions is required within ten (10) business days of the date of this email. Given the Christmas break I am happy to extend this timeframe to January/February depending on your schedules, please let me know.

In addition I have also attached the fees payable as a result of specifically the advert in the Messenger Newspaper.

Feel free to contact me today 18/12. I will be on leave after today, returning 6 January 2016.

Thankyou, Kind Regards,

Daniel Pluck
Planning Officer
Strategic Investment
Department of Planning, Transport and Infrastructure
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Roma Mitchell House 136 North Terrace Adelaide SA 5000 • GPO Box 1815 Adelaide SA 5001 •
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The Strategic Assessment and Investment group will **close** for two weeks over the Christmas and New Year period, starting from close of business **FRIDAY 18 DECEMBER 2015** and reopening **MONDAY 4 JANUARY 2016**. We wish you a safe and happy festive season and look forward to working with you again in 2016.





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